



# KNUTSON FARMS

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## Environmental Impact Statement project

### DEIS Comment Index

#### Tribes

Entity	Name	Date	Source
Muckleshoot Indian Tribe	Reynolds, Patrick	3/15/2024	Email
Muckleshoot Indian Tribe	Fox, Martin	3/15/2024	Email
Puyallup Tribe of Indians	Anderson, Lisa	3/15/2024	Email

#### Agencies

Entity	Name	Date	Source
City of Sumner	Kosa, Michael	3/14/2024	Email
Pierce County Parks Department	Odell, Tiffany	3/15/2024	Email
Pierce County Planning & Public Works	Gaffney, Sean	3/14/2024	Email
State of Washington Department of Ecology	Thomas, Joe	3/14/2024	Email
Tacoma-Pierce County Health Department	Dilworth, Erin	3/15/2024	Email
Washington State Department of Transportation	Larson, Andrew	2/22/2024	Email

#### Organizations

Entity	Name	Date	Source
Applicant: Running Bear Development Partners, LLC	Archer, Margaret	3/15/2024	Email
Applicant: Running Bear Development Partners, LLC	Berry, Tim	3/15/2024	Email
Applicant: Weinman Consulting	Weinman, Richard	3/14/2024	Email
Environmental Coalition	Kirkland, Kirk	3/15/2024	Email
Foothills Rails-to-Trails Coalition	Miles, Shayla	3/15/2024	Email
ForeverGreen Trails	Leveen, Larry	3/14/2024	Email
Friends of Pierce County	Johnson, Larry	3/14/2024	Email
Puget Soundkeeper Alliance	Furman, Kelse	3/15/2024	Email
Puyallup River Watershed Council	Dillon, Angela	3/15/2024	Email



## Individuals

### A

Name	Date	Source
Aaron, Daniel	12/14/2023	Webform
Aaron, Daniel	3/1/2024	Email
Adams, Jessica	12/14/2023	Webform
Alhadeff, Annie	3/15/2024	Email
Alhadeff, Anjovon	1/17/2024	Verbal Comment
Alhadeff, Anjovon	3/12/2024	Verbal Comment
Anderson, Ms.	12/15/2023	Webform
Anspach, Jason	1/28/2024	Webform
Anspach, Caleb	3/5/2024	Webform
Anspach, Jennifer	3/5/2024	Email
Arend, Christine	2/21/2024	Webform
Arianna	2/17/2024	Email
Armatis, Ashley	1/11/2024	Webform
Ashley, Renee	12/14/2023	Webform
Austin, Laura	12/14/2023	Webform

### B

Name	Date	Source
Baez, April	3/14/2024	Webform
Baez, April	3/12/2024	Verbal Comment
Baghirov, Raechelle	2/12/2024	Webform
Baghirov, Rae	12/18/2024	Email
Barr, Adam	12/14/2023	Webform
Barry, Vanessa	2/21/2024	Webform
Beck, Ed	3/4/2024	Email
Beck, Jane	3/11/2024	Email
Beeler, Kris	2/15/2024	Email
Belcher, Lore	3/15/2024	Email
Benkovich, Julie	1/31/2024	Voicemail
Bennett, Debi	1/4/2024	Webform
Bennett, Andrea	1/25/2024	Webform
Bennett, Andrea	3/7/2024	Webform
Bennett, Danielle	3/14/2024	Email
Berg, David	3/15/2024	Email
Bero, Diane	1/4/2024	Webform

Bittenbender, Caryl	1/21/2024	Webform
Bledsoe, David	1/17/2024	Webform
Blondino, Sandra	1/22/2024	Webform
Blythe, Leanne	3/11/2024	Email
Booth, Michelle	1/8/2024	Webform
Bowers,	2/26/2024	Email
Bowes, Amanda	1/30/2024	Webform
Bowser, Mitch	2/24/2024	Email
Braaten, Carole	1/17/2024	Voicemail
Braaten, Carole Sue	1/17/2024	Verbal Comment
Braaten, Carole	3/12/2024	Verbal Comment
Braaten, Carole	3/15/2024	Email
Braeger, Andrew	2/14/2024	Webform
Bristow, Libbie	1/30/2024	Webform
Bristow, Libbie	3/15/2024	Email
Bryan, Tana	1/6/2024	Webform
Bryan, Tana	1/17/2024	Webform
Bryan, Tana	2/13/2024	Email
Buchanan, Sherry	2/26/2024	Email

### C

Name	Date	Source
Carr, Christine	2/13/2024	Email
Case, Scott	12/15/2023	Email
Castillas, Sue	3/12/2024	Verbal Comment
Cecchetto, Caitlyn	2/2/2024	Webform
Chalk, Jimmy	2/13/2024	Email
Chisholm, Chris	2/22/2024	Webform
Chisholm, Chris	1/17/2024	Verbal Comment
Ciocca, Anthony	2/4/2024	Webform
Coleman, Mary	12/15/2023	Webform
Colombo, Joseph	1/11/2024	Webform
Colombo, Joe	1/17/2024	Verbal Comment
Conley, Joe	1/26/2024	Email

Conley, Joseph	3/12/2024	Webform
Conley, Lauren	3/12/2024	Webform
Cook, Charlene	3/6/2024	Webform
Coonley, Lauren	3/12/2024	Webform
Crane, Sarah	2/13/2024	Email
Crawford, Mark	3/14/2024	Email
Cribbin, Vicki	1/6/2024	Webform
Crivello, Jayme	12/15/2023	Webform
Crouch-Klein, Kathleen	1/23/2024	Webform
Cuenca, Phil	2/19/2024	Email
Cunningham, Andriana	3/14/2024	Webform
Cutshall, Al	1/22/2024	Webform

#### D

Name	Date	Source
Davis, Laura	12/14/2023	Webform
Davis, Nancy	2/26/2024	Webform
Davis, Penny	3/6/2024	Email
Davis, Penny	3/12/2024	Verbal Comment
De Groot, David	2/24/2024	Webform
De Groot, David	2/24/2024	Webform
De Groot, David	3/4/2024	Webform
DeGroot, David	2/26/2024	Email
DeGroot, David	1/17/2024	Verbal Comment
DeGuseppi, Elise	3/4/2024	Webform
DeRosier, DeAnn	1/28/2024	Webform
Desteman, Monica	2/10/2024	Voicemail
Diaz, Grace	2/27/2024	Webform
Dildine, Ryan	2/26/2024	Email
Dittus, Lynette	3/13/2024	Email
Dooley, Linda	1/17/2024	Webform
Downes, Nathaniel	2/5/2024	Webform
Downs, Timothy	2/14/2024	Email
Durgin, Sam	3/15/2024	Webform

#### E

Name	Date	Source
Eaton, Robert	12/23/2023	Webform
Eckert, Carol	12/19/2023	Webform
Edmonds, Craig	2/26/2024	Webform
Edson, Heather	2/22/2024	Webform
Eims, Penny	2/26/2024	Email
Elly, Dr.	3/15/2024	Email
Engels, Tracy	3/15/2024	Email
Engels, Tracy	3/12/2024	Verbal Comment
England, Katie	2/26/2024	Webform
Erickson, Lucas	12/14/2023	Webform
Erickson, Sarah	2/13/2024	Email

#### F

Name	Date	Source
Fahnstrom, Barbara	12/14/2023	Webform
Field, Kim	1/12/2024	Webform
Field, Kim	3/14/2024	Email
Fitzgerald, Chuck	2/13/2024	Email
Fitzgerald, Chuck	3/15/2024	Email
Fleming, George	2/19/2024	Email
Fortner, Tirzah	1/23/2024	Webform
Fraser, Tom	3/14/2024	Email
Fuchs, Ashley	12/14/2023	Webform

#### G

Name	Date	Source
Geldien, Wendy	2/13/2024	Email
Gibson, Marie	2/20/2024	Webform
Girl, Banana	12/17/2023	Webform
Gray, Roan	1/8/2024	Webform
Green, Daniel	1/30/2024	Webform
Grefalda, Emily	3/14/2024	Email
Gustaves, Clay	3/14/2024	Webform



## H

Name	Date	Source
Hackbarth, Sandra	3/13/2024	Webform
Hale, Debra	3/15/2024	Email
Hamilton, Kat	3/2/2024	Email
Hampton, Monae	3/13/2024	Email
Hansen, Robert "Doc"	3/9/2024	Webform
Hansen, Doc	3/12/2024	Verbal Comment
Hartmann, Carey	2/24/2024	Email
Hassett, Leslie	1/18/2024	Email
Hassur, Mike	3/6/2024	Email
Hassur, Michael	3/14/2024	Email
Healy, Robert	3/13/2024	Webform
Helgeson, Trevor	1/20/2024	Webform
Hembrow, Juanita	2/17/2024	Email
Henderson, Hunter	12/18/2023	Webform
Henley McKinnon, Lauren	3/5/2024	Webform
HenleyMcKinnon, Lauren	3/5/2024	Webform
Hernandez, Leticia	2/1/2024	Webform
Hernandez, Heather	2/21/2024	Webform
Hobbs, Amy	1/25/2024	Webform
Hoebelheinrich, Carolyn	3/14/2024	Webform
Hoff, Gary	3/9/2024	Email
Holmes, Holly	3/15/2024	Email
Hopkins, John	1/18/2024	Email
Hopkins, John	3/13/2024	Email
Hopkins, John	3/12/2024	Verbal Comment
Hopp, Vicky	2/25/2024	Email
Huckee, Tricia	3/15/2024	Email
Hunt, James	2/24/2024	Email
Hunt, Bart	2/24/2024	Email
Hunter, Joanna	1/29/2024	Webform

## J

Name	Date	Source
Jacobson, Charles	3/1/2024	Email
Jasionkowski, Kim	1/22/2024	Webform
Johnson, Brett	3/15/2024	Webform
Johnson, Brett	1/17/2024	Verbal Comment
Johnston, Sondra	12/16/2023	Webform
Johnstone, Fred	1/25/2024	Webform
Josten, Elizabeth	2/14/2024	Email
Judah, Liesl	12/16/2023	Webform

## K

Name	Date	Source
Kajca, Peter	1/24/2024	Webform
Kajca, Laurie	1/25/2024	Webform
Kajca, Peter	1/26/2024	Webform
Kajca, Laurie	2/26/2024	Email
Kajca, Peter	3/12/2024	Verbal Comment
Kenyon, Steven	12/20/2023	Webform
Kempff, Paul	3/15/2024	Email
Kibiger, Lynae	1/4/2024	Webform
Kibiger, Lynae	3/12/2024	Email
Kirk, Bethany	12/14/2023	Webform
Kirk, Bethany	1/7/2024	Email
Kirk, Bethany	1/8/2024	Email
Kirk, Bethany	3/12/2024	Webform
Kohli, Mary	2/26/2024	Email
Kunze, Amalia	2/11/2024	Webform

## L

Name	Date	Source
Lane, Staley	1/30/2024	Webform
Lee, Keun-Hae	1/29/2024	Webform
Leisner, Andrew	2/26/2024	Webform
Levy, Judith	2/28/2024	Email
Linden, Krista	3/14/2024	Webform
Lindholm, Michael	1/30/2024	Webform
Loney, Kelly	2/21/2024	Email

Loney, Kelley	3/12/2024	Verbal Comment
Luppino, Janae	12/14/2023	Webform

## M

Name	Date	Source
Maahs, Kathy	1/29/2024	Webform
Macadangdang, Craig	2/28/2024	Webform
Mahoney, Roberto	2/5/2024	Webform
Mamic, Helen	3/7/2024	Email
Marckmann, Cydne	3/13/2024	Email
Martineau, Nicole	3/15/2024	Email
Maston, Teresa	1/3/2024	Webform
McDonald, Mary	2/28/2024	Email
McKean, Paul	2/15/2024	Email
McNamara, Karen	1/17/2024	Verbal Comment
Metzger, Bud	3/12/2024	Verbal Comment
Meysenburg, Shelly	1/6/2024	Webform
Michael, Jordan	1/17/2024	Webform
Michael, Jordan	1/17/2024	Webform
Michael, Jordan	1/17/2024	Email
Miller, Pat	3/2/2024	Email
Miller, Dayna	3/14/2024	Email
Mix, Brendon	3/13/2024	Email
Mocorro Powell, Ashley	1/17/2024	Verbal Comment
Mohlman, Bryan	2/29/2024	Webform
Moreno, Cherie	1/17/2024	Webform
Morita, Jay-me	3/2/2024	Email
Mosley, Jackson	1/3/2024	Webform
Myrick, Bob	2/13/2024	Email

## N

Name	Date	Source
Nagai, Laura	3/9/2024	Email
Neermann, Juli	3/12/2024	Webform

Neighbors, Mariah	3/11/2024	Webform
Neshyba Nara, Veronika	1/7/2024	Webform
Neshyba Nara, Veronika	1/7/2024	Webform
Neville, Nick	3/6/2024	Email
Newell, RaeDeen	1/23/2024	Webform
Nguyen, Theda	1/12/2024	Webform
Nickey, Karen	3/11/2024	Email
Nilsby, Juanita	2/29/2024	Email

## O

Name	Date	Source
O'Brien, Tom	3/3/2024	Email
Otsuka, Edward	1/17/2024	Verbal Comment

## P

Name	Date	Source
P, Audrey	3/12/2024	Email
Pacheco, Cheryl	1/29/2024	Email
Paine, Lesley Blair	2/2/2024	Email
Palmer, Connie	1/23/2024	Webform
Palmer, John	1/5/2024	Email
Palmer, John	3/15/2024	Email
Palmer, John	3/14/2024	Webform
Palmer, John	3/15/2024	Webform
Palmer, John	1/17/2024	Verbal Comment
Palmer, John	3/12/2024	Verbal Comment
Palo, Nannette	3/12/2024	Verbal Comment
Palumbo, Dawn	1/12/2024	Email
Pantastico, Hoa	3/14/2024	Email
Paradis, Sara	1/13/2024	Webform
Parshall, Chris	1/20/2024	Webform
Patterson, Christine	3/12/2024	Email

Paulsen, Camille	3/15/2024	Email
Paulsen, Dirk	2/26/2024	Email
Pederson, Carrie	2/20/2024	Webform
Peranzi, John	1/10/2024	Webform
Perrin, Christine	3/15/2024	Email
Perry, Ron	3/12/2024	Verbal Comment
Peters, Michael	12/19/2023	Webform
Peters, Becky	1/23/2024	Webform
Peterson, Andrew	1/30/2024	Webform
Peterson, Cynthia	3/14/2024	Webform
Piacenti, Jordan	2/6/2024	Webform
Pickard, E. J.	3/14/2024	Webform
Pierce, Art	12/14/2023	Webform
Pierce, Art	3/14/2024	Webform
Pingeon, Kayley	2/7/2024	Webform
Pitigliano, Audrey	3/11/2024	Webform
Porter, Kaaren	2/21/2024	Webform
Powell, Elaine	2/19/2024	Webform
Powell, Elaine	3/14/2024	Email
Prociw, Tracey	2/21/2024	Webform

## R

Name	Date	Source
Ragan, Michael	2/12/2024	Webform
Reaves, Renae	1/13/2024	Webform
Reeves, Lisa	2/21/2024	Webform
Reeves, Lisa	2/28/2024	Email
Rhodes, Scott	1/17/2024	Webform
Richeson, Daryl	2/26/2024	Email
Richeson, Daryl	2/26/2024	Email
Richeson, Jaime	3/14/2024	Webform
Riley, Kathleen	3/15/2024	Email
Rink, Stacia	3/15/2024	Email
Rink, Stacia	3/15/2024	Email
Roberts, Stephanie	1/12/2024	Webform
Rometsch, Paul	1/12/2024	Email
Rometsch, Paul	1/17/2024	Verbal Comment
Rometsch, Paul	3/12/2024	Verbal Comment
Romwro, Laura	1/6/2024	Webform

Rose, Any	1/6/2024	Webform
Ross, Markee	2/2/2024	Webform
Ross, Jean	1/2/2024	Email
Ross, Jean	1/2/2024	Email
Ryan, Kyle	12/17/2023	Webform

## S

Name	Date	Source
S, Ben	1/7/2024	Webform
Saarela, Linda	1/25/2024	Webform
Saarela, Linda	2/13/2024	Email
Saarela, Linda	3/12/2024	Verbal Comment
Sanders, Monica	12/14/2023	Webform
Sanderson, Cindy	1/16/2024	Webform
Sawyer, K	1/12/2024	Webform
Schneider, Robin	3/15/2024	Email
Schumock, Phil	2/13/2024	Email
Seely, Art	3/12/2024	Verbal Comment
Selby, John	2/24/2024	Email
Shein, Stephanie	2/24/2024	Email
Sheldon, Kathryn	1/13/2024	Email
Sheldon, Kathryn	1/23/2024	Email
Sheldon, Kathryn	2/1/2024	Email
Sidor, Kyle	2/26/2024	Email
Simons, Anneliese	12/16/2023	Email
Skibba, Allison	12/14/2023	Webform
Slama, Mike	3/15/2024	Webform
Smith, Preston	2/16/2024	Email
Smith, Scott	2/17/2024	Email
Sosa, Shelly	2/21/2024	Webform
Spadafore, Angela	3/14/2024	Webform
Spencer, Danielle	3/14/2024	Webform
Stamon, Kris	3/8/2024	Email
Stamon, Kris	3/12/2024	Verbal Comment
Stanley, Stephanie	1/12/2024	Webform
Stueve, Bridget	1/4/2024	Webform
Sundahl Jr., David	3/14/2024	Email
Sutton, Nathan	3/11/2024	Webform

**T**

Name	Date	Source
Talbott, Robin	3/14/2024	Webform
Talbott, Chris	3/16/2024	Webform
Taylor, Tom	3/15/2024	Email
Taylor, Tom	3/12/2024	Verbal Comment
Teter, Sandra	2/28/2024	Webform
Teter, Eber	2/28/2024	Webform
Thompson, Fay	2/3/2024	Webform
Treacle, Terri	12/15/2024	Email
Treacle, Terri	2/13/2024	Email
Trott, John	2/27/2024	Webform
Trott, John	12/23/2023	Email
Turner, Cass	2/13/2024	Email
Turner, Cass	2/27/2024	Email
Turner, Jennifer	3/15/2024	Email
Turpin, Theresa	1/24/2024	Email

Wetzel, David	3/12/2024	Verbal Comment
Whiteside, Kristal	1/11/2024	Webform
Wickett, Jody	1/17/2024	Webform
Wickoren, Michael	3/14/2024	Email
Wilkins, Alixandre	1/17/2024	Webform
Willard, Tiffany	3/14/2024	Webform
Williams, Teena	2/21/2024	Webform
Williams, Maren	3/15/2024	Email
Williams, Gail	1/17/2024	Verbal Comment
Winger, Michael	1/17/2024	Verbal Comment
Wisness, Sarah	1/3/2024	Webform
Wisness, Sarah	1/3/2024	Webform
Wolff, Sara	2/26/2024	Email
Wolken, Ann	12/15/2023	Webform
Woodke, Mark	3/10/2024	Email
Woodke, Michelle	3/10/2024	Webform

**V**

Name	Date	Source
Valencia, Emilio	1/17/2024	Webform
VanLierop, Anne	1/10/2024	Webform
Vernon, Pamela E	3/1/2024	Webform
Vines, Forrest	1/2/2024	Webform
Vittetoe, Barbara	1/30/2024	Email

**Y**

Name	Date	Source
Yanasak, Chris	2/12/2024	Webform
Yanasak, Chris	2/13/2024	Email
Yost, Debra	2/26/2024	Email
Yost, Deb	3/12/2024	Verbal Comment

**W**

Name	Date	Source
Walker, Staci	1/6/2024	Webform
Wallace, Scott	3/14/2024	Email
Waltier, Christie	2/19/2024	Webform
Wambold, Bailey	1/7/2024	Webform
Wambold, Bailey	1/20/2024	Webform
Wambold, Bailey	2/5/2024	Webform
Wambold, Bailey	1/17/2024	Verbal Comment
Wambold, Bailey	3/12/2024	Verbal Comment
Warhol, Carolyn	2/3/2024	Email
Wertz, Amy	3/12/2024	Email

**Z**

Name	Date	Source
Zimmerman, Joni	3/14/2024	Webform

**Anonymous**

Name	Date	Source
Anonymous	1/11/2024	Email
Anonymous	1/11/2024	Email
Anonymous	2/13/2024	Email
Anonymous	2/13/2024	Email



## Multiple

Name	Date	Source
Protect Puyallup	3/15/2024	Email



**Muckleshoot Indian Tribe**

**Commenter:** Reynolds, Patrick

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Patrick Reynolds

Muckleshoot Indian Tribe Fisheries Division, Habitat Program

39015—172nd Avenue SE

Auburn, WA 98092

March 15, 2024

Puyallup City Hall

Attn: Knutson Farms DEIS comments

333 South Meridian

Puyallup, WA 98371

RE: KNUTSON FARMS INDUSTRIAL PARK PROJECT, DRAFT ENVIRONMENTAL IMPACT STATEMENT, PIERCE COUNTY, WASHINGTON, PUYALLUP RIVER BASIN.

Dear Officials:

I reviewed the Draft Environmental Impact Statement (DEIS) and related appendices concerning the proposed Knutson Farms Industrial Park Project (Project).

The Project proposes to obliterate farmland for up to 2.6 million square feet of building area in seven warehouses on the approximately 188-acre Knutson Farm property located within unincorporated Pierce County, Washington, and the UGA of the City of Puyallup.

The Project proposes a stormwater outfall that will impact the Puyallup River floodplain and channel migration zone (CMZ). The Project will impose additional constraints for future levee setback projects envisioned by Pierce County as integral to the County's Flood Hazard Management Plan. The Project also proposes a trail and warehouses within riparian areas that will adjoin future levee setback projects envisioned by Pierce County.

#### Technical concerns

These technical comments on behalf of the Habitat Program at Muckleshoot Indian Tribe Fisheries Division (MITFD) seek protection of aquatic and riparian resources, and preservation of means to restore those resources in interest of fish production.

We request City of Puyallup require the Final Environmental Impact Statement to identify specific and effective mitigation for Project impacts to floodplain, CMZ, and riparian environments, or absent that, deny approval of the Project under authorities of WAC 197-11-660, which specifies substantive authority and mitigation.

WAC 197-11-768 explains that "Mitigation" means:

- (1) Avoiding the impact altogether by not taking a certain action or parts of an action;
- (2) Minimizing impacts by limiting the degree or magnitude of the action and its implementation, by using appropriate technology, or by taking affirmative steps to avoid or reduce impacts;
- (3) Rectifying the impact by repairing, rehabilitating, or restoring the affected environment;
- (4) Reducing or eliminating the impact over time by preservation and maintenance operations during the life of the action;
- (5) Compensating for the impact by replacing, enhancing, or providing substitute resources or environments; and/or
- (6) Monitoring the impact and taking appropriate corrective measures.

The DEIS proposes insufficient mitigation for Project impacts to floodplain, CMZ, and riparian environments, including this:

- ER-3. Develop Geotechnical Assessment from a WA Licensed Geotechnical Engineer.
- SW-1 Evaluate the outfall erosion issues prior to Hearing Examiner hearing and prior to County and Hearing Examiner approval and final Project permitting and take corrective action as needed to redesign, repair, or relocate the stormwater outfall structure or components of the Project-wide stormwater management plan in relation to future flow increases from the Project site.
- SW-2. Re-evaluate current stormwater management strategy.

These proposed mitigation measures merely yield future assessments, evaluations, or plans that should be evaluated in the EIS, not afterward. They comprise speculative mitigation outcomes for material environmental impacts, absent specific requirements for mitigation specified by WAC 197-11-768, such as avoiding, minimizing, rectifying, reducing, eliminating, compensating, or monitoring impacts for appropriate corrective measures.

## Conclusion

We request City of Puyallup require the FEIS to identify specific and effective mitigation, as described in WAC 197-11-768, for impacts to floodplain, CMZ, and riparian environments, rather than accept the speculative mitigation proposed in the DEIS via future assessments, evaluations, plans (e.g., ER-3, SW-1, SW-2).

We incorporate here by reference all previous comments submitted by MITFD to Pierce County or other agencies regarding environmental review or permitting for proposed development in the Plan area. My colleague Dr. Martin Fox will forward that correspondence separately, and we request that you reconsider all recommendations therein, whichever remain outstanding for action in the DEIS.

Thank you for the opportunity to comment on the DEIS for Knutson Farms Industrial Park Project, and for your attention of need to define effective mitigation for Project impacts in the FEIS.

Please feel free to contact me or Dr. Martin Fox (copied on this email) with any questions about these matters.

Sincerely,

Patrick Reynolds

Muckleshoot Indian Tribe Fisheries Division

Habitat Program

**Muckleshoot Indian Tribe**

**Commenter:** Fox, Martin

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear Officials:

To follow up on Mr. Reynolds letter below, I am providing two comment letters we sent on previous Notices. Although some concerns brought forth in these comments have been satisfactorily addressed, several outstanding concerns remain. For example, the development footprint, new levee facility, and a recreational trail eliminate riparian protections, which should begin at the outer edge of the Channel Migration Zone as per WAC 222-23-010, WAC 222-16-010. To support the importance of these riparian protections, the Puyallup and Chambers Watersheds Salmon Recovery Lead Entity has since produced The Salmon Habitat Protection and Restoration Strategy for the Puyallup and Chambers Watersheds (2018), which is designed to provide a scientific framework for identifying priorities and strategies to support protection and restoration of salmon habitat. In this document, an area identified as the Core Salmon Habitat and Flood Zone Protection Corridor is provided, which is deemed essential to preserve and protect natural river processes that promote salmon habitat. The Knutson Farms Industrial Park Project is proposed to be built immediately adjacent to this corridor, with no riparian buffer as recommended by this strategy (see below figure). In order for this corridor to function properly, the recommended 200-ft riparian buffer is needed to provide shade, wood inputs, and other functions necessary to maintaining conditions known to provide habitat that supports salmon production.

Thank you for this opportunity to provide comment on the DEIS for this project, and we look forward to working with the City to resolve our concerns in order to protect our fisheries resources.

Sincerely,

Martin J. Fox, Ph.D.

Fisheries Biologist

Muckleshoot Indian Tribe Fisheries Division

39015 172nd Ave SE

Auburn, WA 98092

253-876-3121

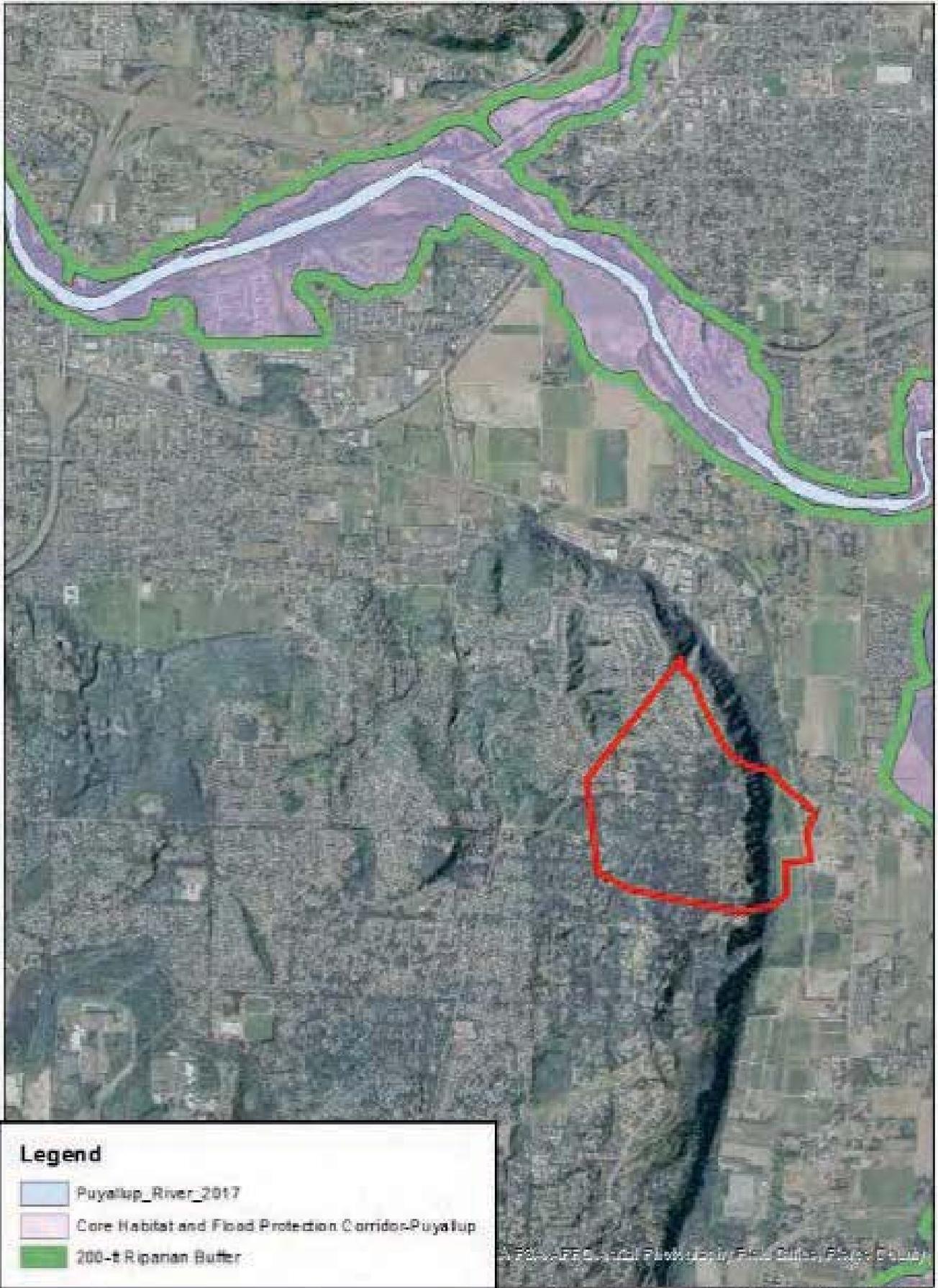
[martin.fox@muckleshoot.nsn.us](mailto:martin.fox@muckleshoot.nsn.us)

## References

Puyallup and Chambers Watersheds Salmon Recovery Lead Entity. 2018. Salmon Habitat Protection and Restoration Strategy for Puyallup and Chambers Watersheds. Pierce County, WA.

Washington Forest Practices Board. 2004. Standard methods for identifying bankfull channel features and channel migration zones. Board Manual Section 2. Department of Natural Resources. Olympia, WA

# Puyallup River Mile 9-12.5





## MUCKLESHOOT INDIAN TRIBE Fisheries Division

39015 - 172<sup>nd</sup> Avenue SE • Auburn, Washington 98092-9763  
Phone: (253) 939-3311 • Fax: (253) 931-0752



May 10, 2017

Mr. Dennis Hanberg  
Responsible Official  
Pierce County Planning and Land Services  
2401 S. 35th St., Suite #2  
Tacoma, WA 98409

**RE: KNUTSON FARMS PROPOSED WAREHOUSE/INDUSTRIAL PARK, MITIGATED DETERMINATION OF NONSIGNIFICANCE: ENV. APP. #792210, FAMILY APP. #: 792206, 792212, 7922136, 840137.**

Dear Mr. Hanberg,

Our Habitat Program staff have reviewed the Mitigated Determination of Non-Significance and updated materials for the Knutson Farms warehouses project referenced above. In addition to comments we sent previously for this project, we offer the following additional comments in the interest to protect and restore the Tribe's treaty-protected fisheries resources.

### Project design changes and additional impact concerns

We appreciate the applicant's efforts to modify and reduce some of the development and infrastructure impacts upon the Puyallup River floodplain and channel migration zone (CMZ). However, as proposed, the project will still encroach into the Puyallup River floodplain and CMZ encroach into an identified levee setback site, and substantially compromise opportunities to restore a viable riparian buffer that provides shade, wood recruitment, and other riparian functions to the river, particularly if the proposed pedestrian trail is placed along margin of mapped floodplain and CMZ areas. To address these concerns, we recommend that the proposal be further modified to avoid development within the floodplain and CMZ to retain the opportunity for a future levee setback project with a functional riparian area that allows for broader river processes and restoration of salmon habitat. The proposal should be modified as follows:

- Remove all stormwater infrastructure from mapped floodplain and CMZ areas,
- Add a 175-foot riparian buffer zone along landward margin of mapped floodplain area, and
- Relocate proposed pedestrian trail within landward edge of 175-foot riparian buffer zone.

Based on recent discussion, we understand that Forterra would be interested to purchase land needed for a 175-foot riparian buffer along the landward margin of mapped floodplain area.

### **Project information missing from the MDNS materials**

The Master Application for Knutson Farms Industrial Park applies to Administrative Design Review and Short Plat (Note: the Pierce County PALs provided link at <https://palsonline.co.pierce.wa.us/palsonline/permitinfo?applPermitId=792210> appears not to be functional). In reviewing materials under this permit number, it does not appear that requirements for Pierce County's Administrative Design Review have been met. For example, an adequate site plan with all graphic items, including trail systems, limits of construction showing existing and proposed grade changes, does not appear to be available. We obtained a copy of the site plan from Pierce County Parks who is working with the developer to incorporate a pedestrian trail system.

A complete and accurate description of proposed floodplain infrastructure is also needed for evaluation of project impacts to fisheries resources and the CMZ, including compliance with ESA requirements per the National Marine Fisheries Service's Biological Opinion for FEMA's National Flood Insurance Program. Therefore, we recommend that Pierce County require revisions to the application materials to address these deficiencies and re-notice this project to reviewers, including the Muckleshoot Indian Tribe Fisheries Division, accordingly.

### **Conflicts with Pierce County Flood Plan**

The revised proposal also conflicts with the 2013 Pierce County Rivers Flood Hazard Management Plan (FHMP). This plan identifies the Knutson Farms floodplain as a compensatory storage area for an \$11M flood control project (see attached), which is also part of a \$341M flood control plan proposed by the U.S. Army Corps of Engineers and Pierce County (see attached). The revised NOA includes new floodplain infrastructure (i.e. storm water outfalls and possibly trails) that will limit or prohibit County/Corps plans for compensatory flood storage in the project area.

The revised proposal also conflicts with the proposed Levee Setback Project Site #5 (a.k.a., the 'Sumner Levee Setback') (Reinhart and Abbe, 2014) which is located adjacent to the proposed development site and includes a portion of one of the proposed warehouses (Building A) (Figure 1). Further, the proposed development extends close to mapped floodplain and CMZ areas, leaving insufficient space to restore a functional riparian buffer along the setback site. The remaining riparian buffer would be further constrained if the proposed Foothills Trail is sited along the perimeter of development, as illustrated in Figure 1. The warehouse development proposal should be modified to provide a sufficiently wide riparian buffer (minimum of 175 feet) along the levee setback site. The Foothills Trail should be located within landward edge of a 175-foot riparian buffer at margin of the identified levee setback site. These previously identified concerns are still outstanding. Pierce County should acquire the mapped floodplain and CMZ areas in the project area for implementation of the levee setback project identified here.

### **Incomplete Wetlands Analysis**

The applicant has not adequately demonstrated that no wetlands exist on the site. Based on the figure provided in appendix B2 “Pierce County Critical Areas Map” within the Critical Areas Assessment Report (Soundview Consultants, Sept. 2016), wetlands are mapped within footprints of the proposed buildings B, D, and E. These wetlands are also identified in Pierce County’s Supplemental Wetland Inventory (courtesy of the Pierce County GIS data portal, 2016) (Figure 2). The wetlands analysis in the Critical Areas Report states there are no wetlands in these identified areas but fail to provide adequate supporting information, particularly the assessment of hydric soils. According to Soundview’s wetland analysis, the soil plots sampled do not coincide with any of these mapped wetlands (Figure 2). Soil plots should have been sampled in these mapped areas to demonstrate there are in fact no wetlands at these locations.

### **Stormwater**

The environmental checklist states that “*stormwater will be discharged directly into the Puyallup River through one existing outfall that will be upgraded and one proposed new outfall to the Puyallup River.*” Both of these outfalls and their maintenance over time will limit channel migration in mapped CMZ areas, contrary to current requirements of the Federal Emergency Management Agency (FEMA). Specifically, FEMA identified compliance options available to affected communities, noting minimum criteria for CMZs that specify “*No activity is allowed that limits the natural meandering pattern of the channel migration zone*” (see: [https://www.fema.gov/media-library-data/1383594225852-50e43955144099bf0eb34d2e5c5b3839/FAQ\\_Compliance\\_Options.pdf](https://www.fema.gov/media-library-data/1383594225852-50e43955144099bf0eb34d2e5c5b3839/FAQ_Compliance_Options.pdf)). The stormwater infrastructure planned for the proposed warehouse development project is inconsistent with FEMA’s efforts to protect listed salmon species in the Puget Sound region.

### **Conflicts with Alderton-McMillin Community Plan**

As noted in the Master Application for the Knutson Farms Industrial Park, the proposed development is within the Alderton-McMillin Community Plan area. This plan describes policies and action steps for desired future conditions that focus on three main areas: improving hillside development; flooding, and stream restoration.

This Community Plan states: “*It is the goal of this community plan to sustain the health, beauty, and function of the unique natural ecosystems of the valley for future generations. The valley’s abundant rivers and wetlands, fertile soils, clean air, forested hillsides, and open spaces provide numerous recreational opportunities, homes and habitat for fish and wildlife, and a healthy local food supply for the urban areas. The valley’s natural resources should be protected and where degraded, should be restored.*”

The Alderton-McMillin Community Plan adopted environment policies that prioritize alleviation of flooding, reduction of erosion and sedimentation, improvement of water quality and aquatic habitat, and

stream corridor restoration. One of the plan's action items include

*“Work with the Public Works and Utilities Department – Water Programs Division to prioritize property acquisition within flood hazard areas.”*

Industrial development of the Puyallup valley's floodplain and farmland is not identified as a focus, whereas the implementation of the aforementioned levee setback project more aligns with these objectives.

### **Recommendations**

Pierce County should require modifications to the proposal as we have recommended above. The County should specifically preclude any new infrastructure in mapped floodplain and CMZ areas. Infrastructure should be setback a minimum of 175 feet from the edge of the proposed levee setback to accommodate a riparian buffer, and the pedestrian trail if desired (Figure 3). Stormwater from this project should be routed into a regional treatment facility rather than directly into the river to reduce risks to water quality and limit channel migration processes. In addition, the County should prioritize property acquisition of mapped floodplain and CMZ areas within the project so the levee setback project for the site can be fully implemented.

We appreciate the opportunity to review this proposal and request that Pierce County provide a written response to these concerns. If you have questions, please contact me at 253-876-3121.

Sincerely,



Martin Fox, Ph.D.  
Fisheries Biologist

CC: Marcia Lucero, Pierce County  
Russ Ladley, Puyallup Tribal Fisheries  
Janet Curran, NMFS  
Chris Conklin, WDFW, Region 6  
Kim Van Zwalenburg, WDOE, Southwest Region

### **Reference**

Reinhart, M.A. and T. Abbe. 2014. Update of Levee Setback and Flood Plain Reconnection Feasibility Study, Puyallup, Carbon, and White Rivers, Pierce County, Washington. Prepared for: Pierce County Public Works & Utilities, Surface Water Management, Tacoma, Washington. Natural Systems Design, Inc. Seattle, WA

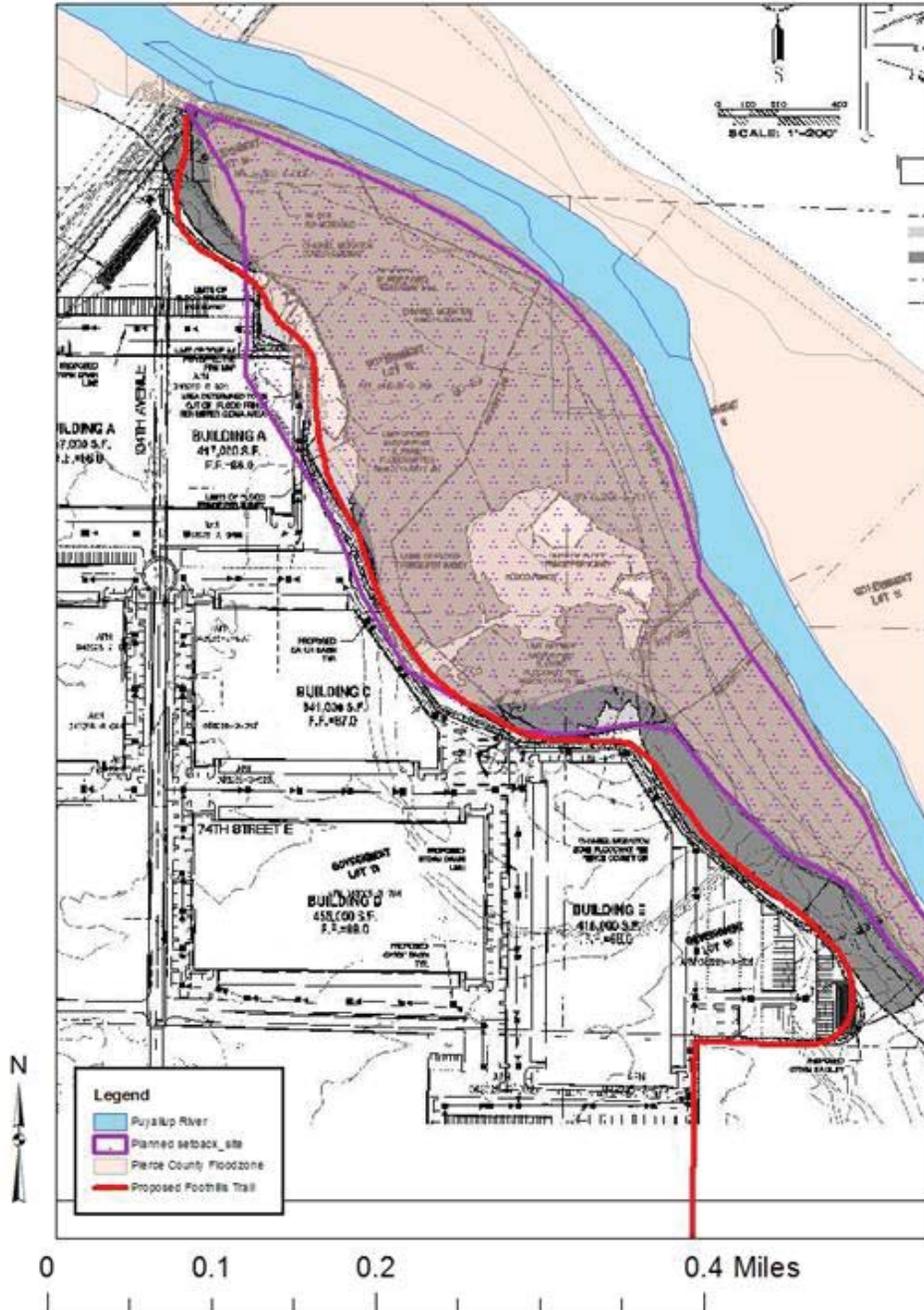


Figure 1. Knutson Farm Warehouse Site development plan shown with proximity to the Pierce County 100-year flood plain, the proposed the Sumner Levee Setback site (project #5 of Reinhart and Abbe 2014), and the currently proposed location of a pedestrian trail.

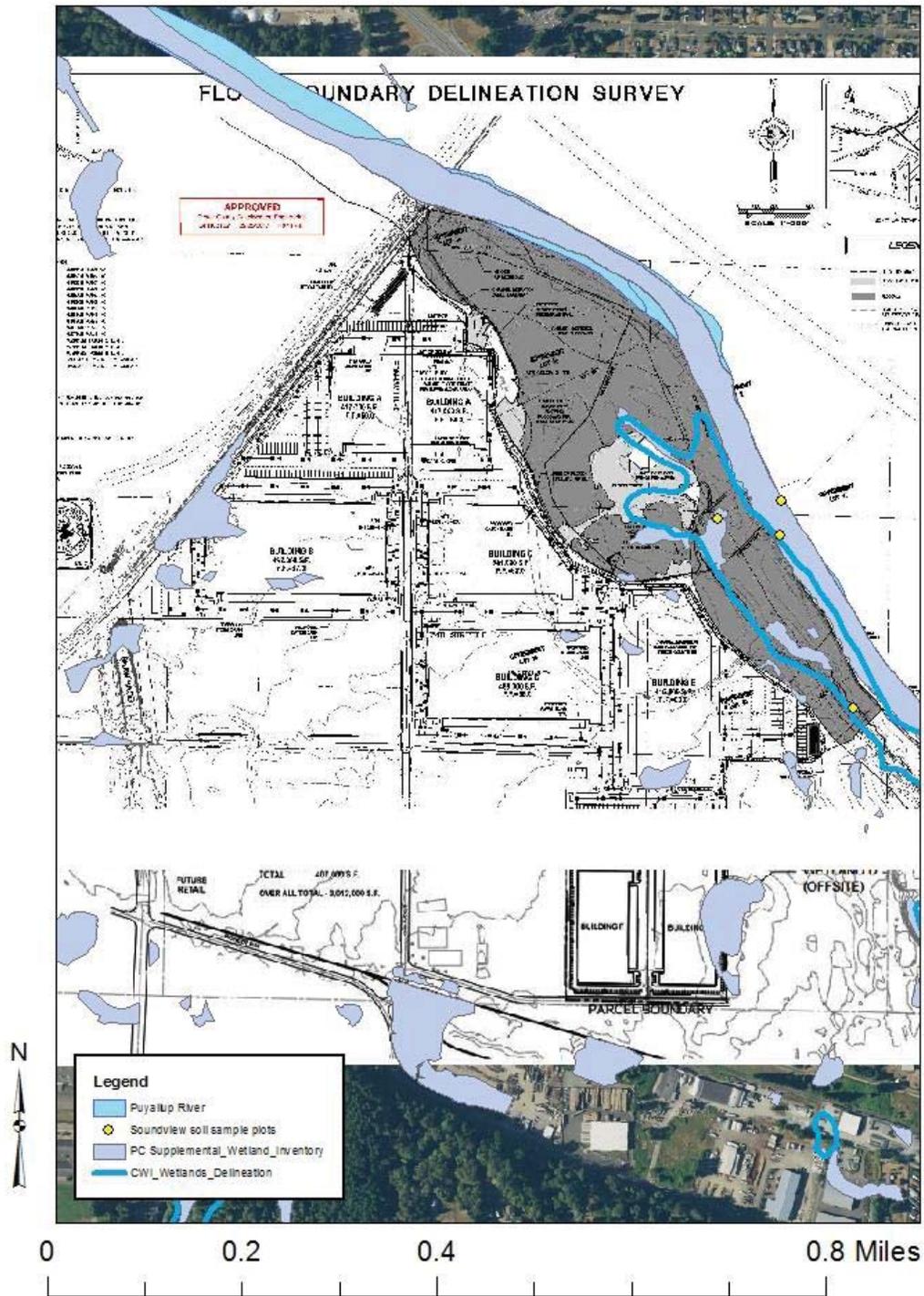


Figure 2. Map of wetlands depicted on Pierce County Supplemental Wetlands Inventory and the CWI delineations. Only the CWI wetlands are acknowledge as wetlands in Soundview Consultant’s Critical Areas Report (SEP, 2016).

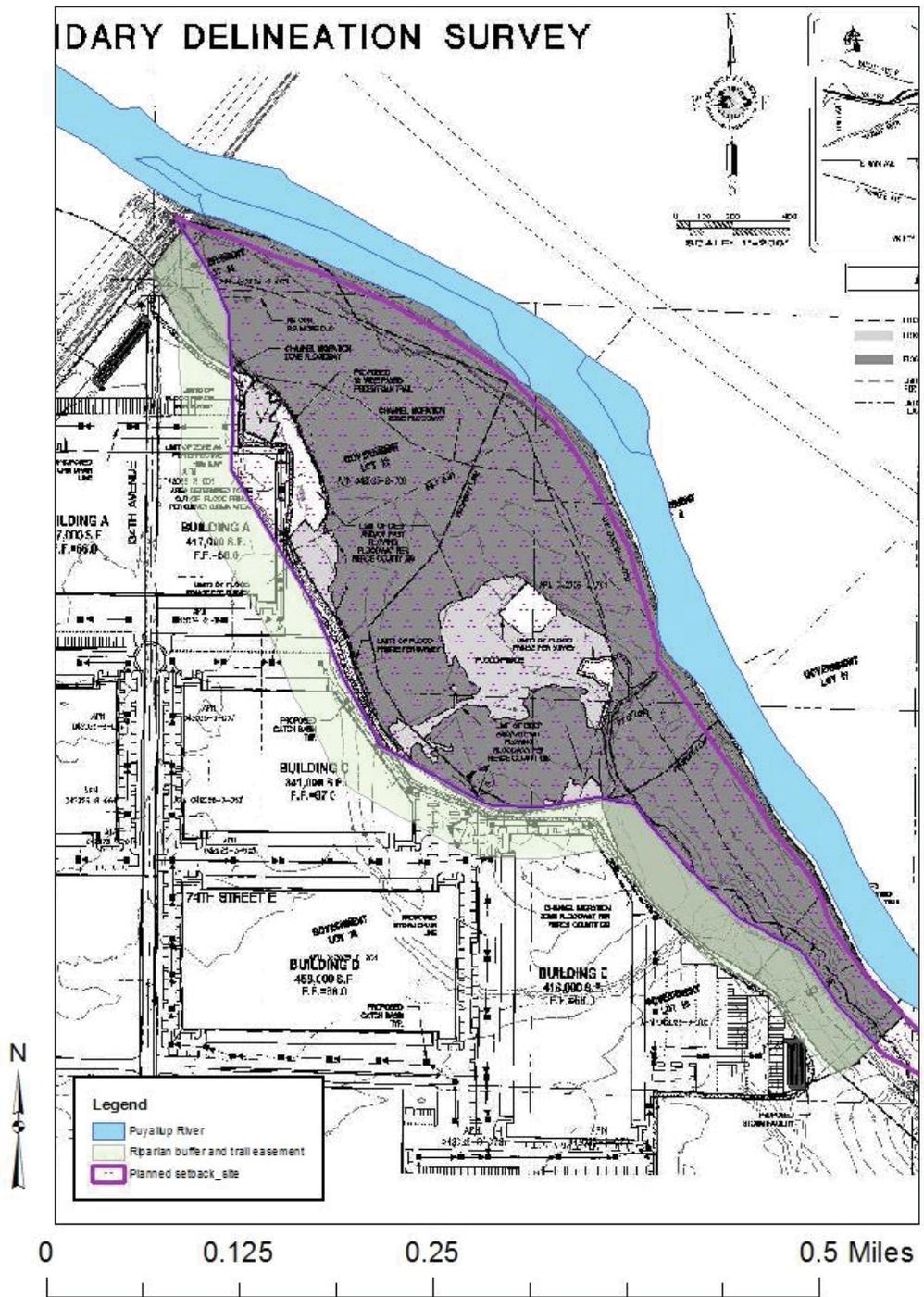


Figure 3. MITFD recommends a 175- development setback to accommodate a 150-ft riparian buffer and space for the planned 25-ft pedestrian trail to remain outboard of this riparian buffer.



## MUCKLESHOOT INDIAN TRIBE Fisheries Division

39015 - 172<sup>nd</sup> Avenue SE • Auburn, Washington 98092-9763  
Phone: (253) 939-3311 • Fax: (253) 931-0752



October 24, 2016

Ms. Marcia Lucero  
Project Manager  
Pierce County Planning and Land Services  
2401 S. 35th St., Suite #2  
Tacoma, WA 98409

**RE: KNUTSON FARMS, UPDATED NOTICE OF APPLICATION, #792206, ADR63-14 / SD36-14 (#792206 / 840137 / 792212 / 792210 / 792213).**

Dear Ms. Lucero,

Our Habitat Program staff have reviewed the updated Notice of Application (NOA) materials for the Knutson Farms warehouses project referenced above. This project proposes new warehouses on current farm land and the construction of new infrastructure in the Puyallup River floodplain and channel migration zone (CMZ). We recommend that the proposal be modified to avoid development within the floodplain and CMZ, and to retain feasibility for a levee setback project that allows for broader river processes and restoration of salmon habitat. We offer the comments below in support of our recommendation, and in the interest of protecting and restoring the Tribe's treaty-protected fisheries resources.

The Puyallup-White River Basin supports fisheries resources of cultural, subsistence, and economic importance to the Muckleshoot Indian Tribe. Chinook, coho, chum, and pink salmon, steelhead and other trout use the basin for spawning, rearing, holding, and migration. The basin includes a portion of the Tribe's Usual and Accustomed Fishing Area (U&A) as defined in *U.S. v. Washington*, 384 F. Supp. 312,367 (W.D. Wash. 1974). Within the U&A, the Tribe retains commercial subsistence, and ceremonial treaty fishing rights, as well as, the authority and responsibility to co-manage shared natural resources with the State of Washington.

### **Submittal Information**

The Master Application for Knutson Farms Industrial Park applies to Administrative Design Review and Short Plat (see: <https://palsonline.co.pierce.wa.us/pals/public/documentView?docSysId=671082>). It does not appear that requirements for Pierce County's Administrative Design Review have been met. These requirements state that site plans are to be drawn to scale and address in writing or through graphic illustration certain items including trail systems, and limits of construction showing existing and proposed

grade changes.

The available documents on file at Pierce County website for this updated NOA are missing grading plans for any of the proposed new floodplain infrastructure (i.e., trail, retaining wall, storm water outfalls, and bio-filtration swale) (see:

<https://palsonline.co.pierce.wa.us/palsonline/#/permitSearch/permit/documents?applPermitId=792206> ).

Also, per an October 12, 2016 email we received from Planning and Land Services staff, the applicant is still working with Pierce County Parks to determine the exact location of the trail. Therefore, the draft maps illustrating the trail location may not be accurate.

A complete and accurate description of proposed floodplain infrastructure is also needed for evaluation of project impacts to fisheries resources and the CMZ, including compliance with ESA requirements per the National Marine Fisheries Service's Biological Opinion for FEMA's National Flood Insurance Program. Therefore, we recommend that Pierce County require revisions to the application materials to address these deficiencies and re-notice this project to reviewers, including the Muckleshoot Indian Tribe, accordingly.

#### **Conflict with Pierce County Flood Plan**

The proposal outlined in the updated NOA also conflicts with the 2013 Pierce County Rivers Flood Hazard Management Plan (FHMP). This plan identifies the Knutson Farms floodplain as a compensatory storage area for an \$11M flood control project (see attached), which is also part of a \$341M flood control plan proposed by the U.S. Army Corps of Engineers and Pierce County (see attached). The proposal outlined in the updated NOA includes new floodplain infrastructure (i.e., trail, retaining wall, storm water outfalls, and bio-filtration swale) that will limit or prohibit County/Corps plans for compensatory flood storage on floodplain in the project area.

The FHMP and the WRIA 10-12 Lead Entity Strategy for salmon recovery have identified levee setbacks as a high priority for flood protection and salmon habitat restoration. In particular, Levee Setback Project Site #5, aka the 'Sumner Levee Setback' (Reinhart and Abbe 2014) is planned in proximity to the proposed development of this site, to which a portion of one of the proposed warehouses will encroach (**Error! Reference source not found.**). Further, the extent of the development to the edge of the topographic break, which is at the extent of the 100-year flood plain/setback project, will provide little room to restore a functional riparian buffer along the setback site. The remaining riparian buffer could be further encumbered if the proposed Foothills trail is situated along this western perimeter as currently proposed by Pierce County Parks. Therefore, we strongly believe that the proposed plan should be modified to provide a sufficiently wide riparian buffer along the levee setback site. Otherwise, the project development should demonstrate how it will not conflict with the levee setback and necessary associated riparian buffer, particularly if the Foothills trail is included in this vicinity.

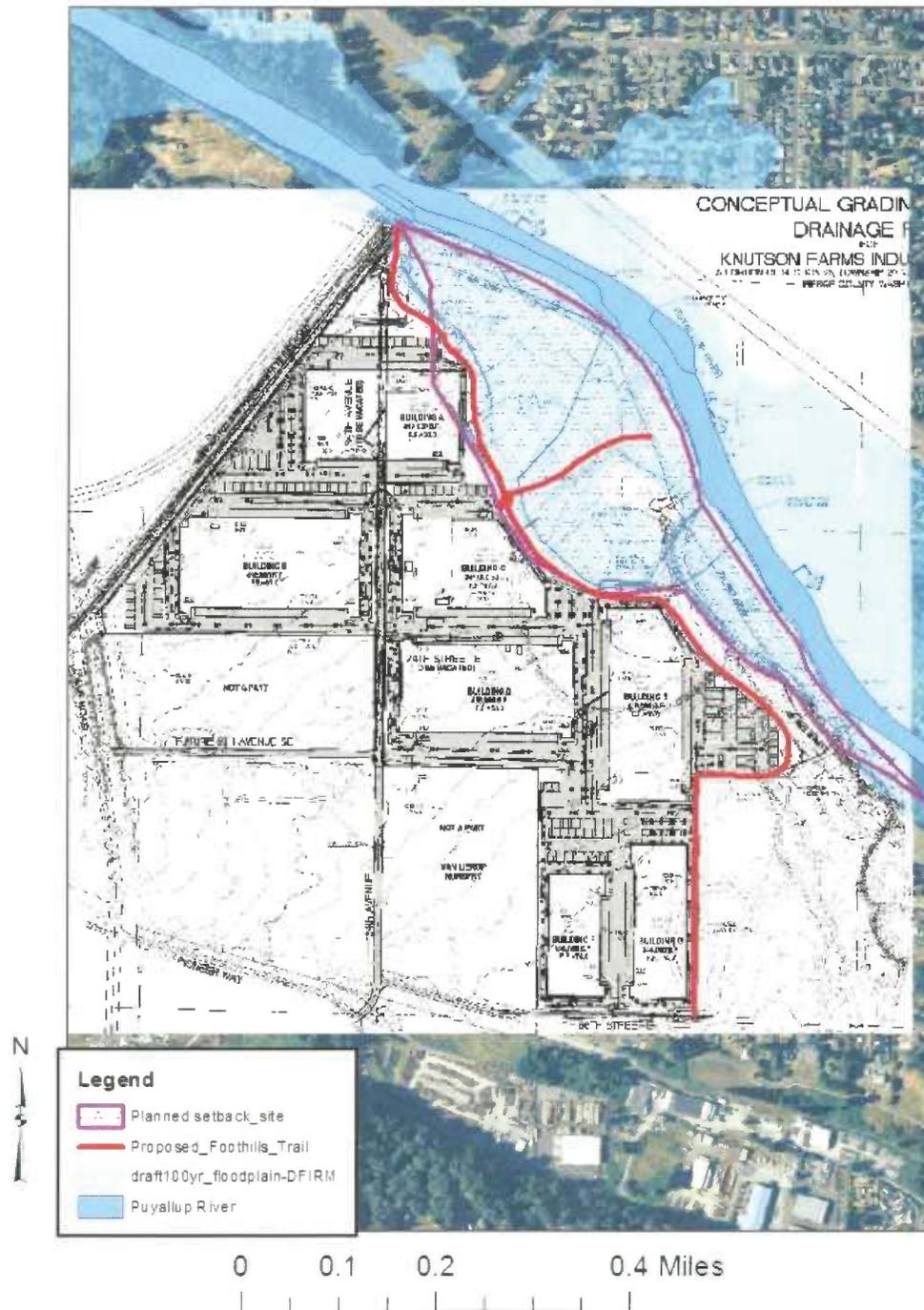


Figure 1. Knutson Farm Warehouse Site development plan shown with proximity to the 100-year flood plain and Project #5, the Sumner Levee Setback site and trail.

### **Conflicts with Alderton-McMillin Community Plan**

The Pierce County Comprehensive Plan includes community plans adopted for specific areas. As noted in the Master Application for the Knutson Farms Industrial Park, the proposed development is within the Alderton-McMillin Community Plan area, which Pierce County adopted in 2008 (see: <http://www.co.pierce.wa.us/DocumentCenter/View/38494> ).

The proposal outlined in the updated NOA conflicts with elements of the Alderton-McMillin Community Plan. This plan describes policies and action steps for desired future conditions that focus on three main areas: improving hillside development; flooding, and stream restoration. Industrial development of the Puyallup valley's floodplain and farmland is not identified as a focus.

The community plan strives to alleviate flooding and restore stream corridor and bank functions for the Puyallup River, thus improving both water quality and quantity issues for fish, wildlife, and people. For example, the plan states: *"It is the goal of this community plan to sustain the health, beauty, and function of the unique natural ecosystems of the valley for future generations. The valley's abundant rivers and wetlands, fertile soils, clean air, forested hillsides, and open spaces provide numerous recreational opportunities, homes and habitat for fish and wildlife, and a healthy local food supply for the urban areas. The valley's natural resources should be protected and where degraded, should be restored."*

The Alderton-McMillin Community Plan also adopted environment policies that prioritize alleviation of flooding, reduction of erosion and sedimentation, improvement of water quality and aquatic habitat, and stream corridor restoration. One of the plan's action items include *"Work with the Public Works and Utilities Department – Water Programs Division to prioritize property acquisition within flood hazard areas."*

Pierce County should require modifications to the proposal outlined in the updated NOA to be consistent with the Alderton-McMillin Community Plan goals and environmental policies. The County should specifically preclude the emplacement of any new infrastructure in the floodplain, including the proposed trail, retaining wall, storm water outfalls, and bio-filtration swale. In addition, the County should prioritize property acquisition of the floodplain in the project area so the levee setback project identified for the site can be implemented fully.

We appreciate the opportunity to review this proposal and look forward to Pierce County's responses to these concerns. If you have questions, please contact Patrick Reynolds at 253-876-3120.

Sincerely,



Karen Walter  
Watersheds and Land Use Team Leader

CC: Russ Ladley, Puyallup Tribal Fisheries  
Janet Curran, NMFS  
Doug Wiedemeier, WDFW, Region 6  
Kim Van Zwalenburg, WDOE, Southwest Region

### **Reference**

Reinhart, M.A. and T. Abbe. 2014. Update of Levee Setback and Flood Plain Reconnection Feasibility Study, Puyallup, Carbon, and White Rivers, Pierce County, Washington. Prepared for: Pierce County Public Works & Utilities, Surface Water Management, Tacoma, Washington. Natural Systems Design, Inc. Seattle, WA

**Puyallup Tribe of Indians**

**Commenter:** Anderson, Lisa

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Attached are the comments from the Puyallup Tribe of Indians.

Lisa A.H. Anderson

Law Office

Puyallup Tribe of Indians



# PUYALLUP TRIBE OF INDIANS



March 14<sup>th</sup>, 2024

**VIA ELECTRONIC MAIL TO [COMMENT@KNUTSONFARMSEIS.ORG](mailto:COMMENT@KNUTSONFARMSEIS.ORG)**

Puyallup City Hall  
Attn: Knutson Farms EIS Comments  
333 S. Meridian  
Puyallup, WA 98371

RE: P-19-0141 Knutson Farms Industrial Park Project, DRAFT Environmental Impact Statement/Environmental Analysis—Comments of Puyallup Tribe of Indians

Thank you for the opportunity to comment on the Knutson Farms Industrial Park Project (“KFIP” or the “Project”). These comments are submitted by the Puyallup Tribe of Indians, (“Puyallup” or the “Tribe”). As the City of Puyallup is aware, the Puyallup Tribe has extensive interests in the Puyallup River and its floodplain. As set forth in detail below, the Puyallup Tribe is generally opposed to the further industrialization of the River, its floodplain, and the lands on which Puyallup have resided for millennia, given the extreme negative impacts that the Tribe has already suffered, and will continue to suffer from existing conditions. The Draft Environmental Impact Statement (“DEIS”) for the KFIP, while inadequate in some areas, identifies a number of significant environmental impacts, many unmitigable, requiring denial of the Project.

## INTRODUCTION AND INTEREST OF THE PUYALLUP TRIBE

The Puyallup are the original inhabitants of the City of Puyallup and surrounding areas, calling these lands home since time immemorial. The lands and waterways were historic village sites, hunting and fishing grounds, seasonal sites, burial grounds, and are vitally important to cultural and natural resources. As signatories to the Medicine Creek Treaty, the Puyallup Tribe reserved rights in the Puyallup River, rights that continue to the present and that the Puyallup Tribe works to protect. These rights cannot be abrogated, except by Congress. Courts have consistently upheld treaty rights such as those retained by the Puyallup Tribe. As co-managers of aquatic and fisheries resources in and around the Puyallup River, the Puyallup Tribe has serious concerns about this Project and the significant negative impacts it will have on the River, the floodplain, wetlands, and all species of fish in the River.

We find the scale and sprawl of this development to fly in the face of the intent of the Growth Management Act and the intent and purpose of the State Environmental Policy Act (“SEPA”). We find that siting a massive development next to the Puyallup River will continue to

bear increasing risks and ongoing impacts to the river, water quality and the fishery. SEPA was intended to manage growth, curb urban sprawl, and preserve open and natural spaces. We need to stop our antiquated ways of building massive developments adjacent to rivers and accommodating growth to the detriment of the natural environment, our fish runs, and the culture, health, and economic stability of the Puyallup Tribe.

## SEPA REQUIREMENTS

In adopting SEPA, the Washington Legislature declared the protection of the environment to be a core state priority.<sup>1</sup> This policy statement, which is stronger than a similar statement in the federal counterpart of the National Environmental Policy Act (“NEPA”), “indicates in the strongest possible terms the basic importance of environmental concerns to the people of the state.”<sup>2</sup> Washington courts have consistently held that SEPA overlays and supplements all other state laws.<sup>3</sup> “SEPA...overlays local ordinances and must be enforced even where a particular use is allowed by local law or policy.”<sup>4</sup>

The point of SEPA is to fully analyze the environmental impact of projects that have a significant impact on the environment.<sup>5</sup> The primary purpose of an environmental impact statement “is to ensure that SEPA’s policies are an integral part of the ongoing programs and actions of state and local government.”<sup>6</sup> SEPA “sets forth a state policy of protection, restoration and enhancement of the environment.”<sup>7</sup> This is often characterized as the “look before you leap” concept, meaning that an agency must ensure that environmental effects are known and carefully considered before it is too late.<sup>8</sup> An EIS is *required* for any action that has a significant effect on the quality of the environment.<sup>9</sup> Significance means a “reasonable likelihood of more than a moderate adverse impact on environmental quality.”<sup>10</sup>

The scope of impacts that must be examined under SEPA, similar to NEPA, includes direct, indirect, and *cumulative* impacts.<sup>11</sup> (emphasis added.) SEPA regulations define impact

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<sup>1</sup> RCW 43.21C.010.

<sup>2</sup> *Leschi v. Highway Comm’n*, 84 Wn.2d 271, 279-80 (1974).

<sup>3</sup> *Adams v. Thurston County*, Wn. App. 471, 475, 855 P.2d 284 (1991).

<sup>4</sup> *Cook v. Clallam County*, 27 Wn. App. 410, 415, 618 P.2d 1030, 1033 (1980). See also *Victoria Tower P’ship v. City of Seattle*, 59 Wn. App. 592, 597 and 599-600, 800 P.2d. 380, 383 (1990).

<sup>5</sup> RCW 43.21C.031(1).

<sup>6</sup> WAC 197-11-400.

<sup>7</sup> *Polygon Corp. v. City of Seattle*, 90 Wn.2d 59, 63 (1978); RCW 43.21C.010.

<sup>8</sup> See also *Marsh v. Oregon Natural Res. Council*, 490 U.S. 360, 371 (1989). Washington courts will look to federal case law interpreting and applying National Environmental Policy Act (“NEPA”) for guidance in interpreting and applying SEPA. See, e.g., *ASARCO v. Air Quality Coal.*, 92 Wn.2d 685, 709 (1979); *Kucera v. State Dep’t of Transp.*, 140 Wn.2d 200, 215-16 (2000); *Gebbers v. Okanogan PUD No. 1*, 144 Wn. App. 371 (2008).

<sup>9</sup> WAC 197-11-330.

<sup>10</sup> WAC 197-11-794.

<sup>11</sup> WAC 197-11-792.

as “the effects or consequences of actions.”<sup>12</sup> Agencies must “carefully consider the range of probable impacts, including short-term and long-term effects and shall include those that are likely to arise or exist over the *lifetime of a proposal or, depending on the particular proposal, longer.*”<sup>13</sup> The range of impacts to be analyzed, direct, indirect, and cumulative impacts, may be wider than the impacts for which mitigation measures are required of applicants.<sup>14</sup> The analysis is not limited to the jurisdictional range of the project proponent.<sup>15</sup> Direct, indirect, and cumulative impacts to be assessed include growth induced by the proposal.<sup>16</sup> The review must address and analyze alternatives to the proposed action, including a “no-action” alternative and alternatives with lessened environmental impact.<sup>17</sup> The range of impacts examined must address mitigation separate and distinct from (and additive to) alternatives and other analysis.<sup>18</sup>

In determining the significance of a project’s impact, the responsible official, here the City of Puyallup, must consider impacts that may appear marginal together in order to assess their significance; must take into account impacts to threatened or endangered species and their habitat; must take into account impacts to cultural resources; and impacts that involve unknown risks to the environment (e.g. the potential to exacerbate effects of climate change.)<sup>19</sup>

“The primary function of an EIS is to identify adverse impacts to enable the decision-maker to ascertain whether they require either mitigation or denial of the proposal.”<sup>20</sup> An agency may impose mitigation measures that are “reasonable and capable of being accomplished”.<sup>21</sup> Potential adverse impacts from mitigation measures themselves must be identified and assessed (unless there will be a full SEPA environmental review specific to the mitigation measure).<sup>22</sup> Where an EIS identifies significant environmental impacts that cannot be sufficiently mitigated (or where the mitigation itself causes adverse impacts), SEPA directs and authorizes denial of the Project.<sup>23</sup>

## COMMENTS

### I. THE PROJECT WILL HAVE SIGNIFICANT ADVERSE ENVIRONMENTAL IMPACTS TO WATER QUALITY, WETLANDS, AND FISH.

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<sup>12</sup> WAC 197-11-752.

<sup>13</sup> WAC 197-11-060(4)(c) (emphasis added).

<sup>14</sup> WAC 197-11-060(4)(e).

<sup>15</sup> WAC 197-11-060(4)(b).

<sup>16</sup> WAC 197-11-060(4)(d).

<sup>17</sup> RCW 43.21C.030(c)(3) and (e); WAC 197-11-792(2)(b); WAC 197-11-440(5).

<sup>18</sup> WAC 197-11-060(5) and (6).

<sup>19</sup> WAC 197-11-330(3).

<sup>20</sup> *Victoria Tower Partnership v. City of Seattle*, 59 Wash.App. 592, 601, 800 P.2d 380 (1990).

<sup>21</sup> WAC 197-11-660.

<sup>22</sup> WAC 197-11-660(2).

<sup>23</sup> WAC 197-11-660.

The large-scale development, even a reduced intensity alternative, has significant impacts to local environmental and cultural resources. Converting 188 acres of agricultural lands to 2.6 million square feet of warehouse and parking will fundamentally alter the natural landscape. The conversion of these lands to hardened surfaces will alter the hydrology and water quality of the area with cascading effects on salmon habitat and other critical areas.

A. Impacts from Impervious Surfaces.

The DEIS identifies multiple adverse and significant impacts to water quality, hydrology and wetlands. These impacts are primarily the result of the enormous increase in impervious surfaces---roofs, roads, parking, and pathways---proposed for the Project (and the overall cumulative increase in impervious when those surfaces are added to the existing Viking warehouse project and the intense industrialization in the lower Puyallup watershed.) Even the smaller-footprint alternative (Alternative 2) results in 1.8 million square feet of developed footage and attendant impervious surfaces. This huge amount of impervious surfaces will adversely affect water quality and wetland hydrology.

Impervious surfaces result in large amounts of stormwater runoff as identified in the DEIS. That runoff is known to carry many pollutants, including nutrients, metals, and various chemicals. The most concerning of these to the Tribe is 6PPD-q. The DEIS recognizes some of the research on 6PPD-q, but should update the research because studies are published almost monthly. The current state of knowledge is that this pollutant washes into streams and is lethal to fish, including species of fish in the Puyallup that are listed as threatened under the Endangered Species Act (“ESA”), including chinook, bull trout, and steelhead, fish in which Puyallup has treaty rights. Puyallup also co-manage coho salmon in the area, a fish to which 6PPD-q is particularly deadly. Even minute amounts of this tire chemical from roads or parking areas will harm salmonids. And, this Project will add to an already large cumulative burden of these extreme toxins from the huge amount of paved surfaces in the region and the lower areas of the Puyallup River.

Additional pollutants of concern are BOD5, ammonia, and fecal coliform, all pollutants for which the Puyallup River is either not meeting water quality standards and/or for which a Total Maximum Daily Load (“TMDL”) cleanup plan was developed in the 1990s. All of these pollutants are likely to increase with the increase in surface runoff.

It must be noted that under 40 C.F.R. § 122.4(i) no new source or discharge of pollutants is allowed to be permitted under the Clean Water Act where the source or discharge will contribute to an existing violation of water quality standards unless a wasteload allocation for those pollutants has been developed, and *has room for the new pollutants, and the wasteload allocation will still be met even with the new source or discharge*. There is no acknowledgement or assessment of this increase in these pollutants or 6PPD-q and whether the TMDL’s wasteload allocation has “room” for them and/or is being met.

The Project's vast amounts of impervious surfaces will also adversely affect wetland hydrology on site. As the DEIS discloses, the increase in impervious surfaces will substantially increase the amount of surface runoff relative to infiltration. (Natural areas infiltrate most precipitation while the built environment causes much of it to runoff). Change to the timing and volume of groundwater recharge will affect wetlands which correspondingly harms wildlife that utilize the wetlands. The DEIS appears to state that the three on-site wetlands will actually be starved of water over time due to the significant changes in site hydrology and lack of infiltration necessary to maintain healthy wetlands. This is contrary to no net loss concepts that direct that wetland functions and values must be maintained for wildlife, for flood control and for water quality functions. Depriving onsite wetlands of water that would infiltrate naturally (or that may be carried from impervious surface containing pollutants) will change the function and nature of wetland and it must be fully avoided. Destruction or negative changes to on site wetlands may also increase downstream flooding due to routing of water to the river and away from wetlands and their natural holding capacity.

#### B. Direct Impacts to Wetlands.

The DEIS also discloses direct adverse impacts to at least one and maybe two on site wetlands. About one-third of Wetland D will be lost directly to fill under the proposed Project. Filling Wetland D will have all of the adverse impacts described above in terms of lost function and values of the wetland and it too must be avoided.

#### C. Cumulative Impacts.

The DEIS discussion of cumulative impacts from the large increase in impervious surface is insufficient in that the cumulative impacts are not fully analyzed and discussed. While the extensive problems and environmental damage from the Viking warehouse outfall are identified and discussed, the DEIS simply states that the Project will make those impacts much worse. It does not appear to quantify the cumulative impacts or fully discuss the synergistic impacts of adding yet more impervious, causing yet more erosion and damage to the shoreline and how that may actually accelerate the environmental damage. It remains unclear how much stormwater volume is anticipated to be discharged from both the Viking and the KFIP site outfalls. What are the stormwater volume estimates expected via both the Viking and KFIP outfalls for the 2-year, 10-year, and 100-year storms?

The DEIS completely fails to discuss the cumulative impacts of adding the impervious surface within the Puyallup watershed regionally. The area of Pierce County surrounding this area of the Puyallup River to the mouth of the River in Commencement Bay is heavily developed, much of it industrial and/or the type of commercial development which includes significant amounts of impervious surfaces. The DEIS fails to address this larger cumulative impact on the water quality and floodplain of the Puyallup River and Commencement Bay, as

well as smaller tributary streams in the area. Without that analysis, any mitigation discussion or assessment is incomplete.

## II. PROPOSED MITIGATION, TO THE EXTENT THERE IS ANY, IS WHOLLY INADEQUATE.

Very little to no mitigation is proposed for the preferred Project alternative and what is proposed is wholly inadequate to address and mitigate the significant environmental impacts described in the DEIS (and herein.) Even the "Alternative 2" with its reduced Project footprint only reduces the impacts in size or total quantity, but does not render them insignificant, and leaves substantial negative impacts unmitigated. Although the reduced intensity alternative (Alternative 2) is surely preferential to reduce site impervious surfaces, it will not eliminate them. Further, the site layout and configuration of the warehouses (per Figure 4-3 in the DEIS) remain unclear. It is mentioned that the square footage of the of the facilities would be reduced by 31% (from 2.6 million square feet to 1.8 million square feet), but a revised site plan is needed and should be included in the FEIS that shows how many warehouses are included in this alternative and how they will be configured on-site.

While Puyallup urges the permitting entities to fully reject this damaging project and not permit it, Puyallup discusses below the minimum mitigation needed to attempt to address the negative effects of this Project.

### A. Remove the Existing Stormwater Discharge.

The DEIS discusses at length the complete failure, and extreme damaging results of that failure, of the Viking outfall (which are possibly illegal) from an earlier stage of the development. The proposed stormwater outfall is located within the mapped severe channel migration zone of the Puyallup River. There is indication that high water events over topped the bank and have altered the sediment regime at the outfall location. In addition, the flow from the current outfall pipes is too heavy for the current conditions. Visual observations of the concrete pad that drains the outfall pipe shows erosion which has separated the pad at the bank. Additional erosion from the heavy flows has cut away the channel and compromised the native plantings and overall stability of the site. The proposed "mitigation" of the extreme damage from this outfall (which may itself violate 40 C.F.R. § 122.4(i) as well as stormwater NDPES permit terms) is itself potentially damaging and, given the assessment of past attempts to "fix" the problem, likely to fail. Rather, the entire outfall must be removed and all runoff from the Viking development rerouted to infiltration treatment and away from the River. This must be "step one" in any mitigation plan for the Project site. Anything less is bound to fail and will simply add damage on top of damage to the River and shoreline.

The absolute minimum mitigation for the existing outfall is complete redesign, *not* riprap and continuing with already failed measures. The area for addressing runoff from both Viking and the Project should in fact occur entirely out-side the flood plain, be located on the upper

terrace where design features will be more or less permanent and readily monitored. This new design will necessitate daylighting a significant portion of the buried conveyance so that treatment and vegetation are not constantly threatened and impaired by natural river dynamics. This is the design that at a minimum, should have been required originally, but was not required in the end to purely to serve the financial interest of the developer. Maximization of developer profits is not a proper environmental impact mitigation consideration. The outfall canal or treatment pond, or whatever name is finally designated must be sized according to the discharge rate and required treatment area necessary to meet water quality standards. It's time to recognize that the current design is and will always be inadequate and ineffective regardless of how much riprap is placed or the number of willows planted.

Page 4-51, the comment about "Durable and effective bank stabilization " is not appropriate. Durable often implies riprap or similar gravity-mass hardened features that simply send or reflect erosive energy to other locations downstream. The entire site is artificially constrained by a RR bridge that acts like bottleneck and levee confinement that simply doesn't allow for adequate channel sinuosity and energy dissipation that would otherwise be realized under natural conditions.

The heat maps provided in the WCI report (Knutson Farm Scour Analysis, Figures 8-24) illustrate the increased channel velocities over a range of flows caused by river channelization. Until this much larger disorder is resolved in the form of a levee setback project, erosion and bank slope failures will continue to threaten the outfall area as well as the entire levee system on both sides of the river.

Tribal Fisheries does not agree with the WCI recommendation (Page 39) and reiterated on page 4-63 of the DEIS, to install riprap at the outfall embankment. Too much of the Puyallup River has been altered with this material and this treatment represents a single purpose mindset that is entirely antagonistic to salmonid habitat, stock recovery and properly functioning riverine and riparian conditions.

#### B. Infiltrate All Runoff from the Project and Including From Viking Development.

While the existing stormwater outfall site is a calamity of design errors, we believe the real threat lies in the discharge water quality itself. Every effort must be made to treat, filter, clean, settle and polish any and all waters being discharged regardless of the release rate. The applied treatments must also provide for the uptake and treatment of 6PPD-q. The developer does not have the luxury of claiming that since the site geology is not ideal for infiltration, all excess storm water will be discharged directly to the river. The Puyallup River cannot absorb any more pollutant load than is already occurring. The absence of load capacity in the river eliminates any opportunity to discharge untreated stormwater.

The primary concern is infiltration at the site. All runoff from the site, and including from the Viking site (see above), both roofs and paved surfaces, *must* be fully-infiltrated. There is no other acceptable mitigation, for protection of wetlands, protection of the River shoreline, and most especially for protection against pollutants, particularly the highly-toxic 6PPD-q. All infiltration must be designed and operated in accordance with the latest research (primarily from Washington State University Stormwater Center at Puyallup) regarding removal of 6PPD-q. The Tribe also refers the City and other regulators to the Center for Watershed Protection's innovative Site Design Handbook. The Center is well known for its innovative thinking on how to manage stormwater with regard to minimizing impervious surfaces, optimizing site infiltration and sizing and siting stormwater infrastructure. We recommend referring to the Center's stormwater site design principles available on their website at [www.cwp.org](http://www.cwp.org)

We also want to emphasize that any revised drainage plan for complete infiltration should include modeling calculations of expected volumes of storm flow from both the Viking and KFIP outfalls, at storms at least up to the 100 year event, so that the infiltration trenches have sufficient capacity to operate successfully, and that system bypasses don't occur except in the most extreme circumstances.

While it may be argued that roof water runoff is cleaner than parking areas, both sources require treatment. Roofs have been shown to collect a variety of harmful airborne pollutants that may deposit for months during dry periods only to be flushed off during rain events creating a potent and potentially lethal concentration of contaminants similar to parking lots and which therefore necessitate the same filtration and treatment prior to discharge.

We are also concerned about the extensive trail system proposed to traverse the site, as detailed on Figure 4-40. Structures and pavement in or adjacent to critical areas such as wetlands or within floodplains alter site hydrology substantially and adequate mitigation rarely addresses the impact adequately. Any pedestrian trails contemplated as part of this project should be moved away from site wetlands and the floodplain and relocated to the high terrace. Low tech methods such as an ordinary gravel paths at grade would work best to preserve some infiltration capacity and obviate the need for additional structures being located where they shouldn't be in the first place.

Finally, the DEIS, per § 4.3, claims infiltration at the site is limited due to geomorphic conditions. Ability to infiltrate *must* drive, and potentially limit, the Project. The build out condition *cannot* exceed the infiltration capacity of the site. Furthermore, the infiltration capacity cannot negatively affect existing wetlands or wetland hydrology. The DEIS mentioned that grading often compromises the porosity of loamy soils. If this should happen, the wetlands that rely on the shallow aquifer will likely go dry for part or all of the year. The grading plan developed for this site needs to specifically address this concern.

C. Reduce Footprint and Infiltrate to Avoid All Wetland Direct Impacts.

Preserving wetland hydroperiods, and diminishing 6PPD pollutants loads and other stormwater pollutants from pavement runoff is critical in protecting river water quality and the tribe's fishery. We agree that infiltration trenches should be situated upslope of all site wetlands and constructed to *at least* meet the minimum performance and threshold requirements per the approved Pierce County Stormwater Management Manual. The 2018 drainage plan that is memorialized in relevant figures in the DEIS needs to be revised to reflect alternative locations for the proposed infiltration trenches to protect the hydroperiods of all wetlands on the site and to filter pollutants prior to discharge to the river.

The revised infiltration trench system must be sized accordingly. The revised drainage plan should include a construction schedule when stormwater infrastructure will be installed and working effectively to ensure wetlands aren't dewatered during site construction and operations at the site.

In 1999, the Puyallup Tribe Fisheries Department proposed that portions of the Knutson Farm site now characterized in this DEIS as the Low Terrace area, be designated as a levee setback restoration site. See Attachments. It is reassuring to see that this area is proposed for protection even if site build-out proceeds. However, it is time to more formally designate this area for a greater purpose and include a timeline for when and where farming practices will occur and where they will not so that a long-term riparian corridor can be designated and established including planting plans. We understand that a farm covenant was identified in a 2017 preliminary short plat written order. It purportedly allowed a 10-year lease for farming activity on the KFIP site. It is unclear as to the status of the lease moving forward, and when farming is planned to cease on the site. The 1999 levee setback plans that the Tribe first proposed and were recently updated in 2022 by ESA are attached to this comment letter for reference. The Tribe would be more than happy to support grant application opportunities to build the project, as it would provide not only habitat and fishery benefits but flood storage as well.

III. THE PROJECT WILL HAVE SIGNIFICANT ADVERSE ENVIRONMENTAL AND ENVIRONMENTAL JUSTICE IMPACTS FROM INCREASED TRUCK TRAFFIC THAT CANNOT BE MITIGATED.

The DEIS has made some effort to estimate truck traffic to and in the immediate vicinity of the Project site, but in constraining its review to a largely regulatory approach (i.e. what is and can be regulated), it is not comprehensively assessing the real, even unregulated, air impact, particularly from mobile source diesel particulates. (Again, under SEPA, the City must assess and discuss environmental impacts that are beyond the scope of its, or any regulatory entity's, jurisdiction.) This is especially true in the DEIS' finding that the increase is "not significant." This blanket statement does not appear to assess the full import of cumulative impacts in this

very heavy truck traffic larger region between the Project site and the Port. With I-5, fast-developing cities, increasing Port traffic (particularly from other proposed large warehouses in the region such as the South Tacoma project) the DEIS' limited look and vague assurances regarding things like diesel particulates is not sufficient and does not adequately address the potential cumulative air impacts on the surrounding community.

The DEIS also does not even mention the possibility of airborne tire particulates. It is known that 6PPD-q is deposited on paved surfaces in particulate form. It is known that the 6PPD-q is thereby carried by stormwater into area waters. But it is also true, and research is developing, that in dry periods, road dust/particulates can become airborne and redeposit elsewhere. Research on potential human exposure and health impacts is still developing. One study has suggested a connection between 6PPD-q exposure and Parkinson's disease in humans. The DEIS must include and assess, as best it can, this developing science and the cumulative impacts of adding this much more 6PPD-q dust to the region.

These impacts cannot be mitigated (absent a proposal like complete electrification of all truck traffic to and from the Project) and none have been proposed other than Alternative 2 which simply somewhat lessens the diesel particulates but may do almost nothing to lessen the cumulative air and health impact. Coupled with the extreme significant adverse impacts to water and wetlands, the unmitigable air impacts dictate denial of the Project.

#### IV. THE DEIS COMPLETELY FAILS TO ASSESS ENVIRONMENTAL JUSTICE AND/OR OVERBURDENED COMMUNITIES, INCLUDING THE PUYALLUP TRIBE OF INDIANS COMMUNITY.

It does not appear that the DEIS has given any consideration to environmental justice or overburdened community impacts, particularly impacts to the Puyallup Tribe community. Overall, the Environmental Justice process requires that no minority or low-income population group should bear a disproportionate share of potential adverse environmental and socioeconomic impacts resulting from major projects. The U.S. Environmental Protection Agency ("EPA") makes available and encourages the use of its EJScreening tool, readily accessed on EPA's website. This tool allows mapping of communities of color, indigenous communities, economically-disadvantaged communities, by age range, by existing pollution impacts, and other potentially relevant categories, in order to assess whether a project may affect a community that is already subjected to challenges. Nowhere in the DEIS does it appear this tool or anything like it has been utilized in order to assess whether this region already bears more than its fair share of environmental and/or other development burdens.

What is plainly evident, even without the tool, is that this region, including the Puyallup Tribe's lands, are heavily burdened already with industrial and commercial development and the potential air and health impacts that come from that development. Health issues in the Puyallup Tribe indicate that development may be a factor in inequitable health outcomes for

tribal members. Adding thousands of truck trips to this equation exacerbates an already inequitable situation. According to a November 19, 2019 article in the Tacoma News Tribune, "Pierce County is on the EPA's 2018 priority project list for places with the highest emissions from diesel engines. The Puget Sound region also ranked in the top 5 percent of the nation for potential cancer risk from air toxins in the EPA's 2011 National Air Toxics Assessment, with diesel emissions identified as the predominant source of cancer risk." The impacts to the overburdened community including the Tribe, its members, and the surrounding residents, must be examined.

The impacts to the Tribe's treaty resources in the Puyallup River are also inadequately addressed (to the extent they are at all) within the DEIS. The Puyallup River is named for the Puyallup Tribe of Indians and the Tribe has used the River for food and cultural purposes since time immemorial. The Puyallup River has been cumulatively burdened with attendant extreme negative impacts on the Tribe and its culture and treaty rights. This must be identified and analyzed within the DEIS in detail and by seeking information specifically from the Tribe.

The use of the EJScreening tool and discussion of its information is the *minimum necessary* for assessing potential environmental justice impacts. The DEIS must be revised to include this information and analysis.

## CONCLUSION

Because of the significant and numerous adverse environmental impacts to the Puyallup River, its floodplain and wetlands, to treaty and ESA protected fish and to the health and well-being of the Puyallup Tribe, many of which will not be or cannot be sufficiently mitigated, the Puyallup Tribe urges the City and other regulators to DENY this Project. We do not support such high intensity development at the shoreline of the Puyallup River, especially without any enhancement or protection of ecological functions. This type of development is inconsistent with the natural features of a river that hosts endangered species and should not be allowed in such close proximity to those resources. At a minimum, all of the mitigation measures outlined by the Tribe above, must be required and sufficiently monitored and enforced into the future.

Sincerely,



Lisa A.H. Anderson  
Environmental Attorney  
Puyallup Tribe of Indians

Attachments: Sumner Setback Levee Materials

## Site 6. Sumner Setback

T20N, R4E, Section 25

### Location

The site is located on the left bank at R.M. 10.8. The site is bounded by the Puyallup River to the north, 134<sup>th</sup> Ave to the west and 74<sup>th</sup> St. on the south side.

### Existing Conditions

#### Physical Conditions

The site is river floodplain approximately 220 feet wide, with a large (20 acre) wetland occupying former river channel. A slope paralleling the river rises 20-35 feet, defining the floodplain and the practical limits of the restoration action. A railroad trestle at the downstream end of the site dictates constraints in that portion of the site. Evidence of flooding over the levee was observed during site investigations. A review of Pierce County records indicates the site is within the service area of the Webstone Water Company. Contact with the water company did not indicate any underground utilities in the levee. A water pipeline crosses the Puyallup River immediately upstream of the site. The County does not, however keep complete records of exact utility locations and inquiries to separate utilities and municipalities in the project vicinity would be necessary to identify any constraints.

#### Biological Functions

The floodplain portion of the Sumner Setback site is currently in agricultural use as pasture/hayfield. The wetland portion of the site likely provides waterfowl staging habitat during the winter and spring. The site provides urban open space and otherwise likely provides functions similar to upland meadow.

### Restoration Opportunities

#### Physical Restoration Opportunities

The restoration action here would consist of a levee setback and wetland enhancement. Up to 1800 feet of levee could be moved back as far as the toe of the existing slope, depending on the amount of excavation and aquatic habitat desired. Maximum excavation (lowering the site 5 feet for the entire length) to the toe of the slope would require removing 320,000 yds<sup>3</sup> of material. A 100ft wide channel in the site would require removing 33,500 yds<sup>3</sup> of material. Some excavation will be necessary outside of the existing wetland, depending on final restoration design. This site presents an excellent opportunity for wetland construction along an elevation gradient. Existing contours could be used to create different hydrologic regimes required by a variety of wetland types.

#### Functions provided by Restoration

Pulling back the levee and allowing the river to meander through former channels will increase side channel, braid and backwater habitats. River substrate at this reach is currently gravel and cobble. Braided channels offer improved **upstream migration** conditions, **wintering habitat**, **summer rearing habitat** and potential (though limited) **spawning** for some species. Shallow water areas in emergent vegetation would provide **foraging and predator avoidance** habitat for juvenile fish. Channel capacity would increase along this stretch.

#### Restoration Costs

This site has been assessed for \$280,000.00. However, purchase of the entire site may not be necessary. The floodplain portion of the site is 40 acres and currently not in intensively farmed. Excavation costs will vary with design. The maximum excavation costs are approximately \$1.6 million. Levee pullback costs are for 1800ft of

## Site 6. Sumner Setback

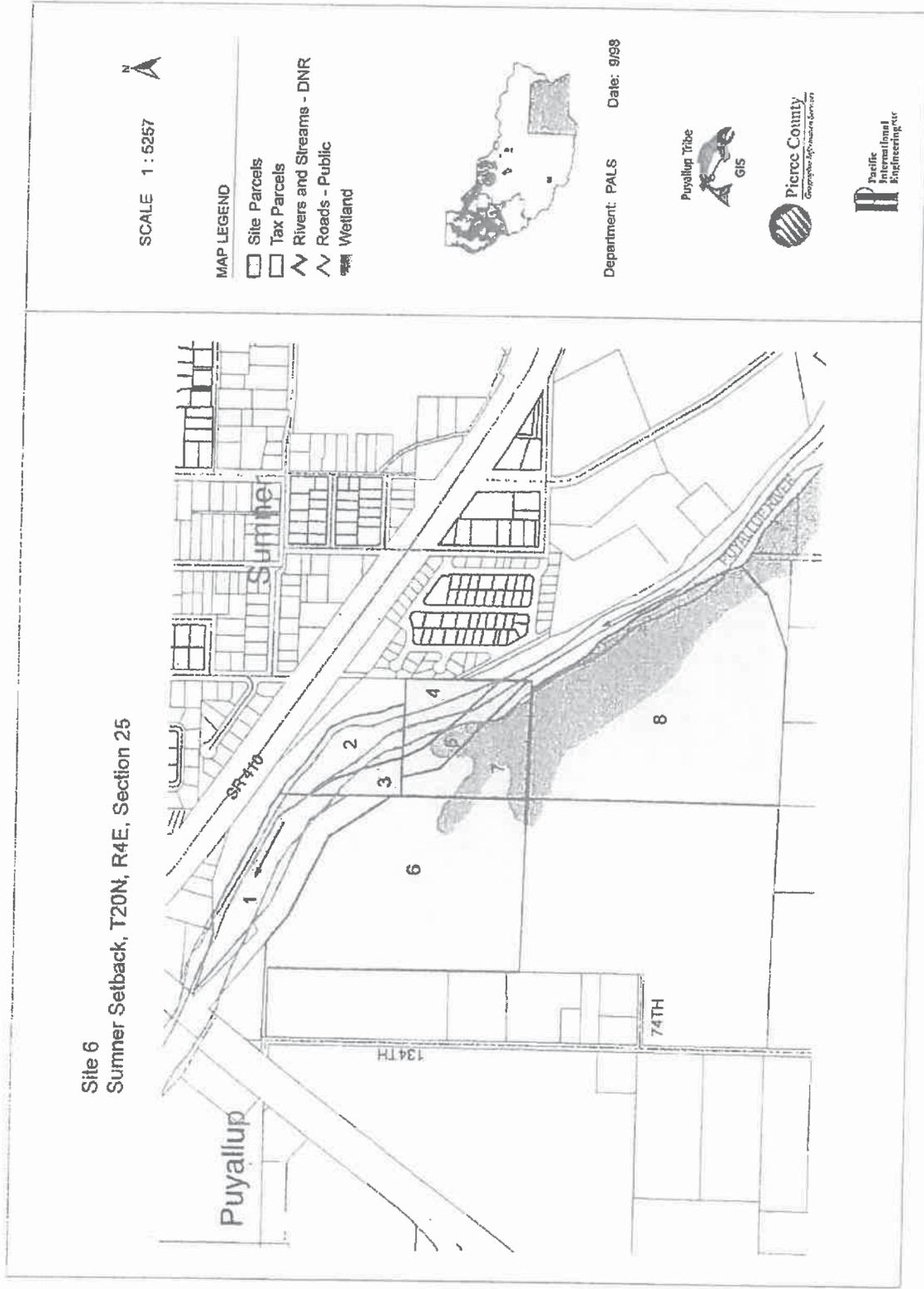
levee. If the site is excavated to the toe of the slope, slope revetment costs would approximate levee construction costs.

Parcel Acquisition	\$280,000.00
Channel excavation (320,000yds <sup>3</sup> )	\$1,600,000.00
100ft-wide (33,500 yds <sup>3</sup> )	\$235,000.00
Levee/Revetment	\$720,000.00
Design and permitting	\$95000.00
Contingency (15%)	\$143,000.00
WA State Sales Tax (8%)	\$84000.00

Design and permitting costs are dependent on the degree of setback. The excavation costs assume the material can be stored on-site. Some costs could be reduced by selling the topsoil.

### Summary

- 78-acres site occupied by 20-acre wetland.
- Large floodplain area in agricultural use along Puyallup River.
- Levee pull back to topographic feature would result in 40 additional acres of aquatic habitat.
- High visibility project potential – Near downtown Sumner.
- 10.8 miles up the Puyallup River.



Site 6  
Summer Setback, T20N, R4E, Section 25



SCALE 1 : 5257



MAP LEGEND

- Site Parcels
- Tax Parcels
- Rivers and Streams - DNR
- Roads - Public
- Wetland



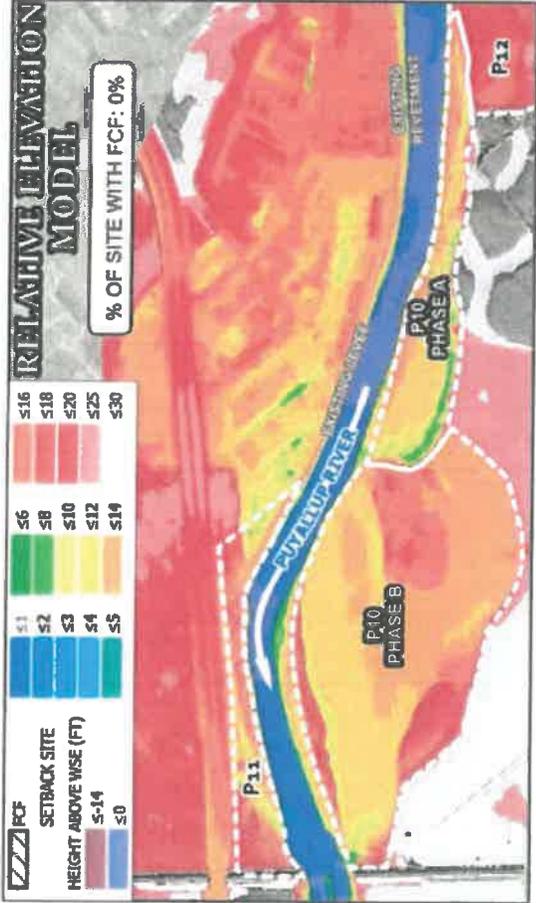
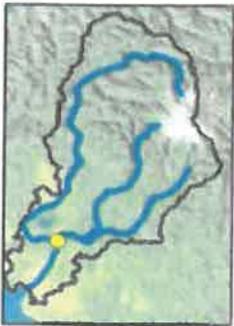
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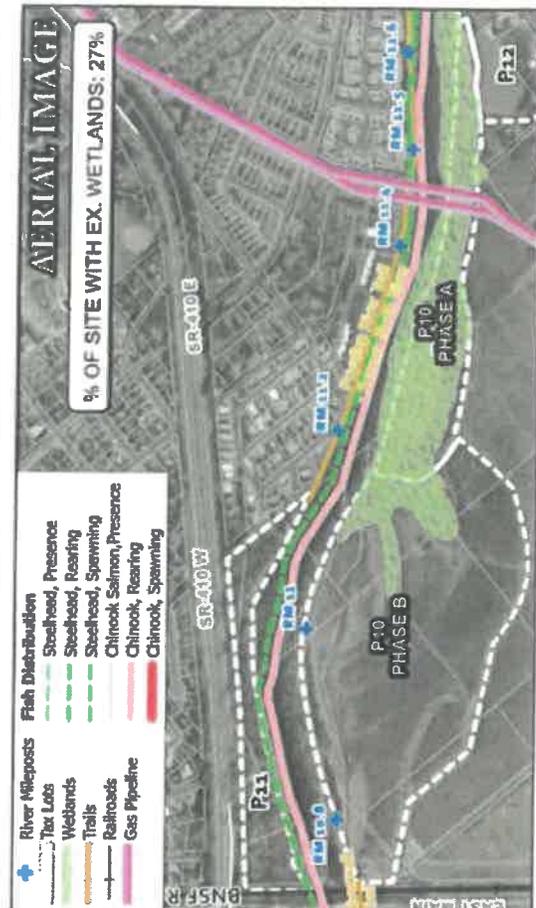
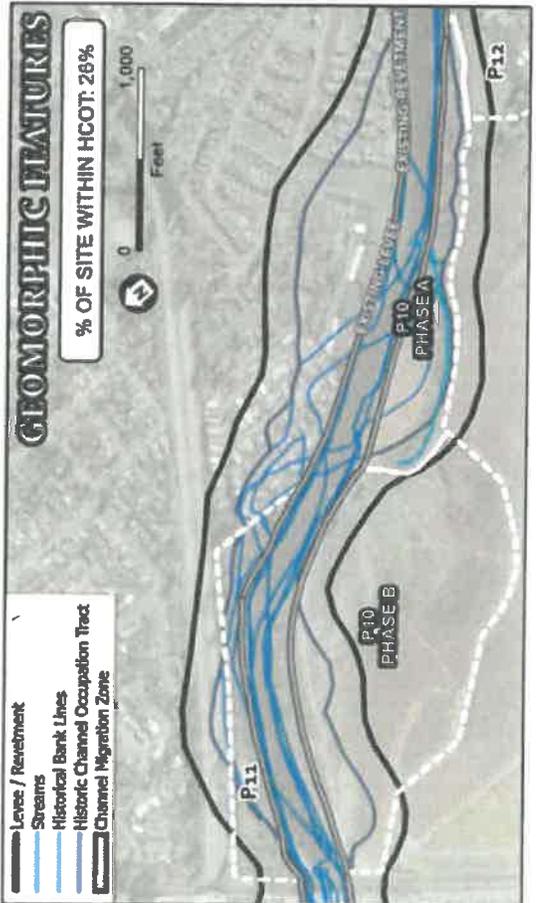
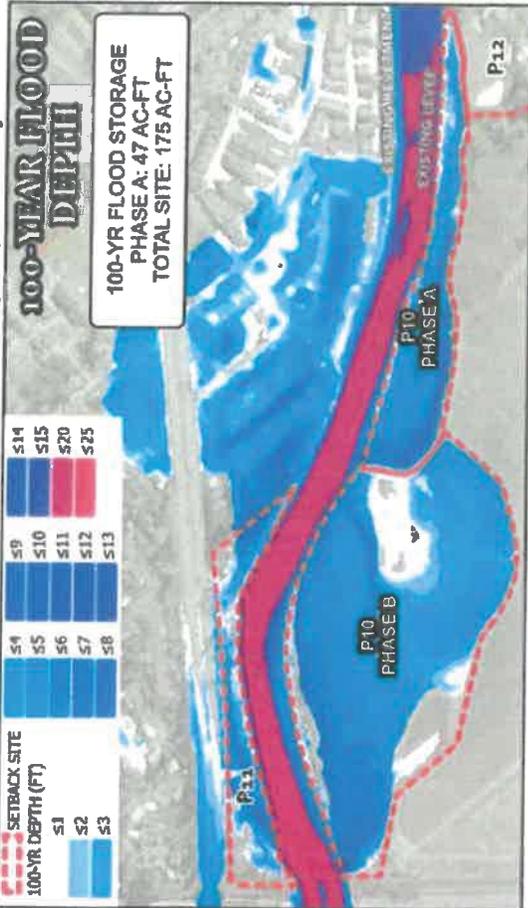


# Summer Setback #P10

System/Bank: Puyallup - Left Bank  
 Nearest Township: Sumner  
 River Mile: 10.7 - 11.6  
 Coordinates: 47.19 N, 122.25 W  
 Acreage: 55.2



The Summer Setback is located on the left bank of the Puyallup River directly upstream of the East Main Bridge. The site occupies 55 acres and spans 5,000 linear feet with farm & agricultural tax parcels, but contains several County owned parcel. Existing wetlands are identified as an opportunity to preserve and enhance habitat and improve flood storage. Due to size and parcel acquisition, a phasing opportunity is identified to sequence site development for a smaller setback project containing the majority of identified wetlands located within the footprint of the historic channel. A gas main is identified to cross underneath the site, requiring evaluation for a possible conflict.



# Summer Setback #P10

## OPPORTUNITIES

- Significant peak flood storage.
- Phase site development in upstream and downstream projects with temporary cross-levée. Existing wetlands located in Phase 1 provide opportunity to preserve and enhance habitat.

## CONSTRAINTS

- Evaluate potential impacts to gas main.
- Perform scour evaluation at East Main Ave. bridge to evaluate project influence on bridge hydraulics.
- 5 property acquisitions identified to maximize available flood storage.
- Coordination with warehouse development on adjacent parcels and associated infrastructure, including a stormwater outfall east of the bridge.



## SITE DEVELOPMENT

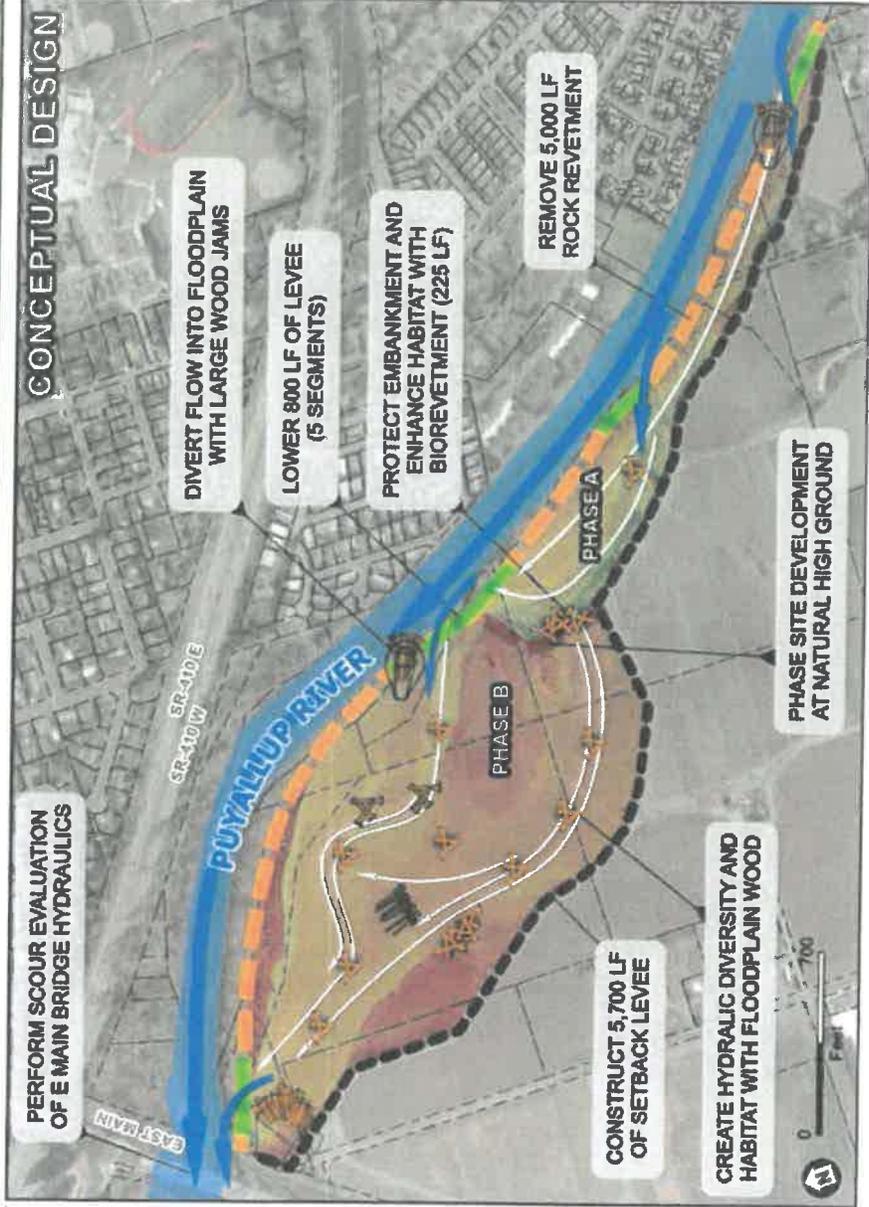
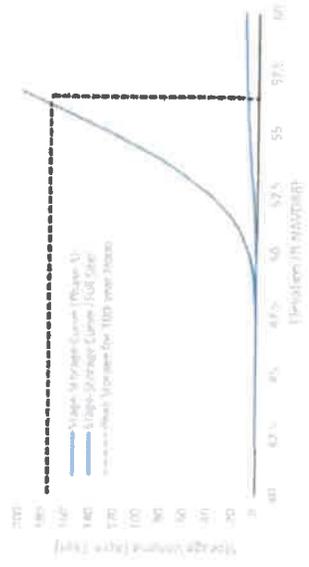
Total Site Area: 55 Acres  
 No. of Pierce County Parcels: 5  
 No. of Parcel Acquisitions: 5  
 Length of Revetment Removal: 5,000 LF  
 Total Planning Level Project Cost: \$21,700,000  
 Planning Level Cost per Acre-Foot: \$125,000

## LEGEND

- |  |                 |  |                       |  |              |
|--|-----------------|--|-----------------------|--|--------------|
|  | WOOD STRUCTURES |  | TEMPORARY CROSS LEVEE |  | RIVER FLOW   |
|  | LOWER LEVEE     |  | FLOW PATHWAY          |  | EXIST. CREEK |
|  | SETBACK LEVEE   |  | REMOVE REVETMENT      |  |              |

## FLOOD STORAGE SUMMARY

Flood Storage for 100-Year Event (Phase A): 47 AC-FT  
 Flood Storage for 100-Year Event (Total): 175 AC-FT  
 Average 100-Year Flood Storage per Acre: 1.5 AC-FT / AC



**PUYALLUP RIVER  
SITE P10 (SUMMER SETBACK - KNUTSON LEFT BANK)**

Estimate Description: Construction Cost Estimate

1/28/2012

Engineer: G. Wolowick, M. Nelson, H. Snow

Reviewed by: Jon Ambrose

BID ITEM	DESCRIPTION	QTY.	UNIT	UNIT PRICE	TOTAL AMOUNT	NOTES
<b>GENERAL REQUIREMENTS AND REMOVALS</b>						
0001	MOBILIZATION	1	LS	\$790,000	\$ 790,000	10% of Subtotal
0002	TEMPORARY TRAFFIC CONTROL	1	LS	\$50,000	\$ 50,000	Flags and PGMS required.
0003	TREE PROTECTION, SALVAGE, AND REMOVAL	10.7	AC	\$10,000	\$ 107,000	
0004	CLEARING AND GRUBBING	10.7	AC	\$25,000	\$ 267,500	Construction Access, Levee Removal, Wood Installations
0005	REMOVE RIPRAP REVETMENT AND HULL	5,000	LF	\$150	\$ 800,000	Assume 38-inch and Full Height of Levee Prism per 2020 UDAF.
0006	CONSTRUCTION SURVEYING	1	LS	\$25,000	\$ 25,000	Based on Similar Bid Tabulations
0007	UTILITY RELOCATION	0	LS	\$50,000	\$ 1,000	No Utilities Currently Identified.
0008	REMOVE CONCRETE AND DEMOLITION	0	SY	\$50	\$ 0	No Pavement Removal or Demolition is Identified.
<b>SITE ACCESS &amp; EROSION AND SEDIMENT CONTROL</b>						
0009	TEMPORARY SITE ACCESS AND STABILIZATION	2	EA	\$25,000	\$ 50,000	2 x Construction Access Point (Large Site)
0009	TEMPORARY EROSION AND SEDIMENT CONTROL (TESC)	10.7	AC	\$16,000	\$ 168,500	
0010	TEMPORARY WATER ISOLATION	1	LS	\$125,000	\$ 125,000	Assume \$25,000 per 1000 LF
0011	DEWATERING	1	LS	\$75,000	\$ 75,000	Assume \$12,500 per Large Wood Structure Installed Adjacent River.
<b>EARTHWORK AND LEVEE CONSTRUCTION</b>						
0012	LEVEE EXCAVATION	10,700	CY	\$20	\$ 214,000	Lower Existing Levee / Selective Breaching.
0013	FLOODPLAIN GRADING	0	CY	\$25	\$ 0	No Floodplain Grading Identified at this Location.
0014	LEVEE FILL (IMPORTED)	81,400	CY	\$26	\$ 2,035,000	
0015	LEVEE ACCESS ROAD (CSBC)	3,150	TN	\$35	\$ 110,250	Levee Prism Access Road
0016	EXCESS MATERIAL HAUL	12,840	CY	\$25	\$ 321,000	Haul excess material to waste (total excavation x20% swell factor)
<b>FLOODPLAIN AND HABITAT RESTORATION</b>						
0017	BIOREVETMENT	0	LF	\$600	\$ 0	No Biorevetment Identified at this Location.
0018	LARGE WOOD STRUCTURES	1	LS	\$550,000	\$ 550,000	Apex Jam, Deflector Jam, Side Channel Wood.
0019	SCOUR PROTECTION ROCK	10,620	TN	\$65	\$ 694,125	Scour protection at setback levee prism / biorevetment
<b>PLANTING AND SITE RESTORATION</b>						
0020	RIPARIAN AND UPLAND PLANTING	8.5	AC	\$15,000	\$ 127,500	Assume Setback and Access Road to be Restored
0021	TOPSOIL (IMPORTED)	8.6	AC	\$20,000	\$ 170,000	Assume Setback and Access Road to be Restored
0022	HYDROSEEDING	10.7	AC	\$6,000	\$ 64,200	Assume all Disturbed Areas.
Direct Item Construction Subtotal					\$ 8,827,075	
Estimate Uncertainty Contingency 20%					\$ 1,765,415	
Contingency Subtotal					\$ 5,182,490	
WA State Sales Tax 8.6%					\$ 704,554	
Construction Subtotal					\$ 8,597,044	
Total Estimation of Probable Construction Cost (Rounded)					\$ 4,800,000	
Assessed Property Value					\$ 4,855,800	From Pierce County Assessor Portal (\$ Parcels)
Property Acquisition					\$ 3,282,800	Assume 60% Markup
Public Outreach					\$ 25,000	
Engineering and Design (25%)					\$ 2,225,000	
Permitting (1.5%)					\$ 135,000	
Construction Management and Inspection (10%)					\$ 680,000	
Total Estimation of Probable Cost for Project (Rounded)					\$ 22,600,000	

- Notes:**
- The Estimate Uncertainty Contingency is to address potential known unknowns in this construction cost estimate. This does not include additional contingencies that may arise during construction (such as weather) or contingencies for additional engineering during construction that could arise due to unexpected site or design issues.
  - This cost estimate has been prepared for guidance in project evaluation from the information available at the time of preparation and for assumptions stated. The final costs of the project will depend on actual labor and material costs, actual site conditions, productivity, competitive market conditions, final project scope and schedule, and other variable factors. As a result, the final project costs may vary from those presented above.
  - Cost and quantity estimation assumptions are provided in Appendix C of the 2021 Levee Feasibility Update.

**City of Sumner**

**Commenter:** Kosa, Michael

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Hello,

Please see the attached for the City's [Sumner] DEIS comments. If there are any questions or concerns, feel free to contact me.

Regards,

Michael



CITY OF  
**SUMNER**  
WASHINGTON

1104 Maple Street, Sumner WA 98390

March 14, 2024

Puyallup City Hall  
Attn: Knutson Farms EIS comments  
333 South Meridian  
Puyallup, WA 98371  
[comment@knutsonfarmseis.org](mailto:comment@knutsonfarmseis.org)

RE: Knutson Farms DEIS Comments – City of Sumner

To Whom it May Concern:

The City of Sumner (City) has completed a review of the published Knutson Farms DEIS for the proposed development (Site) and is providing the following comments. Attention to our input is greatly appreciated, and I would like to acknowledge and thank City of Puyallup staff for reaching out directly to the City during the comment period to discuss the project prior to the comment period deadline.

Below are comments related to the project:

Comments regarding *Knutson Farms Industrial Park Project Traffic Analysis Report (Final)*, December 2023 prepared by HDR:

**Comment 1, Trip Distribution Page 10, 12-13:** The description on page 10 of the traffic report on the trip distribution does not disclose the basis of the distribution assumptions. Additional detail should be provided on the sources and assumptions for the trip distribution, and those sources and assumptions should be reevaluated per the discussion below.

Based on a coordination meeting with the City of Puyallup and HDR, we understand that the basis of the trip distribution is existing traffic counts and considers the majority of freight traffic coming to/from the Site to be to nearby Regional Manufacturing and Industrial Centers (MICs) in Frederickson and the Port of Tacoma. However, this freight traffic distribution did not account for the much closer Sumner-Pacific MIC located less than 2 miles away and is only a short 5-minute drive from the Site. The City believes that some or even most of the Site freight traffic could originate or arrive at the Sumner-Pacific MIC, causing a significant amount of traffic. The Sumner-Pacific Regional MIC, the largest in Pierce County, has both national and international industrial uses and attracts over 6000 local and regional trips daily. With Amazon warehousing infrastructure and a Costco regional distribution facility, it is likely that complementary or direct supporting warehouse space may be a tenant for the Site. This would cause a dramatic increase in traffic over projections and overwhelm Sumner's Traffic Avenue, Sounder Station and Sumner



CITY OF  
**SUMNER**  
WASHINGTON

1104 Maple Street, Sumner WA 98390

Town Center, a designated Countywide center, with stopped traffic for hours every day, impacting regional transit commuters, business activity, and increasing carbon emissions. Truck travel to the MIC would be expected to use Traffic Avenue, which is a T-2 truck route. Even traffic leaving the Site and heading to SR 410 via the Traffic Avenue ramps will have a dramatic impact on City of Sumner traffic, as there will be additional vehicles at the SR 410 interchange that will impede travel into/out of the City via Traffic Avenue and into and out of the Sounder station for both buses and vehicles.

While the use of traffic counts provides an understanding of current travel patterns, regional consideration should be given to the location of the Site. Freight heading to/from destinations north of the Site such as the Port of Seattle and Snoqualmie Pass via Interstate 90 will have to utilize the very congested regional transportation system including SR 167. Due to daily backups on an overcapacity SR 167, the City of Sumner experiences regional cut-through traffic with freight and commuters alike avoiding the congestion along SR 167 by utilizing City of Sumner local roads that were not built to handle this level of traffic. Although WSDOT has future improvement projects for southbound SR 167, funding is not in place at this time and the timeline is unknown. Even with all planned improvements, the long-term travel demand modeling for the study area and the City of Sumner shows regional cut-through will continue even after state highway improvements are complete (*SR 167 Master Plan, Planning and Environmental Linkages Study, June 2023*). Given the location of the Site and the horizon year of 2026, it is anticipated that the Site will significantly add regional cut-through vehicles to the City street network. The draft Transportation Management Plan forecast modeling being developed by the City of Sumner in coordination with the 2024 City of Sumner Comprehensive Plan Update indicates that commuters will use the SR 167/24th Street E interchange and travel via W Valley Highway E to Traffic Avenue/E Main Avenue to access the Site. There is also the very practical consideration that no matter what models say or how you sign routes, every driver now uses a phone to map their route, and every mapping app directs vehicles from this location to any points north with the first step of “Get on WA-410 West in Sumner...”

The current distribution of 5 percent of the traffic to/from Sumner appears to be an underestimate given regional congestion and the MIC. The proposed project trip distribution should be adjusted to consider regional travel patterns and truck travel to/from the Sumner MIC before reassessing the Site’s impact to intersections north of the Site toward and into Sumner. **The City does not support the 5 percent of trips to/from Sumner, as it finds this to be inaccurate. The City expects this to be significantly higher due to the factors described above. The City feels that a mistake in this assumption could have significant implications depending on the final use and tenants at the Site. The need for additional mitigation measures should be considered after this reanalysis is complete.**



**Comment 2, Puget Sound Gateway Program:** The Puget Sound Gateway Program is a significant project in the study area that combines the SR 509 Completion Project in King County and the SR 167 Completion Project in Pierce County to complete critical missing links in Washington state's highway and freight network. Construction of the Gateway Program is anticipated to be completed by 2029. The EIS Record of Decision for the SR 167 portion of the project was completed in 2007 and has been updated as appropriate since that time.

While the City understands the horizon year of the proposed project is 2026, the Gateway Program has been planned for decades and will change travel patterns to and from the Site. This project will alter potential impacts and require different mitigation measures for the proposed project. Given that the Gateway Program would be completed 3-years after the project horizon year and it is likely that the proposed project will be phased and may not be fully completed and occupied by 2026, a sensitivity analysis of the model should be conducted to determine a before-and-after assessment of traffic patterns due to Gateway Program completion to evaluate changes in project impacts and mitigation measures. The EIS should consider if required mitigation measures should be adjusted assuming short-term impacts could be mitigated with the Gateway Program while long-term impacts could result with the anticipated changes in travel patterns. **The City does not feel that the Gateway Program currently under construction by WSDOT has been analyzed and accommodated in the provided Traffic Analysis.**

**Comment 3, Mitigation Measures Page 58:** The traffic report identifies mitigation measures for the study intersections along Traffic Avenue including the SR 410 interchange. The mitigation measures should consider the following:

- **Traffic Avenue/Cannery Way & Traffic Avenue/State Street** – The Sumner traffic signal equipment is old and outdated. There is no interconnect along the Traffic Avenue corridor. In order to implement the proposed traffic signal retiming and coordination, the traffic signal equipment at both intersections will need to be upgraded and signal interconnect will need to be installed along the corridor. In addition, the City is planning to install a HAWK signal on Traffic Avenue at Maple Street. The HAWK signal should also be interconnected as part of this project. The traffic study should include the HAWK signal in the VISSIM analysis of the proposed traffic mitigation measure to confirm that the proposed improvements mitigate the project impact with consideration of the new signal. The City has provided the City of Puyallup with the HAWK signal plans and is available to answer questions to assist in the VISSIM modeling. Additional WSDOT and BNSF coordination will be required to extend coordination to the two signals at SR 410 ramps and also to coordinate with the two at-grade railroad crossings. **The City supports the proposed mitigation in concept, but the need identified does not accurately reflect the full scope of improvements that will be required.**
- **E Main Avenue & SR 410 Westbound/Thompson Street** – The traffic report proposes to restripe Thompson Street to provide only one left-turn lane instead of two left-turn lanes and then eliminating the split phasing at the intersection. The existing channelization was part of the traffic improvement the City implemented

**Pierce County Parks Department**

**Commenter:** Odell, Tiffany

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Good morning,

Attached is a comment letter from Pierce County Parks regarding the Knutson Farms Draft EIS issued by City of Puyallup.

Best,

Tiffany Odell, Senior Planner



Pierce County Parks

EVERYDAY ADVENTURES

Chambers Creek Regional Park  
Environmental Services Building  
9850 64th St W  
University Place, WA 98467

March 15, 2024

Puyallup City Hall  
Attn: Knutson Farms EIS comments  
333 South Meridian  
Puyallup, WA 98371

RE: Knutson Farms Draft EIS Comments

To Whom it May Concern:

Thank you for the opportunity to provide comments on the Knutson Farms Draft EIS. Pierce County Parks is a key stakeholder to this project, both as an adjacent landowner and potential recipient of the trail dedication identified in the development plans. The Foothills Trail to Riverwalk Trail connection through this property provides a key connection for the Regional Trails Plan and the longstanding vision of a “Tacoma to Tahoma” trail system in Pierce County. This connection also provides an important active transportation corridor connecting east Pierce County communities with limited active transportation infrastructure and transit to Pierce County’s economic growth centers and regional transit connections. While we support the development of a trail through this property, we have concerns about the potential impacts of industrial development on recreational users, the active transportation system, nearby residents and businesses, and the environment.

Regarding selection of alternatives, Pierce County Parks objects to Alternative 1, which would require rail crossing the Foothills Trail and largest trailhead in the trail system. Pierce County Parks would not approve any proposal for rail crossing the trail and removing the trailhead, and therefore does not find this alternative to be viable or reasonable. The significant impact to the safety of trail users is too great for us to consider granting any such easement, which would be required to implement this alternative.

Pierce County Parks requests that additional analysis be completed regarding potential impacts to recreational users during construction and operation of this project, as follows:

1. **Freight, vehicle, and rail traffic impacts to recreational users during construction and operation.** We recommend additional study and analysis of potential vehicle, pedestrian, and cyclist conflicts in all alternatives to users of Foothills Trail, Puyallup Trailhead, Van Lierop Park, and the proposed trail route through the project site. We recommend consideration and study of a possible prohibition of vehicular access to the site via 80<sup>th</sup> St. E/8<sup>th</sup> Ave. SE where these conflicts would take place. There are access points to the northwest of the site that could

accommodate vehicle traffic and avoid impacts to recreational area users accessing recreation sites via 80<sup>th</sup> Ave. E.

2. **Air quality impacts of the site on recreational users.** We recommend additional study and analysis of air quality impacts associated with vehicle trips generated by this development with specific consideration for the heavy recreational use of this corridor, especially given the deeper respiration during physical activity and the presence of recreational users of all ages and abilities. This analysis should include possible buffers not only to limit noise from the site, as discussed, but also adequate to reduce air quality impacts to users of the Foothills Trail, Puyallup Trailhead, Van Lierop Park, and the proposed trail through the project site.
  
3. **Trail routing that avoids impacts to critical areas and environmental hazards.** The proposed trail routes studied in the Draft EIS appear to conflict with on-site critical areas, especially floodplains and wetlands. Placement of the trail should provide a safe space for trail users outside of such hazards, especially where hazards are associated with deep and fast flowing water. As the potential custodian of the trail connection, Pierce County Parks should be consulted regarding trail location and design to ensure construction based on best practices for user safety and trail maintenance and operations efficiency.

Thank you for the opportunity to provide comment. We would be happy to further discuss these recommendations as you develop the Final EIS.

Sincerely,

A handwritten signature in black ink, appearing to read "Roxanne Miles", with a long, sweeping underline that extends to the right.

Roxanne Miles, Director  
Pierce County Parks

cc:

Cort O'Connor, Deputy Prosecuting Attorney, Pierce County  
Sean Gaffney, Planning Manager, Pierce County Planning & Public Works

**Pierce County Planning & Public Works**

**Commenter:** Gaffney, Sean

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Good morning,

Attached you will find a cover letter and compilation of comments from Pierce County Planning & Public Works regarding the Knutson Farms Industrial DEIS issued by the City of Puyallup.

Thanks,

Sean



PierceCountyWa.gov/PPW

**Jen Tetatzin, PE, PMP** - Director

March 14, 2024

Puyallup City Hall  
Attn: Knutson Farms EIS comments  
333 South Meridian  
Puyallup, WA 98371  
[comment@knutsonfarmseis.org](mailto:comment@knutsonfarmseis.org)  
(sent via email and regular mail)

Subject: **Comments Provided- Draft EIS (DEIS) | Knutson Farms Industrial Park  
(PROJECT ID P-19-0141)**

To Whom It May Concern:

Thank you for the opportunity to provide comments on the Draft EIS. Attached you will find a compilation of comments from Pierce County Planning & Public Works.

Feel free to reach out to me if you have any need for additional clarification or information, at 253-798-2724, or [sean.gaffney@piercecounitywa.gov](mailto:sean.gaffney@piercecounitywa.gov).

Best regards,

A handwritten signature in black ink, appearing to read "Sean Gaffney".

Sean Gaffney, Manager  
Planning and Public Works, Land Use and Environmental Review

SG:sl

cc: Chris Beale, Senior Planner, City of Puyallup, 333 South Meridian, Puyallup, WA 98371  
Cort O'Connor, Deputy Prosecuting Attorney

## **Knutson Industrial Park DRAFT EIS**

### **Comments from Pierce County Planning and Public Works**

#### **General Comments**

**1. The DEIS places inappropriate emphasis on the City of Puyallup's (City) comprehensive plan and regulations.**

- a. The City's comprehensive plan is not applicable to the project site. The County's EC zoning, which is established through its own comprehensive plan, allows the proposed use.
- b. The discussion of the City's development regulations and comprehensive plan is confusing and unhelpful. It appears the City is incorrectly assuming that the County will apply the City's comprehensive plan goals and development regulations to a project located in the unincorporated areas of Pierce County.
- c. Overall, there is more analysis and discussion of the City's comprehensive plan than the applicable County plans and development regulations.
- d. There are multiple attempts within the DEIS to implement the City's comprehensive plan and zoning regulations via SEPA onto a project site located in unincorporated Pierce County.
- e. Land use goals from the plans appear to be cherry picked to support opposition to the project.
- f. Land use goals are treated as restrictive zoning or development regulations.
- g. Annexation is discussed as if it will be happening in the near future when there is no indication that annexation will occur prior to final decision on land use applications.

**2. The DEIS does not present reliable or adequate information for proposed mitigation measures.**

- a. There is a lack of analysis of existing mitigation measures in the Pierce County Code (PCC). It does not appear that WAC 197-11-660(1)(g) was taken into consideration by the authors of the DEIS. It is not always clear where the mitigation measures are coming from or why they are being proposed.

- b. Proposed Mitigation measures appear arbitrary, unsupported by evidence-based findings, or contrary to Pierce County plans and regulations.
  - i. The DEIS proposes extensive mitigation measures for this project which are purportedly consistent with the City's goals, plans, and regulations but those same mitigation measures are missing from commercial and industrial developments within the City limits.
  - ii. The DEIS lacks citations to source documents and therefore, the source for the identified impacts is not clear or unknown. For example, the discussion regarding potential impacts to hydrology and groundwater lack citations to source materials and therefore, do not appear to be supported by evidence based findings.
  - iii. The DEIS contains an extensive discussion of impacts from the flow of City water through an outfall located within unincorporated Pierce County, but the document does not include any discussion of what City did or is doing to mitigate impacts caused by development within City limits.

**3. The DEIS is inconsistent and confusing.**

- a. The identified impacts and the proposed mitigation measures do not always match up with each other. For example, "Significant Unavoidable Adverse Impacts" are preceded by discussions of mitigation measures that would avoid those impacts. In other areas, mitigation measures are proposed for impacts that are identified as having no significant impact. There is an overall lack of internal logic and consistency.
- b. It would be helpful if the DEIS findings are arranged so that Significant Adverse Impacts and the associated proposed mitigation are clearly identified.
- c. The DEIS advocates for the retention of Agricultural Use while arguing that this same agricultural use is responsible for impacts that will continue to increase in the form of pesticides, damage to wetlands, and erosion. While then continuing to suggest mitigation that is counter to preserving agricultural land or preventing pollution sources.

**4. The DEIS is not organized or formatted for practical use, but instead is unwieldy and difficult to use in a meaningful manner.**

- a. The DEIS is overlength. An EIS is required to be clear, concise, and to the point per WAC 197-11-400 and 197-11-425(2). The draft EIS exceeds the 150 page limit set under WAC 197-11-425(4).
- b. The DEIS is repetitive in parts. For example, there are multiple and duplicative descriptions of the project. In the discussions of hydrology, surface water, and ground water, the same language is repeated over and over. The entire document should be more concise and readable.
- c. Figures throughout the DEIS do not properly scale and often contain incorrect or out of date information.

### Specific Comments

1. Pagination is off or inconsistent. Each section starts with a page 1 except for Section 3 "Project Description". Table of Contents includes "Error- Bookmark not defined" in some of the spots where the page numbers should be.
2. **PDF p. 26** (page 1-5) The overlay depicted in **figure 1-3** is confusing and repeated throughout the DEIS. Several alleged "site constraints" are combined with "view corridors" and "trail corridors" that are not recognized in the Pierce County Code. The diagram and its "site constraints" appear to be a representation of what the City would like to see happen and is based on the City's comprehensive plan which is not applicable to the project site.
3. **PDF pp. 165-167** (pages 4-71 thru 4-73) Section 4.2 Surface Waters: Wetlands. The 2021 SCJ Alliance report has not been formally submitted to the County by the applicant for review. The SCJ alliance report was attached as an appendix to the DEIS. The SCJ Alliance wetland delineation report findings conflict with the 2016 delineation report from Soundview Consultants. The 2021 SCJ Alliance report does not contain adequate data for the County to determine the accuracy of the report findings or to resolve the conflicting findings in the two reports.
4. **PDF pp. 30-69** (Table 1-1 Summary of Impacts by Resources and Alternative)  
Throughout the entire table there is a failure to clearly identify specific impacts which need to be addressed.
  - a. Hydrologic impacts are speculative and based on incorrect assumptions of where development would be occurring. Wetlands A, B, and C are all within protected buffers (Floodplain, shoreline jurisdiction, and wetland buffers).
  - b. Table 1-1 does not clearly identify project impacts.

- c. Table 1-1 does not clearly identify which mitigation measures would be required by PCC and what additional mitigation measures are being proposed by the City.
  - d. Table 1-1 Some categories may be outside of stated scope of DS. Health and Safety as shown in the table is more the purview of L&I and OSHA. WAC 197-11-408 requires that the DEIS be prepared according to the scope decided upon by the lead agency in its scoping process.
5. **PDF p. 132** (page 4-38) Land Use Impact: Tables 4-6 & 4-7 appear to be incorrectly labeled and cited. (Both tables on page.)
  6. **PDF pp. 134-136** (pages 4-40 thru 4-42) Pierce County Comprehensive plan analysis: Pierce County Shoreline components missing.
  7. **PDF pp. 120-141** Section 4.2.2: This seems like a general list and not an analysis.
  8. **PDF pp. 142-170** Section 4.2.3: Proper analysis of this section cannot be completed without the appropriate project area data being included. Areas included in as development are well within protected floodways, shoreline and wetland buffers.
  9. **PDF p. 145** (page 4-51) paragraph one: Also consider WAC 173-26-231 Shoreline Modifications which addresses armoring to protect a structure and any modification of the shoreline. This WAC also covers Shoreline stabilization and the principles to build, maintain, or modify.
  10. **PDF p. 146** (page 4-52) Assertions: The City could have appealed the HPA or contacted WDFW with concerns about the work. This sounds like a disagreement between licensed engineers. City experts reviewed the plans which were prepared by a licensed engineer.
  11. **PDF p. 204** (page 4-110) Section 4.2.6 is labeled “Significant Unavoidable Adverse Impacts”, but in other parts of the DEIS, mitigation measures are proposed to mitigate those impacts.
  12. **PDF pp. 310- 313.** Table 4-20 is entitled “Applicable Regulations” but then refers to City plans and regulations that are not applicable to the project. There is no current annexation application, and no pre-annexation zoning map has been developed or adopted by the City. Annexation is not anticipated anytime in the near future. Continued references to City plans and regulations are confusing and not helpful.
  13. **PDF p. 313** Table 4-21 : Each of the City of Puyallup designations in this table represents Future Land Use map designations from the CPCP, but no pre-annexation zoning map has yet been developed or adopted by the City. Continued references to City plans and regulations are confusing and not helpful.

14. **PDF p. 314-315:** (pages 4-220 thru 4-221) The allegations in this section are not supported by the documentation provided. R1903 is a City resolution that should be included as an Appendix or associated document. Citation of Cardwell, 2008 is from what documentation? Is this a quote, is it from an email, is it from a study or official document? Those agreements discussed in the above should be properly referenced and produced in order to provide accurate emphasis.
15. **PDF p. 317** Figure 4-41: This overlay is confusing and deceptive. It is not clear why the city's future land use zoning, which has not been officially developed or adopted, is being included in the DEIS. The overlay represents the City's goals and desires rather than applicable County zoning and regulations.
16. **PDF p. 320** (page 4-226) As discussed above, the City does not have adopted pre-annexation zoning designations on the parcels located within the Project site. City zoning designations of land in the UGA will be determined at the time of annexation, if annexation were ever to occur. Until that time, County zoning governs development that occurs in the unincorporated areas of Pierce County.
17. **PDF p. 322** (page 4-228) No Action Alternative: Areas zoned EC are to be used for commercial and industrial uses per the Alderton McMillan (AM) Community Plan. Moreover, AM LU 8 recognizes and designates the McMillin Park of Industry area for rural industrial uses. The DEIS impact analysis appears to assume that no commercial or industrial use is allowed in EC zone within the AM community plan area and this is an incorrect assumption. Additionally, prime farmland in unincorporated Pierce County has been zoned ARL or FL which is not recognized or discussed in the DEIS.
18. **PDF pp. 323- 324** (page 4-229 thru 4-230) The project is consistent with County zoning and future land use designations, but the project is inconsistent with some of the City's future goals and desires for this area. There are many elements that are in alignment with County's Comprehensive Plan and Community Plan. Even under the City's plan, large portions of the project site would likely be heavy commercial and car parks resulting in a large quantity of the very non-point source pollution the City wishes to avoid.
19. **PDF p. 325-342** (pages 4-231 thru 4-247) The basis of some of the City's consistency determinations is not clear. The City's Future Land Use Map (FLUM) as referenced in Table 4-21 states that "Various zoning designations could apply upon annexation; no pre-annexation zoning map has yet been developed or adopted by the City." But in other parts of the DEIS, the City's future land use plans and goals are portrayed as something that is final and complete and forms the basis of the City's consistency analysis.

20. **PDF pp. 323-342** (pages 4-231 thru 4-247) The County does not agree with the City's conclusion that the proposal is inconsistent with the County's comprehensive plan or development regulations. The zoning for the project site is EC which was established via the County's comprehensive plan and allows the proposed use. The goals and policies contained in Table 4-22 appear to be cherry picked in order to build an argument of inconsistency instead of a fair and balanced analysis. The City has failed to take into account the permit application review process and existing mitigation measures contained in Pierce County development regulations.
21. **PDF pp. 323-342** (pages 4-231 thru 4-247) Additionally, due to ongoing appeals and litigation initiated by the City, the County has been unable to take action on the pending project applications. The City asserts that lack of "approved" plans supports findings of "inconsistent" or "inconclusive" without recognizing that the litigation has prevented the County from reviewing and approving plans until the environmental review is complete. Furthermore, it appears that the City has failed to request or obtain the desired plans from the applicant in order to complete its own environmental analysis.
22. Additional specific comments regarding the City's consistency analysis are as follows:
- a. **PDF p. 325** (page 4-231) Environmental Element Goal ENV-8: This is above and beyond current BMP's as Ecology has yet to release a BMP related to oxidant pollutant. This is also above and beyond latest and best science. Ecology's standards can be found at; <https://ecology.wa.gov/Waste-Toxics/Reducing-toxicchemicals/Addressing-priority-toxic-chemicals/6PPD>. Also, this is saying the proposal is inconsistent with the policy, but these would be covered by many of the development permits required for a development of this kind.
  - b. **PDF p. 325** (page 4-231) Env-9: It is not inconsistent as explained by County floodplain manager Dennis Dixon in consultation meetings between the City and the County.
  - c. **PDF p. 326** (page 4-232) Env 15: Stormwater design will be required at application of site development permits and would need to be properly designed prior to issuance of permits.
  - d. **PDF p. 326** (page 4-232) Env-15.5: PCC requires these issues to be evaluated during review of wetland and shoreline applications. The county already has requirements as stated above to review for these impacts.

- e. **PDF p. 326** (page 4-232) LU-4.2.6: Historically, the County attempts to downzone the project area were met with resistance from the City who requested that the EC zoning be maintained. The City's new planning goals also allow for warehouse and other commercial and industrial development at the project site, so the finding of "inconsistency" is not accurate. The fact that the project proposal does not align exactly with the City's new vision for the project site does not mean that proposal is, on the whole, inconsistent or that coordination has not occurred.
- f. **PDF p. 326** (page 4-232) LU-44 and LU 44.2: Improper citation these should be LU 54 and LU 54.2
- g. **PDF p. 327** (page 4-233) All mentions of LU 4X should be LU 5X incorrect citation of comprehensive plan throughout.
- h. **PDF p. 327** (page 4-233) LU-44.5 and LU 44.11: The consistency determination is not accurate. AM LU-8.8 addresses specifically which kind of industrial uses should be allowed.
- i. **PDF p. 328** (page 4-234) LU-46.1 should be LU-55.1 The finding entered here should be the same for 54.5 and 54.11. It is unclear why the DEIS deviates so drastically policy to policy.
- j. **PDF p. 328** (page 4-234) LU-47 should be LU-56 The finding of "inconsistent" seems counter to the discussion that is in the determination. It is stated that all modes are being provided but there will not be a desired connection east west to Van Lierop Park. Based on the provided discussion, it should be consistent. Additionally, PCC has requirements for connectivity for pedestrian and bicycle traffic integrated into our design manuals and regulations this has not been analyzed in this DEIS.
- k. **PDF p. 328** (page 4-234) Reference to LU-47 should be LU-56.X in the document. Incorrectly cited.
- l. **PDF p. 328** (page 4-234) LU-47.5: Last section of this is difficult to understand. Is the determination that views to Van Lierop Park will be impacted or that views from Van Lierop Park will be impacted? Was the park established prior to proposal with the intent to preserve views or after proposal? The impact is not quite clear in writing. Previous images show the impacts due to views originating from the park is that what is being communicated?
- m. **PDF p. 328** (page 4-234) LU-47.8: A finding of "inconclusive" is not accurate. LU 56.8 is consistent with county development regulations and policy.

- n. **PDF p. 329** (page 4-235) General: Findings of “inconsistent” do not take into account Pierce County development regulations or the County’s permit review process. For example, if a project is within shoreline jurisdiction, then a far more expansive master plan is required. Most developers do not provide a master plan until such time as a majority of their permits for wetland studies, steep slope, shoreline review and other such possible impacts are better understood. That is primarily to avoid planning some component that is not allowed by regulations, such as a pedestrian pathway constructed of hard surfaces in the floodway.
- o. **PDF p. 329** (page 4-235) LU-47.9: LU-57.9 is the correct citation. The finding of “inconsistent” is perfunctory and does not take into consideration Pierce County development regulations or permit application review processes. Additionally, it appears the City’s findings of inconsistency are based on the City’s failure to obtain the plans needed to complete its environmental analysis.
- p. **PDF p. 329** (page 4-235) LU-47.X: References to LU-47.X should be LU-57.X
- q. **PDF p. 330** (page 4-236) Goal LU-4 and LU-4.2.6: This Inconsistent finding is difficult to understand based on historic planning that has occurred in this area. Historically the County attempted to downzone the area to Rural 10 but did not pursue this option at the request of the City. The City’s Future Land Use Map (FLUM) as referenced in Table 4-21 states that “Various zoning designations could apply upon annexation; no pre-annexation zoning map has yet been developed or adopted by the City.” But in other parts of the DEIS, the City’s future land use goals are portrayed as something that is final and complete. It cannot be both.
- r. **PDF p. 330** (page 4-236) PR-1.3: Consistency Determination: Inconsistent. The City asserts that the Project would disrupt the existing park system that supports region-wide park and recreation opportunities, including within the City of Puyallup and the City of Sumner. This does not seem accurate. There is a regional trail connection that has been proposed. Offsite improvements could also be the result of review analysis. It is unclear how the proposal is inconsistent based on the DEIS documentation.
- s. **PDF p. 330** (page 4-236) PR-5: Consistency Determination. While it appears that views will be impacted from Van Lierop Park it is unclear why there is no discussion of view conflicts originating from commercial uses to the west as they are nearly just as impactful.
- t. **PDF p. 330** (page 4-236) PR-5.7 Buffer facilities from incompatible uses: The inconsistent finding here is disregarding the PCC review required for approval. Saying this is inconsistent is once again determining that this proposal doesn’t meet something that is mitigated by PCC regulations which require buffering to be in place for any approval of site development.

- u. **PDF p. 331** (page 4-237) PR-10: This analysis is not clear. It states that the trail will connect existing facilities but is inconsistent because the proposed trail is not on the Pierce County Parks, Recreating, and Open Space Plan (PROS). Applying that same logic, Puyallup Riverwalk and Van Lierop park should not be connected within the County as they are not covered by PROS either. This is a strange determination.
- v. **PDF p. 331** (page 4-237) PR-10 and PR- 17. The City appears to be assuming that it is the applicants' responsibility to provide a connected system of trails and to develop a regional trail route. While the applicant is proposing to assist with those efforts based on the proposal presented by the City, there does not look to be a conflict. The AM community plan allows for commercial and industrial uses in the EC zones.
- w. **PDF p. 331** (page 4-237) PR-17, 19, 19.3: The City is determining that non-existent portions of the proposal are "inconsistent." This inconsistent determination appears to argue that the City of Puyallup would like a trail connected directly with the shoreline, within a floodway and introducing even more environmental impacts.
- x. **PDF p. 331** (page 4-237) PR-19.3: It is stated repeatedly by the City of Puyallup that the proposal does not include a pedestrian connection to the shoreline. If it is not proposed, then it is inapplicable not inconsistent.
- y. **PDF p. 331** (page 4-237) PR-21 The County disagrees that the proposed trail is inconsistent with Goal PR-21. This is not inconsistent. It is inapplicable.
- z. **PDF p. 332** (page 4-238) AM LU-1, LU 1.2: This stated inconsistency is only viable when confusing land use, zoning and character. When comparing the entirety of the Land Use portion of the Alderton-McMillan plan the EC zoning is supposed to consist of commercial and industrial uses with a preference for agricultural associated uses. Additionally, per the AM community plan, AM LU-1 applies to Rural Residential zone and not to the EC zone which is designated for industrial uses.
- aa. **PDF p. 332** (page 4-238) AM D-1 and 1.X: Once again the City's determination of inconsistency is inaccurate. There may be a misunderstanding on what agricultural land is defined as within the comprehensive plan and what agricultural activities (current use) are. The preservation of agricultural lands are those lands zoned Agricultural Resource Land (ARL) and Farm Land (FL). The complaints levied here would also be resolved during site development review i.e.: LID design and development criteria required by the storm design manual, landscape buffers required due to 18J design regulations, and outdoor lighting plan under 18J design standards.

bb. **PDF p. 333** (page 4-239) 18S.40.050 PCC Commercial, Civic and Industrial PCSMP:  
This entire section is attempting to make administrative decisions on a project that is yet to be re-reviewed by the County. There also does not appear to be any coordination with the applicant to determine these particular or specific components. Additionally, References to 18S are erroneous as the shoreline applications are vested to former PCC Title 20. It appears that the City has failed to obtain the plans necessary to complete its environmental analysis and then defaults to a finding of “inconsistency.”

23. **PDF p. 342** (page 4-248) LU-1: This is unclear, but it appears that the City is operating under the assumption that the City’s development regulations should apply to development outside of their jurisdiction. Also, once again the FLUM is mentioned as existing which contradicts earlier statements in the DEIS. The County’s policy priority to preserve Agricultural Resource Land is not impacted by this proposal.
24. **PDF p. 342** (page 4-248) LU-3: This is covered by PCC and Design Regulations
25. **PDF p. 342** (page 4-248) LU-4: Conservation Easement is already being provided and has been proposed.
26. **PDF p. 343** (page 4-249) Alternative 1-Rail Transport: As the applicant said there is no desire to develop this as a rail associated development it is unclear how this was determined to be an alternative. Under Operations Impacts it is stated “Alternative 1 would be similar to those described for the proposed Project in that they would be consistent with County zoning and future land use designations, but inconsistent with the City’s future land use designations.” This has not been reflected in previously presented documentation within the DEIS.
27. **PDF p. 343** (page 4-249) end of Paragraph 3 states that Alternative 1 would cause significant environmental impact due to conflict with land use plans, policies, or regulations. Then, within the mitigation section, it is stated that the impacts are less than significant. This inconsistency within the DEIS makes it difficult to determine what is being communicated.
28. **PDF p. 349** (page 4-255) 4.6.3 Affected Environment: Residential Areas: it is listed here that single family residences to the east would have their view to Puyallup River impacted. This view is not previously listed in 4.6.3. This does not appear to be an impact as the homes to the east are predominantly on the east side of the river and the development would not block the view to the river.

29. **PDF p. 350** Figure 4-45: KOP 2,3, and 4 would not be impacted in the way the narrative describes. KOP 3 would still have a view of Rainier and the Puyallup River KOP 2 is looking the wrong direction for Mount Rainier and still has an unobstructed view to the north of Puyallup River. KOP 4 is directly viewing and already established structure and would not be affected by the expansion listed in this DEIS.
30. **PDF p. 357 to 359** (pages 4-263 thru 4-265) AES-2: This is presented as an attempt to enforce City of Puyallup's regulations and policies as a form of mitigation. An environmental impact statement analyzes environmental impacts and must be used by agency decision makers, along with other relevant considerations or documents, in making final decisions on a proposal, rather than attempting to impose one agency's goals and desires upon a project located outside of their jurisdiction.
31. Aquifer Recharge per PCC 18E.50 should be referenced wherever critical areas are listed or referenced. Aquifer Recharge is discussed later in the document, but not in the initial outline of PC critical areas to be addressed.
32. **PDF p. 147** (Page 4-53) states "the current PCSWDM allows for direct surface stormwater outfall to the Puyallup River after 'basic' water quality treatment". This is not accurate. Enhance Treatment is required for projects that discharge directly to fresh waters that have aquatic life.
33. **PDF pp. 148, 149** (Pages 4-54 & 4-55): Why compare the old single event model 6-mo 24-hr storm to the current continuous simulation model 91% volume requirement? Stating the PCSWDM allows storms larger than the 6-mo 24-hr storm event to go untreated is not only incorrect, but disingenuous.
- 34. Proposed mitigation associated with stormwater management is confusing, unsupported by evidence based findings, or contrary to Pierce County stormwater regulations.**
- a. Several mitigation items are noted as "should" be done. Mitigation should be supported by evidence-based findings that require specific mitigation and are consistent with existing County development regulations.
  - b. Wetland hydroperiod protection is incorrectly referenced repeatedly through the DEIS. Wetland Category, habitat score and sensitive species are the main criteria set to determine how wetlands are protected per PC Stormwater Manual Min. Requirement #8. Wetland "C" is the only Category II wetland that potentially requires hydroperiod protection. Wetlands A, B & D only require General Protection and Protection from Pollutants per PC Stormwater Manual.
  - c. Note that this project is subject to PC Stormwater Manual Minimum Requirement #5, List #3 as feasible.

- d. Remove all reference to the 2019 Ecology Stormwater Manual. Stormwater requirements are not vested for this project. This project will be regulated by the Pierce County Stormwater Manual in effect at time of site development permit issuance.

35. **PDF p. 4** (page iii)

- a. The "Fact Sheet" is not showing Pierce County Planning and Public Works as a single entity.
- b. Under the list of county permits it is not showing the approved 2016 Flood Boundary Delineation Survey (permit 844049) that finds a larger regulated flood hazard area than what is on the FEMA mapping.
- c. The list is missing the Aug 2022 Boundary Line Adjustment (permit 995208) that changed the parcel boundaries. The flood hazard area and parcel boundaries should have been utilized in the DEIS.

36. **PDF pp. 15-21** (pages xiv-xx) Under the Acronyms and Abbreviations listing:

- a. ft<sup>3</sup>/s is listed as cubic feet per second where "cfs" is the more commonly accepted acronym and is the term used in FEMA's Flood Insurance Study.
- b. MS4 is listed as "municipal storm sewer system" which only has three "s" whereas the NPDES permit calls out MS4 as a "Municipal Separate Storm Sewer System", note the word separate is missing in several citations in the document.
- c. SMMWW is listed as "Stormwater Management Plan for Western Washington" but cited in the document as the more commonly understood "Stormwater Management Manual for Western Washington". The Pierce County Stormwater and Site Development Manual - PCSWSDM is not mentioned but would be the governing manual for development rather than the Ecology manual.

37. **PDF p. 26** Figure 1-3 The Figure shows constraints including CMZ, SMA, Riparian Buffer, Erosion Hazard area and Shoreline Buffer but does not list the County regulated Deep and/or Fast Flowing water (DFF) Floodway, the FEMA defined floodway or the Channel Migration Zone (CMZ) Floodway which covers much of the area riverward of the "proposed pedestrian trail" in yellow that includes many of the "proposed trail" in green. There is a proposed trail (in green) showing a crossing of the Puyallup River. This proposal should be cited as it would be extremely difficult to construct given the multiple floodways that are up to 1,200 feet wide in this location.

38. **PDF pp. 30-31** (pages 1-9, 1-10) Table 1.1 Section 4.1 The table emphasizes the Channel Migration Zones which is only one of three Pierce County regulated floodways. The Deep and/or Fast Flowing water (DFF) Floodway has a much larger impact on the project and acts to extend the Shoreline Management jurisdiction farther from the ordinary high water or channel migrations zone areas. In the Volcanic section, a mitigation recommendation is to comply with Pierce County Code Title 18E.60. In the case of flood hazards a similar mitigation strategy to comply with Pierce County Code Title 18E.70 is not listed. The DEIS is silent on where the County has required, during preliminary short plat permitting, to avoid the flood hazard area with one exception that allowed for a stormwater management outfall.
39. **PDF p. 35** (pages 1-14 thru 1-16) Table 1-1 Section 4.2 The table discusses Wetlands impacts but then combines Floodplains and Shorelines into one section and then the first paragraph of this crowded table is reiterating that floodplain wetlands were discussed in a previous section. The table then has additional sections for "Riverbank Flood and Erosion", "Floodplains " and "Shorelines". In none of these is there a discussion of risk from river flooding, deep and/or fast moving water (that is a life safety issue), the FEMA defined floodway to ensure conveyance or any part of the purposes of Title 18E.70.010. A simple recognition that flooding actually causes harm and should be avoided would be helpful. The initial submittal for this project included structures in the floodway(s) and due to the implantation of the County's critical areas ordinance Title 18E.70 the project was modified to remove all development from the flood hazard area except the previously noted stormwater outfall. A mitigation strategy that recommends complying with Title 18E.70 is expected in this DEIS but is grievously missing. It should be noted that any trail in the lower flood hazard area is a potential life safety risk, could have a short term life due to channel migration risks and would have ongoing maintenance issues including sediment disposal after flood events. A mitigation item that should be listed is that the project is proposing a quitclaim to the County that will facilitate the construction of a long planned setback levee in this location that would reconnect the floodplain and restore natural river processes.
40. **PDF p. 75** (page 3-6) Table 3-1 lists parcel 0420252006 owned by Pierce County PPW but does not describe how this public property is part of the project or impacted.
41. **PDF pp. 114-116** (pages 4-20 thru 4-22) Channel migration zones are discussed and the DEIS identifies the study done for Pierce County in 2003 and the ordinance that prohibits development in the areas identified at severe risk. In many other parts of this document city standards are held up and compared to the County standards, but in this area, none are mentioned. The DEIS should note the Critical Area Ordinances enforced by the County that are more restrictive than the City's.

42. **PDF pp. 124-125** (pages 4-30 thru 4-31) The DEIS gives a broad overview of the FEMA National Flood Insurance Program but does not mention the requirement to be in the program (44 Code of Federal Regulations parts 59 & 60) but lists a guidance document that has no binding authority. The DEIS also mentions the Endangered Species Act but does not mention the National Marine Fisheries Services' Biological Opinion written to FEMA where following the minimum NFIP regulations leads to a jeopardy determination. The 2008 NMFS Biological Opinion list required reasonable and prudent alternatives for FEMA and any NFIP participating community in the Puget Sound area to follow.
43. **PDF p. 132** (page 4-38) The DEIS mischaracterizes the Pierce County flood hazard code. It says that "In general, new development in a flood zone is discouraged, but may be allowed with proper engineering, mitigation and floodproofing." PCC Title 18E.70.040 B (floodway) and C (flood fringe) prohibit new development except for a few special circumstances. This section also leaves out the important PCC 18E.40 Fish and Wildlife reference document of the NMFS Biological Opinion that is utilized as best available science (see 18E.10.140 Appdx\_A F.15).
44. **PDF p. 139** (pages 4-44 thru 4-45) This section is described as a comparison between the City and County flood hazard codes. It does not mention the Deep and/or Fast Flowing water (DFF) Floodway in the County code that has a significant impact on this project, nor that the City has no comparable life safety standard. The County's DFF Floodway standard is based on a Bureau of Reclamation report that established a relationship to depth and velocity where a child or small adult could be swept away and perish. The County also has a CMZ Floodway standard, and neither is part of the PMC Chapter 21.07. Pierce County Code also requires critical areas to be placed in a protected tract, hence the projects proposed large tract that will be preserved for open space.
45. **PDF p. 142** (page 4-48) Figure 4-8 is not showing the Pierce County regulated flood hazard areas. In 2016 the County required a flood boundary delineation survey that showed all the regulated flood hazards including the maximum extent of the floodplain, based on elevation, that exceeded the FEMA mapped areas in places. Additionally, the mapping included the CMZ and DFF floodways which as contiguous floodways extending from ordinary high water extend the shoreline management boundary. This graphic should be replaced with the approved flood survey document. This graphic does show the surveyed "project boundary" that excludes the historic channel oxbow of the Puyallup River, which should be shown on all project graphics.
46. **PDF p. 166** (page 4-72) Figure 4-15 this is labeled as showing the on-site floodplain but does not show the surveyed flood hazard areas as mentioned in the earlier comment.
47. **PDF p. 169** (page 4-16) Figure 4-16. This figure is showing an inaccurate floodplain boundary.

48. **PDF pp. 170-171** (pages 4-76 thru 77) The floodplain impacts make no mention of flooding on the opposite bank where SR-410 is closed during large floods which causes significant traffic impact within the City. Additionally, a senior mobile home park repetitively floods, both of which are mentioned in the County's 2013 River Hazard Management Plan. The County has numerous setback levee studies (2008, 2014, and 2021) that identify this site a potential project. This DEIS lists none of these studies.
49. **PDF p. 258** (page 4-164) The DEIS is redundant in repeating its generalizations of the critical area ordinance and the floodplain code specifically. This does not appear to add to what was written on page 4-38.
50. **PDF p. 271** (page 4-177) Figure 4-34 is misleading as it does not show the regulated flood hazard areas on the site.
51. **PDF p. 278** (page 4-184) The DEIS references PCC Chapter 18E.70.040.A.1, a provision in the flood code that is reserved for "New construction done by or for Pierce County". The citation is not applicable to this project that is a private development.
52. **PDF p. 375** (page 4-281) The DEIS needs to describe the potential impacts and long term viability to their REC-4: Modify the Site Plan to Provide a New Trail Location. The proposed trail location is in the life safety DFF Floodway and where a future set-back levee is planned. It is also an area identified at severe risk of channel migration. The DEIS extensively addresses the sedimentation and maintenance issues near the stormwater outfall, this recommendation to place a new facility in a similar location should be evaluated with the same rigor.
53. **Outfall.** Throughout the DEIS, erosion near the existing outfall is portrayed as a negative impact. This is based on an assumption that the existing riverbank will stay in its current location. This assumption is faulty because it fails to recognize the biological importance of riverbank erosion on healthy river systems, the migrating nature of the Puyallup River, and the County's future plans to construct a setback levee to restore the connection between the river and its natural floodplains. The County's floodplain engineers were not consulted about these issues prior to the DEIS being issued which has resulted in an incomplete and unbalanced analysis of the impacts of the outfall.
54. DEIS Section 4.9 "Transportation" General Comments
- a. The DEIS generally fails to identify adopted City or County standards or policies for identified recommended mitigation measures.

- b. The DEIS uses queue lengths and travel times as “measures of effectiveness,” but fails to clearly describe the methodology for these measures or explain how these measures were used to identify project impacts and potential mitigation. The DEIS also fails to link these measures of effectiveness with adopted standards and policies.
- c. The DEIS does not connect specified road improvement mitigation with the project action impacts and does not provide analysis explaining how the recommended mitigation fully mitigates identified impacts.
- d. It is unclear if the roadway “proportional factor” identified as a mitigation measure is tied to any adopted City or County standards or policies. Further, costs and scope of identified roadway corridor improvement project are not specified so it is difficult to evaluate if the capital improvement project components are tied to Knutson Farms Industrial project impacts. Project components and associated costs should be described and quantified, at least in terms of a range of costs that the roadway proportional factor would be applied.
- e. There appears to be redundancy between specific road improvements identified as mitigation, pavement improvements and the fee in lieu of mitigation. This needs be evaluated to ensure that mitigation is not duplicated and is proportionate to project impacts.

**State of Washington Department of Ecology**

**Commenter:** Thomas, Joe

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Chris Beale, SEPA Contact:

Thank you for the opportunity to comment on the Knutson Farms Industrial Park Project (Lead Agency File No(s). p-19-0141). Ecology's comments are attached.

[ Statewide SEPA Register No. 202305877 ]

Have a great day,

Joe Thomas



STATE OF WASHINGTON  
**DEPARTMENT OF ECOLOGY**

Southwest Region Office  
PO Box 47775, Olympia, WA 98504-7775 • 360-407-6300

March 14, 2024

Chris Beale, SEPA Contact  
City of Puyallup  
Development Services Department  
333 South Meridian  
Puyallup, WA 98371

Dear Chris Beale:

Thank you for the opportunity to comment on the draft environmental impact statement for the Knutson Farms Industrial Park Project (p-19-0141) as proposed by Tim Berry. The Department of Ecology (Ecology) reviewed the environmental checklist and has the following comment(s):

**SOLID WASTE MANAGEMENT: Derek Rockett (360) 995-3176**

The applicant proposes to demolish an existing structure(s). In addition to any required asbestos abatement procedures, the applicant should ensure that any other potentially dangerous or hazardous materials present are removed prior to demolition. It is important that these materials and wastes are removed and appropriately managed prior to demolition. It is equally important that demolition debris is also safely managed, especially if it contains painted wood or concrete, treated wood, or other possibly dangerous materials. Please review the "Dangerous Waste Rules for Demolition, Construction, and Renovation Wastes," on Ecology's website at: [Construction & Demolition Guidance](#). All removed debris resulting from this project must be disposed of at an approved site. All grading and filling of land must utilize only clean fill. All other materials may be considered solid waste and permit approval may be required from your local jurisdictional health department prior to filling. Contact the local jurisdictional health department for proper management of these materials.

**TOXICS CLEANUP: Diana Ison (360) 999-9593**

This property is within a quarter mile of three known or suspected contaminated sites. The sites are Puyallup Landfill A, Facility Site Identification (FSID) # 49172; Hwy 410 at Traffic Ave Overpass, FSID # 58749; and Pasquier Panel Products Inc, FSID # 16398677. To search and access information concerning these sites see <http://www.ecy.wa.gov/fs/> and <https://fortress.wa.gov/ecy/gsp/SiteSearchPage.aspx>. If contamination is suspected, discovered, or occurs during the proposed development of a warehouse complex on this 188-acre property, testing of the potentially contaminated media must be conducted. If contamination of soil or groundwater is readily apparent, or is revealed by sampling, the Department of Ecology must be notified. Contact the

Chris Beale  
March 14, 2024  
Page 2

Environmental Report Tracking System Coordinator at the Southwest Regional Office at (360) 407-6300. For assistance and information about subsequent cleanup and to identify the type of testing that will be required, contact Diana Ison with the Toxics Cleanup Program at the Southwest Regional Office at (360) 999-9593.

**WATER RESOURCES: Charlotte Lattimore (360) 407-6066**

Under RCW 90.03.350, a Dam Safety construction permit is required for those dams or ponds that can impound a volume of 10 acre-feet or more of water or other liquids above ground level. If the Knutson Farms Industrial Park project includes the construction of a stormwater pond or ponds, or any other impoundment that can meet or exceed the above referenced criteria, you will need to apply for a dam construction permit. To determine if a Dam Safety construction permit is required for your project, the applicant must submit a set of construction plans to:

WA Department of Ecology  
Dam Safety Office  
P.O. Box 47600  
Olympia, WA 98504-7600

The construction permit application can be found by entering the following link into your search engine:

<https://apps.ecology.wa.gov/publications/summarypages/ecy07038.html>

For additional information, please contact Charlotte Lattimore by e-mail at [clat461@ecy.wa.gov](mailto:clat461@ecy.wa.gov) or by telephone at (360) 407-6066.

Ecology's comments are based upon information provided by the lead agency. As such, they may not constitute an exhaustive list of the various authorizations that must be obtained or legal requirements that must be fulfilled in order to carry out the proposed action.

If you have any questions or would like to respond to these comments, please contact the appropriate reviewing staff listed above.

Department of Ecology  
Southwest Regional Office

(JKT:202305877)

cc: Derek Rockett, SWM  
Diana Ison, TCP  
Charlotte Lattimore, WR

**Tacoma-Pierce County Health Department**

**Commenter:** Dilworth, Erin

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Please find attached comments from the Tacoma-Pierce County Health Department on the Knutson Farms Industrial Park EIS (PROJECT ID P-19-0141).

Thank you,

March 14, 2024

Record ID: SR0291361

ATTN CHRIS BEALE  
CITY OF PUYALLUP  
CBEALE@PUYALLUPWA.GOV

**RE: SEPA Review, Knutson Farms Industrial Park P-19-0141**

Dear Chris Beale:

The Tacoma-Pierce County Health Department's Environmental Health Program received the above mentioned checklist on December 19, 2023 and has the following comment(s):

The Tacoma-Pierce County Health Department's mission is to protect and improve the health of all people and places in Pierce County. As part of our mission, the Health Department tackles known and emerging health risks through policy, programs, and treatment to protect public health. As part of our Healthy Community Planning Program, we work with policy makers, planners, and community members to build sustainable and healthy communities.

The Health Department advances healthy communities by:

- Encouraging land use and transportation planners to think about people, prosperity, and the planet.
- Getting affected communities to help shape the planning process.
- Addressing health inequities.
- Reducing health risks.

While some aspects of a healthy community are regulated in local, state, or national legal frameworks, many determinants of health that come from Healthy Community Planning are not. We acknowledge these limitations as we offer the information and recommendations below related to the project in review. As noted, some comments are our recommendations to advance health equity, and should not be construed as legal requirements, while others are tied to the Health Department's Environmental Health Code, Pierce County Code, and/or the Washington Administrative Code.

#### Air and Transportation

High levels of diesel exhaust from heavily trafficked roadways around warehouse facilities are unhealthy for neighboring residents, especially mothers and children. These health effects include low birth weights and increased death from cardiovascular disease. Increased pollution created by additional diesel truck trips to/from this area may degrade air quality. To mitigate potential harms, we recommend the following considerations in project design and implementation:

1. Restrict idling with a no idle zone policy.
2. Encourage trucks to switch to battery use if idling is necessary.
3. Encourage use of truck routes that avoid schools and daycare centers.

#### Environmental Health - Noise

Air brake pressure releases, tonal backup alarms, and loading dock activities associated with warehousing activities have a high probability of disturbance to nearby residents. Prolonged exposure to this type of noise pollution common at warehouse facilities can cause hearing impairment, stress, high blood pressure, anxiety,

depression, poor sleep, and many other health problems. To mitigate potential harms, we recommend the following considerations in project design and implementation:

1. Increased insulation in warehouse building to limit noise escaping.
2. Prohibit the use of Jake Brakes (brake retarders) in surrounding neighborhoods.
3. Encourage use of electric trucks. Electric motors make less noise and pollution than diesel trucks.
4. Install vegetated barriers or hedges around the perimeter of the facility.

All activities during construction and ongoing operation must comply with Pierce County Code Chapter 8.76.

## Light and Glare

Many warehouse projects include flashing green and red lights and overhead broad-spectrum lighting. Prolonged exposure to this type of light pollution common at warehouse facilities can cause loss of sleep, increased stress, and alterations to circadian rhythms resulting in altered hormone production. To mitigate potential harms, we recommend the following considerations in project design and implementation:

1. Use light limiting equipment like motion-detectors, dimmers, timers, and shields.
2. Use fewer lights overall.
3. Use LED lights to reduce energy consumption and increase tenant's ability to focus or direct light.

## Energy and Natural Resources

Substantial energy consumption and emissions may result from this project, which may negatively impact local climate goals, and can potentially exacerbate heat island effects in neighboring areas. To mitigate potential harms, we recommend the following considerations in project design and implementation:

1. Retain as many significant trees on site as possible.
2. Install pervious pavement.
3. Use electric heating instead of gas heating in the proposed structure.
4. Install solar power and/or green roofs. Green roofs cool in the summer and insulate in the winter. Solar panels could allow the tenant to create their own energy, reducing stress on the electrical grid.

Thank you for the opportunity to respond.

Sincerely,



Erin Dilworth  
Health Promotion Coordinator III  
Environmental Health Division

**Washington Department of Transportation**

**Commenter:** Larson, Andrew

**Source:** Email

**Date:** 2/22/2024

**Comment:**

Hi Chris,

Here are the comments from Traffic for the Vissim model and EIS document.

Andrew Larson, PE

Development Services Engineer (he/him/his)

(360) 900-9541

Olympic Region

**PLANS, SPECIFICATIONS AND ESTIMATE  
Review Comment Disposition Form**

Project Title:		SR 410 – Knutson Farms EIS		Job Charge #:	
Reviewer (name & office) Sarah Bogue – Traffic Design Kun Yi – Traffic Design Daniel Grimm – Traffic Design Date of Review Comments: 12/20/2023			Responses By: Date of Disposition:		
Comment No.	Sht or Pg.	Review Comment	Status Code	Designer's Response	
1.	GEN	Please include traffic counts from SR 162 in appendices.			
2.	PDF p19	For clarity, consider adding "-" (minus) sign for displaced trips from single family houses.			
3.	PDF p26	Per WSDOT Vissim protocol, all state facility segments must have GEH < 3.0 ( <a href="https://wsdot.wa.gov/sites/default/files/2021-03/TrafficOps-VISSIM-Protocol.pdf">https://wsdot.wa.gov/sites/default/files/2021-03/TrafficOps-VISSIM-Protocol.pdf</a> ).			
4.	PDF p28	For clarity, add legend to Table 6 for "XX (XX)".			
5.	PDF p34	Consider adding LOS standard for SR 162 and SR 512.			
6.	PDF p94	For information only: SR 410/Main Street has been identified on the ADA inventory as multiple features being noncompliant.			
7.	Section 5.2/PDF p90	Abnormal weather and improvements in vehicle technology are not reliable predictors of safety performance and should not be listed at all. On the other hand, project improvements typically have reliable and quantifiable CMFs used for predictive analysis of safety performance, and each should be listed and applied for each proposed mitigations such as the signal phasing at SR 410/E Main Ave, new signal at Shaw Rd E/5th Ave E, roundabout at SR 162/80th St E.			
8.	Section 5.2.1 to 5.2.4/PDF p90 to p91	In short, the key difference between these scenarios (A, B, and D) are how much traffic volume changes, and which intersections the traffic volumes change at. The qualitative assumption seems to be that "No significant safety impacts are expected" from any of Scenarios A, B, and D. However, I believe for at least no action & scenario A, a predictive analysis should be done for to provide support for this claim. The quantified analysis should provide predictive crash frequency for all study intersections as it is an indicator of future performance, not just the 31 of 35 that existed since 2015. The current qualitative evaluation is general speculation and should not blanket every intersection in the study. If scenario A shows no significant difference than no action scenario, then scenario B and D will follow the same conclusion as the latter have objectively less traffic volume			

Status Code Legend: A = Incorporated B = Open/Under Review C = Evaluated/Not Incorporated D = Beyond Scope/Not Evaluated  
All "B" and "C" responses require explanatory comments.

**Olympic Region**

**PLANS, SPECIFICATIONS AND ESTIMATE  
Review Comment Disposition Form**

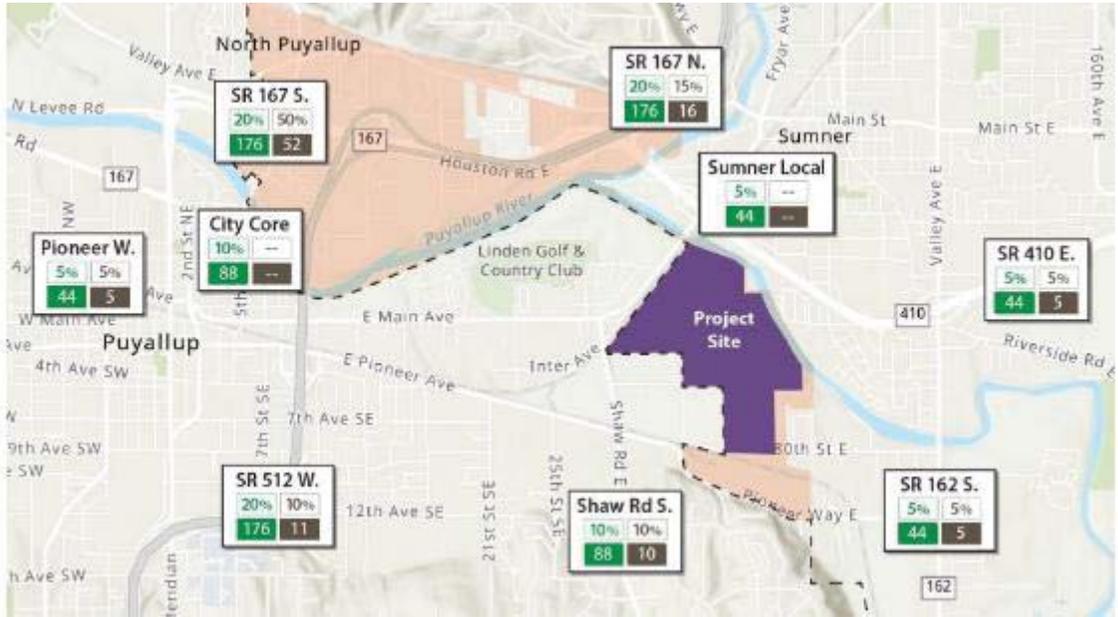
Project Title:		SR 410 – Knutson Farms EIS		Job Charge #:	
Reviewer (name & office) Sarah Bogue – Traffic Design Kun Yi – Traffic Design Daniel Grimm – Traffic Design Date of Review Comments: 12/20/2023			Responses By:  Date of Disposition:		
Comment No.	Sht or Pg.	Review Comment	Status Code	Designer's Response	
		and area of impact. Scenario C and E are currently unlisted in this section, but as described in the previous comment, there are quantifiable CMFs available for the proposed improvements and such CMFs should be applied to the predictive analysis results of Scenario A.			
9.	PDF p14	Please further document the COVID adjustment done on 2021 traffic data.			
10.	PDF p18	Please document sources or methodology for project trip distributions. Was a planning model used? Engineering judgment?			
11.	PDF p42-81	LOS and Delays reported here do not always match their counterparts in Attachment C (PDF p402 onward).			
12.	PDF p80	Consider further labeling in Tables 48, 49 explicitly stating how Scenario E has a roundabout at 80 <sup>th</sup> and SR-162. This mitigation will trigger the need for an ICE.			
13.	PDF p74	Please clarify why Scenario D has less delay than No Action for SR 410/E Main. Does Scenario D have signal optimization?			
14.					

Status Code Legend: A = Incorporated    B = Open/Under Review    C = Evaluated/Not Incorporated    D = Beyond Scope/Not Evaluated  
 All "B" and "C" responses require explanatory comments.

**EIS Document Comments**

Comment #10:

“Please document sources or methodology for project trip distributions. Was a planning model used? Engineering judgment?”



(Above is from PM distribution of trips in Scenarios A, C, PDF page 20)

Comment #12:

“LOS and Delays reported here do not always match their counterparts in Attachment C (PDF p402).”

33. SR 162 & 80th St E	WSDOT	Unsignalized	35.0	C	27.0	C
34. SR 162 & SR 410 EB	WSDOT	Signal	16.5	B	21.4	C
35. SR 162 & SR 410 WB	WSDOT	Signal	20.5	C	20.9	C

Notes: Ave = Avenue; Blvd = Boulevard; Rd = Road; sec = second; St = Street

(Above is from Table 49, Scenario E PM, PDF page 81)

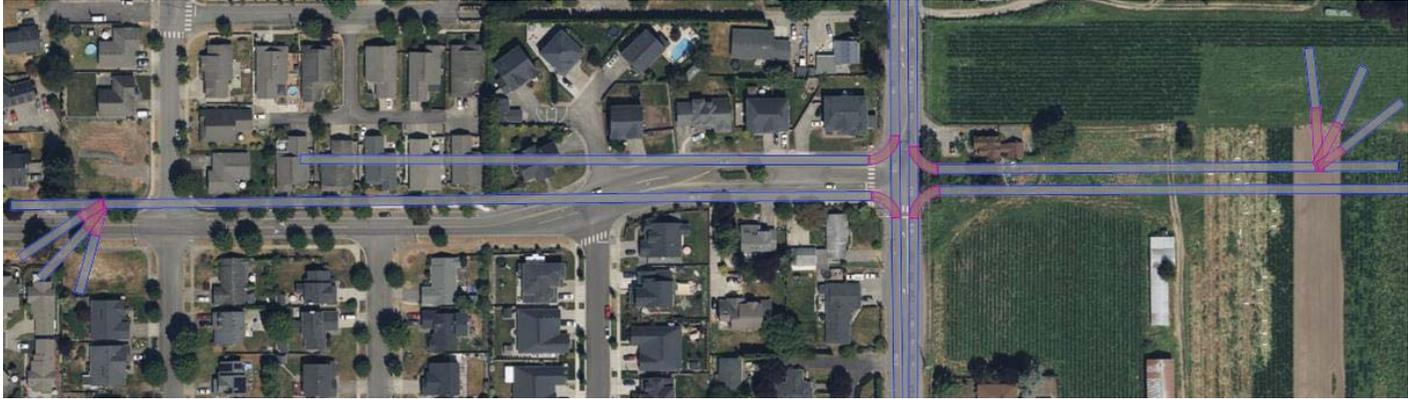
3300	SR 162 & 80th St	46.5	E	S
3400	SR 162 & SR 410 EB	24.1	C	S
3500	SR 162 & SR 410 WB	21.8	C	S

(Above is from Intersection Summary, PM Peak Hour, Scenario E, PDF page 404)

## Vissim Model Comments

### Comment #1:

“143rd currently doesn’t have an east leg at SR-162. Are these links meant to balance volumes along SR-162? The east leg does not interact with southbound SR-162. Is the west leg here also for volume balancing?”



(Above is from Scenario E PM model)

### Comment #2:

“Node results in model outputs do not seem to match what was listed in the report.”

SimRun	Tim...	Movement	QLen	QLenMax	Vehs(All)	Pers(...	L...	LOSVal(All)	VehDelay(All)	P
36	Average 180...	3200: SR 162 at Pioneer	78.63	1258.84	1955	1955		3	27.07	
37	Average 180...	3300: SR 162 at 80th St - 280@2497.8 - 265@16.0	171.19	1085.97	0	0				
38	Average 180...	3300: SR 162 at 80th St - 280@2497.8 - 10430@70.3	171.19	1085.97	148	148		3	21.40	
39	Average 180...	3300: SR 162 at 80th St - 280@2497.8 - 10433@80.3	171.19	1085.97	825	825		3	23.05	
40	Average 180...	3300: SR 162 at 80th St - 488@101.7 - 489@16.7	0.00	0.00	54	54		1	0.97	
41	Average 180...	3300: SR 162 at 80th St - 10431@0.6 - 265@16.0	13.05	314.22	508	508		1	8.00	
42	Average 180...	3300: SR 162 at 80th St - 10431@0.6 - 10430@70.3	13.05	314.22	25	25		1	4.71	
43	Average 180...	3300: SR 162 at 80th St - 10431@0.6 - 10433@80.3	13.05	314.22	0	0				
44	Average 180...	3300: SR 162 at 80th St - 10435@2.1 - 265@16.0	11.52	132.90	76	76		4	26.95	
45	Average 180...	3300: SR 162 at 80th St - 10435@2.1 - 10430@70.3	11.52	132.90	0	0				
46	Average 180...	3300: SR 162 at 80th St - 10435@2.1 - 10433@80.3	11.52	132.90	0	0		2	11.50	
47	Average 180...	3300: SR 162 at 80th St	48.94	1085.97	1637	1637		3	17.42	

(Above is from Scenario E PM model)

Comment #

Reported results differ in Synchro for No Build (2026) and Scenario A (2026)

HCM 6th Signalized Intersection Summary  
3: SR 512 WB & Pioneer

Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↕↕	↕	↕	↕↕	↕	↕↕
Traffic Volume (veh/h)	487	96	440	503	93	84
Future Volume (veh/h)	487	96	440	503	93	84
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	529	104	478	547	101	91
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	1185	232	374	1421	713	634
Arrive On Green	0.40	0.40	0.53	0.53	0.40	0.40
Sat Flow, veh/h	3056	580	794	3647	1781	1585
Grp Volume(v), veh/h	316	317	478	547	101	91
Grp Sat Flow(s),veh/h	1777	1766	794	1777	1781	1585
Q Serve(g_s), s	5.8	5.9	12.1	4.1	1.6	1.6
Cycle Q Clear(g_c), s	5.8	5.9	18.0	4.1	1.6	1.6
Prop In Lane		0.33	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	711	706	374	1421	713	634
V/C Ratio(X)	0.45	0.45	1.28	0.38	0.14	0.14
Avail Cap(c_a), veh/h	711	706	374	1421	713	634
HCM Platoon Ratio	1.00	1.00	1.33	1.33	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	9.9	9.9	16.1	7.3	8.6	8.6
Incr Delay (d2), s/veh	2.0	2.1	145.0	0.8	0.4	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.2	2.2	18.3	1.3	0.6	0.5
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	11.9	11.9	161.0	8.1	9.0	9.1
LnGrp LOS	B	B	F	A	A	A
Approach Vol, veh/h	633		1025		192	
Approach Delay, s/veh	11.9		79.4		9.0	
Approach LOS	B		E		A	
Timer - Assigned Phs	2		4		8	
Phs Duration (G+Y+Rc), s	22.5		22.5		22.5	
Change Period (Y+Rc), s	4.5		4.5		4.5	
Max Green Setting (Gmax), s	18.0		18.0		18.0	
Max Q Clear Time (g_c+1), s	3.6		7.9		20.0	
Green Ext Time (p_c), s	0.5		2.8		0.0	

Intersection	Intersection Summary - PM Peak Hour							
	Existing 2021		No Action 2026		Scenario A		Scenario B	
	Delay (sec/veh)	Estimated LOS	Delay (sec/veh)	Estimated LOS	Delay (sec/veh)	Estimated LOS	Delay (sec/veh)	Estimated LOS
Traffic Ave/Friar Ave & Main St/Cannery Wy	37.7	D	63.7	E	71.7	E	76.1	E
Traffic Ave & State St	2.9	A	44.8	D	69.3	E	80.1	F
E Main Ave & SR 410 WB /Thompson St	28.6	C	60.1	E	103.1	F	111.4	F
E Main Ave & SR 410 EB	17.3	B	35.7	D	157.4	F	168.7	F
E Main Ave & 5th Ave NE	7.3	A	7.4	A	154.6	F	136.5	F
E Main Ave & Shaw Rd E	16.6	B	21.1	C	56.9	E	88.3	F
E Main Ave 15th St SE	9.3	A	10.2	B	10.8	B	10.9	B
E Main Ave & 5th St SE	13.9	B	15.2	B	16.2	B	15.9	B
E Main Ave/W Stewart Ave & 2nd St NE	9.8	A	10.6	B	11.5	B	11.3	B
N Meridian Ave & SR 167 EB	24.3	C	30.0	C	30.1	C	30.1	C
N Meridian Ave & SR 167 WB	9.7	A	12.8	B	12.9	B	12.9	B
N Meridian Ave & Valley Ave NE	49.0	D	138.4	F	138.4	F	138.4	F
E Pioneer & SR 512 WB ramps	23.7	C	34.4	C	21.0	C	19.8	B
E Pioneer & SR 512 EB ramps	8.9	A	15.7	B	11.2	B	11.1	B
E Pioneer & 13th St SE	10.3	B	11.2	B	13.7	B	13.3	B
E Pioneer & 15th St SE	10.7	B	11.6	B	12.6	B	13.2	B
E Pioneer & 21st St SE	9.3	A	9.6	A	10.2	B	4.3	A
E Pioneer & 25th St SE	16.2	C	17.5	C	74.9	F	44.6	D
E Pioneer & Shaw Rd E	38.9	D	49.2	D	84.0	F	138.3	F
E Pioneer & 33rd St SE	9.7	A	13.8	B	14.4	B	17.7	F
33rd St SE & 8th Ave SE	7.4	A	8.1	A	8.6	A	163.4	F
Shaw Rd E & Highlands Blvd	19.9	C	36.3	E	51.5	F	216.7	F
Shaw Rd E & 16 th Ave SE	25.7	D	33.1	D	42.4	E	69.0	E
Shaw Rd E & 23rd Ave SE/Crystal Ridge Dr SE	24.5	C	41.1	D	39.9	D	109.8	F
Shaw Rd E & Forest Green Blvd	15.4	C	22.3	C	22.0	C	17.1	B
Shaw Rd E & Manorwood Dr	15.4	C	26.2	D	21.3	C	15.3	B
Shaw Rd E & 39th Ave SE	32.6	C	75.7	E	70.2	E	44.7	D
Shaw Rd E and 5th Ave SE	1.1	A	1.3	A	62.7	E	120.1	F
33rd St SE & 9th Ave SE	0.1	A	0.5	A	6.6	A	6.6	A
Shaw Rd E & Safeway Driveway	11.4	B	13.5	B	14.6	B	75.4	E
80th St & Driveway	1.3	A	1.2	A	7.6	A	5.9	A
SR 162 and W Pioneer	20.7	C	24.3	C	25.3	C	25.2	C
SR 162 & 80th St	20.7	C	28.3	D	85.5	F	12.6	B
SR 162 & SR 410 EB	12.8	B	15.9	B	16.1	B	16.6	B
SR 162 & SR 410 WB	15.4	B	20.4	C	21.5	C	21.8	C

Intersection Summary	
HCM 6th Ctrl Delay	49.0
HCM 6th LOS	D
2026 - No Build 3:10 pm 02/05/2024	

Intersection Summary	
HCM 6th Ctrl Delay	105.2
HCM 6th LOS	F
Scenario A - PM Peak 2026 10:35 am 02/01/2024 Baseline	

**Applicant:** Running Bear Development Partners, LLC

**Commenter:** Archer, Margaret

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Greeting.

Attached are comments from the Knutson Farms Industrial Park Applicant Running Bear Development Partners, LLC. Also attached is a letter from Tim Berry presenting proposed new mitigation measures for the Knutson Farm Industrial Park project to be considered with the Applicant's comments. The attached comments are supplemental to the comments submitted yesterday by the Applicant's consultant Richard Wienman.

Thank you for your consideration. Please let me know if you have difficulties opening or accessing the attachments.

Margaret Archer

Margaret Y. Archer  
Direct: (253) 620-6550  
E-mail: marcher@gth-law.com

March 15, 2024

Chris Beale  
City of Puyallup  
Knutson Farms Industrial Park Project  
Designated EIS Contact Person  
Puyallup City Hall  
333 S. Meridian  
Puyallup, WA 98371

Meredith Neal  
City of Puyallup  
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Puyallup City Hall  
333 S. Meridian  
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(Sent via email to [comment@kuntsonfarmseis.org](mailto:comment@kuntsonfarmseis.org); [CBeale@PuyallupWA.gov](mailto:CBeale@PuyallupWA.gov); and [MNeal@PuyallupWa.gov](mailto:MNeal@PuyallupWa.gov))

RE: Knutson Farms Industrial Park Draft Environmental Impact Statement

Dear Mr. Beale and Ms. Neal:

We represent Running Bear Development Partners, LLC, the Knutson Farms Industrial Park (“KFIP”) Applicant. This letter is presented as additional comment on the Draft Environmental Impact Statement (DEIS) the City issued as the project’s SEPA Lead Agency on December 14, 2023. This comment letter is more general in nature and should be considered with the detailed DEIS comments submitted under separate cover on March 14, 2024, by the Applicant’s EIS Consultant Richard Weinman, Weinman Consulting, LLC, and the March 15, 2024, letter from Tim Berry, through which KFIP presents new proposed mitigation measures to address certain environmental concerns raised in the DEIS.

More specifically, this comment letter focuses on KFIP’s more general and overriding concern that the City of Puyallup is improperly exploiting its role as the SEPA Lead Agency to advance its own planning and political agenda. More specifically, the City is attempting to employ the

Reply to:

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Seattle, WA 98101 (206) 676-7575 (fax)

EIS process to unlawfully impose its own Comprehensive Plan, vision, and regulations on the Knutson Farms property. In doing so, the City has failed to achieve the core purpose of an EIS, which is to conduct an unbiased, data-based analysis of probable significant adverse project impacts, reasonable alternatives, and possible mitigation that complies with SEPA and other statutory and constitutional law.

It is challenging to cull from the 500+ page DEIS which of the discussed impacts are potentially **significant** project impacts (the only category of impacts that require study under SEPA), and what specific mitigation measures are recommended to address potentially **significant** project impacts, much less how and to what extent identified potential measures will mitigate identified impacts to a level of nonsignificance. Notably, volunteered mitigation measures that KFIP communicated to the City in 2019, which measure are substantial for the KFIP proposal. The City never mentioned, much less appropriately considered the volunteered mitigation. When potential mitigation measures are identified, the City fails to show a nexus between the potential mitigation and potentially significant project impacts. The City likewise fails to demonstrate that suggested measures are proportionate to project impacts and were properly formulated with consideration of Pierce County's zoning and Pierce County's mitigating development regulations as SEPA requires. Instead, the City's DEIS presents potential mitigation measures and a "Reduced Intensity Alternative" that, in large part, are designed to address inconsistencies between the City's Comprehensive Plan and the County's Comprehensive Plan. The DEIS purports to acknowledge that the Knutson Farms property is subject to Pierce County's zoning and regulations and not the City's, but then repeatedly elevates and applies City policies and regulations to the project in disregard of County policies and regulations. Moreover, as discussed below, the City has done so based on false or improper premises.

Our client has long been concerned that the City would use the SEPA process to advance its own agenda and obstruct the KFIP project. The concern is well founded. Soon after our client submitted the revised project application in March 2016, which reduced the project size, the City's leadership, staff and attorneys actively mobilized its opposition to the KFIP project. The Council dedicated more than an hour of its regular meeting on May 10, 2016 to publicly declare their displeasure and formulate a strategy to challenge the project, which strategy included galvanizing public opposition. Reflective of this position, one Council Member stated:

I would like to see that we get some talking points put up on the website so the people can more than just, I don't like it. I mean, I don't know that there's anybody besides developers that like warehouses.

The Council's message was received. Public records reveal that City Engineer Mark Palmer emailed his staff after the meeting concluded: "Defin[it]e interest from most of the council to prevent Knutson development through the SEPA process."

After the City prevailed in subsequent litigation to gain Lead Agency status in 2019, Mayor John Palmer publicly confirmed the City's intent on his Facebook page.

After a long process with the landowners and the County, the City adopted a plan for this area (part of Puyallup's urban growth area) in 2009. Unless the developer alters its proposal, the next step is to develop an EIS and mitigate this proposal to be more consistent with the City's land use plan and not allow trucks to bring Shaw Road to a standstill.

Alarmed by this statement, KFIP's attorney wrote to the City Attorney on October 14, 2019:

We understand that this development has become highly politicized. Following the Supreme Court's decision, Mayor Palmer implied on Facebook that the City will exploit its official role as Lead Agency and use the EIS process to leverage proposal changes to conform to the City's zoning rather than the zoning applicable to the project. ... We trust that City staff, while in the role of Lead Agency, will not allow the process to be politicized, but will engage in normal discussions with our client regarding the scope and use of studies already completed, including studies prepared for the SEPA appeal hearing before the Pierce County Examiner, so that the EIS may be prepared efficiently and consistent [with] SEPA rules.

Unfortunately, KFIP's expressed concern was more than a worry; it was prescient. The City employed the EIS process exactly as Mayor Palmer stated it would be. The City "develop[ed] an EIS and [seeks to] mitigate this proposal to be more consistent with the City's land use plan."

But the DEIS does not comply with SEPA and does not provide a useful tool upon which the County can rely to make its permitting decisions. The DEIS proposes mitigation measures and alternatives that Pierce County, as the permitting agency, cannot lawfully impose upon the KFIP project. While SEPA authorizes the County, as the permitting jurisdiction, to exercise substantive SEPA authority to impose mitigation measures, SEPA also directs that the County may do so only to the extent directly attributable to the identified significant adverse project impacts. RCW 43.21C.060; WAC 197-11-660(1)(d). SEPA also requires that potential mitigation measures must be formulated with consideration of relevant regulatory schemes - where existing regulations adequately address a proposed project's probable specific adverse environmental impacts, no further mitigation should be imposed. RCW 43.21C.240. Moreover, SEPA directs that imposed mitigation must be based on adopted written policies of the County. WAC 197-11-660 1(a). Finally, State statutes and constitutional law requires the

County to make a project-specific determination that each imposed mitigation measure is “reasonably necessary” as a result of an identified project-specific impact. RCW 82.02.020; *Isla Verde Int’l Holdings, Inc. v. City of Camas*, 146 Wn.2d 740, 755, 764, 49 P.3d 867 (2002). Thus, each mitigation measure must be specifically linked to and proportionate to the development’s actual impact. *Id.* Indeed, if the County attempted to impose upon KFIP the “Reduced Intensity Alternative” (and the associated substantial open space designed to benefit the City), the action would not only be contrary to applicable law, but would constitute an unconstitutional taking of the Knutson Farms property without just compensation.

KFIP urges the City to course correct before issuing the Final EIS. But as KFIP has expressed repeatedly in the four years that followed the 2019 court decision authorizing the City to assume the Lead Agency role, the City must work expeditiously to complete the EIS as SEPA requires. RCW 43.21C.0311; WAC 197-11-948(3). We have long passed the two-year period that our State Legislature considers an appropriate amount of time to complete the EIS process. The SEPA regulations also urge the SEPA Lead Agency to issue the Final EIS within 60 days following the close of the DEIS comment period. WAC 197-11-460.

Nonetheless, KFIP recognizes that the significant deficiencies in the DEIS cannot realistically be corrected in 60 days. However, KFIP does expect the City to work as efficiently as possible to correct the deficiencies; and we believe that this can reasonably be completed within 120 days.

Frankly, many of the deficiencies could have been avoided had the City consulted with KFIP during the review process in the preceding years.<sup>1</sup> The KFIP team stands ready to meet with the City to discuss mitigation measures, especially with regard to traffic mitigation, which as presented in the DEIS, were too vague and ambiguous to allow KFIP to conduct any meaningful evaluation for comment. We also are available to discuss KFIP’s New Proposed Mitigation as presented in Mr. Berry’s March 15, 2024 letter. KFIP hopes that, moving forward, the City will accept our request to confer.

The remainder of this letter addresses some of the foundational assumptions that the City incorrectly used to justify an extraordinary and atypical SEPA review process that elevated the

---

<sup>1</sup> KFIP appreciated receiving a preliminary copy of the DEIS in October 2019, but even then, the City advised that it would not accept substantive comment from KFIP until the DEIS was issued and the formal comment period opened. By email dated May 4, 2024, City Attorney instructed KFIP:

The City is willing to extend the applicant review period as requested as long as you understand that this will extend the issuance of the draft EIS by a comparable period of time. Also, the applicant review period is not a time for substantive comments that would result in revisions to the draft EIS. It is possible that comments might result in correction of scrivener’s errors but consideration of any substantive comments as well as any resulting revisions will likely be taken into consideration during the normal comment period along with any other comments that are received from the public.

KFIP adhered to the City’s instruction.

City's political vision, zoning and regulations over and with disregard to Pierce County's zoning, policies and regulations.

**The Court Decision Regarding Lead Agency Status Was Not Critical Of The County's Environmental Review. And It Did Not Contemplate That The City's SEPA Review Would Disregard And Jettison The County's Review And Conclusions Without Due Consideration.**

Prior to the City assuming the SEPA Lead Agency role, Pierce County conducted extensive environmental review of the KFIP proposal. The County evaluated the project impacts, applied its own regulations to determine if they were sufficient to mitigate significant impacts and, imposed additional measures as necessary to mitigate impacts to a level of nonsignificance. Ultimately, the County's environmental review process was enhanced by a nearly three-week County appeal hearing in 2017, which allowed the City to present expert testimony on traffic and critical area impacts. After review and evaluation of the City's, the County's, and KFIP's combined expert environmental study, the Pierce County Hearing Examiner concluded that the County's review and conclusions regarding required mitigation measures generally complied with SEPA and local regulations. But the Hearing Examiner also modified some of the County's imposed conditions to further address the City's concerns and, particularly to better mitigate traffic impacts.

Significantly, in October 2019, despite that the County's MDNS and Preliminary Short Plat approval (and all the conditions they imposed) were voided by the court's Lead Agency decision, KFIP confirmed in writing that it voluntarily accepted all MDNS and Short Plat approval conditions, as modified by the Pierce County Hearing Examiner, formally incorporating these volunteered mitigation measures as elements of the KFIP project. Unfortunately, the DEIS fails to consider or even acknowledge the volunteered mitigation measures.

Instead, the City infers in the DEIS, and continues to infer in its communications with the public, that Pierce County's prior environmental review was inadequate, requiring the City to disregard the prior evaluation and essentially conduct a wholly new review. Not only is this untrue, but the City's failure to consider the County's evaluation of the mitigating effect of County regulations contravenes one of SEPA's mandates.

KFIP acknowledges that the City prevailed in the legal contest for Lead Agency status and, as a result, the City rightfully assumed the role. But that victory did not license the City to proceed with a review that contravenes SEPA's requirements and purpose.

Importantly, in concluding that Puyallup is an agency with jurisdiction qualified to assume the SEPA Lead Agency role, our appellate court did not conclude that the County's environmental review was inadequate or that the environmental study presented by KFIP was insufficient. In

its first decision on the Lead Agency issue, the court acknowledged the significant environmental study that had already been done:

As required by the Pierce County Code and the County's environmental review under SEPA, the Applicants obtained and submitted professionally prepared studies analyzing the potential impacts and mitigation measures including a traffic impact analysis; a critical areas assessment report; flood surveys and studies including a flood boundary delineation survey, conceptual flood plain compensatory storage plan, compensatory flood plain volume table, and flood plain cross sections; a preliminary storm drainage report; and a geotechnical engineering report.

*City of Puyallup v. Pierce County*, 8 Wn. App.2d 323, 330, 438 P.3d 174 (2019). The court later clarified that, while it concluded that the City was procedurally entitled under the SEPA regulations to assume the Lead Agency role, the scope of the court's decision "is narrow:"

[T]his court ruled that Puyallup had jurisdiction to assume lead agency status after the County issued an MDNS, which rendered the MDNS void. See 8 Wn. App. 2d at 351-52. But the court did not opine that the County's SEPA review leading to the MDNS violated SEPA.

*City of Puyallup v. Pierce County*, 20 Wn. App. 2d 466, 474, n. 2, 500 P.3d 216 (2021), as amended on reconsideration in part (June 1, 2022), rev. denied, 200 Wn. 2d 1010, 518 P.3d 207 (2022).

The court also confirmed that invoking the Lead Agency assumption process did not "prevent reliance on information gathered or reviews generated during the prior process." *Id.* at 472. And, when the City asked the court void all prior environmental study and review and affirmatively authorize the City to effectively start its EIS review from scratch, the court unequivocally denied the request. The court of appeals concluded: "Neither the regulations nor the case law supports the scorched earth approach Puyallup included in its proposed order. Because such an order is contrary to law, we refuse to instruct the trial court to adopt it." *Id.* at 474.

But the City nonetheless did employ such a scorched earth approach. The City's approach not only unnecessarily cost KFIP detrimental delay and expense, but it also served to undermine the quality of the DEIS.

Proper SEPA review is not conducted in a vacuum, but must consider other local and state laws that provide environmental protections. SEPA standards and policies are not elevated

above specific zoning ordinances. *Victoria Tower Partnership v. City of Seattle*, 59 Wn. App. 592, 600, 800 P.2d 380 (1990). Rather, SEPA is an overlay of law that supplements – fills gaps in – existing regulatory and statutory law as necessary to ensure deliberate consideration of environmental review in the land use permitting process. *Id.*; *SEAPC v. Cammack II Orchards*, 49 Wn. App. 609, 615, 744 P.2d 1101 (1987); *Bellevue Farm, supra*, 100 Wn. App. at 353. Thus, an EIS must evaluate significant impacts accordingly; and potential mitigation measures in particular must be formulated with consideration of relevant regulatory schemes and appropriately tailored so as not to create duplicative environmental review. RCW 43.21C.240. Project environmental review should not require additional studies or mitigation where existing regulations have adequately addressed a proposed project's probable specific adverse environmental impacts. *Id.* See also, RCW 36.70B.030 (Intent-Findings-1995 c 347 §§ 404 and 405). SEPA compliance is thus achieved when the local jurisdiction determines that a project's environmental impact will be mitigated through its own development regulations to meet SEPA requirements. *In re King County Hearing Examiner*, 135 Wn. App. 312, 325, 144 P.3d 345 (2006); *Anderson, supra*, 86 Wn. App. at 302. See also, Butler & King, 24 Washington Practice, *Environmental and Law Practice*, § 16.16 (2007).

Contrary to SEPA's mandate, the DEIS review fails to consider the mitigating effect of Pierce County's critical area regulations. Not only did the City fail to consider the prior County evaluation and application of County regulations to the KFIP project, the City largely disregarded County regulations altogether. It also failed to consult with the County. This review was not sanctioned by the appellate court decision on Lead Agency. To the contrary, the City's failure to appropriately consider County regulations is contrary to SEPA.

**The City Council Resolution No. 1903 Regarding Potential Future Agreement To Set Aside Agricultural Land Was Never Implemented And Is Not A Legitimate Basis For Any Mitigation Measure Or The "Reduced Intensity Alternative."**

In the DEIS and in its town hall meetings, City staff has represented that, in 2004, Puyallup and Pierce County entered a "verbal agreement" to set aside at least 160 acres of land within the Puyallup UGA for farmland. City staff has further represented that this "verbal agreement" led to the adoption of Puyallup Resolution No. 1903 in 2004, which the DEIS states "required adoption of an approach to planning the area that would set aside 160 acres of land for farming." (DEIS at page 4-221.) City Staff has represented that Puyallup's 2009 Comprehensive Plan included the Rural Buffer Residential (RBR) designation, which was implemented with the Agriculture, Residential and Open Space zoning district that Puyallup now seeks to impose on the KFIP project, even though the site remains in Pierce County's jurisdiction. The City has publicly claimed that the County failed to implement its side of this alleged agreement. At the January 11, 2024 virtual town hall, City staff represented that the "Reduced Intensity Alternative" was effectively intended to enforce the supposed verbal agreement with the County.

The City's representations are inaccurate and have misled the public. Notably, while the DEIS references City Resolution No. 1903, it does not append the Resolution. The document belies the City's representations.

This resolution was adopted when Neil VanLierop and Roger Knutson were considering petitioning for annexation. The City authorized circulation of the annexation petitions for all properties in the vicinity. Pierce County subsequently commenced a comprehensive plan amendment that would have removed the VanLierop and Knutson properties from the UGA. Through Resolution 1903, the City asked Pierce County to leave the UGA (and with that the EC zoning) intact to allow the annexations planned at that time to proceed forward. The City publicly committed in the Resolution No. 1903 that, in return, the City would complete a Pre-Annexation Development Agreement/Master Plan for the entire annexation area.

Resolution 1903 set forth specific proposed terms for a Pre-Annexation Development Agreement for the Knutson and VanLierop Annexation Area. Those terms did, as Puyallup has recently represented in the DEIS process, include a plan to preserve approximately 160 acres for farmlands or open space. But both Knutson and Van Lierop not only had the benefit of industrial zoning for their properties, they had both already filed development applications that vested to the County's EC zoning. The City's "land set-aside vision" could only be achieved if these private property owners voluntarily forfeited their property rights.

Resolution 1903 thus acknowledged that the property owners would not sacrifice their development rights and agree to annexation without compensation for the sacrificed property rights. To entice the landowners to participate in the desired development agreements, the Proposed Terms of Pre-Annexation appended to Resolution No. 1903 included a plan for compensation to Knutson and Van Lierop. Specifically, the Proposed Terms of Pre-Annexation provided:

As consideration for dedications/reservations /open space set-asides made in the target area, landowners/partners would receive development rights/credits/infrastructure bonuses, subject to further negotiation, that could be used to develop other sites with higher densities for residential uses or credits for system development and other infrastructure charges associated with commercial uses, possibly in other parts of the City;

\* \* \*

Owners would receive development incentives in return for dedications;

Properties to the West of 134<sup>th</sup> would be zoned commercial, and those owners dedicating property in accordance with the pre-

annexation development agreement will receive credits for parts or all of Water, Sanitary and Storm System Development Charges and Capital Facility Charges. This will constitute purchase of development rights from affected property owners; ...

Of course, the development agreement never came to light, the owner incentives were never adopted, and Resolution 1903 was never implemented by the City. In response to a direct inquiry on the issue, the City informed Roger Knutson by letter dated July 6, 2010, that the City would not enter into the contemplated development agreement. Notably, in that same letter, the City informed Knutson:

We respectfully note that Resolution 1903 was a resolution and not an ordinance. The distinction is important: An ordinance is the “local law of a municipal corporation, duly enacted by the proper authorities, prescribing general, uniform and permanent rules of conduct, relating to the corporate affairs of the municipality.” In contrast, a resolution is less solemn and formal than an ordinance and, “generally speaking, is simply an expression of the opinion or mind of the body concerning some particular item of business or matter of administration coming within its official cognizance.” (Legal citations omitted, emphasis in original.)

Ultimately, in the absence of the promised development agreement and associated compensation, Knutson rightfully chose to decline the requisite consent to be annexed.

Of course, the industrial zoning designation Employment Center (“EC”) has been attached to the Knutson Farms property since the 1990s and that zoning designation remains in effect today. Regardless of the political desires of the respective jurisdictions to change the zoning when Resolution 1903 was adopted, that never happened. By no means was there any agreement with Pierce County that can be enforced. No laws were passed and no written agreements were signed. The City has no legal authority whatsoever, under SEPA or otherwise, to “enforce” a purported “verbal agreement” with Pierce County, especially at the expense of a private landowner’s property rights.

**Annexation Is Neither Imminent Nor Likely In The Foreseeable Future And Does Not Provide A Lawful Basis For The City To Use The EIS Review Process To Impose City Zoning Upon The KFIP Project**

Though the Knutson Farms property is within the Urban Growth Area and borders the Puyallup city limits, no portion of the property is situated within the City. The City repeatedly infers throughout the DEIS that annexation into the City is certain. But the property has not been annexed in the 30 years that followed the UGA designation in 1994. Moreover, annexation of

a site of this size and value, if it is to occur, will likely require owner consent. See RCW 35A.14.100-.150. Annexation in the foreseeable future is speculative at best. Given the City's enduring opposition to this project, such consent is unlikely. Regardless, because the project is wholly within unincorporated Pierce County; and the Knutson Farms property is not subject to a pre-annexation agreement or joint planning agreement. It is beyond dispute that County regulations exclusively apply. The City's efforts to impose the City's zoning on the KFIP project is unlawful.

Finally, even if the Knutson Farms property was annexed, the City would annex the property subject to KFIP's development application that is vested to the Pierce County Code in effect at the time of application. Annexation, even if it were to occur, would not strip KFIP of this valuable and constitutionally protected property right without payment of just compensation as our constitution requires. The City cannot invoke the SEPA review process to unlawfully impose its own zoning and vision and effectively take the Knutson Farms property.

**The DEIS Unlawfully Affords Priority Status To The City's Van Lierop Park Over KFIP's Property Rights.**

In addition to elevating and placing undue emphasis on the City's Comprehensive Plan, the DEIS also affords the Van Lierop Park priority status over the KFIP project, which again, is wholly consistent with the County's industrial zoning. The DEIS especially emphasizes project impacts to views from Van Lierop Park. The DEIS seeks to impose substantial open space requirements on the KFIP project, taking KFIP's property to create expansive view and trial corridors to mitigate these impacts.

But the DEIS fails to acknowledge that the KFIP vested project application pre-dates the Van Lierop Park. The potential impacts of the future development consistent with the neighboring industrial zoning were well-known to the City when in planned for and created the park – and the KFIP project in particular was known to the City. Notably, the concept plan for Van Lierop Park and the viewshed encompassing Mt. Rainier, were first presented at an open house in 2017, years after the complete KFIP project application vested. A “context graphic” presented at the 2017 open house confirmed this fact. The graphic explicitly identifies the “future Knutson industrial development” and, presents a plan for that known future development, depicting tall, solid vegetation that “buffers the park from future industrial development.” The City's 2018 SEPA checklist for the park's Phase 1 design and construction, acknowledges that views of Mt. Rainier could be obstructed by unidentified “future industrial development.” The same SEPA checklist also embraced the road and stormwater improvements that will necessarily accompany the expected KFIP development to successfully avoid the imposition of measures that otherwise would have been required to mitigate traffic and stormwater impacts from the City's planned park development and use.

Gordon Thomas Honeywell<sup>LLP</sup>

March 15, 2024

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As significant, there is no regulatory authority to impose such restrictions on KFIP's land use rights. The City cannot invoke the SEPA review process to unlawfully impose open space to augment its park; it certainly cannot do so without payment of just compensation.

Again, KFIP genuinely hopes the City will work as efficiently as possible to correct the DEIS deficiencies. The KFIP team not only offers but will welcome any opportunity to meet with the City to discuss its comments or discuss mitigation measures. KFIP hopes that, moving forward, the City will accept our request to confer.

Sincerely,

A handwritten signature in blue ink that reads "Margaret Y Archer". The signature is written in a cursive, flowing style.

Margaret Y Archer

William T. Lynn

cc: Jen Tetatzin, PC Director of Planning & Public Works  
Sean Gaffney, PC Planning Manager  
Cort O'Connor, PC Deputy Prosecutor  
Joe Beck, Puyallup City Attorney  
Client

## **Knutsen Farms Industrial Park New Proposed Mitigation Measures**

### **Wetland Hydrology Protection**

This is not required for this project as the onsite wetlands do not meet the criteria for Method I and Method II and could simply be protected with general protection measures due to the ratings of the onsite wetlands and partially offsite location of the one category II wetland. This complicates/limits a full Method I analysis and would normally not be required per the manual guidance. However, the applicant proposes to conduct Method II flow modeling of onsite wetlands being discharged to as mitigation to ensure hydroperiod protection.

### **Stormwater Outfall Mitigation**

#### **Sediment Management**

Develop a sediment management plan with Pierce County that addresses the ongoing deposition within the outfall Site. This may include periodic removal of sediment or redirection of flows to reduce sediment accumulation. This would preferably include removal of the ecoblock divider the city required as it is inhibiting dispersion of flow and sediment movement within the outfall structure. If the City insists, the ecoblock wall would remain.

#### **Bank Stabilization**

The primary purpose of the proposed stabilization measures is to address the current issues of bank erosion and sediment deposition while enhancing the ecological integrity and resilience of the Puyallup River's riparian ecosystem. These measures are designed to work in harmony with the river's natural dynamics, offering both immediate and long-term solutions to stabilize the riverbank, protect the outfall infrastructure, and mitigate the impacts of altered hydraulic conditions. The following proposed approaches are consistent with Washington Department of Fish and Wildlife Integrated Streambank Protection Guidelines (WDFW 2003).

**Rock Slope Protection (RSP):** Implement rock slope protection in coordination with Pierce County and permitting agencies at the toe of the eroded banks upstream of the outfall to immediately reduce further erosion. Rock would be placed along the toe to the height of the ordinary high-water mark. The RSP should be designed to dissipate energy and withstand the river's hydrodynamic forces during a 100-year flood event. Note: the potential for a future setback levee in this area may limit the extent of RSP the County and/or permitting agencies will desire.

**Vegetated Soil Lifts:** For the restoration of the eroded levee prism upstream of the outfall, it is recommended that soil lifts, an established bioengineering technique, be designed and constructed. Soil lifts involve the construction of horizontal layers of compacted soil, enveloped in geotextiles or coir fabric, which are stacked to rebuild the levee profile. This method stabilizes the slope by combining the mechanical support of the compacted soil with the reinforcement of the geotextiles. The soil lifts should be seeded with native grasses and interplanted with deep-rooted native shrubs to further enhance stability. The roots of these plantings will interlock with the soil layers, creating a living, self-reinforcing structure that provides erosion resistance and improves ecological value. Over time, the vegetation will mature, increasing the cohesive strength of the levee and providing additional benefits such

as wildlife habitat and aesthetic enhancement. This technique ensures a more sustainable and ecologically friendly approach to levee restoration, promoting resilience to future hydrologic events and geomorphic processes.

**Robust Willow Plantings:** Augment the existing vegetative measures by enhancing willow plantings. Select willow species and planting configurations that are known for rapid root maturation and high tolerance to hydraulic stress.

Together, these measures aim to restore the geomorphic balance of the river, ensuring the stability and functionality of the outfall structure, and contributing to the overall health and sustainability of the river ecosystem. These elements will be designed in harmony with the Pierce County proposed levee setback to ensure the outfall contributes to the larger scale non-compensatory floodplain restoration still in the planning process. The integration of these techniques and long-term restoration planning reflects a holistic approach to river management, combining engineering precision with ecological sensitivity, thereby aligning with contemporary best practices in river restoration and environmental stewardship.

### **Floodplain Reconnection**

Commit to cooperating with Pierce County to implement the proposed levee setback to increase floodplain connectivity and create additional functional habitat, providing a more sustainable solution to flood risk management.

### **Monitoring and Adaptive Management**

**Monitoring Program:** Establish a monitoring program to track the effectiveness of the stabilization measures. This program should include regular surveys of bank conditions, sediment levels, and vegetation health.

**Adaptive Management Strategy:** Prepare an adaptive management plan that allows for responsive adjustments to the stabilization measures based on monitoring results and changing river conditions.

**Hydraulic Modeling:** Undertake hydraulic modeling to better understand the flow patterns around the outfall and the impacts on bank stability and how any proposed stabilization measures will integrate with the Pierce County proposed levee setback at the site. Use the modeling results to inform potential design modifications.

All of these measures will be coordinated with Pierce County and other appropriate regulatory agencies.

### **Stormwater Treatment and Source Control Approach to Address 6PPD**

The proposed project will implement two of the treatment BMPs listed as having a high potential for treatment effectiveness in DOE's 2022 analysis. The outfall was designed to disperse stormwater through a vegetated flow path, and removing the ecoblock divider required by the city is recommended to achieve a more effective dispersion through a vegetated flow path as originally designed. In addition, the final stormwater plan will incorporate Biofiltration into the proposed enhanced stormwater design. With implementation of these two BMPs the stormwater design will

exceed current regulatory standards and reduce potential 6PPD and 6PPDq impacts consistent with current regulatory guidance.

In addition, the proposed project will implement DOE's high effectiveness source control BMP of frequent street sweeping per DOE's 2022 guidelines to intercept and remove tire particulates before they enter the onsite stormwater system.

### **Stormwater Management**

**Additional Groundwater Monitoring:** Complete additional groundwater monitoring to confirm winter high groundwater elevations, confirm infiltration feasibility and verify design infiltration rate for infiltration /dispersion system for building rooftop areas along upper bench.



April 26, 2017

Barghausen Consulting, Inc.  
Attn: Dan Balmelli  
18215 72<sup>nd</sup> Avenue South  
Kent, WA 98032

**RE: Knutson Farms Short Plat/Administrative Design Review/Administrative Use Permit/Shoreline Substantial Development/Site Development/Site Plan Review and Building Permits, Application Nos. 792206, 792210, 792212, 7922136, 840137**

Dear Mr. Balmelli:

Enclosed/attached you will find a Mitigated Determination of Nonsignificance (MDNS) issued by Pierce County on the above referenced proposal.

The MDNS is also being sent to agencies other than Pierce County Departments that may be requiring permits for this proposal, in accordance with WAC 197-11-340.

Sincerely,

A handwritten signature in black ink that reads "Marcia Lucero".

Marcia Lucero  
Project Manager

ML:ld  
4 Knutson Farms MDNS.docx

Enclosure/attachment

c: Knutson Farms, Inc., 16406 78<sup>th</sup> Street East, Sumner, WA 98390-2900  
Reviewing agencies that received Environmental Checklist/site plan on December 4, 2014, with revisions on June 20, 2016.



## Mitigated Determination of Nonsignificance (MDNS)

**Environmental Application Number:** 792210

**Project ID Number:** 469640

**Family Application Number(s):** 792206, 792212, 7922136, 840137

**Parcel Number(s):** 0420252002, 2003, 2012, 2700, 2701, 3007, 3036, 3057, 3063, 3064, 3702, 3703, 3704, 3705, 0420261012, 4014, and 4033 (17 parcels)

**Action:** Knutson Farms Short Plat / Administrative Design Review (ADR 63-14) / Administrative Use Permit / Shoreline Substantial Development (SD36-14) / Site Development / Site Plan Review and Building Permits

**Proposal:** Development of a warehouse/industrial park facility on a proposed seven lot short plat for future buildings totaling approximately 2,600,000 square feet. The project includes associated grading, paved parking, and required infrastructure on a 162-acre site in the Employment Center (EC) zone classification.

**Location:** 6719 134<sup>th</sup> Avenue East, Puyallup, WA, within Sections 25 and 26, T20N, R4E, W.M. in County Council District No. 2.

**Proponent:** Knutson Farms, Inc.

### Conclusions of Responsible Official:

The Responsible Official concludes that a Mitigated Determination of Nonsignificance (MDNS) may be issued for this proposal. This is based upon staff review of the environmental checklist and attachments, other information on file with Pierce County, and County regulations governing the project. The MDNS is supported by plans, policies, and regulations adopted by Pierce County for the exercise of substantive authority under SEPA. The following are the County adopted policies and/or findings which support the MDNS.

Pierce County Planning and Public Works has reviewed a Traffic Impact Analysis (TIA) prepared for the project by Transportation Engineering NorthWest (TENW) dated February 10, 2017, and determined the following mitigation is required to adequately address the project's impacts.

### Mitigation:

The Responsible Official has determined that the proposal will not have a probable significant impact on the environment, and an Environmental Impact Statement (EIS) will not be required under RCW 43.21C.030(2) (c), only if the following conditions are met. This decision was made after review of a completed environmental checklist, other information on file with Pierce County, and existing regulations. This information is available to the public on request. These mitigation measures are required as authorized under the Substantive Authority of SEPA in accordance with the guidelines contained in Section 18D.10.080 of the Pierce County Code and shall be implemented by the applicant.

If the mix of land use types and sizes change from what is presented in the revised TIA dated February 10, 2017, prepared by TENW, the applicant shall retain a transportation consultant to conduct a study to determine if there would be any increase in peak hour trips generated by any proposed change in use. The study shall be submitted to the County, which will have the right

to distribute to adjacent jurisdictions for review and comment, and which will have the authority to require additional transportation improvements proportional to any increase in peak hour trips beyond those projected in the above-referenced TIA. Any mitigation identified by the County through a potential future study would be required to be completed, and all construction inspected and approved by the appropriate agency prior to certificate of occupancy issuance associated for the change in mix of land use type or size.

- If not already constructed, the applicant will design and construct 5th Avenue SE to City of Puyallup roadway standards between Shaw Road East and 33rd Street SE prior to final building inspection on the first building in the Knutson Farms Short Plat.
- The applicant will design and construct roadway improvements to 33rd Street SE (134th Avenue East) south of 5th Avenue SE to 80th Street East to City of Puyallup road standards prior to final building inspection on the first building in the Knutson Farms Short Plat.
- If not already constructed, the applicant will design and construct roadway improvements to 134th Avenue East north of 5th Avenue SE within the Puyallup City limits. The applicant will design and construct the necessary road improvements to gain access to Shaw Road East, as well as the full street improvements along 134th Avenue East north of 5th Avenue SE consisting of 32 feet of pavement width (two 12-foot lanes with 4-foot paved shoulders), curb/gutter, and 6-foot wide sidewalks prior to the final building inspection on the first building.
- The applicant will design and construct a traffic signal at the Shaw Road East/5th Avenue SE intersection prior to occupancy of the first building.
- All truck traffic generated by the Knutson development will be restricted from using the 33rd Street SE/134th Avenue East corridor south of 5th Avenue SE, and will be limited to using Shaw Road East only via 5th Avenue SE for access to/from the development site.
- The applicant shall contribute \$75,000.00 to the City of Puyallup for trail crossing improvements at the intersection of East Pioneer and 134th Avenue East/33rd Street SE prior to the final building inspection on the first building.
- To mitigate impacts to queues on Shaw Road East between 12th Avenue SE and 23rd Avenue SE, the applicant shall contribute \$600,000.00 to the City of Puyallup to help fund the City of Puyallup's planned capital project to widen Shaw Road East prior to the final building inspection on the first building.
- To mitigate impacts to the queues along East Main Avenue and Shaw Road East, the applicant shall contribute \$500,000.00 to the City of Puyallup to help fund a new traffic signal at the intersection of 5th Avenue NE and East Main Avenue prior to the final building inspection on the first building.
- To mitigate traffic impacts to the SR-410 interchange, the applicant will contribute a proportionate share cost of \$1.0 million dollars toward future improvements at the SR-410/East Main Avenue/Traffic Avenue interchange prior to occupancy of the first building within the Knutson Farms Short Plat. This proportionate share cost shall be paid to the City of Sumner.

This Mitigated Determination of Nonsignificance (MDNS) is issued under WAC 197-11-340(2). The lead agency will not act on this proposal for 14 days from the date of issue. Comments must be submitted by close of business on the comment deadline date. The Responsible Official will reconsider the MDNS based on timely comments and may retain, modify, or, if significant adverse impacts are likely, withdraw the MDNS. If the MDNS is retained, it will be final after the expiration of the comment deadline. No permits may be issued, and the applicant shall not begin work, until the comment deadline has expired and any other necessary permits are issued.

- This MDNS is issued after using the optional MDNS process in WAC 197-11-355. There is no further comment period on the MDNS. Appeals must be filed within 14 days of the issue date.
- This MDNS is issued under WAC 197-11-340(2); the lead agency will not act on this proposal for 14 days from issue date. Appeals must be filed within 14 days of the end of the comment period.

**Responsible Official:** Dennis Hanberg  
**Position/Title:** Director  
**Address:** 2401 South 35th Street, Room 2, Tacoma, WA 98409

**Staff Contact:** Marcia Lucero/Project Manager  
mlucero@co.pierce.wa.us Phone 253-798-2789

**Issue Date:** April 26, 2017

  
\_\_\_\_\_   
for Dennis Hanberg, Responsible Official

**Appeal:**  
Pursuant to RCW 43.21C.075, Section 18D.10.080 of Title 18D, Pierce County Development Regulations-Environmental and Chapter 1.22 of Title 1, General Provisions, decisions of the Responsible Official may be appealed. Appeals are filed with appropriate fees and Notice of Appeal at the Planning and Land Services Department, located at the Development Center, in the Public Services Building, 2401 South 35th Street, Room 2, Tacoma, WA 98409. You should be prepared to make specific factual objections. Appeals must be filed within 14 days of the expiration of the comment deadline.

Note: The issuance of this MDNS does not constitute project approval. The applicant must comply with all other applicable requirements of Pierce County, federal, and state agencies, and/or the Hearing Examiner prior to receiving construction permits.

**Pierce County Online Permit Information:**  
<http://palsonline.co.pierce.wa.us/palsonline/permitinfo?applPermitId=792210>



Margaret Y. Archer  
Direct: (253) 620-6550  
E-mail: marcher@gth-law.com

October 14, 2019

Via email ([JBeck@ci.puyallup.wa.us](mailto:JBeck@ci.puyallup.wa.us)) and mail

Joe Beck  
City of Puyallup  
333 S. Meridian  
Puyallup, WA 98371

RE: *Knutson Farms Industrial Park – EIS Process*

Dear Joe:

The purpose of this letter is to request the City immediately commence the scoping process for the Knutson Farms Industrial Park EIS if it has not already done so. It is our expectation that Knutson Farms will have the opportunity to work with the City on a reasonable process to facilitate expeditious preparation of the EIS.

You will recall that on May 7, 2019, I emailed the City's attorneys requesting the opportunity to discuss the EIS process for this project. We asked that the City proceed with the EIS scoping while the Petition for Supreme Court review was still pending. My client purposely did not seek a stay because we did not wish to delay the continued review of the project.

The City responded that Knutson Farms' administrative appeal filed with the Puyallup Hearing Examiner in May 2017 "would have to be resolved before arrangements for EIS preparation could get underway." To eliminate that perceived hurdle, Knutson Farms withdrew the administrative appeal on June 25, 2019, and again requested that the City commence the EIS process. Of course, the Supreme Court issued its decision denying the Petition for Review on September 4, 2019.

The Legislature has directed lead agencies to prepare the EIS in an expeditious manner. The process must commence. The questions presented in my May 6, 2019 email remain to be addressed and we request the opportunity to discuss the following issues.

- Who does the City anticipate will prepare the EIS? What is the process for selecting the consultant?

Reply to:  
Tacoma Office  
1201 Pacific Ave., Suite 2100 (253) 620-6500  
Tacoma, WA 98402 (253) 620-6565 (fax)

Seattle Office  
600 University, Suite 2100 (206) 676-7500  
Seattle, WA 98101 (206) 676-7575 (fax)

- What will be the scope of the EIS?
  - Does the City anticipate raising issues beyond those raised in the SEPA appeal?
  - Does the City intend to analyze issues other than those it argued in its appeal?
- SEPA both generally and under WAC 197-11-948 directs the lead agency to aspire to prepare the EIS expeditiously – what does the City believe expeditiously will mean for this process? What is the schedule?
  - Will existing studies be utilized as authorized by WAC 197-11-600 and if so to what extent? Will the information presented at the two week Examiner proceeding be used as a starting point? How will the various expert reports already prepared be used?
- Knutson Farms has accepted the modified conditions as presented by the Examiner, including the restriction allowing no truck traffic south of Pioneer Way. We presume those additional conditions will be considered as elements of the proposal in the Lead Agency's EIS analysis. Is that correct? Will there be an opportunity to discuss those mitigation measures and potentially other additional measures as part of the EIS scoping process? Examples include without limitation:
  - Rather than prepare additional study on whether Wetland D is or is not a regulated wetland, Knutson Farms is willing to instead mitigate for fill of Wetland D as if it is a Category III wetland.
  - In light of the City's position regarding certain financial contributions required by the MDNS to mitigate traffic, are there alternative mitigations measures to which those funds would be better directed?
- How does the City envision communication will occur with regard to the scoping, completion and review of the EIS?
  - Can we devise a process to address potential disagreements efficiently and quickly?

We understand that this development has become highly politicized. Following the Supreme Court's decision, Mayor Palmer implied on FaceBook that the City will exploit its official role as Lead Agency and use the EIS process to leverage proposal changes to conform to the City's zoning rather than the zoning applicable to the project:

After a long process with the land owners and the County, the City adopted a plan for this area (part of Puyallup's urban

Gordon Thomas Honeywell<sup>LLP</sup>  
October 14, 2019  
Page 3

growth area) in 2009. Unless the developer alters its proposal, the next step is to develop an EIS and mitigate this proposal to be more consistent with the City's land use plan and not allow trucks to bring Shaw Road to a stand still.

We trust that City staff, while in the role of Lead Agency, will not allow the process to be politicized, but will engage in normal discussions with our client regarding the scope and use of studies already completed, including studies prepared for the SEPA appeal hearing before the Pierce County Examiner, so that the EIS may be prepared efficiently and consistent SEPA rules.

Thank you for your assistance in this matter. We look forward to working with the City in the immediate future toward expeditious preparation of the EIS.

Very truly yours,

A handwritten signature in cursive script that reads "Margaret Y. Archer".

Margaret Y. Archer

MYA:mya

cc: Tim Berry  
Bill Lynn  
Peter Eglick  
Josh Whited

**Applicant:** Running Bear Development Partners, LLC

**Commenter:** Berry, Tim

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Greeting.

Attached are comments from the Knutson Farms Industrial Park Applicant Running Bear Development Partners, LLC. Also attached is a letter from Tim Berry presenting proposed new mitigation measures for the Knutson Farm Industrial Park project to be considered with the Applicant's comments. The attached comments are supplemental to the comments submitted yesterday by the Applicant's consultant Richard Wienman.

Thank you for your consideration. Please let me know if you have difficulties opening or accessing the attachments.

Margaret Archer

## Running Bear Development Partners, LLC

7701 Forsyth Blvd., Suite 900  
St. Louis, MO 63105  
(314) 862-7080

March 15, 2024

Chris Beale  
City of Puyallup  
Knutson Farms Industrial Park  
Designated EIS Contact Person  
333 S. Meridian, 2<sup>nd</sup> Floor  
Puyallup, WA 98371

Jen Tetatzin  
Pierce County  
Director of Planning & Public Works  
2702 S. 42<sup>nd</sup>, Suite 109  
Tacoma, WA 98409

RE: Knutson Farms Industrial Park, Pierce County Project ID No. 469640  
Proposed New Mitigation Measures

The Knutson Farms Industrial Park team has completed a detailed review of the Draft Environmental Impact Statement (DEIS) issued by the City of Puyallup as the project's SEPA Lead Agency on December 14, 2023. Detailed comments on the DEIS are being submitted under separate cover from our EIS Consultant Richard Weinman, Weinman Consulting, LLC, and our attorneys, Bill Lynn and Margaret Archer, Gordon Thomas Honeywell, LLP. The purpose of this letter is to propose new mitigation measures to address environmental concerns raised in the DEIS. As the Applicant, I request that the proposed new mitigation measures, along with previously proposed additional mitigation measures, be included as additional elements of the Knutson Farms Industrial Park ("KFIP") project and, accordingly, that the measures be considered in the Final EIS.

Initially, I wish to reconfirm our prior stated commitment to comply with all mitigation measures set forth in both Pierce County's April 26, 2017 Mitigated Determination of Nonsignificance ("MDNS"),<sup>1</sup> and Pierce County's Short Plat approval, as modified by the Pierce County Hearing Examiner in his November 21, 2018 Preliminary Short Plat and SEPA appeal decisions. On October 14, 2019, through correspondence from our attorneys,<sup>2</sup> we confirmed our voluntary commitment to incorporate the modified conditions into the KFIP project, even though substantial portions of those decisions were voided by subsequent

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<sup>1</sup> For convenience, I have enclosed with this letter a copy of the County's April, 26, 2017 MDNS.

<sup>2</sup> I have also enclosed a copy of attorney Margaret Archer's October 14, 2019 letter to attorney Joe Beck.

litigation.<sup>3</sup> Without listing all the previously imposed conditions, as modified by the Examiner and accepted by the Applicant, the previously volunteered mitigation includes:

- To mitigate impacts to the queues along East Main Avenue and Shaw Road West, KFIP shall either install a new traffic signal at the intersection of 5<sup>th</sup> Avenue East and East Main Avenue or shall show to the satisfaction of the responsible official that the signal improvement is fully funded prior to the issuance of any occupancy permits.
- KFIP shall to the best of its ability prohibit the use of Shaw Road south of Pioneer Way East as a truck route.
- To mitigate significant adverse traffic impacts KFIP will contribute to a proportionate share cost of 1.0 million dollars toward future improvements at the SR-410/East Main Avenue/Traffic Avenue interchange this proportionate share cost shall be paid to the City of Sumner. No occupancy permits shall issue for improvements on the KFIP short plat until such time as said interchange improvements are completed.
- KFIP shall make additional financial contributions to the City for specified road improvements. (We also offered to meet and confer with the City to discuss redirecting the funds to different traffic mitigation projects if the City deemed that appropriate.)
- KFIP and the County shall provide the City a copy of the final storm drainage plan and a two-week period in which to comment prior to its final approval by the County. (However, the Examiner also appropriately confirmed that the decision to approve or disapprove the plan remains solely that of the County. We note Judge Murphy's LUPA Order sustained Pierce County's interpretation of the Stormwater Manual and subdivision regulations that it is appropriate to defer submission of detailed drainage plans to final short plat approval.)

We also offered, despite the disagreement regarding appropriate categorization of Wetland D, to mitigate fill of this wetland as if it is a Category III wetland in lieu of the additional wetland delineation study ordered by the Examiner. We remain so committed to mitigate Wetland D. Of course, some of the modified mitigation conditions related to traffic were enhanced through the Declaration of Restrictive Covenant that I signed on July 20, 2022.

Again, the above voluntary mitigation measures should be treated as additional elements of the KFIP project. It appears that many were not considered in the DEIS. The previously agreed-to voluntary mitigation conditions should be appropriately considered in formulating recommended and potential mitigation in the Final EIS

In addition to the previously volunteered mitigation, KFIP now also proposes the appended Knutsen Farms Industrial Park New Proposed Mitigation Measures be included as additional elements of the KFIP project. The appended proposed new measures focus upon and are intended to mitigate stormwater concerns raised in the DEIS. My team is available to confer

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<sup>3</sup> Certain specific decisions set forth in the Examiner's November 21, 2018 decision on the preliminary short plat were affirmed by Judge Edmond Murphy in an October 18, 2023 Order on LUPA Appeal filed under Pierce County case no. 19-2-06362-4 in response to a Land Use Petition Act appeal file by Puyallup ("LUPA Order").

with both the City and the County to discuss any of the proposed mitigation measures. My team is also available to meet and confer with the City to discuss fees in lieu of traffic mitigation based upon the "Roadway Proportional Factor" discussed in the DEIS. As presented, the scope and costs of the improvements to which the "Roadway Proportional Factor" may be applied are too ambiguous and vague to allow KFIP to offer meaningful comment at this time. But we would welcome the opportunity to sit down with the City and the City's traffic consultants to explore opportunities for agreement on traffic mitigation.

Thank you for your consideration. We presume that the proffered additional mitigation conditions will be considered as elements of the KIFP project in the City's continued work on the EIS as Lead Agency.

Sincerely,



Tim Berry  
Running Bear Development Partners, LLC

Enclosures

cc: Sean Gaffney, Pierce County Planning Manager  
Cort O'Connor, Pierce County Deputy Prosecutor  
Meredeth Neal, Puyallup SEPA Responsible Official  
Joe Beck, Puyallup City Attorney  
Richard Weinman, Weinman Consulting, LLC  
Bill Lynn / Margaret Archer, Gordon Thomas Honeywell, LLP

**Weinman Consulting**

**Commenter:** Weinman, Richard

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Dear Ms. Neal & Mr. Beale:

I am submitting comments on the Knutson Farms Draft EIS in 2 electronic files: a comment letter and a companion table of comments (Attachment 1).

My thanks to the City of Puyallup for the opportunity to comment.

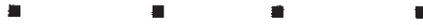
Richard Weinman

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# **Weinman Consulting, LLC**



March 18, 2024

City of Puyallup

Meredith Neal, SEPA Responsible Official & Mr. Chris Beale, EIS Contact Person

333 S Meridian

2nd Floor

Puyallup, WA 98371

Dear Ms. Neal & Mr. Beale:

I am submitting this comment letter and attachments on the City's Knutson Farms Industrial Park Draft EIS (DEIS) on behalf of the applicant, Running Bear LLC. I am a planner/land use consultant with 40+ years of experience specializing in land use policy analysis and State Environmental Policy Act (SEPA) compliance. My SEPA experience includes preparing, managing, editing and performing 3<sup>rd</sup>-party peer review for several hundred SEPA documents that address programmatic actions and almost every type of development proposal, including industrial master plans. I have also acted as a SEPA Responsible Official and regularly provide strategic advice to public agencies on SEPA issues. My comments are based on that experience and expertise with SEPA. I have had no prior involvement with the Knutson Farms Industrial project or the Applicant. My role was only to review the DEIS and to provide my comments. Other professional consulting firms have provided input on technical issues including Barghausen Engineers, Soundview Consultants (SVC) and Transportation Engineering Northwest (TENW), which I refer to in this letter.

This letter contains the essence of our comments on the Draft EIS. The attached matrix (Attachment 1) provides further documentation, including specific examples, of the multiple errors and inconsistencies in the DEIS environmental analyses. This letter highlights and provides representative examples of the range of errors and flaws identified in Attachment 1. Note that the comments in the attachment also identify new proposed mitigation measures that the applicant will voluntarily propose.

## **Overview**

As stated in the SEPA Rules, one of the most basic and overarching purposes of an EIS is to "provide an impartial discussion of significant environmental impacts..." (WAC 197-11-400(2)).

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March 14, 2024

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As will be shown in examples from almost every section of the DEIS document, the Knutson Farms DEIS does not meet this most basic SEPA requirement. A lack of objective impact analyses permeates the DEIS. Analyses are frequently superficial, inaccurate or based on incomplete information, and many conclusions are unsupported and/or results-oriented and slanted towards a pre-determined conclusion.

The City seems to be using its role as SEPA lead agency as license to use the EIS process to make its long-range vision for the Knutson Farms site dominant and to determine environmental mitigation requirements for project review based on its own vision and preferences. In support of this view, the DEIS repeatedly misapplies and elevates the City's land use policy and regulatory framework, which does not apply to a project that is located outside the City's jurisdiction. While paying lip service to Pierce County's authority, evaluations of regional policies in the DEIS are ultimately attacks on Pierce County's underlying zoning for the Knutson Farms site. The DEIS does not consider the mitigation that Pierce County regulations will require during project review and permitting. The end result of this approach is that many elements of the Knutson Farms project which reflect inconsistencies between County and City zoning are incorrectly characterized as being significant impacts of the proposal. Representative examples of this pattern of flaws are identified below.

### **1. SEPA Requirements**

SEPA-related issues present in the DEIS include the description of the proposed action, alternatives and several atypical or abnormal approaches to document organization and the content of required EIS subsections.

#### *Proposed Action*

- The DEIS mischaracterizes the proposal. It uses an incorrect site area (188 in the DEIS vs. 167 in the preliminary short plat); and the Summary chapter contains an incomplete list of the Applicant's Objectives which were provided to the City during EIS preparation.
- The current location of the recreational trail was established based on previous input from Pierce County. The DEIS now finds this location to be inconsistent with its shoreline recreation goal and wants it to be moved closer to the shoreline.
- The Proposed Action discussion also assumes that the project could potentially include a broad range of industrial uses that are not proposed or contemplated by the applicant; in fact, these potential uses have been formally excluded by the recording of a covenant as requested by the City. These misstatements are repeated multiple times in the document. It is standard practice to consider the definition of uses proposed by the

applicant and evaluated in the environmental document as a boundary for what can be developed without undergoing supplemental SEPA review and revising the master plan. Pierce County confirmed the list of proposed uses.

### *Alternatives*

Issues with the EIS alternatives include the following:

- Alternatives 1 and 2 do not meet the Applicant's Objectives and, therefore, are inconsistent with a basic SEPA requirements for alternatives (see WAC 197-11-440(5)).
- Alternative 1 is similarly inconsistent with the SEPA Rules because it does not reduce impacts relative to the Proposal (WAC 197-11-440(5)(a)).
- Assumptions about the No Action alternative vary in different sections of the document, particularly in regard to statements about future land use. In general, agricultural use is incorrectly framed as being environmentally benign and the impacts of continued agricultural use are underestimated.
- City staff has represented at public meetings that the City intends to recommend a Preferred Alternative in the Final EIS. If that is correct, that would be an unprecedented use of SEPA for a project proposal in my experience. This is not a situation where the City has a need or the authority to make a choice among alternatives. The City can and should, of course, consider the information in the EIS to inform its own permit decisions relating to utilities and access, to the extent it has discretion for these permitting decisions, but the City's preference among alternatives is not determinative or binding on other agencies.
- Alternative 2 (Reduced Intensity) is apparently constructed to reflect the City's vision and desires for development and mitigation of the site, notwithstanding its lack of authority to implement this vision through permitting. This approach might be useful for a programmatic action such as an EIS for a subarea plan, but which is unusual for a project EIS. In this case, the City as lead agency has no reason to select a packaged preferred alternative (revised proposal and mitigation combined), since the City is not the primary permitting agency. It would be more useful to the ultimate permitting agency (Pierce County) if the EIS presented mitigation measures as a list, uncoupled from individual alternatives and without the measures that are based on the City's inconsistent zoning preferences for development of the project site.

### *DEIS Format and Content*

There are numerous irregularities in the organization and format of the DEIS. While innovation in SEPA documents is not necessarily bad or wrong, the oddities in the Knutson Farms DEIS appear to also further the DEIS pattern of emphasizing the City's own land use vision and policies and denigrating Pierce County's applicable policies, zoning designations and regulations.

- The *Fact Sheet* does not identify where comments should be sent, when the comment period ends, and when public meetings are scheduled (WAC 197-11-440).
- Impact discussions are scattered throughout the DEIS in brief, incomplete snippets of analysis of City policies and land use designations (primarily in the Land Use/Plans & Policies section). As discussed further below, these analyses are frequently incomplete and/or based on flawed results-oriented interpretations. A conventional EIS Land Use section is omitted and, consequently, there is no coherent analysis of land use patterns and the city-wide urban context of land use.
- The DEIS does not clearly identify or discuss "Significant Unavoidable Adverse Impacts" (WAC 197-11-440(6)(c)(v)), thereby implying that all identified impacts can be avoided or mitigated. This implication is incorrect; some unavoidable impacts would likely occur under any development scenario/action alternative and for the No Action alternative as well. These should be identified. Some unavoidable impacts may not be significant, and should be distinguished from impacts that are significant and can be mitigated; these distinctions are consistent with the overall purpose of SEPA.
- Mitigation measures are specific to each EIS alternative rather than provided as a comprehensive list of measures that could be applied to each alternative. Similarly, there are no distinctions recognized among mitigation measures included in the proposal, mitigation required by regulations (i.e., Pierce County regulations), and additional measures that are otherwise "recommended" by the EIS preparer (WAC 197-11-440(6)(c)(iv)). This approach will make it more difficult for Pierce County, the primary permitting agency, to consider the environmental analysis in its decision making. Similarly, mitigation measures proposed or accepted by the Applicant, which are substantial for the Knutson Farms proposal, are never mentioned.

## **2. Lack of Objective Analysis, Pervasive Bias, Erroneous Conclusions**

As noted previously, one of the most basic and overarching requirements of an EIS is to "provide an impartial discussion of significant environmental impacts..." (WAC 197-11-400(2)). The

Knutson Farms DEIS is replete with examples of analyses that reveal an apparent pre-determined intent to consistently find fault with the project's purpose, land use and design. This lack of objectivity is particularly evident in its deliberate and erroneous analysis of land use policies; the policies are typically off-point, but the findings of inconsistency serve to denigrate the proposal and/or to gratuitously criticize Pierce County's prior land use planning and zoning decisions. Selected examples are provided below; additional examples are included in Table 1.

- The analyses of policies that were cherry-picked from the Pierce County Countywide Planning Policies (CPPs) is a prime example of the problem. The selected policies are inapplicable to the proposal and the analysis is results-oriented, erroneous and demonstrate an approach that is anomalous for a professionally prepared SEPA document. As defined in the Growth Management Act (GMA, RCW 37.70A.210), the CPPs are general and regional in scope and are intended to guide and coordinate city and county comprehensive planning. It is inappropriate to use general, regional policies to evaluate an individual site-specific development proposal. It highlights the DEIS writer's misunderstanding of GMA, apparent inexperience with SEPA land use policy analysis, and/or evidences a misguided mission to find inconsistency any place it can. Examples include the following:
  - The analysis repeatedly interprets individual CPPs to be applicable to the proposal (e.g., LU-44 and LU 46) and concludes that Pierce County's EC zoning of the Knutson Farms site is inconsistent with CPPs. The DEIS then extends this conclusion to the Knutson Farms proposal and concludes that it is inconsistent with the CPPs. This reasoning is circular and flawed.
  - Similarly, Pierce County's Shoreline Master Program (SMP) Policy B-3, which generally "encourages" mixed use throughout the jurisdiction, is misinterpreted as *requiring* mixed use, and the analysis therefore concludes that the proposed single use industrial project is inconsistent with the policy.
  - The same policy generally "encourages" provision of open space, but the DEIS determines that the proposal is inconsistent with the policy because the analyst finds fault with the location or design of the open space that is provided.
- The collateral attacks on prior County land use decisions are inappropriate and irrelevant. They are unnecessary statements that avoid accepting and acknowledging that the proposed action is consistent with applicable zoning, notwithstanding that the proposal may be inconsistent with the City's long-term vision for the unincorporated potential annexation area. These findings of inconsistency are erroneous.

- Similarly, the DEIS selects numerous Puyallup Comprehensive Plan land use goals and policies that provide general direction for the City's own planning and inappropriately uses them to evaluate the proposed action. For example, Goal CC-2 generally describes the city-wide land use character as mixed-use but is interpreted in the DEIS to conclude that a single use project like Knutson Farms is inconsistent with the goal. Obviously, every project in Puyallup is not mixed-use in nature; are all single use developments in the City inconsistent with this Comprehensive Plan goal? The DEIS reads Pierce County SMP Policy B-3, which generally encourages mixed use development, in the same manner and concludes that a single use project is inconsistent. Other examples of this type of shallow, results-oriented and erroneous analysis of land use policy are identified in Attachment 1.
- The proposal is located within unincorporated Pierce County and is not currently subject to a pre-annexation agreement or joint planning agreement; the "potential" annexation area in which it is located may or may not be annexed to the City in the future. The DEIS includes numerous instances, however, where findings of project inconsistency are based on the fact that (1) the City and County have not conducted joint planning and agreed on future land use for the unincorporated potential annexation area; and (2) Pierce County's current land use designation and zoning of the site are not consistent with the City's future land use/zoning for the potential annexation area. Any interjurisdictional disagreement about land use planning between the City and County is beside the point and should not influence the analysis and conclusions of a project EIS. The fact that joint planning has not occurred is a topic for another forum; it could be mentioned once in the DEIS before moving on and focusing on consistency with Pierce County land use and zoning designations.
- Using similarly results-oriented policy analysis, the DEIS concludes that the Pierce County Employment Center (EC) zoning applicable to the Knutson Farms project site is inappropriate, based on an interpretation of the Alderton-McMillin Community Plan that misunderstands or ignores that the plan encourages limited economic development in specific limited locations in the sub-area, as well as maintaining the sub-area's rural character. The County's land use and zoning have been in effect since 1997, and the DEIS's repeated collateral attacks on County decisions are inaccurate and inappropriate; they reflect an apparent strategy to elevate City policy by denigrating County policy.
- The Aesthetics analysis misrepresents and misuses the view protection objectives in the Alderton-McMillin Community Plan. In reality, neither Puyallup nor Pierce County have adopted view protection policies or regulations in their Comprehensive Plans or codes,

and in particular have not identified any specific protected viewsheds through policy or regulations. This includes the viewshed that is used as the basis for the impact analysis and mitigation measure identified in the DEIS.

- It is noteworthy that a concept plan for Van Lierop Park and the viewshed encompassing Mt. Rainier, were first presented at an open house in 2017, which is 3 years after the Knutson Farms project application was submitted to Pierce County and had vested, and 2 years after the Alderton-McMillin Community Plan which confirmed the County's 1997 EC zoning of the Knutson Farms site was adopted. The City was clearly aware of Knutson Farms proposal when the park was being designed; a "context graphic" presented at the 2017 open house identifies "future Knutson industrial development" and shows tall, solid vegetation that "buffers the park from future industrial development." The City's 2018 SEPA checklist for the park's Phase 1 design and construction acknowledges that views of Mt. Rainier could be obstructed by unidentified "future industrial development."
- The Aesthetics section overall is an exercise in misdirection. The basic methodology is not explained (e.g., how the viewshed was defined, for example, and technical criteria used to identify impacts), and it omits any post-development simulations to help identify the extent of impacts resulting from the proposal and options for reasonable mitigation. Elimination of an entire project building that obstructs views of Mt. Rainier is the mitigation proposed (Alternative 2/Reduced Intensity) is unreasonable and without any foundation in adopted policy or regulations.

#### **4. Unsupported Conclusions, Incomplete or Erroneous Analysis**

The analysis of key elements of the natural environment -- Surface Water, Groundwater, Plants and Animals -- contains numerous opinions, errors and unsupported conclusions. Information regarding Noise, Cultural Resources and Public Services is also incomplete or inaccurate. Description of existing conditions, particularly related to the outfall, are also incorrect and are a mix fact and opinion. Several examples are provided below; please refer to Table 1 for additional comments.

- In general, the DEIS contains substantial discussion related to the outfall project that seems misplaced, given that the project was designed and approved previously by the County (with City input) and state agencies. The DEIS concludes that stormwater flows from the KFIP to the outfall would significantly increase erosion and instability at the riverbank. This conclusion is unsupported and is inconsistent with analysis performed by

the Applicant's technical team and provided to the City. Refer to Attachment 1 for additional detail.

- The DEIS states that enhanced water quality treatment that is proposed per Pierce County code requirements would not provide protection of listed species from potential impacts of the 6PPD. This conclusion is an unsupported opinion as the potential water quality impacts of this chemical are still being studied and are not the subject of adopted standards.
- Based on Pierce County regulations, hydrologic monitoring is not required for all on-site wetlands, only possibly Wetland C A site visit in March 2018 did not determine that Wetland D was larger than previously described, contrary to the statement in the DEIS. Other data on this wetland contained in the Wetland Report is inaccurate or incomplete. Refer to Table 1 for additional detail.
- The DEIS states that the OHWM, which was previously approved by multiple agencies, is incorrect and should be modified. This statement is unexplained and unsupported; no justification for this revision is provided.
- Based on the proposed stormwater control system, many impacts should not be considered to be significant and/or unavoidable. As noted previously, the DEIS does not focus its discussion of Significant Unavoidable Adverse Impacts consistent with SEPA requirements.
- The application for the proposal was submitted to Pierce County in 2014 and vested to then applicable regulations. The statement in the DEIS that Pierce County's current Shoreline Master Program applies is incorrect.
- "Prime farmland" is defined by numerous criteria which are not evaluated in the DEIS. It is not clear, therefore, that the site contains prime farmland as stated in the DEIS. In any event, the site is not designated as agricultural lands of long-term commercial significance under the GMA. Impacts to wetlands, shorelines, and floodplains from continued agricultural use of site under No Action are not discussed. Agricultural use is intensive and existing impacts are likely significant.
- The Cultural Resources section should be corrected to identify that the residence and outbuilding located on the site that are recommended eligible for listing on historic registers were demolished in Feb. 2023, under a permit issued by Pierce County.

- The Noise section contains errors in applicable standards and inconsistent and questionable mitigation.
  - It incorrectly identifies the use category of the site. Per PCC 8.76.050, the site should be a Class C EDNA (agricultural use) not Class A EDNA (park-like land use). This raises the allowed noise at the receiving property to 60 dBA.
  - The analysis uses a method for noise analysis from the FHWA Handbook which was developed for highway construction projects not industrial or other types of land uses. No highway construction is proposed as part of this project. Although this method is often used as a proxy method for discerning traffic noise increases in SEPA analyses, it may be inappropriate in a project context with no federal nexus. Mitigation using sound walls may therefore be unreasonable.
  - It is unclear how the noise wall mitigation is derived and whether it's necessary given the Class C EDNA of the site. In addition, a 12-ft. noise wall could block the view corridor from Van Lierop Park and would be inconsistent with the proposed mitigation for view impacts.

## **5. Unclear Basis for Analysis & Mitigation**

The Transportation analysis, while mostly reasonable, raises some questions about its assumptions and the use of impact measures and mitigation approaches that are not based on adopted City standards.

- Two traffic analysis measures of effectiveness (MOEs) described in the DEIS – queue lengths at intersections and travel time – relate to but are not based on adopted City standards and were nonetheless considered. It is unclear how these measures influenced impacts and were used to identify and potential mitigation. Similarly, a “roadway proportional factor” is also identified as a measure for mitigation, but it is unclear how this factor was derived or how it relates to adopted City or County policies for traffic impact evaluation.
- Four large corridor-wide improvements are identified as needed for all the Transportation Scenarios. However, the analysis is unclear as to what specific actions would mitigate impacts or whether any of the improvements would adequately mitigate impacts.

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- The analysis does not demonstrate a nexus between the proposed project and the recommended mitigation or the proportionate share. In addition, the analysis does not clearly link recommended mitigation to City standards or discuss Pierce County policies, impact standards and mitigation programs.

### Conclusion

The DEIS is seriously flawed, undermined by lack of objective and in several cases inaccurate analysis. The analysis and the EIS alternatives seem to reflect advocacy for denying or modifying the proposed action instead of providing dispassionate evaluation. The discussions should be pruned to remove irrelevant policies and to make consistency evaluations more focused, balanced and accurate.

The City's has not clearly understood and distinguished its role as SEPA lead agency from the role of Pierce County as permitting agency. The confusion is evident in the EISs indirect but transparent attempts to usurp the primacy of Pierce County policies and regulations in review of the project and its use environmental analysis to substitute its own vision of future land use for the site. Collateral attacks on County land use decisions are frequent and inappropriate and should be removed.

The FEIS should provide corrected, completed and/or new environmental information, supported by objective analysis, to address the serious defects in the DEIS identified above. In particular, the Final EIS should eliminate the current dominance of the City's future vision for the potential annexation area and give greater weight to Pierce County land use designations, regulatory requirements and environmental programs when evaluating the proposal and considering mitigation.

As noted previously, the applicant intends to revise the Proposed Action to incorporate several new mitigation measures, which are identified in the attached comment table. The FEIS should incorporate and consider these additional measures in its reevaluation of impacts and appropriate mitigation.

Sincerely,



Richard Weinman

Weinman Consulting LLC

Attachment A (comment table)

## ATTACHMENT 1

### Knutson Farms Industrial Park DEIS – Compiled Knutson Farms Team Comments

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
<b>SEPA/EIS Requirements</b>		
All Sections	<b>EIS Alternatives</b>	<b>Inconsistent with SEPA</b> – SEPA directs that EIS alternatives must be reasonable and must meet the Applicant’s objectives but with less impacts than the Proposed Action. Alternatives 1 and 2 do not meet the Applicant’s objectives, as listed in Ch. 2. Financial feasibility of Alternative 1 (cost of construction, cost of rail transport) is questionable; City only considered technical feasibility; environmental benefit is limited to transportation and that is not substantial. Alternative 1 increases impacts relative to the Proposed Action in several areas (e.g., noise, recreation).
All Sections	<b>No Action Alternative</b>	<b>Inconsistent Assumptions</b> – In some sections, the DEIS indicates that under the No Action alternative the site could continue in agricultural uses or develop according to the site’s zoning. In other sections, it is only assumed that nothing would occur onsite. The analysis switches between these different No Action assumptions to select the one that results in the worst impacts for each element of the environment. A consistent, even-handed approach that assumes continuing use of the site should be taken.
<b>FACT SHEET</b>		
Missing	<b>End of DEIS Comment Period, Public Meeting Info., Cost of the EIS to the Public</b>	<b>Omitted</b> – These sections required by WAC 197-11-440 are not included in the Fact Sheet
Pg. i, Pg. ii, Pg. 2-1, Pg. 3-5 and throughout report	<b>Project area</b> – Noted as 188 acres	<b>Inaccurate</b> – The total site area indicated in the DEIS is incorrect. Based on the preliminary short plat, the total site area is approximately 167 acres and the total development area is approximately 126 acres.

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
<b>CHAPTER 2 – INTRODUCTION</b>		
General	<b>Description of Proposed Project</b> – Description of the restrictive covenant as well as the uses allowed under the site’s EC zone.	<b>Overblown, Irregular EIS Approach, Inaccurate</b> – The project description is skewed to suggest that the proposal is ill-defined/purposely vague. Several of the EC zone permitted uses listed in the description are not proposed by Knutson Farms Industrial Park (KFIP) (e.g., off-site hazardous waste treatment and storage facility, recycling collection and processing facility, salvage yards/vehicle storage). As a result, all the possible uses or the maximum impact-generating use under the site’s zoning are studied for each element in the DEIS. This approach is speculative and irregular.
Pg. 2-2, Section 2.2	<b>Regulatory and Policy Context</b> – “... the UGA is expected to annex and develop under designated policies and future land development and growth scenarios consistent with the affected city’s Comprehensive Plan. In this case, the development is subject to the policies in both agencies’ Comprehensive Plans (Pierce County and City of Puyallup); analysis in this EIS subjects the proposed development to analysis of consistency with both Plans. “	<b>Inaccurate, Misleading</b> – Inaccurate description of regulatory and policy context. While future annexation of the KFIP property may be desired, such expectation is speculative and remote. The future potential for annexation is used in the DEIS to artificially elevate City land use policies and designations and to create an impression of conflict. In reality, the City lacks authority to apply its land use designations to the project in place of Pierce County’s, except hypothetically. The proposal is “subject to” the City’s comprehensive plan in only a limited and mostly hypothetical sense. Inconsistencies and/or conflicts between City and County land use policies or designations should be documented as such, not as inconsistencies or impacts of the proposed project. The Final EIS should clarify the City’s authority.
<b>CHAPTER 3 – PROJECT DESCRIPTION</b>		
General	<b>Impacts and Mitigation</b> – Four impact categories were created and used throughout the EIS.	<b>Irregular EIS Format and Approach</b> – Only “significant impact” and “no impact” are defined in SEPA and are standard categories used in EISs. The other categories (“mitigated significant impact” and “less than significant

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
		impact ") have been created and applied by the EIS preparer and have the effect of obscuring impacts.
Pg. 3-9 thru 3-18, Section 3.4	<b>Proposed Project</b>	<b>Incomplete, Irregular EIS Format and Approach</b> – The project description does not provide an even-handed description of the proposal. It includes only limited and selective information on positive aspects, such as: the open space area along the Puyallup River, the pedestrian trail, landscaping, and elements of the project design. Also, mitigation required by regulations are typically assumed to be part of the proposal unless the Applicant proposes otherwise and this category of mitigation is not referenced.
Pg. 3-23, Fig. 3-4	<b>Alternative 2, Reduced Density Alternative</b>	<b>Unclear</b> – This figure is very confusing and isn't introduced well in the text. As an example, what is the difference between the "proposed trail" and the "proposed pedestrian trail"?
Pg. 3-11 thru 3-14, Table 3-3	<b>Industrial Use Category Examples</b>	<b>Inaccurate</b> – A recorded covenant limits future uses of the property, yet the DEIS repeatedly focuses on describing all possible uses in the EC zone, including ones that are not proposed and could not be developed. It is standard EIS practice to address the categories of uses that are proposed and to state that dissimilar uses are outside the environmental analysis and would be evaluated if they are proposed in the future.
<b>CHAPTER 4 – IMPACTS, MITIGATION, SIGNIFICANT UNAVOIDABLE IMPACTS</b>		
<b>All Sections</b>		
All Sections, General		<b>Irregular EIS Format and Approach</b> – Categorization of impacts is odd, confusing, and ignores standard WAC typology. Avoids/refuses to include required category of "Significant Unavoidable Adverse Impacts" and to distinguish between impacts that are significant and unavoidable, or significant and avoidable through mitigation.

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
<b>4.2 Surface Water, 4.3 Groundwater, 4.4 Plants and Animals</b>		
General	<b>Overview of Problems with Stormwater Information and Analyses</b>	<b>Overview of Comments</b> <ul style="list-style-type: none"> <li>• Inaccurate descriptions of affected environment/existing conditions</li> <li>• Inconsistent interpretations of stormwater requirements</li> <li>• Repeated collateral attacks on previously permitted outfall design</li> <li>• Overstatement of significant unavoidable impacts</li> </ul>
Sections 4.1, 4.2, 4.3 and 4.4, General		<b>Irregular EIS Format and Approach</b> – These sections are very technical and do not appear to have been simplified for the reader. In some cases, statements appear to be opinions and lack support for conclusions. Per the SEPA Rules, an EIS should be written in language that can be understood by the lay person (WAC 197-11-400(3)).
Relevant Plans, Policies, and Regulations, Pg. 4-35	<b>Pierce County Stormwater Management and Site Development Manual</b> – “The KFIP stormwater design information describes that enhanced rather than basic treatment will be used prior to releasing stormwater runoff to the Puyallup River (Table 4-4). In addition, the current proposal is to infiltrate roof runoff from four of the warehouse roofs in trenches sited along the top of slope at the northeast edge of the high terrace. However, the infiltration facility design does not provide modeled data to show how the wetland hydroperiods of the onsite wetlands will be preserved by this proposal, as required by the PCSWDM.”	<b>Misleading</b> – Typically, this degree of flow modeling information is provided as part of the final project design and is not required for SEPA review at the master plan stage as long as it can be demonstrated to be conceptually to be feasible. That approach is followed by Pierce County.
Relevant Plans, Policies, and Regulations, Pg. 4-35 - 36	<b>Pierce County Shoreline Master Program</b> – “PCC Title 18S – the current Pierce County Shoreline Master Program was adopted in 2018 and is in the process of being updated (Ordinance 2022-37s, effective December 2022). PCC Title 18S establishes allowed uses, and defines buffers, setback requirements, and mitigation requirements for regulated waterways. PCC Title 18S identifies the Puyallup River at the KFIP site as a Shoreline of the state with a shoreline environmental designation of Conservancy (Pierce County Shoreline Designations maps, October 2019). The regulated	<b>Inaccurate Interpretation</b> – At the time of the initial project applications and vesting of the KFIP project, Pierce County Title 18S was not in effect and should not be applied to the project. At the time of application, Shorelines were regulated under PCC Title 20. Under Title 20, the shoreline management zone extends 200 ft beyond the OHWM and to the outer edge of associated wetlands.

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	<p>shoreline area includes all lands within 200 ft of the OHWM, plus all floodplains within 200 ft of the edge of the floodway and to the outer edge of all associated wetlands.</p> <p>Thus, the entire floodplain and the floodplain wetlands at the KFIP site are in the regulated Shoreline jurisdiction and are subject to Shoreline Master Program (SMP) regulations.”</p>	
<p>Relevant Plans, Policies, and Regulations, Pg. 4-36</p>	<p><b>Pierce County Critical Area Regulations</b> – “Mitigation Sequencing (PCC Chapter 18E.40.050) is required in Pierce County when a developer is considering potential impacts to critical areas. Avoidance of the impact is required if possible. If not possible, the impact must be minimized and mitigated as outlined below. Mitigation for alterations to habitat areas must achieve equivalent or greater biological functions and must address adverse impacts upstream and downstream of the development site.”</p>	<p><b>Inaccurate Interpretation/Opinion</b> – Although mitigation sequencing is generally applicable, there is no specific language in Pierce County code that requires mitigation for adverse impacts upstream and/or downstream of a development site. No upstream or downstream impacts are proposed or anticipated.</p>
<p>Affected Environment, Pg. 4-47 - 48</p>	<p><b>Agricultural Use of Site / Floodplain</b> – “The KFIP site is proposed for future construction of seven warehouses and associated infrastructure. The site is currently actively managed as farmland. It is located on a post-glacial, alluvial terrace located on the left bank of the Puyallup River. There are two terrace features onsite, a high elevation terrace to the southwest, where it is proposed to build the KFIP warehouses (high terrace), and a low elevation terrace to the northeast along the Puyallup River, which is an active floodplain (floodplain). The entire high terrace and parts of the 100-yr floodplain have been regularly plowed and planted with agricultural crops.”</p>	<p><b>Overstated, Misleading and Incomplete</b> – This characterization should be revised to accurately identify the percentage/acreage of the floodplain currently in agricultural use. These areas comprise a majority of the on-site floodplain area and are proposed to be rewilded, which will provide a significant improvement in floodplain and biological function on the site.</p>
<p>Affected Environment, Pg. 4-47 - 48</p>	<p><b>Soundview Consultants Reports Prepared for the KFIP site</b></p>	<p><b>Incomplete</b> – The list of included reports excludes an August 2023 memorandum prepared by Soundview Consultants in response to comments/concerns related to the existing stormwater outfall on the project site. This should be reviewed and included in the list.</p>
<p>Affected Environment, Pg. 4-47 - 48</p>	<p><b>Outfall Construction</b> – “Construction of the outfall has resulted in unpermitted placement of large boulders below the OHWM and increased bank erosion under current conditions. Undercutting at the riverbank has resulted in some materials from the outfall construction – some boulders</p>	<p><b>Inaccurate</b> – This paragraph contains incorrect descriptions, statements, and conclusions. Some medium sized cobbles, which were already in the floodplain prior to construction, were placed along the edge of the new outfall structure and</p>

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	<p>and A-jacks originally installed at top of slope and bioengineered sections of the riverbank slope face – starting to slump and fail. Some of these materials have fallen down the bank and into the river. Ongoing riverbank erosion (described in more detail below) has resulted and will result in impacts to fish and fish habitat in the Puyallup River (Confluence 2023).”</p>	<p>either fell or were placed on the bankfull terrace. Upstream bank erosion occurring on the project site is a result of natural river processes and a shift in the thalweg which occurred around the time of construction, rather than as a result of the outfall. Furthermore, the outfall design provides one of the only off-channel flood refugia for several miles of constrained and levied floodplain. The significant benefit it provides is completely ignored in the DEIS. See SVC’s August 2023 <i>Response to Deficiencies Memorandum</i> provided to the City of Puyallup for additional details/clarification. To date, no concrete or A-Jacks have fallen into the river that the Applicant team is aware of. The Applicant is investigating additional bank stabilization measures that could be installed to mitigate bank erosion occurring upstream to the margin of the outfall. (New Proposed Mitigation Measure)</p>
<p>Affected Environment, Pg. 4-47 - 48</p>	<p><b>Outfall Construction/Riverbank Condition</b> – “The WDFW has been tracking this situation through the hydraulic project approval (HPA) originally issued for the Viking project. WDFW staff met with KFIP consultants onsite at the end of 2022 to assess conditions at the end of a three-year monitoring period at the riverbank in relation to how the riverbank has been impacted by outfall construction. In their 2022 Correction Request concerning the outfall facility HPA, WDFW documented unpermitted placement of several boulders below the OHWM, failure of the plants installed for riverbank impacts mitigation to survive at required rates (minimum required survival was 80%), and documented riverbank erosion where previously installed plantings had been washed away in winter floods. WDFW required repair of the riverbank, through a new HPA. This work is described in detail in the following Section and below.”</p>	<p><b>Inconsistent</b> – These statements are out of date and are not consistent with prior SVC reports and follow-up with WDFW; plant material has been replaced and the correction request has been resolved to their satisfaction per HPA file number 2023-6-161+01 mitigative action report.</p>
<p>Affected Environment, Pg. 4-47 - 48</p>	<p><b>Streambank Stabilization</b> – “To assess impacts of the bank failure and ongoing erosion on listed species and habitat in the river, the City’s fisheries biologist consultant (Confluence Environmental) reviewed the</p>	<p><b>Inaccurate and Misleading Description</b> – Local deposition and erosion were considered in the solution applied per the ISPG and WDFW guidance and consultation. Given the site</p>

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	<p>WDFW HPAs and assessed streambank stabilization repairs that were installed under the most recent HPA in May 2023. In their report (Confluence, August 2023), Confluence noted that streambank stabilization protocols that are to be applied under the WDFW HPA permit are defined in the <i>Integrated Streambank Protection Guidelines</i> (ISPG). WAC 220-660-130 codifies the ISPG, which represents the best available science and provides critical technical guidance for designing and permitting bank stabilization projects in Washington State. The ISPG requires that streambank stabilization projects be grounded in sound scientific and engineering principles. For that reason, a successful bank stabilization design must be engineered to incorporate fluvial geomorphic processes and to address local ecological conditions.”</p>	<p>conditions and because nothing on the actual outfall structure had failed, the objective of the corrective actions was not to armor the bank from further erosion and natural processes occurring. Rather, the objective was to restore vegetated cover on the bank to bring the site back into compliance with habitat restoration goals, while long-term solutions for upstream natural erosion of the preexisting levee could be evaluated, permitted, and addressed. As previously noted, the Applicant is engaged in active consultation with WDFW, Pierce County, and the Puyallup Tribe to identify potential actions to address upstream levee erosion that is beginning to undermine the upstream extent of the outfall. (New Proposed Mitigation Measure)</p>
<p>Affected Environment, Pg. 4-47 - 48</p>	<p><b>Boulder Placement on Streambank</b> – “Unpermitted placement of boulders below the OHWM (a violation of the Clean Water Act and state law) was not addressed in the May 2023 repair work.”</p>	<p><b>Inaccurate/Misleading</b> – Statements about the project permitted by WDFW and subsequent actions at the request of WDFW are consistently misleading and are collateral to the proposal. The EIS consultant’s legal opinion regarding violations of federal or state law concerning a project permitted by another state agency are beyond the scope of an EIS and are inappropriate. WDFW considers this action resolved, and neither the State of Washington nor the USACE are pursuing a violation for these actions.</p> <p>To acknowledge the facts, it should be noted that placement of boulders occurred at the request of WDFW to re-establish willow plantings along the channel margin as an interim fix until larger upstream bank erosion processes could be addressed. That is why only plant material and existing river alluvium were utilized to eliminate negative impacts to fish species and habitat.</p> <p>The cobbles existed along the riverbank prior to being washed onto a bench during highwater from their</p>

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		placement on the perimeter of the outfall per the approved design. They were then replaced at the outfall location by the Applicant.
Affected Environment, Pg. 4-47 - 48	<b>Outfall Design</b> – “The original 2018 design as well as the May 2023 repair work does not protect spawning and rearing habitat in the River, as flood events comparable to past winters are expected to undercut the new installation and continue to erode the riverbank at the outfall. This is expected to lead to delivery of additional boulders and concrete debris from upslope into the Puyallup River, which will further degrade habitat.”	<b>Incomplete, Misleading</b> – This paragraph and its conclusion are misleading and incomplete and should be revised to provide an accurate picture of the positive aspects of the outfall project. The outfall, as designed, provides one of the only off-channel habitat areas with large woody debris and vegetative cover in the mid Puyallup River reach. This is a significant positive result of the outfall that has been excluded from the DEIS analysis. Furthermore, the perimeter boulders harvested during construction existed in the floodplain prior to construction and are, therefore, part of the pre-existing floodplain habitat. Based on multiple geomorphic assessments done on the site, the concrete armor-flex mat is very unlikely to get delivered into the river. It could also be argued that the several cubic yards of fine sediment and silt the outfall has collected during flood events has had a greater beneficial impact on water quality and spawning habitat of listed species than the negative impacts of the few floodplain cobbles which have fallen into the channel.
Affected Environment, Pg. 4-47 - 48	<b>Streambank Protection</b> – Referring to Confluence’s conclusion that: “There is no evidence that the [streambank protection] work was based on sound engineering principles and required hydraulic and geomorphic assessments of erosion risk. City hydraulics experts [NHC, 2023] have evaluated the installation and have indicated that the installation is likely to fail under expected future conditions. More extensive bank stabilization will be required to protect the outfall, leading to additional expense and additional adverse impacts to fish and fish habitat.”	<b>Inaccurate/Misleading and Inappropriate</b> – The DEIS contains repeated, unnecessary, and inappropriate comments and opinions regarding a project that was previously permitted by a state agency. This project EIS is not the appropriate forum for those opinions. The outfall design is consistent with V-1.4.3 Outfall Systems in WSDOE’s stormwater requirements to create an alcove for off-channel fish habitat and was thoroughly reviewed by the City’s engineers and consultants prior to approval and construction. Furthermore, it is effectively providing this habitat benefit as designed currently. Future bank

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		stabilization efforts to the levee upgradient of the outfall could be designed in collaboration with Pierce County and per ISPG guidance to avoid adverse impacts to fish and fish habitat. (New Proposed Mitigation Measure)
Affected Environment, Pg. 4-47 - 48	<b>6PPD Pollutant</b> – “Because the 6PPD pollutant has lethal effects on salmonids at very low concentrations, applying the “basic” or “enhanced” treatment standards alone may not provide enough protection to ensure no harm (i.e., take) to listed species in the Puyallup River near the new outfall. In addition, because this is a new outfall which will introduce new volumes of 6PPD to the river, it presents an increased risk to salmonids relative to pre-outfall conditions. Therefore, it does not maintain or improve the current status quo, but rather will increase the current background level of 6PPD pollution in the river.”	<b>Biased and Unfounded</b> – This statement assumes proposed treatment methods (e.g., enhanced water quality treatment) consistent with the currently adopted Pierce County Stormwater Manual which incorporates the latest version of the DOE manual are ineffective at treating 6PPD. This is scientifically unfounded and reflects the writer’s unsupported opinion. The statement also ignores the fact that the old City outfall directly discharged at the same location without any dispersion or treatment, while dispersion and treatment has been incorporated into the design of the new outfall. Given DOE’s analysis of the effectiveness of stormwater BMP’s at removing 6PPD, the preliminary nature of the current stormwater design, and the fact that DOE’s guidance document clearly states that the movement of 6PPD through stormwater systems is not currently understood, this authoritative statement is premature, not founded in known science, and is speculative. The 2022 DOE BMP effectiveness analysis clearly states in section 4.4 that the effectiveness of stormwater BMPs at removing 6PPD have not been adequately studied to date, so to assume they don’t work in the context of an EIS without the support of scientific research is irregular and unfounded.
Affected Environment, Pg. 4-47 - 48	<b>Impacts of 6PPD on Listed Species</b> – “Protection of listed species is required under federal, state and local law, and in relation to current KFIP site design, this newly identified impact to surface water quality which increases risk to listed salmonids in the river adjacent to the KFIP site suggests a need for reassessment or redesign of KFIP stormwater management plan and/or facilities. Protecting listed salmonids in response	<b>Misleading and Speculative</b> – Currently, per the guidance from DOE cited in the EIS, it is not completely understood how 6PPD and 6PPDq moves through the environment or stormwater systems. Research to date has largely been laboratory-based and standard field sampling protocols have not been approved. DOE has suggested BMPs that

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	to the new information about tire chemicals would also be consistent with Pierce County’s Comprehensive Plan policies for using best available science and adaptive management for critical areas (Goal ENV-14, Goal ENV-15, Policy ENV-15.3).”	<i>may</i> be effective at reducing or eliminating the chemical in stormwater based on current understanding, but these BMPs are not yet required and have not been incorporated into published manuals. BMPs recommended in DOE’s current guidance are proposed to be applied and are adapting to updated information, similar to the approach used on other recent industrial projects in the City and County. Therefore, protection of listed salmonids should be identified to be consistent with the best available science and current regulations. (New Proposed Mitigation Measure)
Affected Environment, Pg. 4-47 - 48	<b>Water Quality in River/Outfall Construction</b> – “As described above, water quality in the river is affected by scouring, erosion, and sediment loads from regular riverine flooding. Some of these impacts are natural and ongoing. However, construction of the outfall structure at the edge of the river has resulted in an increase in bank erosion.”	<b>Inaccurate</b> – This statement is inaccurate. SVC’s August 2023 <i>Response to Deficiencies Memorandum</i> , which was provided to the City, demonstrated that observed upstream erosion is the result of natural river processes and an upstream thalweg shift, and is not due to the outfall structure. The FEIS should incorporate this current information.
Affected Environment, Pg. 4-47 - 48	<b>Removal of Pre-Outfall Bank Vegetation/Sediment Deposition</b> – “New sediment deposits within the outfall structure from regular river flooding and scouring and subsequent erosion impacts at the top of bank at the edge of the structure have resulted from the removal of pre-outfall bank vegetation (riverine buffer vegetation) and from the loss of mitigation plantings on the riverbank (willow wands). Lack of effective protection of the riverbank at the downslope edge of the outfall structure has exacerbated baseline scouring along the riverbank.”	<b>Misleading Unsupported Conclusion/Opinion</b> – Sediment deposition within the outfall is expected, particularly while designed stormwater flows are not yet occurring. On the active side of the outfall, you can readily observe the deposition of sediments during flood events, which are flushed out as the flows decrease. This process is natural in any confluence area and to be expected in a high bedload system with an alcove-designed outfall fitted with soft armoring. This design is consistent with the DOE manual preferences for outfalls listed in V-1.4.3, and was fully reviewed and approved by the City, County and state agencies prior to construction. No data supporting these conclusions is provided in the DEIS.

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Affected Environment Pg. 4-47 - 48	<b>Erosion Impacts on Railroad Trestle</b> – “Over time, riverbank erosion at the outfall could have secondary impacts to the railroad trestle, located directly downstream from the KFIP site outfall structure (Figure 4-9).”	<b>Inaccurate</b> – The City’s consultants have confirmed that the railroad trestle is likely not at risk from the KFIP outfall structure. See NHC report. The statement in the DEIS should be corrected.
Affected Environment Pg. 4-47 - 48	<b>Energy Dissipation Measures</b> – “Energy dissipation measures are needed to protect the riverbank from erosive impacts caused by stormwater flows from the outfall.”	<b>Inaccurate/Misleading</b> – The entire outfall structure is an energy dissipation structure that includes boulder clusters, large woody debris, and vegetation to disperse energy from outfall flows.
Affected Environment Pg. 4-47 - 48	<b>Thalweg Characteristics in Scour Analysis</b> – “From field observations and as indicated from Google Earth photos, the EIS team and NHC hydraulics consultants verified that the center of the river channel (thalweg) was being forced to the left bank of the river near the KFIP outfall location due to the gravel bar along the right bank aggrading over time. During medium flows, the thalweg appeared to be running diagonally from right bank to left bank upstream of the outfall location and was directed at the KFIP site riverbank about 200 feet upstream of the outfall structure. They also noted that there was significant erosion along the left bank face of the levee, hydraulic impacts that affect the outfall location. During lower flows, the majority of the force of the thalweg is directed at the bank just upstream of the outfall due to a gravel bar constriction at this location.	<b>Inaccurate/Misleading</b> – It should be acknowledged that the intent of the alcove and vegetated bank design of the outfall is to allow natural river processes to continue and provide off channel flood refuge while diffusing the energy of flows from the stormwater outfall. The fact that the channel has shifted to direct more energy at the outfall is expected, as is the eventual shift away from the outfall to an orientation consistent with pre-disturbance conditions observable in aerial images from the 1990s. It is well documented that erosion in the Puyallup River system fluctuates drastically with bed shifts, and thalweg realignment can happen in a single two-year event due to the extreme bedload of the glacially fed river. If we were to armor the banks every time a thalweg shift was observed, the result would be a continuous rip-rap wall detrimental to fish populations and floodplain functions associated with the river.
Affected Environment, Pg. 4-47 - 48	<b>Bank Stabilization Concept</b> – “SVC’s bank stabilization concept sketch was attached to the HCP.”	<b>Inaccurate</b> – This sketch was attached to the HPA. Strike “HCP” from text.
Affected Environment, Pg. 4-47- 48	<b>Erosion of Bank near Outfall</b> – “Since outfall construction was completed in 2019, much of the bank near the outfall structure has been severely eroded. In Figure 4-8, Figure 4-9 and Figure 4-10 (from the Deficiencies Report), note the areas of severe erosion and scalloping just upstream from the outfall in 2019 and 2020 where there is minimal riparian tree	<b>Misleading</b> – It should be noted that the observed erosion is occurring in the exact location the thalweg has directed itself, independent and upstream of the outfall. This type of erosion is typical of the Puyallup River system when the

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	vegetation (Figure 4-8 and Figure 4-9). Also note the subsequent loss of riverbank on both sides of the central line of Ecology blocks when comparing the 2019 aerial photo (Figure 4-9) to the 2022 site photo (Figure 4-10). NHC noted in both reports that five to ten feet of riverbank was eroded away along the outside edge of the outfall.”	thalweg hits the bank at a steep angle and is an expected natural process.
Affected Environment. Pg. 4-47 - 48	<b>Failure of Streambank Stabilization</b> – “Most of the originally installed streambank stabilization efforts (draped coir fabric and willow wands) on the top of bank and down the sloped face of the river bank at the outfall structure are gone (scoured away during annual rainy season flooding), and some of the A-Jacks at the outside edge of the outfall structure have been undermined and are only prevented from falling into the river by their retaining cables.	<b>Inaccurate</b> – The DEIS statement is incorrect. The A-Jacks are intact and coir matting would be expected to be broken down 5 years post installation within the dense matrix of willow roots on the west side of the outfall.
Affected Environment, Pg. 4-47 - 48	<b>Mitigation for Thalweg Impacts</b> – “City engineers and other permitting agencies (including but not limited to WDFW and Ecology) prefer to first consider use of softer or more natural mitigation measures designed to push the river thalweg away from the left bank and outfall structure – such as barbs or constructed log jams, which would be designed to deflect flow away from the bank and mitigate for the increased shear stress at the edge of the structure while also increasing channel complexity, improving habitat and restoring natural riverine functions.”	<b>Inaccurate Characterization</b> – The implication that the Applicant has not followed generally accepted practices and has not consulted with resource agencies is inaccurate. The Applicant has engaged the Puyallup Tribe, Pierce County, and WDFW to discuss these sorts of solutions for the erosion occurring upstream of the outfall.
Affected Environment Pg. 4-47 - 48	<b>Streambank Erosion</b> – “Figure 4–10. Concentrated flow spilling over, eroding, and undercutting vegetated bank. Photo taken on March 15, 2022, NHC.”	<b>Inaccurate</b> – The EIS analyst has misinterpreted the photo. This photo shows dispersed flow spreading out into multiple fingers through a dense vegetated mat, and the associated caption should be revised. The undercutting is occurring as a result of thalweg alignment and natural process and is providing excellent fish habitat value under the woody vegetated cover.
Pg. 4-59, WDFW Hydraulic Project Approval Technical Review, 5th Paragraph	<b>Lack of surveyed benchmarks in the KFIP design drawings.</b>	<b>Inaccurate</b> – The design plans prepared for the construction of the outfall were based on a detailed field survey of the riverbank and outfall area which was tied to horizontal and vertical benchmarks and datums as required by the permitting agencies.

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Pg. 4-60, 4th Par, WDFW Hydraulic Project Approval Technical Review	<b>5 to 10 ft of riverbank was eroded away along the outside edge of the outfall</b>	<b>Inaccurate, incomplete</b> – Based on additional as-built survey, LIDAR survey, and analysis by the Applicant’s design team during the DEIS process, it was determined that there was insignificant erosion of the original riverbank along the outfall.
Pg. 4-63 Other Outfall Design Issues, bottom paragraph and other areas of Chapter 4	<b>Evaluate changing the approved OHWM from 38.5 to elevation +-42.</b>	<b>Inaccurate, Unfounded, and Inappropriate</b> – The OHWM for the area of the outfall was staked in the field, surveyed, and confirmed by Soundview Consultants at an elevation of 38.5. The OHWM was shown on the design plans that were reviewed and accepted or approved by Pierce County, the City of Puyallup, WA Dept. of Ecology, the WA Dept. of Fish and Wildlife, and the Puyallup Tribe and Muckleshoot Tribe. The outfall has been constructed and accepted by the agencies. There was no mention, comment, or question by any agency during the design and approval process of the outfall regarding the OHWM elevation. Suggesting that the OHWM should now be determined by an alternate method after it has been accepted by all of the agencies and has been constructed is unfounded, is not reasonable, and constitutes a collateral attack on prior agency decisions and permitting
Pg. 4-66, Bottom Paragraph	<b>Installation of Outfall Ecology Blocks to Separate City Flows</b> – This paragraph states “installing the line of ecology blocks was apparently in response to a regulatory need to delineate the Puyallup side of the outfall from the KFIP side.”	<b>Inaccurate, Misleading</b> – The EIS should clarify that the requirement for the installation of the Eco blocks within the outfall design was a specific requirement of the City of Puyallup during the City’s review of the outfall plans in order to separate the City flows from the KFIP flows.
Pg. 4-67, 3rd Paragraph, Other Outfall Design Issues	<b>Details on Outfall</b> – “As of this writing, City requests to the Applicant for structure engineering details specific to this outfall that may be used to assess performance standards have not been met.”	<b>Inaccurate Statement</b> – This narrative is inaccurate and unfounded. The Applicant has provided all surveys, plans, documents, and studies that were prepared for the outfall design and has attempted to address and answer every question from the City regarding the outfall design during preparation of the DEIS. The outfall is a unique design

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		concept which cannot refer to similar designs; therefore, some of the information requested simply does not exist.
Shorelines, Pg. 4-75	<b>Floodplain in Shoreline Zone</b> – “Therefore, the entire floodplain on site to the toe slope of the high terrace, including the floodplain wetlands, is within the regulated Shoreline zone (Figure 4-16).”	<b>Inaccurate</b> – The DEIS incorrectly interprets the extent of shoreline jurisdiction. The shoreline jurisdiction does not include all of the on-site floodplain; the shoreline jurisdiction only extends to contiguous floodplains within 200 feet from such floodways. (Refer to PCC Title 20.) A large portion of the onsite floodplain is located more than 200 feet from the edge of the floodway and is not within shoreline jurisdiction.
Pg. 4-84, 4th paragraph and Pg. 4-88, 4th paragraph	<b>Agreement with Tribe to infiltrate building rooftops</b> – These and other pages within this section discuss the proposal to infiltrate four of the building roof tops along the top of the upper bench, the potential for impacts to the wetland hydrology, impacts to flood plain and other infiltration options. The DEIS narrative indicates that the proposed infiltration of the roof top areas does not solve the 6PPD water quality issue, that there does not appear to be any analysis of infiltration feasibility or groundwater monitoring, the system may have an impact on the slope of the bench, may have an impact on flood storage and may impact the hydro period of the wetlands which has not been analyzed. These sections also indicate there was no consideration for other potential areas of the site that may be feasible for infiltration.	<b>Inaccurate, Incomplete</b> – The proposed infiltration/dispersion system for the four buildings was not intended to address the 6PPD water quality issue since it does not handle any paved areas and also there was no requirement by any of the local or state agencies to provide treatment for the potential 6PPD issue. However, the Applicant’s plan design includes enhanced water quality treatment for all pollution generating pavement areas, which will provide mitigation for the 6PPD water quality issue. Earth Solutions Inc (ESI) completed a report to determine the feasibility of the proposed infiltration/dispersion system along the upper bench. The report includes information on the groundwater monitoring in 3 wells in the area of the system, an initial design infiltration rate and recommendations for setbacks of the system from the slope. This report is considered preliminary and additional geotech analysis, groundwater monitoring, and infiltration testing will be completed during the final design phase. Other geotech reports and analysis completed by ESI for the project and also during construction of the storm and sewer trunk lines concluded that due to the soil types and estimated winter high groundwater elevations, on-site infiltration of stormwater in most other areas of the site would not be

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		feasible. The proposed infiltration/dispersion system should not have any impact on flood storage since the project does not propose any fill in the flood plain, there is no fill proposed in the lower bench where the flood plain is located and the proposed infiltration/dispersion system would have no impact on existing flood storage since it is discharging flows to the ground similar to existing conditions.
Pg. 4-109, Section 4.2.6	<b>Significant Unavoidable Impacts</b> – This section lists a number of items which are considered to be Significant Unavoidable Impacts including surface water quality impacts for the 6PPD chemical, erosion, sedimentation, outfall structure failures and impacts to flood plain wetlands.	<b>Inaccurate, Incomplete</b> – Based on the design proposal for handling stormwater runoff, many of these items should not be considered significant unavoidable impacts since there are additional design measures that can and are being provided in the final design of the project’s stormwater management system that will mitigate impacts, including enhanced water quality treatment of pollution generating pavement areas, infiltration of roof top runoff, outfall maintenance measures and additional TESC measures to eliminate discharge of construction stormwater not meeting DOE discharge requirements.
City of Puyallup Regulatory Review, Pg. 4-171	<b>Shoreline Jurisdiction</b> – “Thus, the entire floodplain and the floodplain wetlands in the City directly adjacent to the Project site are in the regulated shoreline jurisdiction and are subject to PSMP regulations.”	<b>Inaccurate</b> – The DEIS incorrectly interprets the extent of shoreline jurisdiction. Only the floodplain area within 200 feet of the edge of the floodway is within shoreline jurisdiction. The floodplain extends more than 200 feet from the edge of the floodway and is not included within shoreline management area.
Riparian Buffer and Floodplain Habitat Areas, Pg. 4-187	<b>Shoreline Jurisdiction</b> – “The regulated Shoreline Jurisdiction includes all areas within 200 feet of the OHWM at the river, plus all associated floodplains within 200-ft floodway (as mapped by Pierce County), and wetlands on the floodplain.”	<b>Inaccurate</b> – The DEIS incorrectly interprets the extent of shoreline jurisdiction. Shoreline jurisdiction extends to “associated wetlands” but does not extend to a wetland just because it is located within a floodplain.
Riparian Buffer and Floodplain Habitat Areas, Pg. 4-187	<b>Shoreline Jurisdiction</b> – “The Project’s regulated Shoreline Jurisdiction extends from the edge of the river to the outer or landward edge of the floodplain boundary.”	<b>Inaccurate</b> – The DEIS incorrectly interprets the extent of shoreline jurisdiction. Shoreline jurisdiction extends to contiguous floodplain areas within 200 feet of the

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		floodway – but the lands more than 200 feet from the floodway are not within shoreline jurisdiction. See PCC 18S.10.030.A.5.
Riparian Buffer and Floodplain Habitat Areas, Pg. 4-187	<b>Shoreline Jurisdiction</b> – “Approximately 47 acres of the study area are designated as FEMA mapped floodplain (Figure 4-38), all of which falls within the Project site Shoreline jurisdiction.”	<b>Inaccurate</b> – See previous comments. The DEIS incorrectly interprets the extent of shoreline jurisdiction
Table 4-19. Regulated Species with Potential Occurrence in the Study Area, Pg. 4-190	<b>Streaked Horned Lark</b> – <i>Eremophila alpestris strigata</i> Federal: Threatened State: Endangered Local: NA There is designated critical habitat for this species.	<b>Inaccurate</b> – Critical habitat for this species is not designated within or near the study area (USFWS critical Habitat Mapping tool).
Impacts to Wetland Habitat, Pg. 4-197	<b>Mitigation Sequencing for Wetland D Impacts</b> – “Currently, there is no mitigation proposal or permit describing how mitigation sequencing was evaluated to avoid all impacts to Wetland D critical areas, or if by not taking certain actions, impacts could be minimized by limiting the degree or magnitude of the action, or how the impacts will be mitigated. Therefore, until there is an approved mitigation plan addressing Wetland D impacts, any fill at Wetland D as currently proposed would result in a net loss of wetland and buffer area during construction phases. This is a significant impact and is in conflict with no net loss policies at a federal, state, and local level.”	<b>Changed Conditions</b> – It should be noted that the delineation approved by the County, USACE, and HEX determined no impacts were expected . A mitigation plan was not required by Pierce County at the time of application and its absence is not, therefore, a deficiency in the wetland report. All impacts to wetlands can and will be mitigated pursuant to applicable standards.
Impacts to Wetland Habitat, Pg. 4-198	<b>Impacts to On-site Wildlife Habitat</b> – “To ensure that impacts to on-site wildlife habitat are insignificant, preservation and expansion of riparian habitat along the river (which is currently significantly less than the standard 150-foot critical area buffer) and preservation...”	<b>Excessive Mitigation</b> – Pierce County’s Shoreline code does not require this area to be revegetated. The proposed floodplain open space will eliminate farming activities and allow successional species to revegetate naturally. This will result in forest conditions within 10 to 20 years and will allow natural selection of appropriate species to colonize. Replanting of this area was not a requirement; it would be extremely costly, time consuming, and would likely only speed along the vegetation succession process by a few years.

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Operations Impacts, Pg. 4-229	<p><b>Mitigation Plan for Wetland D Impacts</b> – “The Project is inconsistent with development regulations including critical areas (Title 18E). Currently, there is no approved mitigation plan addressing Wetland D impacts, and any fill of Wetland D as currently proposed would result in a net loss of wetland and buffer area during construction phases.</p> <p>This is a significant impact and is in conflict with no-net loss policies at federal, state, and local levels.</p> <p>Protection of listed species is required under federal and local law and in relation to current Project site design.”</p>	<p><b>Changed Conditions</b> – Because Wetland D was located entirely offsite during the permit application and approval process, no impacts to this wetland were identified and no mitigation was required. If Wetland D did in fact extend onsite, and impacts are required, appropriate on-site compensatory wetland mitigation will be provided. Previously, the Applicant voluntarily agreed to mitigate for Wetland D fill as if it was a Category III wetland. There is sufficient area in the northern portion of the site within the floodplain to accommodate such mitigation. (New Proposed Mitigation Measure)</p>
Pg. 4-381, second paragraph, Stormwater	<p><b>New trunkline</b> – “The new 42-inch diameter storm trunk line may not have capacity for the entire Project site runoff.”</p>	<p><b>Unsupported/Inaccurate</b> – The DEIS statement is not supported by analysis or factual information. The 42-inch diameter stormwater trunk line was sized for the entire project basin, including the 38 acres of roof top area of the proposal that are designed to infiltrate and disperse to the lower bench, as well as an off-site area that has been developed such that its stormwater won’t contribute to the trunkline. The sizing of the 42-inch trunk line was based on basin modeling and sizing information provided by the City’s consultant, Brown and Caldwell. Assuming an overflow connection from the infiltration system to the project’s conveyance system was provided, the 42-inch storm trunk line would have capacity for the additional storm flows.</p>
Table 4-22. Project Consistency with Applicable Plans and Regulations. – Environmental Element, Pg. 4-231	<p><b>ENV-8.7</b> – Encourage landowners to maintain and enhance habitat areas. There is no current plan from the Project that identifies maintenance or enhancement of habitat areas.</p>	<p><b>Inaccurate Interpretation of Policy and the Proposal</b> – This is a general policy “encouraging” action and not a regulatory requirement. The Applicant’s proposal includes cessation of all agricultural activities adjacent to the Puyallup River, which will allow the land to go fallow and revegetate naturally.</p>
Table 4-22. Project Consistency with	<p><b>Inconsistent with County Shoreline Master Program</b> – The existing shoreline ecological functions of the portion of the Puyallup River</p>	<p><b>Incomplete/Unsupported</b> – Specific shoreline jurisdiction impairments are not identified and mitigation is not</p>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
Applicable Plans and Regulations, Pg. 4-239	shoreline jurisdiction within which the Project is located are currently impaired. The Project, as proposed, does not include restoration of shoreline ecological functions. Therefore, the Project would interfere with implementation of this policy.	warranted. In addition, the statement does not acknowledge that allowing the floodplain to rewild would likely improve shoreline ecological functions relative to existing and continuing high intensity agricultural use. This change in land use and plant community over time would be a significant restoration of ecological functions.
Applicable Plans and Regulations, Pg. 4-239	<b>Inconsistent with County Shoreline Master Program</b> – There is only one other existing warehouse adjacent to the site (Viking warehouse structure, 0.3 mile from the Puyallup River); there are no other similar uses adjacent to the site. The Project’s need for newly expanded industrial areas adjacent to and within the Puyallup River shoreline has not been established.	<b>Oversimplified Analysis</b> – Refer to the comments in the Land Use portion of this table. This comment on land use is a gross over-simplification of the land use pattern. The limited number of warehouses reflects City and County zoning; if that is a criticism, it should be directed to the City and the County and is inappropriate in the EIS. It is not an impact of the proposal and not an inconsistency with adopted policy and regulations.
<b>4.5 Land Use</b>		
General	<b>Land Use Analysis</b>	<b>Incomplete and Irregular EIS Approach</b> – The EIS contains no Land Use section per se and no coherent analysis of land use impacts, which is a key element of the proposal and the City’s analysis. The discussion is disjointed, selective, and repetitive and focused on misguided interpretations of individual policies. The EIS lacks a coherent and comprehensive discussion of overall land use patterns, transitions of uses, relationship to adjacent uses, and indirect impacts, other than in the plans and policies discussion, which is disjointed and has a micro focus on the site and selected immediately adjacent uses, i.e., rural, agricultural, and parks. The DEIS does not relate the proposed land use to the larger pattern of urban land uses in the city, including an adjacent warehouse .
Pg. 4-229	<b>Construction Impacts, Significant Impact with Mitigation</b> – This significant impact of construction is based primarily on elimination of prime farmland and impacts on other elements of the environment.	<b>Incomplete, Inconsistent</b> – The EIS preparer avoids using the standard EIS terminology for significant impacts that cannot be avoided. The main criteria that is used to determine that the site is prime farmland is soil properties.

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		<p>According to the USDA, other considerations to determine prime farmland should include: land use, frequency of flooding, irrigation, water table, and wind erodibility. The DEIS should also disclose that the property does meet GMA criteria for agricultural resource lands of long-term commercial significance.</p> <p>The discussion is inconsistent and identifies land use impacts of Alternatives 2 and 3 during construction as “mitigated significant impacts” even though they are potentially converting prime farmland, the same as the proposal.</p>
General	<p><b>Overview of Problems with Relationship to Plans and Policies Analysis</b></p>	<p><b>Overview of Comments</b></p> <ul style="list-style-type: none"> <li>• Absence of even-handed, objective analysis; appearance of pervasive bias; twists policy interpretations to find or invent inconsistencies.</li> <li>• Conclusory statements of inconsistency without evaluation or rationale.</li> <li>• Improper application of general policies that are intended to guide comprehensive planning, to review a site-specific proposal.</li> <li>• Conflates policy inconsistencies with land use conflicts.</li> <li>• Collateral attack on appropriateness of County Comp Plan and Community Plan policies and land use designations that permit urban/industrial use on the project site. Very narrow reading/misunderstanding of Alderton-McMillin (AM) Community Plan – sees only the rural/agricultural elements and ignores how plan uses EC designation to achieve some balance with urban/economic uses.</li> <li>• No context or consideration of broader city-wide land use pattern.</li> </ul>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
		<ul style="list-style-type: none"> <li>The entire analysis is hypothetical. Although it is stated (once or twice) that City plans and regulations do not apply, the land use analysis is presented as if the City is in control of permitting; County plans, land use designations, and zoning regulations are subservient afterthoughts.</li> </ul>
Relationship to Plans and Policies, General	<b>Relationship to Plans and Policies Analysis</b>	<p><b>Irregular Format and Approach</b> – Conclusions about the project’s consistency with primarily City plans and policies is repeated throughout this section (and the entire document). As identified below, statements of inconsistency are based on an extremely narrow, shallow, and results-driven interpretation of many policies. Inconsistencies are based on many general policies that are intended to guide county and city planning and are not applicable to an individual site-specific project. Analysis is not objective and creates an impression of pre-judgment. The undue emphasis on City of Puyallup’s plans and policies lacks balance, given that site is located in unincorporated Pierce County. Although the site is within the City’s UGA/potential annexation area, there is no certainty that annexation will actually occur. Plans and policies analyses are standard in EISs but should not supersede other impact analyses as done here.</p>
Relationship to Plans and Policies, General	<b>Purpose of an EIS (WAC 197-11-400)</b>	<p><b>Lack of Impartial Analysis</b> – The SEPA Rules require that an EIS provide impartial discussion of significant environmental impacts (WAC 197-11-400); the City’s DEIS fails this requirement. Statements about the project’s inconsistency with City plans and policies, repeated throughout the document, contain extremely narrow, shallow, and results-driven policy interpretations and give an impression of pre-judgment. Multiple examples of pervasive bias are provided in subsequent sections of these comments.</p>

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Pg. 4-232, 4-233	<p><b>Relationship to Plans and Policies</b> – The EIS preparer identifies several land use plan goals, objectives, and policies with which they consider the project to be inconsistent. Examples include:</p> <p><b>PC Comp Plan (PCCP) Goal ENV-8</b> – Protect the natural environment.</p> <p><b>PC SMP Policy B-3</b> – “Encourage mixed use...”</p>	<p><b>Misapplied</b> – The conflicts and inconsistencies are misapplied for the reasons explained below. The 20 pages of “analysis” (sic) could be simply explained and summarized in two sentences: The proposal is generally consistent with Pierce County land use and zoning designations. The City’s contrary future land use and zoning designations do not apply to the proposal at this time.</p> <p><b>Overbroad Conclusion, Required Mitigation Ignored</b> – This policy is general and intended to guide planning, not to evaluate a site-specific proposal. Asserted inconsistency with this broad policy assumes that required and proposed mitigation will be ineffective in protecting the environment. There is no basis for assuming the failure of mitigation if the project is following legal requirements. (Additional examples where the EIS assumes that mitigation will be ineffective include its discussion of: <b>PCCP Policy ENV-8.4, PCCP Goal ENV-9, PCCP Goal ENV-15, PCCP Policy LU 47.5, AM Policy D-1.1, AM Policy D-1.2, PC Shoreline Policy B-1, PCC SMP 8S.20.040B-3, 6, 7, PCC SMP 18S.30.030B.</b>)</p> <p><b>Misinterpretation of Policy</b> – Non-sensical conclusions, i.e., that a site-specific proposal for a single use is inconsistent with a policy that generally “encourages” a mix of uses within the jurisdiction overall. The policy does not require a mix of uses on any specific site; any single-use proposal could be concluded to be inconsistent with this policy. Similarly, it is illogical to conclude that a project that provides open space is not consistent with a policy that encourages open space, simply because the analyst may prefer an alternative location or design of the open space.</p>

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	<p><b>Note:</b> The following “PCCP” policies are from the Pierce County Countywide Planning Policies.</p> <p><b>PCCP Goal LU-4.2-6</b> – Designate industrial areas in the County and coordinate with the City.</p> <p><b>PCCP Policies LU 44/46, Generally</b></p>	<p><b>Misapplication of General Policies in a Project Context</b> – In general, applying the Countywide Planning Policies (CPPs) to an individual, site-specific development proposal is inappropriate. In the GMA scheme, CPPs are intended solely to establish a regional planning framework to guide city and county comprehensive planning and should not be used to evaluate individual projects (refer to RCW 36.70A.210(1)). They appear to be included, and have been selected, to communicate the City’s historic and ongoing disagreement with the County about a number of land use issues and to create an appearance of project-specific land use/policy conflicts.</p> <p><b>Collateral Attacks on County Land Use Designations</b> – This policy pertains generally to the County’s designation of lands as industrial; the DEIS discussion exemplifies both the City’s disagreement with the County and how the DEIS inappropriately uses analysis of the project to express opinions about collateral matters. City displeasure about lack of coordination on regional planning should not intrude into the EIS. CPPs are not relevant to and shouldn’t be applied directly to a site-specific proposal. (Other examples of collateral attacks on the County include: <b>PCCP Policy LU-44.11, PC Shoreline Policy B-4.</b>)</p> <p><b>Misapplication of General Policies in a Project Context</b> – These policies are intended to direct city and county comprehensive planning and are not intended to be applied to specific projects. (Other examples of misapplication of general policies to the project include: <b>PCCP Goals PR-17 and PR-19, PCCP Policy PR-1.3.</b>)</p>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
	<p>PCCP Policy LU-44.5 – Near historical employment generating operations.</p>	<p><b>Inaccurate Conclusion/Collateral Attack</b> – The DEIS analysis is arguing that Employment Center (EC) zoning is inconsistent with County Comprehensive Plan policies but ignores and withholds that the proposal is consistent with applicable zoning. It is similarly arguing that AM Community Plan industrial designations are inconsistent with the County Comprehensive Plan and, therefore, there is inconsistency between these County plans. This discussion is not relevant to the proposal and is inaccurate.</p> <p><b>Extreme Misinterpretation of Community Plan</b> – This general planning policy is not applicable to an individual site that is designated/zoned for industrial use; the City’s disagreement with the EC zoning applied by the AM Community Plan in 2015 (see pg. A–26 of the AM Plan) is a collateral issue. It is noted that the EC zoning has been in effect since at least 2005. The inconsistency asserted in the analysis is due to the AM Plan’s general desire to retain the rural character in the community but to also allow limited non-rural economic uses. The EC designation/zoning applies to 2 sites in the planning area; while EC is not a “rural” designation, it is not inherently incompatible in character. (Other examples of misinterpretation of the community plan include: <b>AM Goal LU-1, AM Policy PLU-1.2, AM Goal D-1.</b>) It’s true that the Community Plan only designates 2 sites as Industrial, but that is intentional and intended to encourage some economic development. Repeated statements throughout the policy analysis about introducing new facilities into a rural/agricultural environment reflect a misreading of County AM Community Plan intent and policy and appears to be results–driven.</p>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
	<p><b>PCCP Policy LU-44.6</b> – Locate development on properties that are not constrained by significant critical areas such as wetlands, steep slopes, or other environmental factors limiting development potential.</p> <p><b>PCCP Policy LU-47.11</b>– Provide sites with a variety of parcel sizes to accommodate both large and small businesses.</p> <p><b>PCCP Goal PR-5, Policy 5.6</b> – Develop facilities that exemplify sustainable practices, connect to surrounding neighborhoods, universally accessible, safe, and cost effective to maintain. Incorporate scenic viewpoints.</p> <p><b>PCCP Policy LU-47.9</b> – Encourage master planning for industrial areas.</p>	<p><b>Inaccurate</b> – This policy articulates a general principle and does not suggest that sites with some environmental constraints are not appropriate for development. The part of the site on which development is proposed is not constrained by critical areas.</p> <p><b>Inapplicable Policy</b> – This is a general policy that is intended to guide overall planning for development in the City and County. It is inappropriate to apply it to a single site. The EIS discussion exemplifies the EIS preparer’s tendency to reach far fetched and predetermined conclusions of inconsistency.</p> <p><b>Inaccurate Conclusion</b> – The proposed project includes a public trail that connects to the surrounding neighborhood. Adjustments to the location of the trail and scenic viewpoints along the trail could be included in specific development plans for the trail. This County policy does not pertain to viewpoints in the City (e.g., from Van Lierop Park).</p> <p><b>No Accounting for Further Development of Site Plan and Implementation of Mitigation</b> – Many of the positive features of the proposed master plan (e.g., open space, trail, etc.) aren’t taken into account in the application of this policy. The site plan will be developed further during the permit process and appropriate mitigation will be applied. (Other examples of not accounting for further development of the site plan include <b>PCCP Policy PR 5.7, PCCP Policy PR-10, PCCP Goal PR-19, AM Policies D-1.2 and D-1.3.</b>)</p> <p><b>Irregular EIS Format and Approach</b> – The amount and level of analysis of City of Puyallup plans, policies, and land use</p>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
	<p><b>City of Puyallup Goals and Policies</b> – An extensive list of City goals and policies are analyzed in the DEIS and the project is found to be inconsistent with most.</p> <p><b>Goal LU-6, Policy LU-8, Goal LU-11</b> – Goals and policies related to the city’s UGA and annexation within this area; cooperation between jurisdiction to plan for growth; and preservation of rural buffers.</p> <p><b>LU-11.1/11.2</b> – Encourage preserving residential and rural buffer areas and provide for commensurately lower LOSs.</p> <p><b>LU-22.3</b> – Relates to the use of vegetative or landscape berms between industrial and single-family residential areas.</p>	<p>designations for a proposal located in Pierce County is extreme, notwithstanding that the property is located in the City’s potential annexation area. Puyallup land use goals and policies do not and will not apply unless and until the project is annexed to the City, or until a Pre-Annexation agreement is executed. The fact that coordinated planning between the jurisdictions is mandated by policy but has not occurred explains most asserted conflicts between City and County policy regimes, but that is not the fault of the Applicant. A project application is not the appropriate forum for that debate. The bottom line is that City policies and land use designations are not applicable and do not warrant the extreme emphasis and dominant position they receive in the DEIS. The City’s role as SEPA lead agency does not also mean its policies and zoning should dominate the DEIS discussion.</p> <p><b>Misapplication of Goals/Policy</b> – The goal is general and directed to the City; not applicable to an isolated site-specific proposal. These are residual disagreements over past County decisions in the guise of project-specific policy conflicts.</p> <p>Only a portion of the site is designated Rural Buffer. Note that City land use and zoning designate auto-oriented commercial uses next to a park and Rural Buffer.</p> <p><b>Inapplicable Policy</b> – These policies are about buffers not land use.</p>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
	<p><b>LU-22.5</b> – Directs that agricultural lands that are converted to industrial use phased development generate a high number of employees and provide visual amenities.</p> <p><b>LU-24</b> – “Focus most growth...in RECs”</p> <p><b>CC-1.1, CC-1.3</b> – Encourage maintaining the identity and character of established residential neighborhoods and creating sensitive interfaces between residential and non-residential areas.</p> <p><b>CC-1.6</b> – Promotes industrial projects which complement and contribute positively to the community character (e.g., through site design, buffering, and pedestrian experience.)</p> <p><b>Goal CC-2/Policy CC-2.2</b> – Characterizes Puyallup as a built environment with high quality urban design and mix of compatible uses</p>	<p><b>Strained Interpretation</b> – The discussion avoids acknowledging that jobs generated by the project would meet the intent of the policy. Phasing is the actual subject of the policy but is not discussed at all.</p> <p><b>Flawed Interpretation</b> – This interpretation is illogical and unprofessional. By any even-handed, objective reading of this policy, it should be obvious that locating “some” growth outside the RECs is not inconsistent with the policy and does not result in locating “most” growth outside RECs. The City’s land use designations and zoning also allow “some” growth outside its REC, including zoned parcels adjacent to the KFIP site.</p> <p><b>Flawed Interpretation</b> – The discussion reflects the writer’s opinion about the sufficiency of proposed landscaping/site design to maintain the character of established residential neighborhoods; it is an opinion about design and not a conflict in policy. Adjacent “neighborhoods” on 2 sides of the site are commercial, not residential. Accurate analysis requires a fuller land use discussion and clearer distinction between policy, land use compatibility, and design. Similarly, CC-1.6 reflects an opinion about the adequacy of buffering measures without any evaluation or rationale of how/why there is an asserted conflict.</p> <p><b>Unsupported Opinion</b> – Over-generalized, debatable conclusion. Ignores the obvious fact that the site and the park are both adjacent to a high volume roadway and a commercial corridor.</p> <p><b>Misinterpretation of Policy, Unsupported Opinion</b> –</p>

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
	<p><b>CC-2.1</b> – Adopt urban design principles that recognize the unique qualities of different types of uses.</p> <p><b>Goal CC-3</b> – Natural landforms, vegetation, and scenic areas contribute to the City’s identity.</p> <p><b>C-3.2</b> – To the greatest extent possible, preserve significant trees and mature vegetation</p> <p><b>CC-4.5</b> – Allow the use of shared driveways.</p>	<p>This is a general goal that addresses the overall, city-wide land use pattern and does not apply to every individual site. It is inappropriate to conclude that any single use project – whether industrial or residential – is inconsistent with a goal that generally encourages mixed use development. In the absence of a design plan, the discussion nevertheless concludes that project design is not of high quality based on the type of project.</p> <p><b>Misinterpretation of Policy</b> – The policy speaks to urban design but the discussion addresses land use, not design.</p> <p><b>Misinterpretation of Policy</b> – The proposal would not affect natural landforms, vegetation, or scenic areas, but the EIS concludes it is inconsistent with the policy because of interference with the “viewshed” of Mt. Rainier from the north. Neither the City’s Comp Plan nor the City code identify or regulate specific views, viewsheds, view corridors or scenic areas.</p> <p><b>Inaccurate Conclusion</b> – The proposal is consistent with this policy and the discussion should simply state that without equivocating.</p> <p><b>Inaccurate Conclusion</b> – Question how practical or feasible a shared driveway is for an industrial project involving heavy trucks transport.</p>
<b>4.6 Aesthetics</b>		
Pg. 4-254	<b>Methodology</b>	<p><b>Numerous Issues/Questions:</b> Key Observation Points (KOPs) are based only on landscape and exclude the built environment; viewer groups are more expansive than identified and the analysis conflates viewer groups and viewer sensitivity; does not explicitly consider viewer</p>

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		<p>sensitivity; KOP photos do not indicate boundaries of the site; Google photos dated 2021 so there may have been changes; no description of how “prime view corridor” was defined in Figure 4-51, e.g., is it based on 55mm lens/average width of human vision or is it skewed?</p> <p><b>Misreading/Inaccurate Application</b> – The cited policy is for views from Pierce County’s AM Community Plan area; it is focused on views of surrounding ridgelines, vegetation open spaces, farm lands and water bodies from the valley floor. Only Viewpoint KOP 2 is located in the AM Community Plan area. The EIS preparers don’t cite any City view protection policies but apply those from the AM Community Plan (e.g., views of ridgetops and Mt. Rainier) to views within the City (e.g., from Viewpoints KOP 1, KOP 3, KOP 4, and KOP 5). County Comp Plan and code do not contain view protection policies. Based on review of City and County policies and development regulations, and AM Community Plan, while views of Mt. Rainier are stated to be “abundant”, none of the identified views, viewsheds, or corridors, including of Mt. Rainier, are defined, and none are specifically protected. The Puyallup Code’s site plan design principles for manufacturing and commercial zones does not mention view protection; view protection is similarly not mentioned in the code’s design review standards or zone transition standards.</p>
General	<b>Post-development View Simulations</b> – None provided.	<b>Omission</b> – No post development views are included in the view analysis, yet conclusions are drawn about significant view impacts with the project. Due to absence of simulation from KOP 5, the reader is unable to identify extent of obstruction of Mt. Rainier view.
Pg. 4-265	<b>Mitigation AES-2</b> – Includes redesign of the project to include buffers/separators based on impacts to views of Mt. Rainier from Van	<b>Inconsistent</b> – No specific PCC citation is provided for this mitigation, rather general plans and policies are noted.

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	Lierop Park and separation from the adjacent residential uses to the southeast to reduce “significant impacts.” The screening includes a 30-foot Level 3 (full evergreen sight obscuring) buffer area around all areas of the site abutting public park space (e.g., Van Lierop Park).	Implementation of this mitigation could block views of Mt. Rainier from Van Lierop Park, which the City is seeking to protect.  Cites Pierce Co Policy LU 47.5 to support inconsistency. Policy encourages development to “consider visibility” but does not say protect or preserve views or totally avoid any obstruction. Proposed mitigation is not reasonable.
Pg. 4-265	<b>Post–construction Photometric Analysis Required</b> – To ensure energy efficient and non-spill lighting.	<b>Irregular EIS Approach, Unnecessary</b> – Appropriate lighting could be selected and approved through the permitting process and a post-construction photometric analysis isn’t necessary.
<b>4.7 Recreation</b>		
Pgs. 4-275 and 4-279	<b>Figure 4-57 and Figure 4-58</b> – Proposed Action and Alt. 1 parks and recreation figures	<b>Unclear</b> – For clarity, the trails should be labelled “City’s Proposed Trail” (green) and “Applicant’s Proposed Trail” (yellow).
General	<b>Demand for Park and Recreational Facilities</b>	<b>Omitted</b> – Note that as an industrial development, the project would not create much if any demand for recreational facilities.
Pg. 4-277	<b>Mitigation REC-1</b> – Change site plan to eliminate Van Lierop Park prime view corridor obstructions. Prepare visual simulations to show that no obstructions would occur	<b>Potentially Unnecessary, Arbitrary</b> – As mentioned in comments on Aesthetics above, the City has not adopted policies or regulations to protect views or view corridors and has not objectively defined specific view corridors. Similarly, no Pierce County policies or regulations protect views or specific viewpoints or viewsheds. The comment that mitigation must show that “no obstructions would occur” is, therefore, ad hoc, not based on any adopted policy or regulation and may not be reasonable. Why could there not be some partial obstruction of the view corridor? Also, because view simulations are not included in the EIS analysis and given the distance of proposed Building F from KOP 5, it is unclear if this building would block views of Mt. Rainier from the park viewpoint. Fig. 4-50 shows existing

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
		buildings in the distance that do not block views of the mountain. Building F also may not block views from the viewpoint. The EIS should include a post-development visual simulations to show that Building F would/wouldn't block views and the extent of any obstruction. In addition, as noted previously, the KF proposal was vested before the park was planned, so the City was aware of the proposed building layout before creating the view corridor.
Pg. 4-278	<b>Waterfront Access</b> – The proposed trail should be located along the Puyallup River.	<b>Inconsistent</b> – It is acknowledged that the proposed trail is not located along the Puyallup River. The trail was originally located near the river; however, the Applicant received direction from the County to locate the trail away from the river to protect critical areas. As a result, the proposed trail was moved closer to the proposed buildings.
Pg. 4-281	<b>Mitigation Measure REC-5</b> – Provide an east/west trail connection through the site to Van Lierop Park. The Applicant and County should seek input from the City of Puyallup Parks Department and Development and Permitting Services Department as the site plan is revised to meet this mitigation measure.	<b>Unnecessary</b> – Revising the site plan to provide an east/west trail through the site is not necessary, as this connection could occur offsite to the south of the site, along 80 <sup>th</sup> Ave. SE.
<b>4.9 Transportation</b>		
General	<b>Overview of Problems with Transportation Information and Analysis</b>	<b>Overview of Comments</b> <ul style="list-style-type: none"> <li>• Parts of impact evaluation methodology are unclear and deviate from City LOS policies.</li> <li>• Mitigation analysis is incomplete and conclusions are not explained.</li> <li>• Specific improvements and pro rata shares are not identified.</li> </ul>
Pg. 4-308-310 and 4-315-316 and 4-324	<b>Application of Comp Plan Policies section 4.9.4 Traffic Simulation Results</b>	<b>Unclear</b> - This section analyzes the City's transportation LOS policies, including volume-to-capacity (V/C) ratio and Vehicular LOS. It includes two additional traffic analysis "measures of effectiveness" (MOEs) – <u>queue lengths</u> at intersections and <u>travel time</u> – that relate to but are not

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
		City-adopted LOS policies_It is unclear how the MOEs were used to identify impacts and potential mitigation.
Pg. 4-340 - 341	<b>Application of County Comp Plan Policies</b> – Additional Mitigation section.	<b>Incomplete Analysis</b> – The discussion concludes that large corridor-wide improvements would be needed due to roadway corridors exceeding capacity for all Scenarios, including No Action, and because all intersection LOS and V/C ratios cannot be mitigated within target values. Four improvements are identified, but it is not clear what specific actions would be necessary to mitigate impacts of any of the Scenarios or whether any of the improvements would provide adequate mitigation to meet City standards for LOS and V/C ratios for any of the action Scenarios. No cost estimates are provided for the specific corridor-wide improvements, and a proportionate cost is not estimated for any of the action Scenarios, should they be required to contribute.
Page 4-344	<b>Section 4.9.6 Pavement Analysis Results</b> – Roadway corridor improvements include pavement upgrades to meet current road standards.	<b>Unclear</b> – It is unclear if the corridor roadway improvements along Shaw Road, E Main Street, SR 162 and E Pioneer Avenue would include pavement improvements. This should be considered in any future mitigation for the Action scenarios so there is no “double counting” of potential mitigation for pavement impacts and contribution toward potential roadway corridor improvements.
Pg. 4-345 - 348	<b>Section 4.9.7 Mitigation Measures and Table 4-56</b>	<b>Mitigation Analysis Not Complete</b> – The mitigation analysis identifies a “proportional factor” to determine a potential fee-in-lieu cost as mitigation. However, specific improvements to mitigate impacts of the action Scenarios are not identified for the 4 road segments in Table 4-56; similarly, no nexus is established to determine whether improvements are needed to mitigate project actions on the 4 road segments.

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
<b>4.10 Health and Safety</b>		
Pg. 4-458	<b>Proposed Action Allowed Uses</b> – Possible recycling collection and processing facilities, and salvage yards/vehicle storage.	<b>Inaccurate and Hypothetical</b> – Same comment as in Fact Sheet that these are not proposed uses.
<b>4.11 Public Services</b>		
Pg. 4-378	<b>Domestic Water</b> – The highest established uses of domestic water are assumed to be a power plant, large building construction, or airport.	<b>Inaccurate, Hypothetical and Overblown</b> – The power plant and airport cited aren't proposed to be developed onsite. It is unclear what is meant by "large building construction" in this context.
Pg. 4-380	<b>Sewer</b> – Demand analysis.	<b>Omitted</b> – A specific analysis of the project's impacts on sewer service has been deferred to the time of permit application. Therefore, the capacity of the sewer system to handle proposed development, and the potential for the project to need to contribute to improvements, isn't known at this time.
Pg. 4-381	<b>Stormwater</b> – This section indicates that little is known about the proposed stormwater vaults and that they're assumed to be expensive to install and require frequent and expensive maintenance.	<b>Inaccurate</b> – Vaults are not an uncommon way to detain stormwater. It would be reasonable to assume, or to at least acknowledge the possibility, that a maintenance program would be approved and overseen by Pierce County to ensure their performance.
Pg. 4-381	<b>Mitigation Measure PS-4</b> – The City indicates that two additional years of groundwater monitoring is required for adequate design of the stormwater facility.	<b>Unclear</b> – The Pierce County stormwater manual requires only one year of groundwater monitoring to confirm winter high water elevations for detention and infiltration design.
<b>4.12 Cultural Resources</b>		
Pg. 4-402	<b>NRHP, WHR, PCRHP, and/or the PRHP Listing</b> – Residence and out buildings at 7525 134 <sup>th</sup> Ave. E are recommended eligible for listing in the NRHP, WHR, PCRHP, and/or the PRHP.	<b>Inaccurate</b> – The building and outbuildings recommended for listing were removed from the site in February 2023, in accordance with a permit issued by the County.
<b>4.13 Noise</b>		
Pg. 4-406	<b>Table 4-65</b> – PCCP Policies ENV-13.3 and 13.3.1 relate to Joint Base Lewis McChord.	<b>Inaccurate</b> – These noise policies relate to JBLM and have no bearing on the project.
Pg. 4-409	<b>EDNA Classifications</b> – The assessment of noise impacts of the potential project considers the site to be a park-like land use (Class A EDNA) with a maximum allowable noise level adjacent to a residential (Class A EDNA) of 55 dBA.	<b>Inaccurate</b> – Per PCC 8.76.050, the site should be a Class C EDNA (agricultural use) not Class A EDNA (park-like land use). This raises the allowed noise at the receiving property to 60 dBA.

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
Pg. 4-411	<b>Noise Analysis Method</b> – The noise analysis used the FHWA Handbook for the construction noise analysis; it’s unclear if the handbook was used for the operational analysis as well.	<b>Potential Flaw</b> – The method for noise analysis in the FHWA Handbook was developed for highway construction projects not industrial or other types of land uses. No highway construction is proposed as part of this project. Further Pierce County has adopted standards and methods and those must be the basis for review of the proposal. Finally, this method is sometimes used as a proxy method for discerning traffic noise increases in SEPA analyses, it may be inappropriate in a project context with no federal nexus. Mitigation using sound walls based on the FHWA Handbook may therefore be unreasonable.
Pg. 4-415	<b>Mitigation Measure N-3</b> – Noise wall.	<b>Unclear</b> – It is unclear how the noise wall mitigation is derived and whether it’s necessary given the Class C EDNA of the site. Similar to the buffer requirements noted in Aesthetics, a 12-ft. noise wall could block the view corridor from Van Lierop Park.
<b>DEIS Appendix D</b>		
Wetland D Report, Pg. 5	“Results indicated that Wetland D was larger than depicted in the SVC report and large enough to be regulated by Pierce County.”	<b>Erroneous</b> – The farm pond that was present during the original 2015 delineation by SVC, was excavated deep enough to break through the poorly drained silty soil layer and reach a coarser, well-drained sandy layer below, allowing the area to drain effectively. The farm pond was filled after the 2015 delineation was completed, effectively removing the outlet from the farm field and resulting in changed hydrologic conditions. SVC installed groundwater monitoring wells in 2018; data from these wells indicates that the eastern portion of the farm field that is subject to inundation on arials does continue to drain relatively quickly. Therefore, the groundwater monitoring data does not align with the onsite wetland boundary presented by SCJ and is consistent with SVCs delineation.
Wetland D Report, Pg. 8	“The onsite portion of Wetland D is plowed in the late spring and planted in summer after the early season hydrology has infiltrated, evaporated or	<b>Erroneous</b> – The offsite portion of the wetland is noted to have “scant” vegetation even during the summer due to

DEIS SECTION / PG.	EXCERPT FROM OR SUMMARY OF DEIS	COMMENT
	is lost to transpiration from spring and early summer plant growth. Offsite areas to the east are managed as a pasture for livestock. The lowest central area of the wetland has scant vegetation even in mid to late summer, due to long-duration inundation in the Spring, which limits early season plant growth.	extensive ponding; however, the data forms they provide, which appear to have been collected in that area, show excessive vegetation cover with over 100% cover of the shrubs and 200% cover of herbaceous species.
Wetland D Report, Pg. 9	"Figure 6. Hydric soils at the edge of Wetland D."	<b>Incomplete</b> – This photo depicts an exceedingly small sample of soil which appears to have soil colors and redox features that would meet depleted matrix indicators. However, the only way to meet this indicator is to ensure that these colors are present within a certain depth of the soil surface and extending a sufficient thickness. This photo does not prove that. A photo of the entire soil profile with a measuring stick/tape is needed to show such findings.
Wetland D Report, Pg. 10	More than 80% of the wetland plants were Facultative or Facultative Wet species (Table 3). The two Facultative Upland species, oxeye daisy and red clover, were mostly on a small berm along the fence line.	<b>Irregular Approach to Analysis</b> – This percentage appears to refer to the 13 FAC/FACW plants out of the 15 species with wetland indicators – this is not a real measurement used for delineation because it fails to account for the aerial cover of each species and which species are dominant. This is why SVC utilizes the dominance test or prevalence index, when warranted.

**Environmental Coalition**

**Commenter:** Kirkland, Kirk

**Source:** Email

**Date:** 3/15/2024

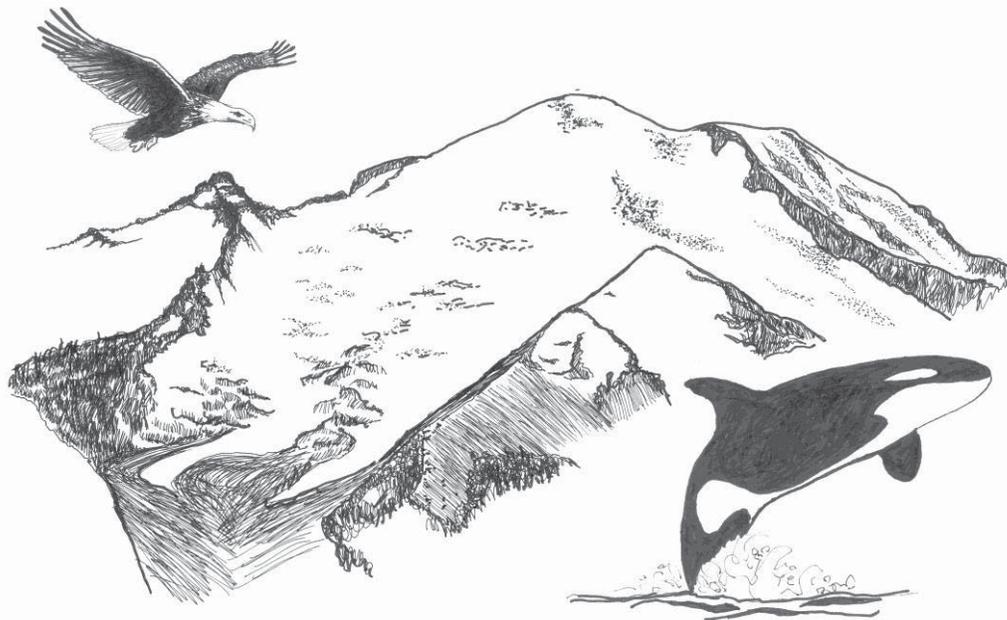
**Comment:**

Hello Chris.

Please find comment letter from the Environmental Coalition on the proposed conversion of Knutson Farm to a Warehouse District.

Can you tell me who will decide on the alternatives and what is the next step in the process of issuing a conditional use permit?

Thank you



## Environmental Coalition

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3114 N. Alder Street, Tacoma, WA 98407

**To: Chris Beale, Project Manager**  
**Date: March 15, 2024**

**Subject Conversion of Knutson Farm & Countywide Planning Policies**

City of Puyallup  
333 South Meridian  
Puyallup, WA 98371

[cbeale@puyallupwa.gov](mailto:cbeale@puyallupwa.gov)

Chris Beale:

Thank you for the opportunity to comment on the Environmental Impact Statement for a Warehousing, Distribution, and Freight Movement facility of up to 2.6 million square feet of building area on the approximate 162-acre Knutson Farm property located within unincorporated Pierce County.

The Environmental Coalition is an environmental group that advocates for environmental protections in growth in Pierce County. One of our primary goals is the preservation of working farms and working forests. The proposed Knutson farm conversion area under consideration for development has just been plowed and the fields are ready to provide another crop this year as they have for decades.

The Environmental Coalition recommends that the City of Puyallup select the NO ACTION alternative in the EIS. Our concerns about this projects conversion of land is listed below

## 1 Farmland Protection:

State law requires protection of farmland. RCW 36.70A.060 directs county to adopt regulations to protect agriculture lands. RCW.36.70A.177 Ag lands of long-term commercial significance discourages non agriculture activities on agriculture land. The conversion of a farm land that has grown daffodils and other farm products since the 1950s is in violation of these state statues, but is also outrageous.

Pierce County [strategic plan for agriculture](#) policies on agriculture land reported in 2006 – “The most intense mix of agriculture and development is taking place in the Puyallup Valley where there has been a steady conversion of open land to development uses. The accompanying table shows the location of Pierce County’s remaining agricultural land.

Pierce County Agricultural Land		
Agricultural Area	Ag Land (acres)	% Inc./UGA <sup>1</sup>
Anderson Island/Nisqually	1,787	0%
Bonney Lake/Buckley	7,290	2.5%
Central County	7,318	5.7%
Peninsula	5,155	0%
Puyallup Valley	6,606	<b>24.7%</b>
Roy/Eatonville	19,723	0.8%
<b>Total</b>	<b>47,880</b>	<b>5.0%</b>

Source: Pierce County Assessor’s Office (2005)

<sup>1</sup> Percentage of agricultural land that is located within an incorporated area or urban growth boundary.

The strategic plans calls for the “Preservation of farmland and nurturing a prosperous agriculture sector will require a sustained policy commitment and well conceived, concerted action.

We urge the city to cooperate with Pierce County to designate this area as farmland which the county failed to do in 2015 when Agriculture lands of significance were redesignate in Puyallup Valley.. Converting this land to a commercial warehouse district of 2.6 million square feet on 162 acres farm property is in violation of Countywide Planning Policies.

We recommend the No Action Alternative of the EIS.

## 2 Future Annexation of this area will not help the city meet growth management’s employment targets:

Pierce [County’s Buildable Lands report](#) (2022) states, “land uses which are concentrated in office environments, or retail and service uses centered in Urban Centers have much higher employment densities that land uses which typically require larger footprints such as warehousing and industrial and manufacturing uses”. How does a large, sprawling warehouse contribute to meeting employment targets?

In proposing to convert this agriculture land under the jurisdiction of Puyallup into a Warehouse District is in violation of Countywide Planning Policy policies agreed to by city of Puyallup and Pierce County in 2022.

"The designation of agricultural lands per the Growth Management Act includes lands that are not already characterized by urban growth and that have long-term significance for the commercial production of food or other agricultural products." [RCW 36.70A.170(1)(a)].

The Washington Administrative Code Section 365-196-480 includes guidelines for designating agricultural lands pursuant to RCW 36.70A.050. Although the Growth Management Act does not expressly require a countywide planning policy on agricultural lands, the requirement was added by the Interlocal Agreement: Framework Agreement for the Adoption of the Countywide Planning Policy (Pierce County Council. The Resolution was first passed as No. R91-172, in September 24, 1991)."

In Particular, the Puget Sound Regional Council VISION 2050 "seeks to permanently protect these key agricultural resource lands. These Multicounty Planning Policies calls for conserving the region's natural resource lands, establishing best management practices that protect the long-term integrity and productivity of these lands, limiting the conversion of these lands, and ensuring that development does not adversely impact these lands."

The City of Puyallup and Pierce County Council are members of the Regional Council and must conform to the Multiplicounty planning policies agreed to in the creation of Vision 2050. And these two jurisdiction are parties of record who agreed on the most recent update of the Pierce County Planning Policies update in Adopted May 17, 2022.

In evaluating the city's Environmental Impact Statement for this project, the city can only abide by the No Action Alternative. Any other decision is arbitrary and capricious and in violation of countywide polices which call for:

AG-7 Jurisdictions choosing to designate agricultural lands, shall coordinate agricultural land preservation policies with other Countywide Planning Policies through:

"7.2 Ensuring that public facility and service extension, even if not directly serving the agricultural lands, do not stimulate the conversion of agricultural land or make its preservation and protection more difficult;

According to the state Court of Appeal Decision in April 2019 between City of Puyallup and Knutson farms, the court recognized that the city had extended services for this Knutson project site which is within the City's sewer area, and a portion of the site is in the City's water service area".

Extending such services is not in accordance with planning policies that direct the city to "avoid the extension of urban services to areas intended for continued agricultural use" and by providing service extension to the area around Knutson Farms, the city did in fact "stimulate the conversion of agricultural land" and did make its "preservation and protection more difficult."

In addition, AG-1 of Countywide Planning Policies calls for "jurisdictions choosing to designate agricultural lands of long-term commercial significance, shall do so using the methodology and criteria stated in WAC 365-190-050. Cities are encouraged to coordinate their agricultural resource lands designations with the County and adjacent jurisdictions and are encouraged to adopt the same criteria.

1.1 Consistency with WAC 365-190-050 shall be based on the following factors:

1.1.1 The land is not already characterized by urban growth;

1.1.2 The land is used or capable of being used for agricultural production; and

1.1.3 The land has long-term commercial significance for agriculture

The proposed conversion of the Knutson farming area is outside the city's boundary which is "not characterized by urban growth". It is currently plowed and planted farm land and it is obviously "used for agriculture production". And since 1950 the property has been used for farming as a "long term commercial significance for agriculture."

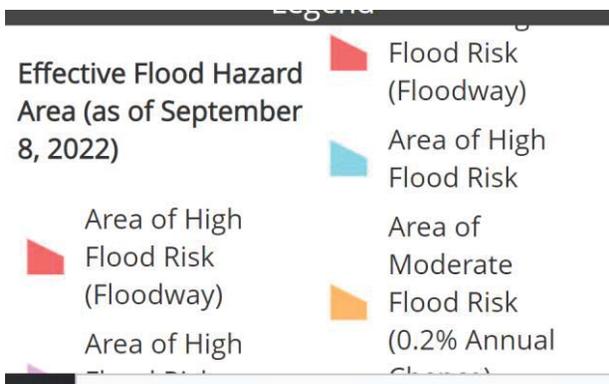
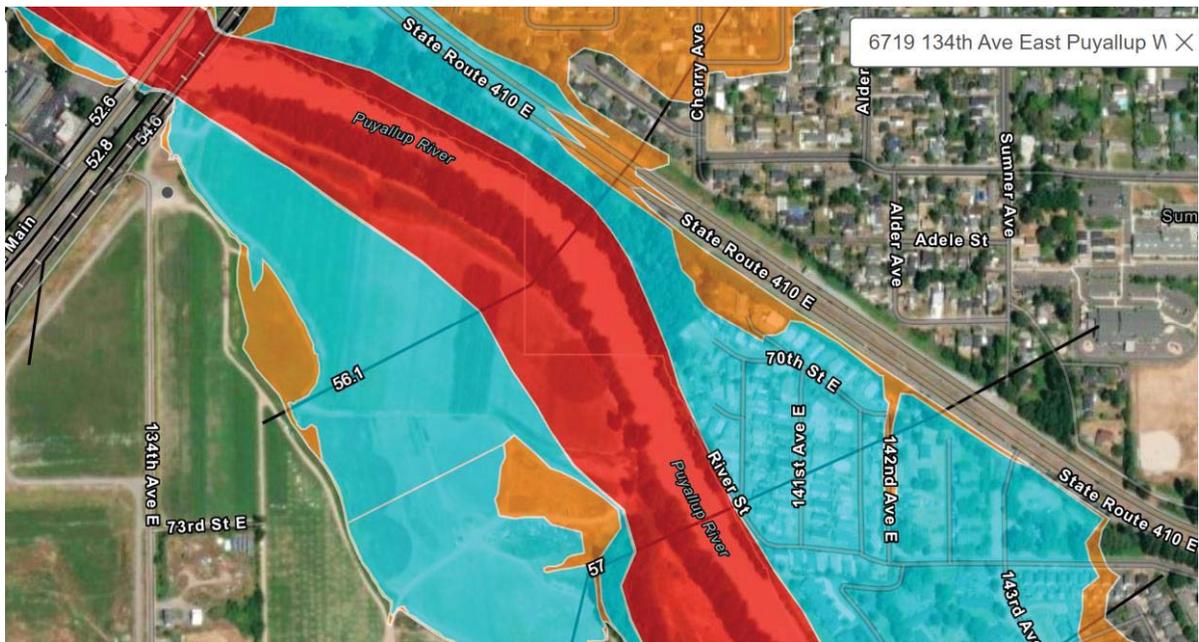
### **3 Flood hazard areas -**

The local office of the U.S. geological Survey has provided Puyallup Valley farmers information about the increase of rainfall in the future to change from 35 inches to 50 inches per year in the coming decades. This flooding is caused by global warming and is already causing floods in other parts of the county.

With no changes in the Mud Mountain Dam, the Puyallup River which starts flowing from the Carbon, Mowich and Puyallup glaciers on Mt Rainier is likely to cause high water in winter flooding that will exceed the river banks along SR 410. The river levy is not high enough to contain the expected flooding.

Siting a warehouse district with large parking lots and building site of 2.6 million square feet which collects rainwater that will add to the water level upstream and cause serious flooding on lower Puyallup Valley farms. In addition, state law prohibits building in floodway and frequently flooded areas.

The proposal would add significant impervious surfaces to an already at-risk area, that is currently acting as flood control for other areas of the Puyallup River. It is irresponsible and potentially dangerous to permit conversion of agriculture lands and allow urban development of this magnitude at this location.



If the Puyallup Planning Department chooses to issue a permit for this project, it will be acting in an arbitrary and capricious manner. It is odd for the city of Puyallup to change agriculture designation of an adjacent jurisdiction to a commercial development. And it is arbitrary for the city to covert Agriculture lands without first honoring the Countywide Planning policies requirement to preserve agriculture lands when annexing productive agriculture lands..

We urge the City of Puyallup to choose the no Action Alternative.

Kind regards,

*Kirk Kirkland*

**253 761 1693 | [kirkkirkland9@gmail.com](mailto:kirkkirkland9@gmail.com)**

**Foothills Rails-to-Trails Coalition**

**Commenter:** Miles, Shayla

**Source:** Email

**Date:** 3/15/2024

**Comment:**

To Whom It May Concern:

Attached is a document which contains the Foothills Rails-to-Trails Coalition's comments regarding the Knutson Farm Draft EIS. We appreciate the opportunity to provide comments on this issue.

Please let us know if you have any questions.

Thank you,

Shayla



FOOTHILLS  
NATIONAL RECREATION TRAIL  
PRIMARY SUPPORTER

**Board of Directors**

March 14, 2024

Dan Bucci  
*President*

Puyallup City Hall  
Attn: Knutson Farms EIS comments  
333 South Meridian  
Puyallup, WA 98371

Theresa Turpin  
*Vice-President*

Pat Johnson  
*Secretary*

Subject: Public Comment on Knutson Farms Draft Environmental Impact Statement

Steve Brown

To Whom It May Concern:

Mark Hamilton

John Hopkins

David Lee

Victoria Lincoln

Nicole Martineau

John Peranzi

Audrey Pitigliano

John Selby

John Warhol

Kim Wells

Janine Williamson

Shayla Miles

*Executive Director*

The Foothills Rails-to-Trails Coalition works to expand, fund, maintain, promote, and advocate for an accessible and inclusive trail system from Mt. Rainier National Park to Puget Sound, fostering recreation, health, active transportation, economic growth, and environmental and historic preservation. The Coalition, composed of dedicated community volunteers, members, and local leaders, has worked for the last 40 years to help create and maintain the Foothills National Recreation Trail, a key regional trail system in Pierce County. We are writing to provide public comment on the Knutson Farms Draft Environmental Impact Statement (DEIS). We appreciate the opportunity to share our perspectives on this crucial matter that has implications for the community and our organization's mission.

The Foothills Trail to Riverwalk Trail connection is the focus of our comments for the Draft EIS, as well as trail-related issues such as the 80th street crossing, connection of the trail to Van Lierop Park, and the naming of the trail connection. **Our goal is for the creation of a safe and attractive trail link from the Foothills National Recreation Trail to the Riverwalk Trail, in line with Pierce County's regional trails plan, as well as a connection to Van Lierop Park.**

We acknowledge the complex nature of this project. While many would like to see No Action for the preservation of open space and recreation, we recognize that the land is in the City of Puyallup's Urban Growth Area, and therefore unlikely to remain undeveloped in the future, given the needs and demands of the growing population of Puyallup. We strongly believe that a trail connection in this location can provide a solution, balancing the need for preserving open space, providing recreation opportunities, and spurring economic development, all within the wetland delineation needs of the area.

**Regarding the Proposed Development:** While we are in support of the developer's proposed concept of a trail, we have several significant concerns about the proposed development being approved as is:

1. **Proposed development conflicts with Puyallup's PROS Plan:** The proposed development is not consistent with the City of Puyallup's Parks, Recreation and Open Space (PROS) Plan, nor its shoreline goals.
2. **Safety concerns for added traffic:** This location is a future key nexus for the Pierce County regional trail system, and the added traffic impacts from the proposed development would be potentially hazardous to pedestrian safety as well as other trail users.
3. **Trail alignment is not safe for users as proposed:** We strongly believe that the trail alignment should be buffered from buildings and/or warehouses, as well as from parking and/or higher traffic areas.
4. **More study needed for the best trail alignment:** Based on the city's PROS plan, the trail alignment would be better suited closer to the shoreline, however more study of the ideal trail alignment would be needed to proceed.

**Regarding the Reduced Intensity Alternative: The Foothills Coalition supports the Reduced Intensity Alternative with the following key considerations:**

1. **Trail alignment:**
  - a. ***Shoreline alignment maintenance considerations:*** While the Coalition would like to see a shoreline alignment to connect with the Riverwalk Trail in solidarity with the Friends of the Riverwalk Group, it should only be pursued if the jurisdiction that will own and maintain the trail agree to this alignment, as well as acknowledges the high maintenance requirements of a trail along the shoreline (i.e. flood impacts, sediment, etc.) and commits to taking on this maintenance responsibility.
    - i. We urge the permitting agency to conduct environmental feasibility on a potential shoreline alignment, given the potential of the trail separating the river from the adjacent wetlands.
    - ii. We also urge the City of Puyallup to consider building up the levee and/or the creation of a boardwalk to best accommodate a shoreline alignment.
  - b. ***Alternative alignment – ensure the trail is set back from warehouses and avoid parking lot trail crossings:*** If the trail alignment is not along the shoreline, we strongly believe that it should be buffered from the proposed warehouses and parking lots for the safety, visibility, and enjoyment of users.
  - c. ***Limit sharp turns and changes in directions on the trail alignment*** for improved safety and visibility of trail users.
  - d. ***Consider additional pedestrian sidewalks and/or a bike lane along the north side of 80<sup>th</sup> St.*** to ensure the safety of users accessing the trail and Van Lierop Park from various parking locations along 80<sup>th</sup> (e.g. East Puyallup Trailhead, Farm 12 parking lot, Van Lierop parking lot, etc.). We suggest these sidewalks be buffered from 80<sup>th</sup> St. using physical boundaries such as a gabion wall or other landscaping elements.

2. **80th St. crossing:** We strongly support one trail crossing on 80th St, though it is not clear where the best and most logical crossing location would be. Considering the fact that this location is the nexus of a regional trail system, a popular Puyallup park (Van Lierop), a popular group of businesses/event venues (Farm 12, Fika, etc.), the new East Puyallup trailhead and added traffic for the area on 80th St., we strongly encourage the permitting agency to require a study to find the best location for a crossing on 80th. An additional pedestrian crossing might be needed to handle pedestrian traffic to Van Lierop and Farm 12.
  1. **Consider lowering the speed limit on 80<sup>th</sup>** as the trail connection will draw an increased number of trail users crossing 80<sup>th</sup> St.
3. **Trail connection to Van Lierop Park:** We strongly believe that the trail should connect with Van Lierop Park, but, crucially, the trail should not run through or cross the park out of concern for intermingling higher-speed trail users with small children and pedestrians enjoying the park. We urge the consideration of a short loop or other means to connect to the park.
4. **Naming of the trail connection:** Long-time community trail activities have referred to this trail connection as the Ernie Bay Connector. We support this name and strongly urge the City of Puyallup, Developer and Pierce County to retain this name.
5. **Acting in good faith:** We support the developer building the trail prior to warehouse buildings as a good faith gesture.

Finally, we encourage ongoing dialogue between the City of Puyallup, Pierce County, Tribal partners, community members, and other stakeholders on this issue.

We appreciate the opportunity to provide input and express the Foothills Rails-to-Trails Coalition's commitment to collaborating for a balanced and sustainable community development. Thank you for considering our comments on the Knutson Farms Draft EIS. We look forward to continued collaboration and open communication to ensure the best outcomes for the community.

Sincerely,



Shayla Miles  
Executive Director  
Foothills Rails-to Trails Coalition  
Phone: 253-224-4963  
Email: [Shayla@foothillscoalition.org](mailto:Shayla@foothillscoalition.org)

**ForeverGreen Trails**

**Commenter:** Leveen, Larry

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Attached, please find our comments on the EIS process. Please let us know if you have any questions.

Thank you.



# ForeverGreen Trails

Connecting Pierce County Communities with Trails

March 26, 2024

## Executive Officers

Roxanne Miles  
President

Bob Myrick  
Secretary

Rob Buck  
Treasurer

## Board Members

Jennifer Dvorak

Kim Field

Cody Geddes

Shayla Miles

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[info@forevergreentrails.org](mailto:info@forevergreentrails.org)

Puyallup City Hall  
Attn: Knutson Farms EIS comments  
333 South Meridian  
Puyallup, WA 98371

Dear EIS review team:

I am writing on behalf of ForeverGreen Trails (FGT), to provide comment on the Knutson Farms EIS. FGT is a 501c3 nonprofit organization that supports the development of a unified system of trails throughout Pierce County.

We lament that the County never followed through on the agreement with the City of Puyallup regarding these parcels as agricultural set-asides. Whether through rezoning or transfer of development rights, the significant value of these agricultural, riparian, and flood zone lands could have been retained for public benefit. They should still be mitigated to the greatest extent possible through restrictions placed on development.

We appreciate that the applicant voluntarily proposed a trail connecting the Foothills National Recreation Trail with the Puyallup Riverwalk Trail. We offer the following comments regarding trail development in/around this area:

- **Align the trail towards the river but out of the floodplain.** Trail users prefer natural settings whenever possible, though we understand that final trail alignment will result from balancing several different issues including wetland preservation, flooding mitigation/prevention, long-term trail maintenance, etc.
- **Alignment should consider all planned regional trail connections.** At/adjacent to the Knutson site there are planned connections to the Puyallup River Trail and the Sumner Rivergrove Trail which crosses the Puyallup River. These might influence the final alignment of the trail (on the Knutson property).
- **Buffer the trail from structures and parking lots** for aesthetics, user enjoyment, and safety. Use of landscaping to separate these uses may be appropriate if it does not result in debris on the trail.
- **Provide direct pathways with minimal changes in direction:** Trail geometry that avoids sharp right-angle turns is desirable for good sight distance and user safety.

- **Provide a safe and logical connection across 80<sup>th</sup> St. E:** Users should enjoy safe and intuitive travel to/from the Foothills Trail and the Knutson Farms properties. It is not clear where/how this is proposed from the images in the DEIS (though perhaps the County's East Puyallup Trailhead project includes part of this connection). Active mode users often suffer at transition points like street crossings unless great care is taken. An additional pedestrian crossing of 80<sup>th</sup> at this location is needed to improve trail user safety. The existing crossing to Van Lierop Park should be retained — active mode users deserve multiple routes to reach their destination (whether trail or park). Traffic increases resulting from the Knutson redevelopment should be directed to 5<sup>th</sup> Avenue SE/Shaw Road E or 33<sup>rd</sup> St. SE/E Pioneer and away from 80<sup>th</sup> St. E for trail user safety.
- **The speed limit on 80<sup>th</sup> St. E should be lowered to 25 mph between 33<sup>rd</sup> St. SE and 139<sup>th</sup> Ave Ct. E.** The trail connection will increase the number of active mode users crossing the roadway. Lowering the speed limit here will reduce the likelihood of collisions due to greater reaction time and shorter vehicle stopping distances. Also, according to the Institute of Transportation Engineers, this speed reduction will lessen the chance of a collision resulting in a fatality by more than half because of diminished impact forces. Other roadway treatments are recommended to reinforce the message of a lower-speed zone (e.g. textured pavement, street trees, narrowed lane widths, etc.).
- **Connect Van Lierop Park to the trail.** The “east-west trail corridor buffer” in the Reduced Intensity Alternative or a combined Williams Pipeline right-of-way could accomplish this while providing park access to warehouse employees. Alternately, requiring the development of a 12-foot-wide shared-use path along 80<sup>th</sup> St. E between the trail crossing and the multi-use path entrance to the park could do so too. This would reduce conflicts between trail users and vehicles in the parking lot while benefitting residents accessing the park via active modes.
- **Combining the trail with Van Lierop Park's “multi-modal loop trail” (MMLT) yields little benefit.** We expect minimal user conflict in a combined trail because the MMLT is on the edge of the park (generating no crossing movements) and trails routinely accommodate users of varying speeds. However, future regional trail connections from Sumner and along the Puyallup River would still need to be accommodated with an additional route like the ones mentioned above. A separate trail east of the park with a direct alignment and connection to the trailhead is therefore preferred.

Regardless of what the preferred alternative is, FGT looks forward to working with all parties on the development of this important trail connection.

Thank you for your consideration,

*Larry Leveen*

## Executive Director

Mission: ForeverGreen Trails supports a system of trails throughout Pierce County, WA, via education, advocacy, promotion, collaboration, and stewardship.

Vision: All of Pierce County's communities are more connected, livable, and economically viable due to a system of trails, sidewalks, bike lanes, and local pathways that provides safe, convenient, non-motorized travel and recreation for all ages and abilities. Residents and visitors use, value, and care for this network because it contributes to their health, wellbeing, quality of life, and local economies.

**Friends of Pierce County**

**Commenter:** Johnson, Larry

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Good Afternoon Mr. Beale,

Please find our attached letter in response to the Puyallup Warehouse Proposal (Knutson Farms EIS).

Please confirm receipt of said attachment.

Thank you,

Larry Johnson

Vice President

Friends of Pierce County



[FOPC@comcast.net](mailto:FOPC@comcast.net)

March 14, 2024

Chris Beale, Project Manager  
City of Puyallup  
333 South Meridian  
Puyallup, WA 98371

[cbeale@puyallupwa.gov](mailto:cbeale@puyallupwa.gov)

**RE: Knutson Farms EIS**

Dear Mr. Beale:

Friends of Pierce County, founded in 2003, is a non-profit environmental group that advocates for environmental protections in growth in Pierce County.

Friends of Pierce County strongly recommends that the City of Puyallup select the **NO ACTION** alternative in the DNS. Our concerns from this project are listed below:

- 1) **Farmland Protection:** State law requires protection of farmland. RCW 36.70A.060 directs county to adopt regulations to protect agriculture lands. RCW.36.70A.177 Ag lands of long-term commercial significance discourages non ag activities on ag land. Pierce County [strategic plan for agriculture](#) policies on agriculture land reported in 2006 – “The most intense mix of agriculture and development is taking place in the Puyallup Valley where there has been a steady conversion of open land to development uses. The accompanying table shows the location of Pierce County’s remaining agricultural land.

Pierce County Agricultural Land		
Agricultural Area	Ag Land (acres)	% Inc./UGA <sup>1</sup>
Anderson Island/Nisqually	1,787	0%
Bonney Lake/Buckley	7,290	2.5%
Central County	7,318	5.7%
Peninsula	5,155	0%
Puyallup Valley	6,606	<b>24.7%</b>
Roy/Eatonville	19,723	0.8%
<b>Total</b>	<b>47,880</b>	<b>5.0%</b>

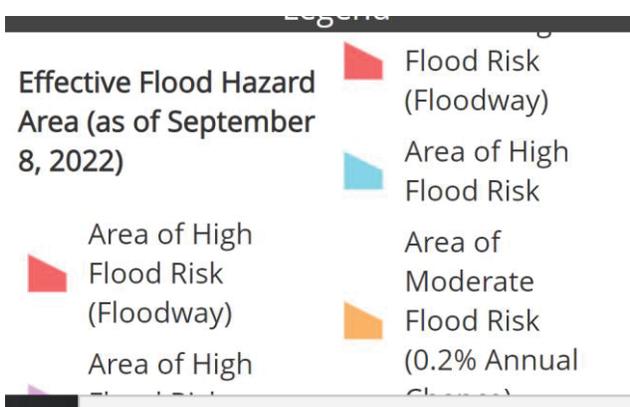
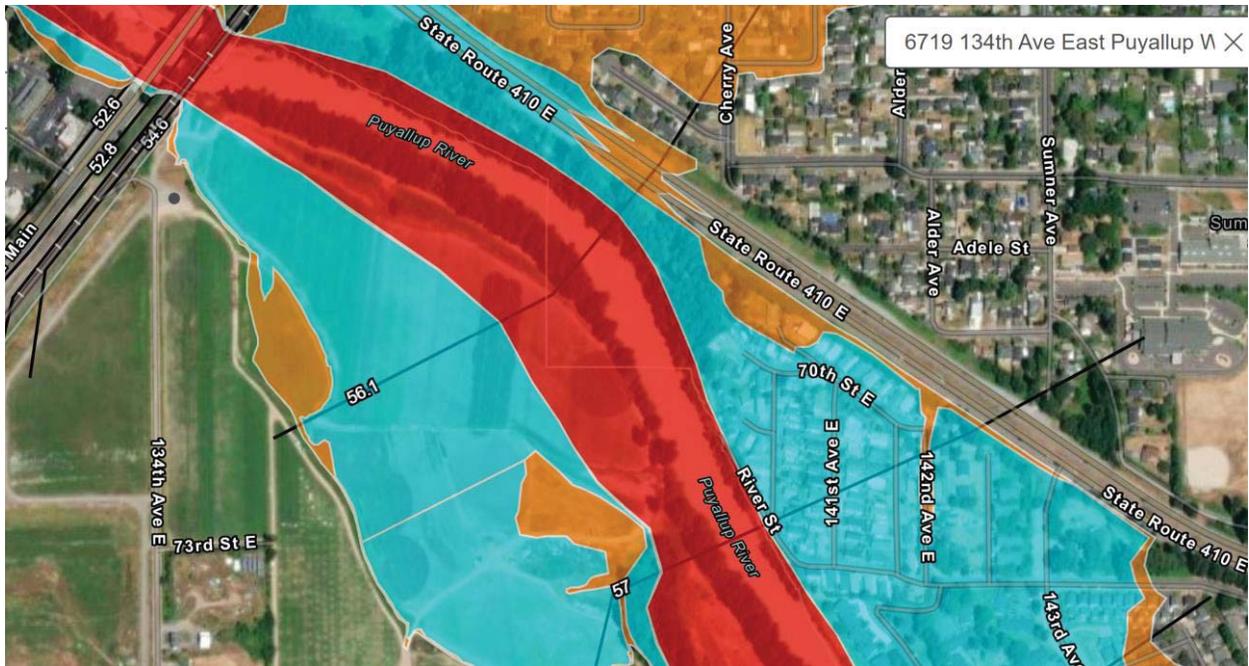
Source: Pierce County Assessor's Office (2005)

<sup>1</sup> Percentage of agricultural land that is located within an incorporated area or urban growth boundary.

“Preserving farmland and nurturing a prosperous agriculture sector will require a sustained policy commitment and well-conceived, concerted action”.

We urge the city to redesignate this area as farmland after Pierce County failed to do so in the past. The city has an opportunity to preserve this fertile land and let it be used for agriculture.

- 2) **Future Annexation into this area will not help the city meet growth or employment targets:** Pierce [Co Buildable Lands report](#) (2022) states, “land uses which are concentrated in office environments, or retail and service uses centered in Urban Centers have much higher employment densities that land uses which typically require larger footprints such as warehousing and industrial and manufacturing uses”. How does a large, sprawling warehouse contribute to meeting employment targets?  
 There is concern that this area would be included in the city’s Urban Growth Area as a result of this project. The site is not currently within the city’s UGA. Allowing this development now can only mean that the city’s UGA will be expanded in the future to accommodate this development. State law requires that jurisdictions, “Identify reasonable measures, other than adjusting urban growth areas.” This is the first step in annexation and we urge the city to look for other areas for a scaled back version of this project within the city limits.
  
- 3) **Flood hazard areas** - State law prohibits building in floodway and frequently flooded areas. The proposal would add significant impervious surfaces to an already at-risk area, that is currently acting as flood control for other areas of the Puyallup River. Flooding is only predicted to increase with climate change. It would be irresponsible and potentially dangerous to allow a construction of this magnitude at this location.



- 4) **Pollution from storm water runoff and loss of flood control measures from natural soil infiltration** - The project site of 162 acres, with 2.6 million square feet of building area, will contain 108 acres of impervious surface or 66% of the site.

The developer’s current stormwater outfall system is failing and the proposal is to add 7 more warehouses to a failing stormwater system. **This cannot be permitted.** Pierce County Stormwater Management and Site Development Manual, Volume I, Minimum Technical Requirements and Site Planning, states, “If the proposed project will result in the addition of new impervious or hard surface amounting to 25 percent or more of existing impervious/hard surface, providing that the site is greater than 1 acre in size with 50 percent or more impervious/hard surface, or the county determines developed site contributes to existing water quality, flooding, or erosion problem, the entire site must be brought up to current stormwater standards”.

An at-risk area for flooding with an at-risk developer is another reason this project should be denied.

- 5) **Risks to Threatened Chinook Salmon** - The increase in runoff from this site will contribute more pollution load into the Puyallup River. National and Oceanic Atmospheric Administration (NOAA) [reported in a letter](#) “New development (e.g., buildings) in floodplains reduces available fish refugia, likely increases stormwater effects on the adjacent stream, and likely becomes a new source of pollutants (e.g., lawn chemicals, stored chemicals, road runoff, etc.)”.  
With a NOAA listed stock in the Puyallup River of Chinook Salmon, this project could present a continual taking of a Threatened species.
  
- 6) **Negative Traffic Impacts** - The traffic impacts of this development are suggested to be as large as 6,000 large vehicle trips daily. In the SEPA checklist for this project, it is acknowledged to be classified as an “environmentally sensitive” area. This proposal does not fit this property.

We urge you to select **the No Action Alternative**. This project does not belong at this location.

Please do not hesitate to contact me if you have any questions.

Thank you for considering our comments.

Larry Johnson

*Larry Johnson*

Vice President

Friends of Pierce County

**Puget Soundkeeper Alliance**

**Commenter:** Furman, Kelse

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Good morning,

Please see attached comments and confirm receipt. Thank you.

Best,

Kelsey



March 15, 2024

Puyallup City Hall  
Attn: Knutson Farms EIS comments  
333 South Meridian  
Puyallup, WA 98371

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P 206.297.7002  
F 206.297.0409

Lead Agency:  
Chris Beale, Senior Planner  
City of Puyallup  
333 South Meridian  
Puyallup, WA 98371

Sent via email: [comment@knutsonfarmseis.org](mailto:comment@knutsonfarmseis.org)

[www.pugetsoundkeeper.org](http://www.pugetsoundkeeper.org)

**RE: KNUTSON FARMS INDUSTRIAL PARK PROJECT DRAFT  
ENVIRONMENTAL IMPACT STATEMENT COMMENT**

Dear Puyallup City Hall:

Thank you for accepting these comments on behalf of Puget Soundkeeper. The Knutson Farms Industrial Park (KFIP) Project, if approved will result in significant adverse impacts and we strongly oppose it. As an organization whose mission is to protect and preserve the waters of the Puget Sound we are particularly concerned that the project is directly adjacent to the Puyallup River and as the DEIS points out, will “result[] in significant adverse impacts to surface water systems.” Pg. 4-111.

Soundkeeper urges the City to consider our comments on the DEIS and deny the proposal. The KFIP project is currently environmentally unsound and the significant environmental impacts, as demonstrated by the DEIS, will not be reasonably mitigated.

**1. THE PROJECT WILL HAVE SIGNIFICANT IMPACTS ON WATER  
AND SPECIES.**

The devastating impacts of building the proposed industrial park include significant consequences for the waters running adjacent to and under the site – including critical protected wetlands, a river where ESA-protected species are present, and a vital aquifer – and for local stormwater management. Because the mitigation measures fail to adequately examine or address these impacts, this project must not be allowed to proceed as is.

**A. The industrial park will introduce toxic tire chemicals into the waters running through and adjacent to the project site.**



Introducing almost 9,000 new vehicles per day into the area (*see* pg. 4-87) will not only significantly worsen traffic, air quality, and increase greenhouse gas emissions at a time when Puyallup should be working to reduce both, but it will also introduce toxic fish-killing chemicals from tires into Puyallup waters.

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Tire manufactures use a chemical called 6PPD to protect rubber elastomers in tires from ground level ozone. 6PPD-quinone is a transformation product of 6PPD. When it rains, stormwater flows over roads and carries road chemicals into nearby waterways.

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Recently, scientists identified 6PPD-quinone as the cause of well documented fish death in the watersheds in urban areas.

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The threat of introducing toxic tire chemicals into waterways is especially likely for the KFIP because it would be sited on critical wetlands, adjacent to Puyallup River, and an important aquifer sits below the area. Chinook, steelhead, bull trout, and coho -- species listed for protection or of concern under the Endangered Species Act -- will be significantly impacted by increased 6PPD/q from the project area. However, the KFIP avoids any effort to minimize this risk:

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*“under the current proposal, potential water quality impacts to listed species in the River during both Construction Phases and Operations Phases from the increase in direct flows to the river from paved areas containing the 6PPD pollutant are neither avoided nor minimized. No effective treatment designed to remove 6PPD from the pavement runoff prior to sending it to the river is proposed.”*

Pg. 4-103 (emphasis added).

Protecting these listed species from this significant adverse impact should be a priority. Especially since the White River merges with the Puyallup River approximately 0.5 mile downstream from the KFIP site and supports the last Spring Chinook salmon run in the South Puget Sound. The DEIS highlights that this issue “suggests **a need for reassessment or redesign** of KFIP stormwater management plan and/or facilities.” Pg. 4-54. We agree. The use of low impact development (LID), and bioinfiltration must be required in the new stormwater systems to better manage runoff from the proposed impervious surfaces before discharging to the Puyallup River. Simply “re-evaluating” and “considering broad application” is not enough. *See* pg. 4-105.

If the KFIP project continues as proposed, it will likely involve illegal “take” of protected species and fail to comply with federal law as noted in the DEIS:

*“The currently proposed Project stormwater management plan does not implement BMPs that may be used to minimize this pollutant prior to discharge into the Puyallup River. With no BMPs using prescriptive infiltration, sorption, filtration or sedimentation treatment, potential for minimizing levels of 6PPD-q (soluble) and fine sediment or tire particles containing 6PPD (solid or precipitate) is low. Without appropriate treatment,*



*research indicates a moderate to high potential for illegal take of listed and sensitive species near the stormwater outfall, and potential for downstream impacts to other species from bioaccumulation.”*

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Pg. 4-181 (emphasis added).

Unless and until KFIP can guarantee the use of LID, bioinfiltration, and assure adequate mitigation and protection to at-risk species, we urge the City to deny this project.

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### **B. The Project undermines stormwater management.**

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The KFIP project will dramatically change what happens to stormwater on the project site’s 188 acres, with a myriad of significant unavoidable adverse impacts. Increasing impervious surfaces results in greater runoff volumes and faster rates and is a major contributor of pollutants. This can lead to significant changes in hydrology and water quality and impacts habitat biological diversity, increases flooding, sediment movement, and surface erosion.

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The project proposes increasing the existing impervious surface from less than 5 percent to more than 75 percent. Pg. 4-144. This means that stormwater at the site that was previously absorbed by soil, native plants, wetlands, etc. would be diverted to run off into the Puyallup River and lead to increased potential of erosion.

The outfall structure was originally built without appropriate assessment of additional hydraulic impacts from significantly greater future KFIP outfall volumes. Pg. 4-50. Construction of the outfall has already resulted in unpermitted placement of large boulders and increased bank erosion under *current* conditions. This will only get worse with increased runoff volumes for the KFIP project.

According to the DEIS, the proposed “stormwater management would lead to faster runoff to the river, and a reduction in stored groundwater volumes below the high terrace on the KFIP site, which currently slows flows to the floodplain and river over time.” Pg. 4-144. It goes on to point out the need for design changes, and without them “on-site wetland hydrology would not be maintained, and the on-site wetland hydroperiods would change over time, eventually resulting in loss or reduction in surface area of on-site floodplain wetlands.” Pg. 4-144.

The project will impede the recharge of the aquifer on which the project site sits. KFIP is proposing to build over an aquifer recharge area, where groundwater is currently able to seep into the aquifer because of the lack of a confining layer. Please note, the maximum impervious surface coverage for the KFIP site is 60 percent. Pg. 4-144. This is in place to protect aquifer recharge areas. Pg. 4-119. The DEIS provides no reasoning or information as to why KFIP can exceed this maximum by 15 percent, or how it will properly do so.



The City cannot approve this project as is. The 60 percent maximum is critical for protecting aquifer recharge areas. While protecting the aquifer now is essential, its importance will grow as climate change causes increasingly long periods of extreme heat and drought.

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Finally, there is scientific consensus that climate change has caused and will continue to cause, intensification of heavy precipitation, including rainfall. With more rainwater accumulating more quickly, existing stormwater systems, especially ones built without proper assessment of additionally hydraulic impacts in the first place, will certainly not be able to handle the level of water introduced during increasingly extreme weather events. The DEIS fails to analyze this issue.

## **2. THE SIGNIFICANT ENVIRONMENTAL IMPACTS ANALYSIS LACKS NECESSARY INFORMATION AND PROPER MITIGATION.**

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An EIS is supposed to provide the decision makers and public with a *complete* and impartial discussion of the proposed project. It is also intended to provide information to agencies, applicants, and the public to encourage the development of environmentally sound proposals.

### **A. The DEIS lacks the necessary information to properly inform the decision maker, other agencies, and public.**

The DEIS shows that the proposal lacks detailed information, testing, or assessments to show the complete picture. *See e.g.* pg. 4-103 (“Potential water quantity impacts to Wetlands A, B, and C during construction phases and operations phases are neither avoided nor minimized, *due to a lack of any information about on-site wetland hydroperiods, as is needed to properly design infiltration facilities that could be used to maintain these wetlands.* Potential water quality and quantity impacts to Wetland D are neither avoided nor minimized, due to the lack of any fill permit review and approval process and lack of an associated approved mitigation plan.”); *see also* pg. 4-104 (noting the lack of scour analysis and assessment); *see also* pg. 4-234 (“due to the clear lack of master planning of the overall Project” it is inconsistent with Pierce County policy. Moreover, there is “a lack of details regarding overall site management and maintenance approaches”); *see also* pg. 4-240 (“The Applicant has not provided building designs and a conclusion” regarding the City’s natural environment element of the comprehensive plan “cannot be made at this time.”).

These are just a few examples where the proposal fails to provide sufficient information to the decision maker, other agencies, and public to make a fully informed decision, properly comment, and encourage the development of environmentally sound proposals.



## **B. The proposal is not environmentally sound and lacks proper mitigation.**

The environmental review process involves the development of mitigation measures that will reduce adverse environmental impacts. Here, the mitigation measures are non-existent or fail to reduce adverse impacts.

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For example, filling at Wetland D will result in direct loss of wetland. Wetlands are important because among other functions, they provide important habitat, flood control, and act as a natural pollutant filter. According to the DEIS there is no fill permit or mitigation plan designed to compensate for the losses associated with wetland filling. Pg. 4-111.

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It also appears, as mentioned previously, that without design changes, “on-site wetland hydrology would not be maintained, and the on-site wetland hydroperiods would change over time, eventually resulting in loss or reduction in surface area of on-site floodplain wetlands.” Pg. 4-144. So, any attempt to claim that retaining wetlands is mitigation for this project is absurd. A proposal that causes ultimate destruction of its “mitigation” simply lacks any mitigation.

Please note, the Project is inconsistent with numerous Pierce County goals and policies. See Table 4.22 (i.e. “there is no current plan from the Project for assessment, repair or replanting to address existing current conditions, including loss of planted habitat mitigation vegetation surround and outside of the outfall structure ...” and “the Project does not include adaptive management for critical areas.”).

An EIS is supposed to provide the decision makers and public with probable significant adverse environmental impacts and mitigation measures that would avoid or minimize adverse impacts. That is not the case here. More mitigation is needed to sufficiently reduce adverse environmental impacts before the City approves the project.

### **3. THE PROJECT LACKS A REASONABLE RANGE OF ALTERNATIVES.**

Alternatives are one of the basic building blocks of an EIS. They should present options in a meaningful way for decision makers. It is important that selected alternatives represent a *range of options* which then provide the decision maker with the option to evaluate and compare the merits of different choices. The DEIS includes a range of options primarily focused on variations of the proposed project intensity. It should include an alternative that truly minimizes environmental impacts, such as those that prioritize reduced density and conservation.

The Applicant’s objectives for the Project include:

- Provide additional manufacturing, warehousing, and shipping capacity in Pierce County;



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- Improve nearby arterial traffic corridors to meet the growing economic demands for such services in the Sumner/Puyallup valley;
- Create new manufacturing/warehousing/shipping jobs in Pierce County; and
- Preserve and integrate open space into development plans for the site to provide for flood storage, habitat, environmental mitigation, and passive recreation.
- Complete construction within 5 years of the issuance of a Final EIS, or by 2029.

Alternative 2 reduces the site footprint of the facility from about 2.6 million square feet to about 1.7 million square feet. Pg. 4-146. There is no specific objective requiring a certain number of warehouses or new jobs. One of the objectives is to preserve the open space for flood storage and environmental mitigation. Alternative 2 addresses all the objectives and preserves the open space the best. If the City approves the KFIP project, it should approve Alternative 2.

While Alternative 2 better fulfills the KFIP objectives but is still not enough because it fails to protect listed species in the Puyallup river from 6PPD and does not address the need to maintain current hydrology sources for wetland habitats during construction. *See* 4-208.

#### **4. THE PROJECT'S PARKING LOT SIZE IS UNREASONABLY LARGE.**

KFIP could reduce some adverse impacts associated with the impervious surface area by decreasing the amount of parking spaces in favor of retaining non-developed area. The DEIS notes that during operations, the seven warehouses are anticipated to employ up to approximately 1,500 individuals. Pg. 3-17. There would be three shifts per day, which would result in approximately 500 employees on the Project site at any time. The proposal, however, includes 1,730 car parking spots and 473 parking spaces for trailers. Pg. 3-9.

The proposal does not specify why it needs *1,230 more* parking spaces than the number of employees on the site at any time. The currently proposed parking lot size is unreasonable and unnecessarily causes more environmental harm. The proposal can significantly reduce parking spaces (thus reducing impervious surface area) and increase open space for more environmental mitigation. This feasibly attains the proposal's objectives but at a lower environmental cost and decreased level of environmental degradation.

#### **5. CONCLUSION**

If the project moves forward as proposed it will result in significant adverse impacts to the environment. As an organization committed to protecting and preserving the waters

**Puyallup River Watershed Council**

**Commenter:** Dillon, Angela

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Please accept the attached comments on behalf of the Puyallup River Watershed Council.

Thank you,



## The Puyallup River Watershed Council

is a tax exempt  
501(c)(3) nonprofit  
Washington corporation

### Board of Directors

Allan Warren  
Angela Dillon, Vice President  
Cindy Callahan  
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Robby Wright, President  
Stephanie Eckard

### Contact

WEBSITE:  
[Puyallupwatershed.org](http://Puyallupwatershed.org)  
EMAIL:  
[Puyallupwatershed253@gmail.com](mailto:Puyallupwatershed253@gmail.com)

March 15<sup>th</sup>, 2024  
Puyallup City Hall  
333 S. Meridian  
Puyallup, WA 98371  
Attn: Knutson Farms EIS Comments

RE: P-19-0141

Thank you for the opportunity to comment on the Knutson Farms Industrial Park Project. The Puyallup River Watershed Council with our mission to restore, protect, and enhance the environmental, economic, and cultural health of our watershed, from Mount Rainier to Commencement Bay would like to recommend the no action alternative.

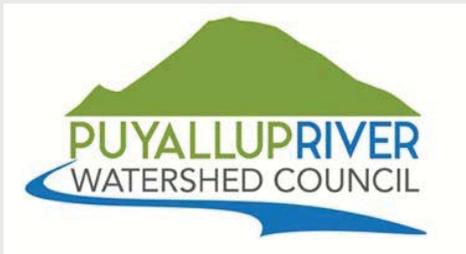
A project of this magnitude with the majority of the site changing from agricultural lands to impervious areas is harmful to the natural landscape with implications for wildlife and ESA listed species, is not consistent with the natural characteristics and aesthetics of the area, and has serious considerations for traffic.

The proposed impervious surfaces are a barrier to the loamy top soils which absorb and convey waters to the onsite wetlands. Changes to hydrology will have negative impacts to the wetlands and puts them at risk to shrink or disappear. The impervious surfaces that collect petroleum, rubber, copper, and many other contaminants including 6PPD-q will accumulate over 188 acres of the proposed project and load into the Puyallup River which hosts all salmonids including ESA listed Chinook, Steelhead, and Bulltrout. The potential for untreated stormwater discharged from the proposed project is a threat to these already endangered species and the water resources and habitat on which they rely. Additionally, storm water that is not infiltrated but rather discharged into the Puyallup River exacerbates flooding and puts pressure on the outfall which has major design flaws and failures.

The outfall was originally positioned too close to the river bank on the lower terrace. Heavy flows out of these pipes has cut away the surrounding area and compromised native plantings and the overall stability of the structure. Regardless of the alternative selected, the outfall should be relocated south onto higher elevations or what is referred to as the upper terrace. The outfall needs to be redesigned so plantings and function are not compromised when the river overtops the bank.

The recommendation on 4-63 of the DEIS to install riprap is not a reasonable solution. Armoring river banks is an outdated method that needs to be retired. Riprap tends to increase the speed of water flow along an armored reach. The Knutson Farm Scour Analysis report indicates that velocity of water at the site is critically high over various AEP flood scenarios. In an already channelized system with the railroad structure confining flow, increasing the strength and speed of water perpetuates erosion on site and downstream, interrupts riparian areas, and simplifies habitat which has negative effects on salmonids that lose refuge from overhanging vegetation and get pushed downstream prematurely.

Figure 4-40 indicates a pedestrian trail on the north side of the development. Pedestrian trails do not coexist well with shorelines. They take valuable real estate along the riverside that could be used to enhance ecosystem functions. Robust riparian buffers should be prioritized over pedestrian trails.



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Mark Heckert  
Mark Palmer, Treasurer  
Robby Wright, President  
Stephanie Eckard

### Contact

WEBSITE:  
[Puyallupwatershed.org](http://Puyallupwatershed.org)

EMAIL:  
[Puyallupwatershed253@gmail.com](mailto:Puyallupwatershed253@gmail.com)

The DEIS section 4.9.2 reports that the proposed project will include up to 8,724 total daily trips including 1,482 heavy vehicle trips. The addition of thousands of vehicles on Shaw Road, Pioneer, and East Main streets will clog already heavy traffic in the area. The pressure on the transportation network will have negative consequences for police and fire services, health care services, schools, and the quality of life for residents.

The Puyallup River Watershed Council cannot support siting 2.6 million square feet of warehouses next to the Puyallup River. High intensity development should stay in areas that are already established for this type of industry. Do not push development into rural areas and destroy valuable agricultural soil while risking water quality, wetlands, salmon habitat, and increasing flood risks. This massive warehouse complex should not be allowed to compete with and degrade already threatened natural resources.

Thank you,

Angela Dillon, Vice President

**Commenter:** Aaron, Daniel

**Source:** Webform

**Date:** 12/14/2023 7:52:27 PM

**Comment:**

Placing warehouses here is absolutely abhorrent in every way. The landscape is beautiful, draws visitors, and keeps Puyallup looking like a beautiful city and not just another Kent. I can't believe anything other than a rail project would be put here. This new warehouse called the "Red Dot Corporation" is an eye sore to all residents and makes our lives actively miserable with all the trucking traffic. Shame on the council, shame on Puyallup Community Development, and shame on Puyallup Public works for even considering this.

**Commenter:** Aaron, Daniel

**Source:** Email

**Date:** 3/1/2024

**Comment:**

My name is Daniel Aaron and I live at [REDACTED]

This "reduced intensity" alternative honestly feels like arguing with a child. I said "NO" to the proposal outright, not "maybe just a few warehouses". When someone says "NO" do you keep going?

I'm insulted, but less so if this is really the only route. You people will never be satisfied though huh? Am I going to have to fight for the flood plain area, the preserved agricultural area, and Van Lierop Park in the future? I'd like to know so that I can start organizing honestly.

- Daniel Aaron.

**Commenter:** Adams, Jessica

**Source:** Webform

**Date:** 12/14/2023 7:30:30 PM

**Comment:**

Please don't do this to the beautiful valley. We were known to have the 3rd richest soil in the country in our Valley. Now you want to cover it with concrete? We don't need to continue to become Fife! This is infuriating and breaks my Puyallup born and raised heart.

**Commenter:** Alhadeff, Annie

**Source:** Email

**Date:** 3/15/2024

**Comment:**

My name is Anjovon Alhadeff. I live on [REDACTED] My address is technically in unincorporated Pierce County and let me tell you I feel like I've been held hostage by them. I will literally be a stone's throw away from one of the warehouses. I am not being dramatic and I am not over emphasizing the fact that I will literally open my front door and see the warehouse. The road that I live on begins the entire residential area and stretches all the way back to the Orting freeway. It's a large residential area that abuts the Van Lierop Park, Farm 12 restaurant, the Step-by-Step foundation, the Puyallup river, along with Riverwalk and Rails to Trails trails and magnificent views of Mount Rainier. I bought my home about eight years ago and was super excited and feeling extremely blessed that I literally get to open my door to see the Daffodil fields of Puyallup.

I was walking my little dog one day passed the field when I saw a giant yellow public use notice sign at which time it explained that this property was going to be used for seven mega warehouses!! There is no way that warehouses fit in this location and this has to be a mistake. So, I called the County to get some more information. I talked to a very snarky and rude woman, who should not be doing business with the public, who basically told me I'm lucky that they're not putting a manufacturing plant in and have a nice day. My heart sank. Unfortunately, this is the attitude that Pierce County has had about the project and how it affects us the taxpayers that live in that neighborhood since the beginning. They will not address us face to face. COWARDS!! Pierce County does not give a shit! It is the county's job to protect those that live here and to ensure that the projects that they allow to happen in this county, do not affect our quality of life. What a shame that the City of Puyallup is fighting for people who don't even live in their city, but our own county doesn't give two shits about what happens to us! I do not understand how it is that the county does not have to come and face us in these public forums and discussions and take accountability. To in some way at least try to poorly justify their stance on letting this happen. It's incredibly insulting to those of us who live directly in the neighborhood to which these warehouses will be built and pay the taxes that they use!! It doesn't take a genius to figure out that this use of the land just doesn't fit. I was floored by the fact that anybody would consider putting something so industrial in such an agricultural and residential type of space.

I won't even go in to all of the comments regarding traffic, noise and pollution, because my fellow citizens have already spoken loud and clear about this. They have very clearly laid out how detrimental these issues are. However, unfortunately, no one in any of the public meetings that I have been to have brought up any kind of solution or mitigation for what this will do to our property values. I demand to know who is going to pay me for my lost resale value??!! Nobody wants to buy a house next to a warehouse!! They don't even want to buy a house that's within 6 blocks of a warehouse because of the traffic, the noise, and the pollution.

Another point that has been poorly talked about is how this will affect the wildlife and the bird migration. I believe that there has not been enough studies about the bird migration in that field. I watch every year hundreds upon hundreds of different ducks and geese and other birds migrate in and out of the area, about two different cycles a year they will migrate to and from. However, this was not discussed at all in the DEIS and I think it is a gross oversight. I believe the county has a responsibility to ensure that they are not damaging the migration pattern of these birds. I'm pretty sure that that is a federally protected stance. There's also an abundance of owls, hawks, coyote and deer.

The DEIS says that one of the warehouses would sit right upon a wetland that is right near my home. I don't need a DEIS to tell you that that is a wetland!! There has been so much water at one point in time that it flooded into my neighbor's property and therefore he had to use a pump to move the water so that it did not flood our properties. It's a wetland! You cannot build upon it. Furthermore, there gas pipe running through the field!! There is no way that you can safely build upon that without a taking it out and what kind of environmental concern with that have? Not to mention the safety factor for flammable material.

It's very simple. This project does not fit here. When you drive through the valley of Auburn, Kent, Sumner, and Fife you see acres upon acres of warehousing. It is ugly. It is gross. It is not conducive to people wanting to live here in the valley of Western Washington. It does not make people want to move here and set down roots here and grow communities here. It drives people away. It pollutes our beautiful landscapes, our clean air and our clean water. Why in the hell do we need more warehouses!! When you drive through these industrial parks within the valley, all you see is "For Lease" signs left and right. We don't need more warehousing in this damn country! What we need is more farmland. Food Insecurities are growing by the tens of thousands every year. Many bee species, which are responsible for pollinating the very food that we rely on, are on the brink of extinction! Can you please tell me how warehouses are going to help bees pollinate food!! How warehouses are going to grow food to feed us!!!

It's illogical, and it's irrational, and it is an injustice to human beings who live here. If those who are responsible for planning and approving this hideous project have a soul you would stand up and do what is right. Pierce County has the power to do what is right here. But will they???..... Probably not. So, knowing that, I implore those who are making the final decision to go with "reduced intensity" alternative that was carefully and thoughtfully been engineered by the City of Puyallup. It was designed to alleviate significant impacts on; traffic, farmland loss, wetland destruction, stormwater runoff killing salmon, the visual, noise, and rural character that impacts Farm 12, Van Lierop Park, Rails to Trails, Foothill Trail and those that actually live in the neighborhood. [image of reduced intensity plan] Please do what is right before it is too late!!!

**Commenter:** Alhadeff, Anjovon

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Okay. Are you ready for this name?

Anjovon Alhadeff. A-N-J-O-V-O-N A-L-H-A-D-E-F-F. I didn't pick it. Anyway, so I live off of 139th Avenue Court East. Literally we're building esses (phonetic) horse pasture, my fricking house. Yeah. I'm not happy. And so I bought my house about eight years ago and I'm walking my dog and I see the big yellow sign that says public use notice. That's where it began. And I start the residential area all behind me and, unfortunately, we are stuck in unincorporated Pierce County. I can see the City of Puyallup sign from my front door. I can see Farm 12 from my front door, and this beautiful field that I get to watch the sun set every night. I called city -- or the county and I got some snarky lady on the phone that basically said, well, you're lucky that they're not wanting to build cars there, but they can do whatever they want. Have a nice day. That's the treatment that I got from the county. So as much as I appreciate everything that City of Puyallup have done, and I've talked to many different council people, John Palmer. I've gone to a lot of different meetings and it's kind of a shame that all of those previous public comments that we've already done are not included. And I think that's not right because a lot of us have sent in a lot of documentation and spent a lot of time logging our comments and our concerns and for those to just kind of not be included is not right. But anyway. I talked to plenty of council members, and I do appreciate everything, absolutely everything that the City of Puyallup has done to the point of legal action to fight over who has rights to the land. I know that wasn't cheap to, you know, the Puyallup taxpayers. So thanks, guys. I would gladly pay that tax, but I'm again stuck in unincorporated Pierce County. So what happens in that land directly affects every single house down 80 to the Oregon Highway. And one thing that is not covered in there at all is our property value. If I want to sell my house with a ginormous warehouse next to it, what do you think is going to happen to my property value? Is there anybody here from Pierce County? From the county? No, didn't think so because they don't give a shit. They don't. And so I don't think we should give them an inch. And if it is that Pierce County or City of Puyallup pulls all of the permits, don't get a damn one of them. It is not okay for them to do that to us, and all those people who live down in here. Traffic is horrendous already. Love Farm 12 but damn, I can barely get through without having to stop and wait for somebody to figure out where they're going to park. Right? It's already bad and it's just a restaurant. It's a joke. It does not belong there. When you look all around you, you have Mount Rainier view. You have the Rails to Trails. You have the Riverwalk Trail. It's a beautiful pasture area. All this beautiful farmland. And we're going to stick warehouses there? It does not fit. I don't think that Puyallup should give them anything. As much as I appreciate all the hard work that's gone into this and we're going to give them the best case scenario that best fits the area and still allow you to have warehouses, it's too much of a concession. Way too much of a concession. We need to fight harder. At one point in time I had talked to a council member and there was talk about -- I can't remember the exact term but like a super bill to where it was put out that maybe Puyallup would buy the land from the developer and expand the Van Lierop Park and be able to annex all of us that are stuck in unincorporated Pierce County. I don't know what ever happened to that. I watched

every measure that came through and referendum. Never saw it on the bill. I'm curious where that idea went. I understand that maybe people who live up in South Hill and up in Meridian up there may not give two shits about what happens down in the

valley. But again, all of us that live down 80th, we just get to sit there and watch it And we have no control over it. Annex the area however possible you can do it so we can at least vote on what happens to our own property values in the area. I'd like to see City of Puyallup put a little bit more effort behind that if possible. This is great, all the work, but it's too much of a concession. The other part that I am very concerned about is I didn't need the EIS statement to tell you that that's a wetland in front of my house. I can see it every winter and every spring. One thing that really wasn't discussed is the wildlife that happens in that area. There's a lot of deer. There's a lot of hawk. A lot of eagles. There's a migration for different types of geese and birds. And coyotes in the area. And so what happens to the wildlife? I didn't see that really brought up in the EIS statement either. There's a lot of runoff. I'm pretty sure my house, my front yard is a wetland. It's ridiculous. I lose fences all the time because of it. I love the portion where they're saying you really probably won't be able to build here because of it but there's more there than just that. And when you start putting buildings in that area, where does that water go? And is it going to come into our land? There's three houses that run off of 139th Avenue on the private dirt road. Is that going to come into our area and is Pierce County going to pay for the damage? So there's way more that needs to happen here. So, you know, as much as I want to say just get rid of F and G because that helps my area, but the whole thing, it's just too much of a concession. So anyway, I think that's about all I wanted to say. Again, I do appreciate everything that City of Puyallup has done but we just can't. Just can't. The answer is no. The answer is no.

**Commenter:** Alhadeff, Anjovon

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Mitigation concerns

**Commenter:** Anderson, Ms.

**Source:** Webform

**Date:** 12/15/2023 11:58:58 AM

**Comment:**

Please limit any warehouses or trains at this site. The green space is greatly needed in this area. Pierce County and the Puget Sound region already have plenty of empty warehouse spaces. We do not need more traffic on these roads or more warehouses at this location. The green space is better for community health and wildlife health to avoid noise pollution. The green space is better for air quality and water quality. The bucolic nature of this area offers a small road with limited noise and ample birdwatching, biking, dog walking, and other recreational pursuits for community members. Please do not sacrifice this green space with a scenic view of the mountains nearby the river for a quick buck.

**Commenter:** Anspach, Jason

**Source:** Webform

**Date:** 1/28/2024 8:40:41 PM

**Comment:**

Puyallup has a rich history of agriculture. Its farm lands should be preserved, allowing Puyallup to remain a city, beautiful and capable of self-sustaining life. I would urge the plan to not be approved on those grounds.

**Commenter:** Anspach, Caleb

**Source:** Webform

**Date:** 3/5/2024 9:15:00 AM

**Comment:**

To whom this may concern, This plan is not a good idea. The reason so many people get up in arms against these new developments is because they know they're not getting a fair trade. No one wants to see some of the only farmland left turned into a warehouse district. The city has had a long history of aggressively developing beautiful natural areas into traffic inducing sprawl, (south hill) often at the expense of the core downtown, which should be the primary area of interest. If the city really feels a need to "grow" then they should be doing it by creating walkable developments zoned for mixed use in the downtown / valley area, not by building warehouses on a farm.

**Commenter:** Anspach, Jennifer

**Source:** Email

**Date:** 3/5/2024

**Comment:**

Good morning! I strongly oppose the proposed project on the Knutson farms property. I have lived in this beautiful region my entire 45 years and it is extremely sad to see over and again the development of land that is our heritage. Puyallup and the surrounding area is so unique. The topography not only provides beauty and peace for our souls but also establishes a sense of pride in our community. Filling natural land with warehouses is an extreme mistake that will have negative impact on the future of our community and our environment.

**Commenter:** Arend, Christine

**Source:** Webform

**Date:** 2/21/2024 5:38:00 PM

**Comment:**

As a lifelong community member, the idea to build more ugly warehouses on beautiful Puyallup soil is an atrocity. There are so many empty warehouses available now (Canyon Road being just one example). Instead of adding more concrete, please consider turning this farmland into a green space or a community park.

**Commenter:** , Arianna

**Source:** Email

**Date:** 2/17/2024

**Comment:**

Hello,

As residents in the Puyallup Highlands we will be directly impacted by these buildings.

Traffic before the warehouse complex construction was bad, pierce county had a traffic study done and the results clearly stated that traffic was already congested up Shaw road, throughout east main as well as smaller roads in the general area. The study confirmed that building the complex would make it worse. I'm shocked that pierce county would continue the project after those results.

This will not benefit the residents of the surrounding area at all. Home values will drop, pollution will increase and impact salmon coming up the Puyallup river and overall be devastating to the farmland and natural in our area.

Thank you for the consideration

Arianna

**Commenter:** Armatis, Ashley

**Source:** Webform

**Date:** 1/11/2024 3:28:41 PM

**Comment:**

No action. Please do not develop this land. Keep as farm land. We don't have enough left. Warehouses are not needed in the Puyallup valley. This will have awful environmental impact as well as traffic impact.

**Commenter:** Ashley, Renee

**Source:** Webform

**Date:** 12/14/2023 6:54:24 PM

**Comment:**

I truly do not understand how more share houses are what we need?! The roads can not support the amount of large trucks that will be on the road. Traffic at the 410/ Traffic Ave intersection already gets backed up ridiculously.the impact these warehouses are going to have on our community is stupendous! This just shows the amount of greed there is in our government thinking about the money before the people who live and use the roads here.

**Commenter:** Austin, Laura

**Source:** Webform

**Date:** 12/14/2023 7:31:28 PM

**Comment:**

What exactly are these building going to be used for? Is it necessary or would the land be better used for the city as actual farm land? While Puyallup is growing, it is still a small town and the people who occupy this area enjoy that small town feel and look. Warehouses, dealerships and complexes going up everywhere, destroying the land and scenery is not the answer the citizens want or need.

**Commenter:** Baez, April

**Source:** Webform

**Date:** 3/14/2024 10:52:00 AM

**Comment:**

I would really like to see a no build alternative and Puyallup telling Pierce County that the city will deny all water and sewer permits to this development and deny use of their roadways. This should be a fail-safe approach as Pierce County has demonstrated they are lacking in any moral and ethical behavior thus far to this project. However, it was mentioned to be "too high of a bar" to propose a "no build alternative." Therefore, priority must be given to mitigating traffic congestion and preserving the natural beauty of the farmland surrounding Van Lierop Park. The Reduced Intensity Alternative preserves the shoreline buffer and removes warehouses A and C from the map, but it is not enough. It is unclear to me if Alternative 2 is complaisant to the warehouses situated next to the park. Certainly, warehouses "F" and "G" cannot be constructed in any way as they completely block or interfere with the Mt. Rainier view from Van Lierop Park and sit on a wetland. These two buildings cannot be allowed to happen. A preferred alternative has to be clear that the buildings north of the park: B, D, and a smaller version of E are the ONLY warehouses that can be permitted to be built. These three warehouses have to be the maximum allowable through a preferred alternative. It also has be made clear that nothing could be built in the future on any areas of site constraints. Traffic revisions recommended in the EIS must occur no matter the outcome, but I am not convinced they are enough to effectively manage traffic flow from the industrial park and expected future growth. For example, the possible McDonald's, 7/11, coffee shop, and housing at the East Town Crossing currently being built across from Safeway on Shaw Rd. Tehaleh is also currently constructing a new multilane road project to better connect its growing community to Orting Highway in the McMillin area. I also worry it would be too hazardous to have heavy semi-truck traffic in an area with increasingly more pedestrians and bicyclists traveling to and from the park and the Foothills Trail. Thank you Puyallup for the EIS and providing the community a voice. Puyallup has to make clear in the final EIS the growth that has happened and future growth within this residential area cannot accommodate seven industrial warehouses. The "New Legacy" of Van Lierop Farm, with its stunning view of Mt. Rainier, is very special to the community and it has to be honored, not destroyed. I urge you to watch Pierce County Television's video on Youtube titled "Van Lierop Farm Begins a New Legacy." Thank you for your time and hard work.

**Commenter:** Baez, April

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Concerns regarding warehouses, Conservation of farmlands, Traffic revisions/concerns, Concerns of reduced alternatives

**Commenter:** Baghirov, Raechelle

**Source:** Webform

**Date:** 2/12/2024 8:20:51 PM

**Comment:**

Please don't build this. We as residents do not want this in our community. The impact on traffic will make it miserable for those living in the Shaw Road area and our local elementary school.

**Commenter:** Baghirov, Rae

**Source:** Email

**Date:** 12/18/2024 10:54:00 PM

**Comment:**

As a local resident I am extremely disappointed that this is still moving forward. We do not want this mega warehouse in Puyallup and it will only make it extremely difficult for residents who have to commute and live here to compete with the large trucks this will bring. The impact for our schools will be devastating for the school buses and families who have to pick up and drop off. Please don't build here.

Raechelle Baghirov

**Commenter:** Barr, Adam

**Source:** Webform

**Date:** 12/14/2023 10:25:07 PM

**Comment:**

Keep the warehousing out of downtown Puyallup. No more apartments either. Keep the Puyallup valley a small town with less concrete. Put the warehouses in fife or tacoma.

**Commenter:** Barry, Vanessa

**Source:** Webform

**Date:** 2/21/2024 7:06:00 AM

**Comment:**

I have lived in the city of Puyallup for 5 years and in the surrounding area for 24 years. Since buying a house in the city of Puyallup, I feel so connected to our town and all that it has to offer. I love the Rainier views when driving from Main st to Shaw rd and walking from Farm 12 down the trail and being surrounded by nature. The thought that all that land could be turned into a bunch of warehouses makes me sick. Too much of our beautiful area had been destroyed by warehouses being built. It adds nothing to the culture and community and destroys what makes the Puyallup valley so special. 100% against this!!

**Commenter:** Beck, Ed

**Source:** Email

**Date:** 3/4/2024

**Comment:**

I'm writing to voice my strong opposition to the proposed warehouses off of Shaw Rd.

When my wife and I were deciding where to raise our family we chose Puyallup because of its blend of suburban and rural areas. It was a nice city in which we felt had a good balance. I've been comfortable with most of the development we've seen in the past 30 years, but these warehouses are monstrosities.

Besides being environmental catastrophes, they destroy the iconic image of Mt. Rainier over Puyallup tulip fields.

I can say without a doubt that with the current plans we would not choose Puyallup as the place to raise our family.

Ed Beck

**Commenter:** Beck, Jane

**Source:** Email

**Date:** 3/11/2024

**Comment:**

City of Puyallup –

Please reject the plan for 7 additional warehouses in the Knutson Farms.

I am very disappointed to see plans for additional warehouses, I had hoped that we had all learned a lesson with the first warehouse on how it changed the character of the area. Previously the area was an attractive area to show visitors to our city, a family and farm friendly area. These were the qualities that had us move to Puyallup.

The area along E Main street where the warehouses are proposed has a nearly unobstructed view of Mount Rainier. This spot was used frequently in pictures of Puyallup, especially when daffodil were in season. With the new warehouses this would not be the view I would want to show people interested in coming to our city.

In addition to the above, an additional 7 warehouses would make the Van Lierop park very unappealing to visit, I am not sure why anyone would be interested in visiting what looks like an industrial parking lot. This would also cause us to lose more farm land, once that land has been paved over we will not be getting it back.

I am not sure that a Missouri developer cares about Puyallup.

Jane Beck

**Commenter:** Beeler, Kris

**Source:** Email

**Date:** 2/15/2024

**Comment:**

To whom it may concern,

I have been a resident of Puyallup for over 25 years and I was a teacher in the Sumner School District for 26 years. I drove the roads to Sumner and past Knutson farms daily and have seen first hand the increase of traffic over the years. I believe it would be a great inconvenience and safety concern for the citizens of Puyallup and Sumner to build more warehouses in the area!

I also walk and ride my bike from Farm 12 parking area and enjoy the beauty of the outdoor space and the bike trail. More warehouses will detract from this special location in our community.

I strongly support th Reduced Intensity proposal for development. We need to maintain our traffic safety and convenience and the beauty of our city.

Thank you,

Kris Beeler

**Commenter:** Belcher, Lore

**Source:** Email

**Date:** 3/15/2024

**Comment:**

I am against the 7 new warehouses. The traffic is already tremendous on the bridge and Shaw Road. During peak hours one has to sit through two or more lights at the bridge and traffic backs up all the way up the hill on Shaw road. The increase in traffic would be a nightmare.

One truck access to the warehouses is on the bridge, yet the turning lane for the access is not much longer than one semi with a long trailer which will also contribute to the backups if there are two semis.

The second access to the warehouses would be next to The Farm 12 restaurant, the start of the walking trail, and Van Lierop Park. The noise and exhaust would ruin the enjoyment outdoors. Farm 12 is a unique restaurant with large outdoor seating and its enjoyment and the enjoyment of the park and trail would be diminished.

I live in the Puyallup Highlands and drive Shaw road to the bridge and also over the bridge almost daily. This will be a nightmare and may affect our property values. I would never have purchased a home in the highlands if I had known this would happen. I am already unhappy with the current backups, but I can't imagine the horror once these warehouses go in and with it the additional tremendous increase in traffic.

My preference would be no new warehouses!!! But if this fails, a reduction to 3 warehouses and 1.00 million sq. ft. and the resulting reduction in trucks/cars would be the lesser evil.

As an aside: I go over the bridge to Sumner several times a week and see these huge Semis with trailers turn from the freeway exit on Traffic Avenue to the small streets towards Sumner. They take up both lanes to make a turn. It is scary. I also see sometimes more than one of them crossing the railroad tracks by The Old Cannery in Sumner, making slow turns across the tracks. By the time they have cleared, the light is read again and I have to sit through another light.

I know the city of Puyallup was not able to prevent this from happening, because it is Pierce County. However, could the City of Puyallup change its zoning so that no large tractor trailers are allowed during certain hours. That may curb the intent for so many warehouses. I don't know if bridge or the

road next to Farm 12 belongs to Puyallup. By limiting the semi truck ours to certain times, e.g. nights, it may help with the traffic.

Sincerely,

Waltraud Lore Belcher

Puyallup Highlands

**Commenter:** Benkovich, Julie

**Source:** Voicemail

**Date:** 1/31/2024

**Comment:**

Hello, this is Julie Bank of it's that's be like boy. B e n k OVI CH a resident of Puyallup. It is atrocious to cover up more Farmland. We need places to drain our water shed to cover with concrete and buildings is horrible. I can't imagine the traffic impact on the area. It's already you can't go between Trail up in Sumner on a weekday and get through all the traffic. I think it's a very poor idea. I'm sad that Knutson are Wanting to make that much money. Anyway, thank you.

**Commenter:** Bennett, Debi

**Source:** Webform

**Date:** 1/4/2024 2:20:17 PM

**Comment:**

Curious to know plans for infrastructure to handle this project. We are already dealing with a traffic issue in this area. Your thoughts please.

**Commenter:** Bennett, Andrea

**Source:** Webform

**Date:** 1/25/2024 12:14:07 PM

**Comment:**

Why this is an insane idea: 1) It is so close to the river and will have so much car traffic that there is no way to keep ground pollution out of the river. 2) There is nothing that could be done to alleviate the amount of traffic this will bring. It is already backed up during rush hour and school pick-up in that area. 3) For a city that prides itself on its small businesses I can't help but see how this is going to be a hindrance to Farm 12. The view alone is a selling point for events. Who will want to get married there when your back drop is a warehouse. Also the traffic will make it that much harder to get into it. Step by Step provides such a vital service we should do everything in our power to help and not hinder them. 3) So many walkers, runners and bikers use the trail there. It seems impossible to be able to guarantee the safety of those using the greenbelt. 4) There are other sites deemed useable for warehouses. Look out towards Five, etc. These places have been zoned specifically for this use. Why try to force this small site into warehouse use. I know we live in a changing world and farm space is often only as valuable as its monetary worth. But sometimes things should be left as is. What happens when our whole world is warehouses and we have run out of greenspace? Puyallup should keep its promise of this land remaining untouched.

**Commenter:** Bennett, Andrea

**Source:** Webform

**Date:** 3/7/2024 9:57:00 AM

**Comment:**

I have some concerns about the impact of these warehouses to our community. Here are some of them: 1) The amount of increased traffic. An estimated 900 to 1,250 large trucks will travel Shaw Road, East Pioneer, East Main, and Highway 162. An estimated increase of 5,000 to 8,000+ cars depending on which proposal is accepted. This traffic congestion does not include the apartment complex (and a McDonald's and 7-Eleven) that has started construction on the southeast corner of Shaw Road and East Pioneer. All of this means a lot of traffic impacting the safe access of students and parents to Shaw Road Elementary School and the Seventh Day Adventist School located on Shaw Road. 2) Impacts to Farm 12/Step by Step and Van Lierop Park. Farm 12 has worked hard to put in a program to help lift low income mothers out of poverty by giving them employable skills. They have built a teaching kitchen, restaurant, event center, and bakery. Building the main access road to the warehouses on the little traveled road on the west side of Farm 12 would greatly increase the noise, traffic congestion, and access to their site. Having a warehouse just north of the restaurant and a warehouse blocking the view of Mt. Rainier, means due to noise and lack of a view that dinners and people planning events would hesitate to book there. 3) Impacts to the Puyallup River. Storm water from the warehouses is to be filtered and run off into the river. The existing warehouse has already experienced some failures to its filtering system. Do we really want to risk contaminated water from trucks and the warehouses going into the river? How hard will the impacts be to the flora and fauna supported by the river? 4) Need for the additional warehouses. Do we need additional warehouses? The existing warehouse on Shaw Road is not fully in use. Another new warehouse was also built to the west of Shaw Road on East Main. It is not fully in use either. There are designated industrial sites nearby (Sumner, Frederickson, and Fife). My understanding is those sites are not fully utilized either. 5) Infrastructure impacts. There would need to be substantial changes to infrastructure on East Main to the freeway on ramps in Sumner, Highway 162 (proposed traffic circle for large truck movement from East Pioneer), East Pioneer, Shaw Road and the roads around Farm 12, Van Lierop Park and the Foothills Trail. We have so many other places that are already set up for warehouses that a no change proposal is my main choice but if we have to have some warehouses, I think the least impactful would be the preferred alternative put forth by the citizen's group. In an ever changing world let's keep some of our land beautiful and unmarred by a modern touch.

**Commenter:** Bennett, Danielle

**Source:** Email

**Date:** 3/14/2024

**Comment:**

The Knutson Farm warehouse proposal is unacceptable for this location! Please see the preferred alternative. This must be scaled back to mitigate traffic impacts, protect farmland, preserve the quality of our wonderful Farm 12 and Van Lierop Park, protect wetlands and wildlife, and avoid killing salmon with harmful stormwater runoff. The attached map is a recommended alternative. [image of reduced intensity alternative]

**Commenter:** Berg, David

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Please adopt the reduced intensity alternative proposed to try to protect existing greenspace, farmland, and parks. The continuous creep of warehouses onto the land in that area, along with the traffic they would add to a corridor that cannot handle existing traffic, should be opposed. The idea that Puyallup will build a new park in the area, and then surround that park with warehouses, is absurd.

David Berg

20 year Puyallup resident

**Commenter:** Bero, Diane

**Source:** Webform

**Date:** 1/4/2024 1:56:31 PM

**Comment:**

I was never for this large truck farm. A roundabout on Hwy 162 & 80th is detrimental to everyone who uses Hwy 162. The traffic will slow to a standstill with semi trucks trying to make the corner, resulting in backups at both Hwy 410/162 interchange and further east on Hwy 162. Currently, traffic is backed up to East Pioneer in the mornings waiting to access Hwy 410. Traffic would be about 30 miles of trucks (truck length x # trips) which will have a severe impact. East Pierce County is already choking with traffic with standstill traffic even on weekends. Another lane needed on Hwy 410, Hwy 167 and Hwy 162. Using rails makes more sense, but must have an overpass over 80th so traffic can move. Improvements to Shaw and E Pioneer will need improvement to accommodate more train traffic. Shaw Rd backs up to Main St. Retiming lights will not be much help as it takes semi truck so long to get going that any gain will not be realized. Losing the view of Mt Rainier should not be allowed for those using the new Van Lierope's Park. This project is impacting everyone who lives or drives through the area, so basically, all in Puyallup/So Hill, Sumner, Orting, Graham/Kapowsin/Bonney Lake. Hwy 162 has never had the attention/money needed to make it a good transportation road for trucks.

**Commenter:** Bittenbender, Caryl

**Source:** Webform

**Date:** 1/21/2024 8:12:58 PM

**Comment:**

Please do not allow all these warehouses to be built on the Knutsen Property. It will cause more congestion than our community can handle. There is a need for community sports fields - why not put up some community place for soccer fields, and other needed fields and recreational play fields or courts for pickle ball. The loss of farmland is also such a concern. Overall, these warehouses would be a blight on this land and the traffic congestion that it will bring to this area is unacceptable and ludicrous.

**Commenter:** Bledsoe, David

**Source:** Webform

**Date:** 1/17/2024 11:58:39 AM

**Comment:**

My concern is that approximately 5 acres of Knutson property abutted against my property on the Puyallup River. I have met with Aron Roger's son in law a several encounters over the last 4 years to maintain there parcel that affects my property, from overgrowth weeds animal infestations and. They never responded or took action to resolve these issues. I do not want to have to also put up with the proposed development that the EIS . Any and all suggestions and support would be incitful!!!

**Commenter:** Blondino, Sandra

**Source:** Webform

**Date:** 1/22/2024 5:15:36 PM

**Comment:**

So looking forward to this connection. I'm in support of completing this link.

**Commenter:** Blythe, Leanne

**Source:** Email

**Date:** 3/11/2024

**Comment:**

Please reconsider the approval of the building of 7 Mega Warehouses for the Knudson Industrial Warehouse Project.

I oppose these large warehouses being built on the Shaw Road Area Farmland. This is a beautiful, lush valley with amazing views that would be better used for Parks, Sports Complexes and smaller businesses that provide services to the residents in the surrounding communities. These warehouses would create extra traffic with large trucks coming and going and add to the gridlock on already traffic congested roads; East Main, Pioneer and Shaw Road. Shaw Road in particular is only 2 lanes from 1 block South of Pioneer to 23rd Avenue and can't handle any more traffic. As it is, the traffic backs up all the way to East Main during the evening commute.

While my preference would be to have no new warehouses, I plead with you to consider building no more than 3 smaller warehouses built closer to the railroad tracks and away from the foothills trail.

Sincerely,

Leanne Blyth

**Commenter:** Booth, Michelle

**Source:** Webform

**Date:** 1/8/2024 8:54:32 PM

**Comment:**

This is a terrible idea. We need more greenspace, not more ugly warehouses. Why doesn't Puyallup, Sumner and Orting incentivise farmers to keep farmland? This is close to the river which floods and erodes. 512 and 167 can't handle more trucks either. Resential landscapes are getting ruined with all of the pollution and overhead these operations cause. I vote no on all of this. Big push to go green with cars but this is ok? It is a crime what you are doing to our area.

**Commenter:** Bowers,

**Source:** Email

**Date:** 2/26/2024

**Comment:**

Building on the most fertile ground in the valley can only be described as greed by the local government to destroy it. Further more the traffic would be impacted by already congested and at capacity roads. You will raise our taxes because of more commercial property and then raise it again when it's determined the infrastructure won't support the increased traffic. Puyallup is large enough and doesn't need growth just to satisfy the city's father's power interest!

**Commenter:** Bowes, Amanda

**Source:** Webform

**Date:** 1/30/2024 5:20:17 AM

**Comment:**

Build in the Port of Tacoma! Farmlands do not need warehouses but the port is set up for the space, the traffic and environment

**Commenter:** Bowser, Mitch

**Source:** Email

**Date:** 2/24/2024

**Comment:**

The fact that this project exists is insane. The ecological damage that this would create without any real economic benefit outside of some out of state Missouri kid is beyond offensive. This will add cars/noise/damage the existing environment and harm existing residents through negative health outcomes, negative quality of life outcomes and negative economic outcomes making Seattle even harder to get too.

This is truly shameful.

Sincerely,

Mitch B. a Puyallup resident for many years

**Commenter:** Braaten, Carole

**Source:** Voicemail

**Date:** 1/17/2024

**Comment:**

Hello, my name is Carol Sue Brayton, B -R -A -A -T -E -N. My cell phone number is [REDACTED] I'm calling to get and obtain a full paper document of the Knudsen Farms, and I noticed that you did not list that the International Building Code has for well over 30 years prohibited building in any liquefaction soil, and that is in their code. And I noticed that you've done a lot of development and you haven't enforced the building code for that. Also, it's been illegal to build in a volcanic eruption area since 1974, the Stafford Act, and as you realize the Growth Management Act, and you are in the Lahar area, and the area is supposed to be zoned, and you can call the GLG Department and State Emergency Management.

You are supposed to zone it commercial farmland or open space. I know I passed the test over 45 years ago. So I would highly recommend that you people get on track because you cannot zone it, and you are 100% liable for every building you have built in the city of Puyallup in Lahar.

There is no Lahar insurance. The volcano insurance is only for ash, and that is outside the Lahar area. So you better have a bond for every single one. And also, like I said, absolutely no building in liquefaction areas.

There's no possible engineering, and you can talk to the state engineering experts and the ones to write complaints to about the engineers who've said that they can build to engineer and design that.

I would highly recommend that you immediately turn every engineer in because it's illegal to build in those areas. And anyway, but I'd like a copy of the document. And also, I know I noticed you didn't mention the two dam break areas.

electron dam and mud mountain dam, and the fact that it can be a nine -magnitude, five -minute shake time, and the 49 earthquake, which liquefied the valley areas. So the valley sunk over 60. That's why the valley is supposed to be zoned commercial agriculture.

So also, the floodway of 1930s, my mom, 98 now, lived through that when the whole entire, from one side of the valley to the other, all the houses were flooded, the things went down, houses were going down, the whole port was entirely flooded, and people lost lives.

In fact, I actually met and know one of the people that her uncle was one of the children killed and swept down river. And there's been an emergency evacuation out, and the United States Geological Survey, not geological, the, U .S.

Army Corps of Engineers decertified the levee back in 2007. They actually came down to Fife, where I live, and they decertified the whole entire levee because it can break and because there's been illegal development, putting pressure against the levee.

So I know, and I was there with the two engineers, and the person that happened to be there speaking from there was my upperclassman from Washington State University Soils. He was a soils expert talking about the levee.

So I think you people need to go look at some laws. And the pipeline setback area is 660 feet each direction, has been since about 2003 and for natural gas. And there is no natural gas pipeline supposed to be anywhere in any liquefaction soil.

It's prohibited because of what happened in the 1989 earthquake in San Francisco when the liquefied area, the area liquefied during the San Francisco earthquake and pipes broke all over. And there was major fliers and gas line breaks.

And they had problems putting it out. I think you need to check your laws and reconsider this because liability is extensive at this point. And again, I would highly recommend calling in some state experts and looking at the design because you don't need it.

And you have violated the National Building, International Building Code for a long time, along with others. Because we are a home rule state, means you are the enforcement for every state law. And I think that you need to know that.

**Commenter:** Sue Braaten, Carole

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Carole Sue Bratten, C-A-R-O-L-E S-U-E

B-R-A-A-T-E-N, Fife, Washington. My major is agricultural science. I also worked on the Growth Management Map over 35 years ago. This is not where we wanted to go. By the way, Washington state holds five of the top

tillable agricultural soils in the entire world. We are standing on it down in the valley. That's where it is. If you want food resources, that's where you have to go. When you destroy them, you destroy the food for everyone, not just this place. I want you to know that is the first and foremost.

Second, there is a geological hazard area. It

means that -- and also, the building codes and a lot of the laws are taken out of context. Every geological hazard area means that the Washington State Department of Geology has determined that those areas are too dangerous to build in for human habitation. That valley soil grows great food crops. It's one of the world's most dangerous liquefaction soils there is. It's high. The water table is high. It causes buildings to topple, sink, and collapse. And it's against the building code, the International Building Code, it has been for over 30 years, to build in any liquefaction area. It's against the Growth Management Act to build in any liquefaction area. It's been against the Stafford Act, which I had passed in the late 1970s, which was written in 1974 by the Federal Government. All seismic hazard areas are illegal. Volcano eruption areas, Mount Rainier erupted in the 1840s. Erupted again in 1870 and one more time. We're not talking recent. You have two dam break areas, Mud Mountain Dam, Electron Dam. If that comes down, you cannot evacuate the people out. I live in Fife. It's scary and terrifying to me. I have warehouses behind me. I can tell you about warehouses. I'm going to have to take my mom and walk out of there if I can make it. I'm surrounded by landslide hazards. I'm in a tsunami zone and a lot more in the lahar. You put more people in danger down here. It's incompetent. This is a no go.

As of September 1990, the Growth Management Act, that developer had a right to change that property and move

his right to develop in a developable area. It's considered unsuitable. Get the experts in. Let me see that engineering seal of somebody who says it is. I'll turn them in. It's time we stop and think of who we're putting safety those people have to evacuate down out of that valley. Now, flood plain engineer Dennis Dixon told me they figure one house for every 20 acres and they can barely get people out during a flood. Think of a lahar.

Farmhouse. Maximilan Dixon, who runs the

Emergency Management, says nobody listens. Nobody. Time has come. We all have to stand up. If you want to do a favor, make your (indiscernible) comment out five miles. There's a liquid natural

gas plant that can explode and hit here. People need to start standing up and saying the word no. It's not that we don't want development; we want competent development. The water aquifer is down below us, but the problem is you're running water into that already decertified -- I know because the United States Army Corps of Engineers decertified that levy in 2007. I was at the 2006 meeting. The person who spoke was my upper classman in soil science. He was part of the teaching group. They told them to stop building warehouses. You're putting pressure against the levy. I was at that meeting. It's time we start to look at what dangers we're putting everybody in. You have to evacuate. Now, those fields, they sink, topple, and collapse, and you say it's a buildable soil. You're a liar. You better have that bond. But if you turn it around and say we realize, we're going to call the State Department of Geology and have the experts come in. What of the buildings are actually not buildable and didn't get built to code? What did the land developers do? That's where we need to go. And it's that dangerous. It's time we stop playing with people's lives. It's not just because I'm an agricultural science major. Almost everybody's dead off of those committees that I was in. I was the youngest person. I expected to see the valley almost all agricultural soil. Why? Because it's too doggone dangerous for people to live in.

I don't want to see a building collapse on workers. I don't want to see a landslide like Oso comedown. I don't want to see the dams break during the nine magnitude earthquake that we're supposed to have in the valley liquify when my grandfather and two uncles worked the Port of Tacoma and my grandfather was in the ship that moved. Three days later off Gig Harbor came a six tsunami. In the 1930s, when my mom was a little girl, she stood and watched with my grandfather as all of the Puyallup Valley, from one side to the other, was flooded so much it took people down the river. People's lives were lost. People's homes were lost. It's time you start looking -- the entire port was flooded. We almost had that in the 2009 evacuation. Ask both Maximillion Dixon and Dennis Dixon. He stood in the emergency -- I talked to him -- he stood in that emergency office. He was the one who was calling whether I-5 got closed. We had a mandatory evacuation out of Fife. Mandatory. You don't get to stay. It's no choice. You leave. How are you going to get everybody out of here right now? That's what I want to see. I want to see a cumulative impact of say each and every house. Each and every thing. How are you getting the children out? Is that school safe to be in in a liquefaction area? Will they collapse on the children? And not only that, the Pipeline Safety Act 2003. Guess what? 660 foot back. What happened to everybody? Nobody's following the rules anymore? We don't care about laws? The tanking for different tanks, like the LNG that's out in Port of Tacoma. Like the tank that's over in Fife. The aboveground tank ended before 1972. My God, people. We've got to start looking at what we've done to ourselves. How many people are in danger from those tanks exploding that don't even know it? Or the pipelines in the liquefaction area? It's time we do a hazard management, a water aquifer runoff, hydrologists. Get the state hydrologists. Get the experts down here to talk to you. Not somebody like me. Not like somebody that others that don't look at the actual impacts. I will tell you, Maximillian Dixon told me

personally, he said, I'll go anywhere they want. I talked to Tim Walsh, who was then considered the national tsunami expert at Department of Geology. I didn't even know who at first I was talking to. He spoke to me for a long time. I found out through a public request of information I was in a tsunami area in Fife. I knew of a lahar. I knew of the liquefaction. I pleaded with them to look at the building codes. Pleading. You cannot make the area stable enough. They will still sink, topple, and collapse. That's why it's prohibited. Thank you for your time. Thank you. I appreciate you.

**Commenter:** Braaten, Carole

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concern

**Commenter:** Braaten, Carole

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear Pierce County Planning Dept. and City of Puyallup Planning Dept.,

1. Under FACT SHEET and elsewhere Critical and vital Information has been left out of the Knutson Farms Industrial Park Project Draft Environmental Impact Statements/DEIS not intentionally but unknowingly Making the DEIS Inadequate according to R.C.W. 36.70A State laws Full copies of state and federal laws full copies of the laws , maps and information for the public in professionally done documents for the people need to be included also is a complete and total Rebuttal to Anything stated in the FEIS after it is finished before closing so citizens and people are allowed to disbut information given:
  - a. R.C.W 86 -**Floodplains/ Floodway** and the federal laws on floodplains 1930's, National Flood Insurance of 1968 Act limits any building whatsoever in Any known Floodway or Flood plain of 500 years scientifically mean it can happen every 7-8 years. The Zoning MUST be Farmland, Commerical Agriculture or opens Space. The definition of Farmland is land surrounded by pieces of Acreage.
  - b. R.C.W. 36.70 Planning act... zoning hazardous earthquake areas/Liquefaction soil - as Commercial Agriculture. 1952 due to the April 13, 1949 Tacoma earthquake that liquefied the entire Port of Tacoma and Puyallup Valley, and elsewhere. Liquefaction Causing building to sink, topple and collapse, cracks, soil sinking, sand boils, sand volcanos, sinking areas by the Puyallup River.
  - c. **Federal law Stafford Act of 1974- Mitigation of hazardous areas Prohibiting building in these areas**
    - i. **Volcano Eruption areas,**
    - ii. **Lahar areas,**
    - iii. **Dam Break Areas**
    - iv. **Mud flow**
    - v. **Debris Flows**
    - vi. **Erosion Hazard- including abandon Oxbows of River Bends**
    - vii. **Earthquake fault lines**
    - viii. **Liquefaction Soils- Soils that cause buildings to sink topple and collapse during major earthquakes**

- ix. **Landslide hazards/Seiche**
  - x. **Floodplains**
  - xi. **Storm Surge**
  - xii. **Tsunami Hazard**
- d. R.C.W. 36.70A - full law Growth Management Act so people can read the entire document.
  - e. **R.C.W. 36.70A Definition Geological Hazard Areas- Are Not suited to any residential, commercial or industrial development due to the threat they pose to human safety. ....**
  - f. Make Note R.W.C on the state Geology Division has them map **All Geological Hazard areas that are to Dangerous for Any residential, Commerical or industrial development**
  - g. **W.AC. 365-190, 195,196,- ---Critical Areas Must be Protected , Agricultural lands Preservation, Geological Hazard Areas, Flood plains, Wildlife habitat, Wetlands and Water Aquifer Recharge Areas**
  - h. **Best Use Land Management , Using Best Available Science not Best Available Revenue for the land developer or local lead agency or County,. R.C.W. 43.21.C safety, human welfare, health, human Quality of life ,etc.**
  - i. R.W.C 43.21C- including public safety, welfare and quality of life- can be used to Deny any land development use by citizens.
  - j. **International Building Code/ IBC- the Code has Prohibited building in any Liquefaction soil for over 30** years this this is well Known by All land developers, builders and Geo Engineers and Geological Technical Engineers and Geologist.... IBC state decades ago that .... The Knutson Farm is unbuildable land for any industrial endangering human life and the developers know this the maps are fully listed in the International Building Code and have been for decades. Zoning is Commerical Agriculture the land developers knowingly purchased land only useable as Farm land they have no come back on the county Planning or other planning whatsoever because they do not have the IBC code book and rely on the developer to up hold the laws which they have NOT.
  - k. **R.C.W Natural Pipeline safety laws and setback codes 2003- No building in any Liquefaction soil also the set backs required of at least 600 or more feet for public safety.**- Only 1 county and 3 cities have up dated their codes for compliance. Any and all natural gas Pipeline in Liquefaction soil should be removed and State and federal Experts in the field should be called in for safety reasons...just due too pipeline safety.
3. City of Puyallup Taking the lead agency is Important in Exposing that Prohibited and illegal building, lack of explaining Immedient Threats of danger, disastrous, injury and harm, extremely harmful impacts, land management, wrong zoning, Accumulative Environmental Impacts and more needs to be taken on by the city of Puyallup Planning dept. what they are doing and going to the state Court can save thousands of lives that have been put in imminent threat of danger by illegal land development by developers..

4. Although well done the Knutson Farms Industrial Park Projects DEIS misses information some which is above for all readers to be adequately and sufficiently informed and to make the DEIS adequate and Comply to and with the laws R.C.W. 36.70A Growth Management Act and W.A.C. 365-190,195, 196 which I personally worked on the round tables that created the law.
  - a. The SEPA /State Environmental does not have to be in the order that is on the SEPA 197 if Any the land site location is found to be in ANY
    - i. **Geological Hazard Area mapped by the State of Washington Division of Geology, the United State Geological Survey/USGS, Soil Conservation maps- if just one hazard is found the property MUST be Zoned either Commerical Agriculture or Open space for Public Safety**
    - ii. Any known 500 year Floodplain/ Flood Way
    - iii. Prime Agricultural Land-Which is the Knutson Farm land,
    - iv. etc or a combination
    - v. Also A look back on properties that may have been illegally developed because Geo Engineers and Land Developers did not state on the documents that the **building could never be sufficiently engineered to be safe and the land was to remain Commerical Agriculture or open Space to**
  - b. Proper Historic information and understanding of dangerous areas and **Geological Hazard that CAN NEVER Be SUUFFIECNTLY Mitigated by Geo Engineering needs to be understood....** Many Geo Engineers will never tell you or explain to the Public or to Local Jurisdictions or Councils this fact about Geological Hazards that are mapped, **but they avoid it that mapped Geological Hazards Never can be Sufficiently Mitigated to be safe. By law in Washington state if anyone misleads either local jurisdictions or councils on these facts they can be Criminal Charged with misleading them.**
6. Evacuation routes maps out of the Valley for dam breaks, Lahars, Flood plains etc that warehouse interfere with a mass evacuation and escape from the Valley floor. More open land is needed for public safety
7. Above are just some of the information that has to be included. Also, Washington state is a HOME RULE STATE and state and federal Agencies that should be commenting have to be invited to Comment.

I personally have more knowledge and understanding of reading and totally comprehending the Draft Environmental Impact Statement/DEIS this needs an Extension of at least 6 months for citizens to be able to obtain full copies of laws , maps and information and to contact other people/agencies that were not included and should be:

1. **State of Washinton, Department of Natural Resource Division of Geology- their Geologist, Geological Engineers, Hydrology , Landslide hazard**

**experts etc.** ask them if any of the land is usable for any building type of building...  
a hay barn is different than a warehouse

2. **State of Washington Department - Emergency Management - Maximillion Dixon or the person that holds that position now.**
3. **Pierce County Emergency**
4. **Pierce county Flood Plain Engineer - Dennis Dixon-** about the January 2009 mandatory Evacuation due to flooding of the entire Puyallup Valley from Orting, Puyallup, Unincorporated Pierce County, Fife, and Port of Tacoma.... Current status he says that just for flooding there should be no greater density than 1 farmhouse per every 20 acres to be able to escape from the valley.
5. federal United States Soil Conservation and Pierce Conservation district and the State of Washington Department of Agriculture.
6. **Puyallup Tribe of Indians - Salmon Impacts a**
7. Federal migratory birds and Water fowls EPA experts - of overwintering habitats.

My background is from Washington State University/WSU I am an Agricultural Science major which entails a broad field of scientific disciplines including but not limited to: Soil Science (many of the classes the same one that building geo engineers/geological technical engineers take, I passed engineering section), Crop science, Hydrology(water)/Drainage/irrigation, Animal Science, Forestry, Agricultural Economics, Statistics, Biology, Botany, Adult Continuing Education and more. I have been Critical Area Certified for years originally dating back to the late 1970's so I have an understanding of Critical areas that are Agricultural lands, Flood plain areas, Geological Hazard Areas/steep slope, that some can kill, injure and harm people.

I am requesting an extension of the period of time for public comment:

8. Many of the agencies both state and federal who have extraordinary expertise on this land site location and surrounding areas were never invited to comment on this site location and need to comment on this land site location and document. These agencies should have been the first to contact.
9. **FACT SHEET** - should have start off with Location and history because both make **the Land Zoning incorrect as Any Urban Growth or Employment Center/ EC or EZ zoning and Prohibit this zoning according to state laws R.C.W 36.70A, W.A.C. 365- 190,195,196, R.C.W 43.21C, R.C.W. 86, Stafford Act of 1974, International Building Code/IBC...Knutson Farms was correctly zoned as Commercial Agriculture and should stay that way complying with state and federal laws.....** why does the Land developer, the County and the Geo Engineers think that the Knutson Farm location is outside of any Geological Hazard Areas Please Explain in detail use state maps and USGS ? Please Supply All of the peer Reviewed by Geologist state and federal in Oral and written documents provide the dates the publication and the reviewers names, their qualification, where and when the presentation happened who did it and when of why you disagree with the State and federal Geologist and when they state and federal geologist changed the maps to agree with the Industrial, commercial and resident Zoning? To comply with W.A.C.

365-190,195,196... year done. Other wise the zoning can not be industrial and must be Commerical Agriculture or Open space.

- a. History of the Area of the site location and surrounding areas making the land site location prohibited, unsuitable and illegal to developed any residential, commercial or industrial zoning and only zoning of Commerical Agriculture or Open Space for:

- i. **For thousands of years the Puyallup Valley has been located in the Volcano Eruption, Lahar, debris flow for Mt. Rainer the last larger eruption was in the later 1840's the largest eruptions 5,000 years ago covers the Valley floor to Port of Tacoma in 30-60 ft of hot mud as Mt. St. Helens did in May 18,1980. Some state there have been minor eruptions in the 1870's 1890's and later...Knutson Farms is in a Geological Hazard Area that can never be suffiencetly Mitigated by Any Geo Engineering whatso ever making the land unsuitable for residential, Commerical or industrial development R.C.W. 36,70A, W.A.C. 365-190,195,196, R.C.W. 43.21C Public Safety.** The Zoning can be Open Space or Commerical Agriculture No Warehouse can be allowed in this area it is Unsuitable and Prohibited... Any geo engineer that says that they can do so needs to be turned in to the State of Washington Engineer Licensing on a Complaint Form which is above .....Provided to me By the state Engineering/ geologist Licensing for the State of Washington.

1. **NOTE\*\*\* Tim Walsh Assitance Geologist (2<sup>nd</sup> in command at the state Geology divison until he retired ) who I have personal spoken with put up All of the Volcano Erutpion Evacuation signs out of the Puyallup Valley in 1987.** Local jurisdiction per state laws on volcano erutpion areas for safety were contacted with all information after the Mt. St. Helens eruption of **May 1980 after the state Legislature ruled in Sept of 1980. Rapid evacuation out of the Valley for the people of the Valley is needed during any major event.** The hillsides could fail. Warehouse now pose a major threat to the evacuation of all the Valley truck congestion mass evacuation slowing rapid escape from the Valley floor. More Farms and open space are needed for Public safety. Are you stating that all the Volcano Eruption Warning Signs Must be taken down and are Wrong and that there is no danger?

- ii. The Puyallup Valley has a history of historic Flooding that has Catastrophically damaged the Valley in the past. The Knutson Farm is located in the 500 year Flood way/ flood plain meaning the land site location can flood every 7-8 years. The area has repeatedly flooded most recently over topping areas of the Levy in 2009 etc.1917 the United States Army Corps of Engineers finished the Straightening of the Puyallup river creating the Levy which Knutsons farms is by that

year the highest known flood went over the banks by over 30ft high from on side of the Valley to the other side . Electron Dam was put in to mitigate the flooding in about 1924 to control flooding of the Puyallup River only to in the 1930's when my mom was a child to watch the Valley flood again in the 2<sup>nd</sup> highest flood Flooding the entire Valley and Port of Tacoma. In January 2009 I and my mom and thousands of other people in the Puyallup River Valley from Orting, Sumner, Puyallup, Fife and Port of Tacoma were under a DECLARED MANADTORY Flood Evacuation which Dennis Dixon was overseeing along with Emergency Management...the Cities were ALL unprepared and it was everyone for themselves by the time emergency locations were set up by unprepared cities.... Knutsons Farms is in the Historic Flood Plain of the Puyallup River Valley and can not be zoned EC or Industrial under the Flood Plain Act R.C.W. 86, Stafford Act of 1974, R.C.W 36.70A Growth Management Act, W.A.C. 365, R.C.W. 43.21C the land was Correctly zoned Commerical Agriculture or must be zoned open space. How Will you evacuate people with the truck traffic congestion during a flood, dam break and lahar answer each?

- iii. Historical water from the higher elevations of the Pierce County flow into the Puyallup river Valley setting in the Valley soil causing a high water table of the soil called Liquefaction. This is the Soil which make the Knutson Farm so good for growing Prime Agricultural Crops. Liquefaction Soil is considered on of the most dangerous Soils in the world because the soil turns to liquid during major earthquake violent shaking causing buildings to sink, topple and collapse especially industrial and high rises. There is No known Geo Engineering for Liquefaction soil the **International Building code** Prohibits building in ANY Liquefaction Soil whatsoever for over 33 years and the San Francisco Earthquake and the Marina District damage. Liquefaction Soil can NOT be zoned residential , commercial or industrial only Commerical Agriculture or open Space. Knutsons Farm was zoned Correctly as Commerical Agriculture and no warehouse can be located in this area The Stafford Act of 1974 prohibits building, R.C.W. 36.70A, W.A.C. 365 -190,195,196 Best Available Science, R.C.W. 43.21C Public Safety. In April 13, 1949 a major earthquake hit the entire area the Puyallup Valley Liquefied and did so again in the 1960's quake, in the 2001 Jan quake the Valley was seconds from liquefaction. Is the Developer, the builders, and geo engineers stating there is no liquefaction soil and the soil is safe to build on? Is Pierce County Planning stating that there is no liquefaction soil and the land is safe to building on yes or no? The zoning is Wrong if you disagree with the state maps you MUST PROVIDE to all the people the new approved maps, the oral reasons, the documentation, who did it

when and where the information was published who peer reviewed the information Provide all please/

- iv. Knutson Farm is located in 2 dam Break areas Electron Dam and Mud Mountain Dam that can fail during a major earthquake event sending the lakes behind the dams flooding into the Valley floor where the warehouse would be located. It has been Prohibited by federal laws to build in any dam break areas due the potential lose of human life those areas are to be zoned Commerical Agriculture or Open space no known Geo Engineering is possible.
  - v. The Puyallup Valley and Knutson Farms is located the major Earthquake region of the Cascadia Earthquake Subduction Fault goes of every 250-300 years January 26,1700 and the Seattle fault line. Both putting out 9 magnitue earthquake at a 5 minute shake time and are over due for the Pacific Northwest these will take down every bridge from Souther British Columbia Canada to Norhtern California and cause all the Puyallup Valley to Liquefy far, far worst than in 1949 causing Catastrophic Failure to especially Commerical and industrial buildings and residential buildings. Knutson Farm can not be zoned urban anything or commercial or industrial any one who states so should be turned into the State of Washington Engineer Licensing so they can investigate the person and remove the license and fine them and investigate all buildings they have signed off on . International Building Code/IBC also has a shake intensity map .
  - vi. The Landslide Hazards on both sides puts the property at risk. Especial over on the Bonney Lake side where a massive Fire Scare burn damaged Vegetation and trees because of the hill elevation and destabilization the hill could give way like Oso, Washington slide only one of the state of Washington Division of Geology or the United States Geological Survey experts with can access this area they have the correct license endorsements. The run out could be massive and toward Knutson Farm site.
- b. The property land site location is right on the Already DECERTIFIED in 2007 because of over building Puyallup River Levy. I was personally at the Valley wide meeting in Fife and Puyallup as the United states Army Corps of Engineers told the Cities and others in January 2006 to Stop building on and by the Puyallup river Levy and by the Oxbows/ the Abandon Puyallup River Channel when the U.S. Army Corps of Engineers strainghted the Levy. My upper classman from Soil Science WSU, was the presenter for the Soil Scienst part along with 2 Engineers. We talked later about the cities and others refusing to stop building putting Hydrological/ water pressure against the LEvy by infilling causing damage to the River Levy causing our Flood Insurance to increase.... Zoning this area industrial is totally endangering down stream citizens on the Levy even more and is reckless endangerment of human life and structures...Any more Building but the Levy can Cause

Levy Failure by the many floods that we have had ... This are must stay Farm Land or open space for public safety.... The Water Run off from these warehouse will run down River all thew Fife Warehouse and other Puyallup warehouse contributing to Valley flooding have not had an Accumulative Impact Statement Done in 30 years. I and thousands of people who have lived in the Puyallup Valley for decades lives are being put endanger of flooding and

- c. A Accumulative Impact of the Entire area is need for the River Levy, Agricultural Soil Impacts and more adding the entire County together. before any further building, for Agriculture, hydrology, food production, Wildlife habitat loss and more... Truck traffic impacts health and safety.
10. The Entire Puyallup Valley according to the United State Soil Conservation maps has some of the Worlds most Valuable Tillable Agricultural Soils. Puyallup Valley soil is 2<sup>nd</sup> only in the Nation to the California Valleys for food and Crop production per acer. World wide it is considered 3<sup>rd</sup> to 5 most Valuable soil for food production and Agriculture. Of the top 20 Agriculture soils in the World , Washington state has 5 of the top tillable Agricultural Soils at one time estimated Washington state could feed the entire Continent of Africa by its self.... Best Use Land Management, NOT best revenue , Using Best Available Science is Required the Worst possible use the this Prime Agricultural Soils is a warehouse. But it is said by some because of the Lahars and flooding we have the 1<sup>st</sup> to the 3<sup>rd</sup> deepest tillable agricultural soil going hundreds of feet deep.
11. The Knutson Farm is located on the Water Aquifer Recharge areas that Must Be protected by state laws and federal laws... Massive areas of the Valley have be put under concrete and asphalt causing the loss of water evaporation going into the upper atmosphere and into the jet stream and
12. The Zoning is ALL Wrong
13. Not enough documentation on Wildlife impacts....
14. Hydrology Impacts down stream, Levy impacts.
15. Quality of life,
16. Noise and pollution from etc Warehouses , the warehouse are right behind my home in Fife the Impacts to life would make up and should be in a entire other Supplemental Impact Statement, Traffic interfering with mass evacuation, noise day and night, water pushed on to other surrounding properties flooding those properties
17. Ther can be no ponds the Water table in the Valley is to high and the warehouse and land development is displacing water off site to surrounding properties, farms lands causing more flooding.

Extend the time for more people to comment especial Food Banks

Thank you,

Carole Sue Braaten

# Complaint Form

File a complaint against a person or business, or report unlicensed activity.

Online: [professions.dol.wa.gov](http://professions.dol.wa.gov)

Email: [complaints@brpels.wa.gov](mailto:complaints@brpels.wa.gov)

Or mail: **Board of Registration for Professional Engineers and Land Surveyors**  
**PO Box 9025**  
**Olympia WA 98507-9025**

Include the following:

- A detailed explanation of your complaint. This should include dates, other parties involved, and a summary of any efforts you have already made to resolve the problem. Describe events in the order they occurred.
- Copies of all documents that relate to the complaint.

For questions or language help call: (360) 664-1561

## Person or business you are filing a complaint about

Profession or type of business		
<input checked="" type="checkbox"/> Engineer	<input type="checkbox"/> Land surveyor	<input type="checkbox"/> On-site wastewater designer/inspector
PRINT or TYPE Name (Last, First, Middle) Hatefield, Alan		License number (if known) unknown not licensed in 2015
Business name (if any) Ecology and Environment, LLC Inc. ///Braemar Technical Service LLC///Chicago Bridge and Iron		
(Area code) Phone number and extension	(Area code) Fax number	Email or web address
Business address 333 SW 5th Ave/// Braemar Unknown///Chicago unknown		
City Portland ///Braemar unknown///Unknown	State OR	ZIP code 14086

## Your contact information

Name (Last, First, Middle) Carole Sue Braaten		
Business name (if any)		
(Area code) Phone number and extension 253-380-1247 cell	(Area code) Alternate phone number 253-922-5431	Email address csbpigeons@hotmail.com
Mailing address 2410 Berry Ln. E.		
City Fife	State Wash.	ZIP code 98424

## Complaint summary

Provide a brief summary of your complaint. Attach an additional sheet if necessary. Puget Sound Energy/ PSE Liquefied Natural Gas/LNG Facility illegal located in dangerous Geological and Seismic Haza
--

The information I have provided above is true and correct, and I have provided all required enclosures to which I have access.

Carole Sue Braaten

TYPE or PRINT Name

**X**

Signature

January 1, 2023

Date

# FLOODS AFTER FIRES

## WHAT YOU SHOULD KNOW ABOUT ALLUVIAL FANS AND WILDFIRES



If you live or work on an **alluvial fan** you could be at risk from potentially dangerous debris flows and floods, especially if you are near a recent wildfire burn area.

### What is an alluvial fan?

Alluvial fans are broad, gently sloping, fan-shaped landforms made of sediment and debris deposited when a stream emerges from steep hillslopes onto a wide, flat valley. Because these streams drain upland areas, alluvial fans are more prone to floods and debris flows. These sudden events can be catastrophic. Debris flows roar down into valleys, sweeping away everything in their path, including huge boulders, homes, and cars.

### Wildfires and flooding

*Wildfires change the properties of soils, causing water to run off more easily. The lack of vegetation also means rain can fall directly onto the ground. This leads to increased flooding.*

### What makes alluvial fans so dangerous?

Alluvial fans are always changing and often have several active and inactive stream channels. The streambed that is currently filled with water may not be the active channel after a flood. A previously safe structure could quickly become inundated following a major storm. A home far from a stream channel could suddenly be right in the path of a surging wall of mud and debris.



## How do I know if I live or work on an alluvial fan?

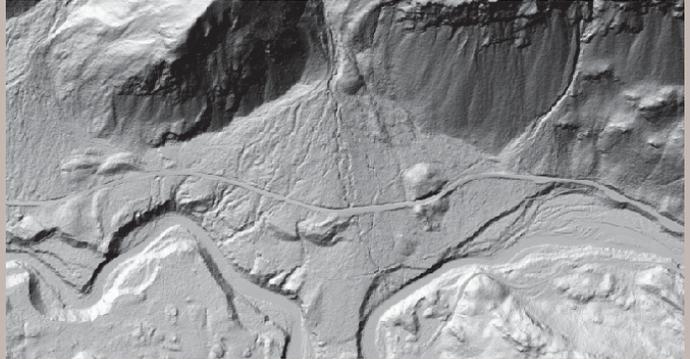


Look for large piles of cobbles and boulders on and around your property that may have been dropped by previous floods.

Fans are seen most clearly on lidar maps, which you can view on our Lidar Portal (<https://lidarportal.dnr.wa.gov/>). Look for broad fan shapes at the edges of valleys.



Explore our Landslide Inventory on the Washington Geologic Information Portal (<https://geologyportal.dnr.wa.gov/>)



Look for streambeds near you that seem to originate from a canyon or gully higher up.

## What should I do if I live or work on an alluvial fan?



Watch for impending storms and be prepared with an emergency plan in case you need to evacuate.



Buy flood insurance if warranted. Your insurance agent can help you make this decision.



Walk your property and identify potential flood pathways. Move outdoor items like furniture, barbecues, and cars to a safer location if they are in a potential hazard zone.



Identify stream channels and culverts in and around your property that may become blocked during a flood. Remove nearby debris, such as rocks, vegetation, and loose garbage.



Work with your neighbors and emergency manager to identify evacuation routes.



WASHINGTON STATE DEPT OF  
**NATURAL RESOURCES**  
WASHINGTON  
GEOLOGICAL SURVEY

### Visit these websites for more information:

- <https://www.dnr.wa.gov/wildfire-debris-flows>
- <https://afterthefirewa.org>

E-mail us: [geology@dnr.wa.gov](mailto:geology@dnr.wa.gov)

Call us: (360) 902-1450

# Geologic Planning in Washington

## WHAT IS GEOLOGIC PLANNING?

Scientists at the Washington Geological Survey (WGS), part of the Department of Natural Resources (DNR), are experts in Washington State geology. Our research touches on many aspects of geology such as geologic hazards, mineral resources, and climate change. These topics are relevant for society, and our work contributes to the safety and economic well-being of Washington’s residents. Geologic planning work involves understanding, sharing, and implementing scientific information with the community. The WGS Geologic Planning Liaison works with a wide range of people to more effectively connect science at WGS with other state agencies, local jurisdictions, and tribes.

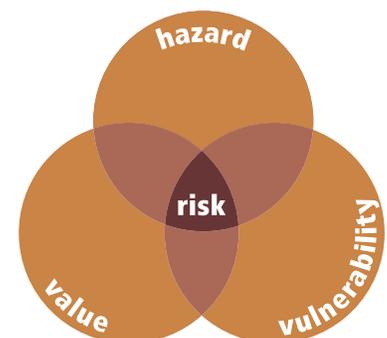


- ▶ Delineating critical areas, including geologically hazardous areas, is required under the Growth Management Act (GMA) as described in Revised Code of Washington (RCW) [36.70A](#) and Washington Administrative Code (WAC) Chapters [365-190 \(Minimum Guidelines\)](#), [365-195 \(Best Available Science\)](#), and [365-196 \(Procedural Criteria\)](#).
- ▶ Counties and cities must use the best-available science to periodically review and update their comprehensive plans, designate critical areas, and adopt regulations to protect critical areas.
- ▶ Planning for aggregate needs and use is required under RCW [36.70A.131](#) and WAC [365-190-070](#). Counties and cities must identify and designate aggregate (part of mineral resources) of long-term commercial significance. The GMA requires that counties and cities use information provided by DNR for their land-use decisions related to mineral resources and Mineral Resource Lands.

<b>How can WGS’s Geologic Planning work help with land-use planning?</b>	Geologic hazards and mineral resource lands mapping assists land-use planners, emergency managers, public works staff, and those who live and work where geologic hazards and mineral resources are located. Local jurisdictions can make better informed decisions about their assets, community safety, and growth management planning using the best-available science.
<b>What does WGS’s Geologic Planning do?</b>	WGS identifies geologic hazards and aggregate mineral resources, provides data, maps, and technical assistance such as training and outreach, and creates other tools that county and city planners can use. The use of the information and the regulatory decisions are made by local governments.
<b>What does WGS’s Geologic Planning NOT do?</b>	WGS does not revise building codes, evaluate development permits, or regulate land-use planning.

## WHAT ARE THE MOST SIGNIFICANT GEOLOGIC HAZARDS?

In Washington State, the most significant geologic hazards are earthquakes, tsunamis, volcanic events, and landslides. Other geologic hazards include subsidence above improperly reclaimed mines and exposure to hazardous minerals like radon. Climate change and additional human alteration of the environment exacerbate impacts to people, property, and infrastructure from natural hazards, increasing the need for hazard identification and efforts to minimize risk and mitigate impacts.





# Liquefaction Susceptibility for the Sumner 7.5-minute Quadrangle, Washington

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by Joe D. Dragovich and  
Patrick T. Pringle  
with a section on liquefaction  
by Stephen P. Palmer

WASHINGTON  
DIVISION OF GEOLOGY  
AND EARTH RESOURCES

Geologic Map GM-44  
September 1995

Partially supported by the  
Federal Emergency Management Agency and the  
Washington Division of Emergency Management

*The information provided in this map cannot be substituted  
for a site-specific geotechnical investigation, which must be  
performed by qualified practitioners and is required to  
assess the potential for and consequent damage from soil  
liquefaction.*



Location of  
quadrangle



WASHINGTON STATE DEPARTMENT OF  
**Natural Resources**

Jennifer M. Belcher - Commissioner of Public Lands  
Kaleen Cottingham - Supervisor

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**Natural Resources**

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Kaleen Cottingham - Supervisor

**Division of Geology and Earth Resources**

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### WASHINGTON STATE DEPARTMENT OF NATURAL RESOURCES

Jennifer M. Belcher—*Commissioner of Public Lands*  
Kaleen Cottingham—*Supervisor*

### DIVISION OF GEOLOGY AND EARTH RESOURCES

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# Liquefaction Susceptibility for the Sumner 7.5-minute Quadrangle, Washington

by Joe D. Dragovich and Patrick T. Pringle, with a section on liquefaction analysis by Stephen P. Palmer

## SUMMARY

The liquefaction susceptibility map of the Sumner 7.5-minute topographic quadrangle\* is based on analyses of 153 geotechnical borings obtained from the Washington Department of Transportation, the Pierce County Department of Public Works, and several private geotechnical firms. We assigned geologic deposits in the study area to one of three susceptibility rankings on the basis of analyses of the geotechnical data and historical reports of liquefaction during the 1949 magnitude 7.1 Olympia and 1965 magnitude 6.5 Seattle–Tacoma earthquakes.

This map and the accompanying analyses are intended to provide land-use planners, emergency-response personnel, geotechnical consultants, building developers and contractors, and private citizens with a qualitative assessment of the potential for soil liquefaction during an earthquake.

*The map is meant only as a general guide to delineate areas prone to liquefaction. This map cannot be substituted for a site-specific investigation to assess the potential for liquefaction and consequent damage for a given project.*

This map cannot be used to determine the presence or absence of liquefiable soils beneath any specific locality because of the regional nature of these maps (as determined by their scale, 1:24,000) and because the data used in the liquefaction susceptibility assessment have been subdivided on the basis of regional geological mapping.

Category I deposits are composed of artificial fill and modified land, post-Vashon (Holocene) alluvium, and the Electron Mudflow. Some historic liquefaction sites are close to abandoned river channel segments of the Puyallup and White Rivers. These areas may have a locally very high susceptibility to liquefaction because of their recent age, high proportion of sand-size sediment, and (or) because of their position in topographically low areas that are likely to have a shallow ground-water table.

Category II deposits, consisting of late Pleistocene sandy glaciolacustrine sediments, Holocene lacustrine

deposits and mass-wasting deposits, are ranked as having a low to medium liquefaction susceptibility. No geotechnical data were available for these deposits. Late Pleistocene sandy glaciolacustrine sediments have a moderate liquefaction susceptibility elsewhere (Palmer and others, 1995). These deposits are dominantly sandy and, because of their deposition during Vashon glacial recession, were not overridden by the glacial ice sheet and compacted. In areas of shallow ground water, these deposits may be susceptible to liquefaction. Holocene lacustrine deposits are primarily composed of peat and silt, with only scattered sandy sections, and would typically be considered as having a low liquefaction susceptibility. However, several peat deposits contain sand layers as much as 4 feet (1.2 meters) thick (Crandell, 1963), which may be liquefiable. Also, Chleborad and Schuster (1990) report a possible instance of liquefaction in a Holocene lacustrine deposit in the Big Soos Creek drainage in the Auburn quadrangle directly north of the Sumner quadrangle.

Mass-wasting deposits include both colluvium and Quaternary landslide debris. Because it typically occurs in small deposits, colluvium was not mapped by Crandell (1963) and Fiksdal (1979) and thus is not shown on Plate 1. Colluvium may consist primarily of loose material deposited by raveling or surface erosion of steep hillsides (for example by rilling), whereas material mapped as landslide debris typically comprises deposits from individually discernible landslides originating from discrete failure planes. Landslides and ground cracks were reported in colluvial deposits during the 1949 and 1965 earthquakes. Although these ground failures may not necessarily have been caused by liquefaction, it seems prudent to consider the possibility of liquefaction-induced ground failures in the mass-wasting deposits, for instance, because they are commonly areas of elevated pore pressure. Moderately sloping deposits of colluvium occur along the bases of many valley wall slopes. However, because these deposits were not mapped at this scale, there may be some small areas of Category II deposits not depicted on Plate 1.

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\* This pamphlet accompanies the liquefaction susceptibility map for the Sumner quadrangle.

Some valley wall areas display irregular topography whose origin is poorly understood. Two large areas (0.14 and 0.28 mi<sup>2</sup> or 0.7 and 0.36 km<sup>2</sup> respectively) of irregular topography in the west half of sections 12 and 13 west of McMillin were mapped as ice-contact collapse features by Crandell (1963); Fiksdal (1979) noted similar ice-contact collapse features. However, on aerial photographs these features appear to be large, deep-seated landslides. A visit to the site revealed locally disrupted pavement but no evidence of recent major slope movement except at smaller scarps adjacent to the valley wall that Fiksdal (1979) identified as active landslides. The latter could be minor scarps or shallow landslides (such as debris avalanches) emanating from the disrupted part of a larger deep-seated failure (that is, a slump-earthflow). If these features are deep-seated landslides, they are at least as old as the Electron Mudflow (which occurred about 600 years ago) because there is no evidence of a landslide deposit on the Electron Mudflow just below the potential slide. We have mapped these features as mass-wasting deposits.

Category III deposits include all other Vashon and older glacial and nonglacial deposits and exposures of the Osceola Mudflow. Category III also includes the Vashon recessional deposits, except sandy glaciolacustrine sediments (see above). Ice-contact stratified drift of Crandell (1963) and ice-contact collapse features of Fiksdal (1979) have been denoted with hachures on Plate 1 because they have never been overridden by ice (and therefore should be less well consolidated than glacially overridden sediments) and (or) may have been loosened during collapse. It seems plausible that these deposits would have a greater liquefaction susceptibility than those that were overridden; the sparse data available, however, show no significant difference. Furthermore, sandy interlayers in areas of shallow ground water could result in locally higher liquefaction susceptibility.

Quantitative evaluation of geotechnical data obtained from the Vashon glacial and the pre-Vashon glacial and

nonglacial deposits indicates they have a low susceptibility to liquefaction.

The Osceola Mudflow (shown with a horizontal line pattern on Plate 1) is a poorly sorted, cohesive lahar from Mount Rainier composed of gravel- to boulder-size clasts in a silty, sandy matrix. Crandell (1971) shows the silt content of the mudflow ranges from 20 to 28 percent and the gravel content ranges from 28 to 34 percent for eight samples. The high silt and gravel content of this deposit should significantly inhibit its susceptibility to liquefaction. Additionally, the upper part of the Osceola Mudflow is highly cemented due to weathering; this impervious cap would tend to diminish any surface manifestation of liquefaction of the underlying material. Crandell (1971) does note that the Osceola Mudflow "becomes highly unstable when disturbed and when near its liquid limit, which ranges from 22 to 30 percent; seemingly, the bearing strength of the deposit would be low in areas of high water table." Geotechnical analyses performed as part of this study suggest that unweathered portions of the Osceola Mudflow may be susceptible to liquefaction.

No instances of liquefaction were reported during the 1949 and 1965 Puget Sound earthquakes in the areas of Osceola Mudflow outcrop, and the outcrop area of these deposits is considered to have a low liquefaction susceptibility.

Liquefaction studies in adjacent quadrangles have mapped Tertiary bedrock as a fourth category with low to no liquefaction susceptibility. However, there are no mapped exposures of Tertiary bedrock in the Sumner quadrangle, and no bedrock was penetrated in any of the geotechnical borings. Bedrock in areas mantled by glacial or both glacial and Osceola Mudflow deposits is probably deeper than 100 feet (31 m). Depth to bedrock in the Sumner quadrangle ranges from 1,400 ft (425 m) to 800 ft (245 m) and increases to the north-northwest (Hall and Othberg, 1974; Buchanan-Banks and Collins, 1994).

## INTRODUCTION

The Washington Department of Natural Resources, Division of Geology and Earth Resources (DGER), is actively investigating earthquake hazards statewide and has received funding from the National Earthquake Hazards Reduction Program to conduct earthquake hazard mitigation studies. DGER has concentrated its technical programs on mapping Quaternary deposits in the Puget Sound region that are subject to seismically induced ground failures. The purpose of this study is to present a map showing liquefaction susceptibility in the Sumner 7.5-minute quadrangle (Fig. 1 and Plate 1); the quadrangle includes part of the alluvial valley along the lower reach of the Puyallup River, parts of its tributaries Fennel Creek and Prairie Creek, the lower reaches of the White River (formerly the Stuck River in the Duwamish valley), and part of an upland glacial drift plain.

Liquefaction occurs when a water-saturated, relatively loose, granular (sandy) soil loses strength during vibratory shaking such as that generated by an earthquake. Below the ground-water table, all the void spaces among soil particles are filled with water. The weight of the overlying soil mass is supported by grain-to-grain contact. Strong shaking during a large earthquake can disrupt the grain-to-grain contact, causing a decrease in the grain support. If strong shaking lasts long enough, the grain structure of the liquefiable soil may completely collapse. If the pore water cannot flow out of the collapsing pore space, the pore-water pressure increases. In the extreme case where the grain support is completely lost, the water pressure must increase to bear the entire weight of the overlying soil mass. At this point the granular soil is liquefied and will behave as a viscous fluid. The liquefied soil is then subject to extreme lateral deformation because it does not provide much resistance to horizontal forces. Youd (1973) provides a good overview and discussion of liquefaction.

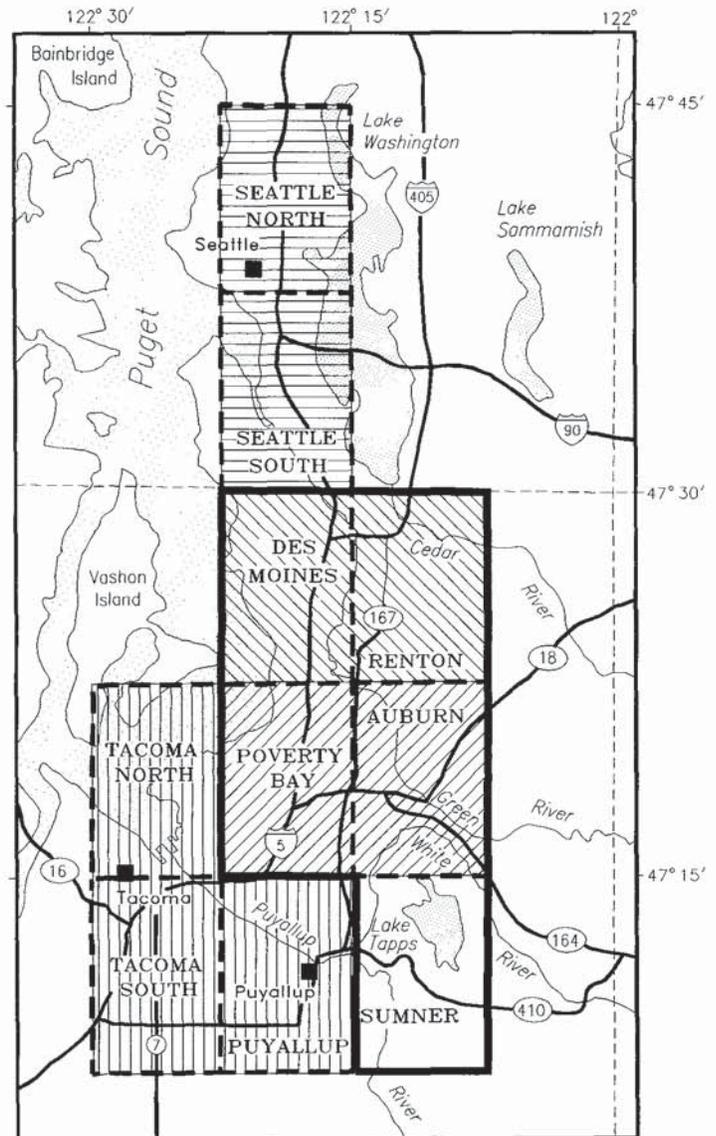
The liquefaction map of the Sumner 7.5-minute quadrangle (Plate 1) and the accompanying analyses are intended to provide land-use planners, emergency-response personnel, geotechnical consultants, building developers and contractors, and private citizens with a qualitative assessment of areas potentially subject to soil liquefaction during an earthquake. The map is meant only as a general guide to delineate areas more prone to liquefaction. *The map is not a substitute for a site-specific investigation to assess the potential for liquefaction and consequent damage for any development project.* Because of the regional nature of the map (scale 1:24,000) and because the data used in the liquefaction susceptibility assessment have been subdivided on the basis of regional geological mapping, this map cannot be used to determine the presence or absence of liquefiable soils beneath any specific locality. *Site-specific geotechnical investigations performed by qualified practitioners are required to make this determination.*

## GEOLOGY OF THE SUMNER QUADRANGLE

The discussion of the geology of the Sumner 7.5-minute quadrangle is a summary of Crandell (1963), Luzier (1969), and Fiksdal (1979). A generalized geologic map of the Sumner quadrangle is shown in Figure 2. Although sedimentary rocks and intrusive rocks of Tertiary age crop out in nearby areas, these rocks are not exposed in the study area and were not penetrated in the borings. Descriptions of these rocks can be

found in Gard (1968), Mullineaux (1970), Walsh (1987), and Walsh and others (1987).

Pleistocene glacial and nonglacial deposits unconformably overlie Tertiary bedrock in the study area (Fig. 2). The youngest of these glacial units was deposited during the Vashon Stage of the Fraser Glaciation (ca. 14,000 years ago). Vashon till and outwash veneer the broad drift plain that defines the Puget Lowland between the Olympic Mountains and the foothills of the Cascade Range. In the study area, river valleys that reach Puget Sound are incised more than 300 ft (92 ft) into the



### EXPLANATION

-  This study
-  Palmer and others, 1995
-  Palmer and others, 1994
-  Grant and others, 1992
-  Shannon & Wilson, Inc., 1993

**Figure 1.** Location map showing the Sumner 7.5-minute quadrangle, Washington, and adjacent quadrangles for which liquefaction studies have been completed.

drift plain. Some of these valleys may be inherited glacial outwash valleys from the Vashon Glaciation (Booth and Hallet, 1993).

Sedimentary and fragmental volcanic deposits younger than the Vashon Drift are also present in the study area (Fig. 2). The 5,700-year-old Osceola Mudflow (Crandell, 1971; Scott and others, 1992) occurs at the surface in the eastern and southern sectors of the Sumner quadrangle and can be identified in the subsurface in the Puyallup and Duwamish (White River) valleys. This mudflow probably originated as an eruption- or explosion-generated debris avalanche (sector collapse) of at least 0.91 mi<sup>3</sup> (3.8 km<sup>3</sup>) (Dragovich and others, 1994) of material that flowed down the upper White and West Fork White Rivers and into the Puget Lowland. According to Crandell (1971), the pre-Osceola White River was a tributary of the Puyallup River that followed the South Prairie Creek valley southeast of Orting. A lobe of the mudflow filled this valley and, at the same time, a larger amount of debris spread across the drift plain, spilling into the Puyallup River valley through Fennel Creek as well as into the Duwamish valley near Auburn, mostly through what is the modern valley of the White River. Some of this material spilled over into the Green River drainage and entered the Duwamish valley through that channel.

On the basis of well records, Luzier (1969) found that the Osceola deposits are 265 ft (81 m) below sea level at a site 4 mi (6.5 km) north of Auburn. The deposit is found at this depth because the Duwamish valley was an arm of Puget Sound at that time (Fig. 3) (see also Dragovich and others, 1994). Crandell (1963) mapped an important surface exposure of the mudflow in a cutbank of the Puyallup River at Sumner (Fig. 2, see also Fig. 5) and suggested that the mudflow extended in the subsurface to Puyallup.

For the Sumner quadrangle, we document the overall distribution of the Osceola Mudflow in the subsurface and connect the subsurface deposits with surface exposures farther east on the glacial drift plain. (Also see Dragovich and others, 1994.) Subsurface identification and correlation of the Osceola Mudflow deposit in the Sumner quadrangle (and adjacent areas) were based on several characteristics markedly similar to those observed in upstream outcrops (Crandell and Waldron, 1956; Crandell, 1963; Scott and others, 1992). The similarities include:

- l extremely poor sorting (Fig. 4A),
- l low density (as determined from blow count data, Fig. 4C; see also Fig. 11),
- l normal grading as shown by amounts sand/sand+silt+clay with depth (J. Vallance, McGill University, written commun., 1994),
- l a large proportion of angular clasts from Mount Rainier,
- l numerous wood fragments,
- l a gray color with mottled yellow patches and sulfurous smell,
- l similar stratigraphic position (Figs. 5 and 6), and
- l a thickness range and mudflow top gradient (paleoslope) and elevation range consistent with both upstream surface exposure information and the drastic thinning and deepening of the mudflow off delta

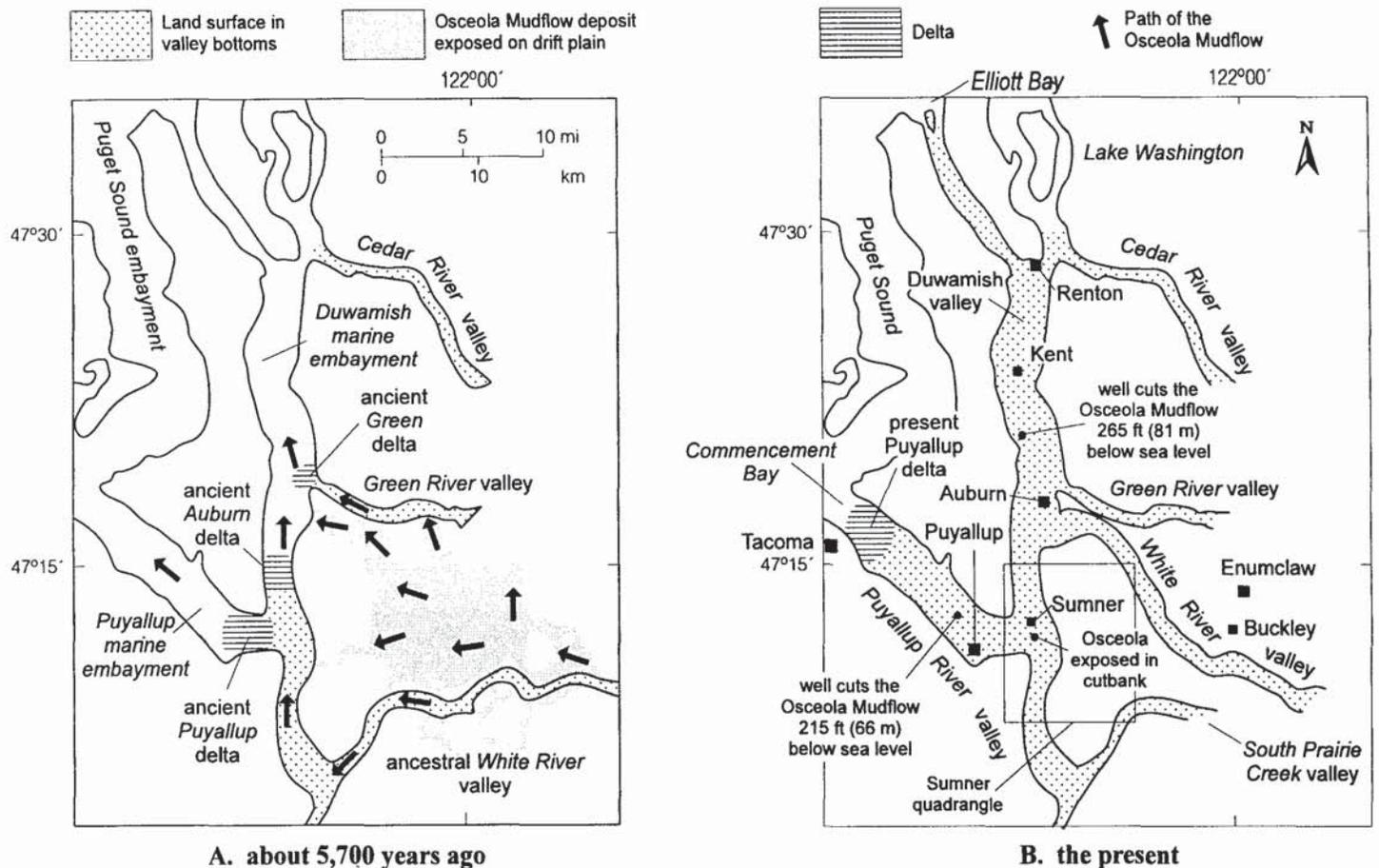
**Figure 2.** Generalized geologic map of the Sumner 7.5-minute quadrangle, Washington. Asterisks are approximate locations of individual boreholes; filled triangles are clusters of closely spaced boreholes (number of boreholes superscripted); squares are the approximate locations of the centers of various communities.

<b>EXPLANATION</b>	
af	Artificial fill; undifferentiated
Qa	Quaternary alluvium; stratified clay, silt, sand, and gravel
Qls	Quaternary landslide deposits; largely massive diamicton*; includes slump-earthflow, debris avalanche, and talus deposits
Qlp	Lacustrine deposits; stratified silt, sand, and peat
Qem	Electron Mudflow; diamicton*; volcanic rock fragments (largely Mount Rainier provenance) in clayey, sandy matrix; deposited about 530 years ago (radiocarbon years)
Qom	Osceola Mudflow; diamicton*; volcanic rock fragments (largely Mount Rainier provenance) in clayey, sandy matrix; deposited about 5,000 years ago (radiocarbon years)
Qcl	Ice-contact collapse features and (or) ancient landslides; mainly stratified drift; hummocky, irregular surface. Mapped in part as Qpo and Qpv by Crandell (1963). Amount of overlying postglacial slumping undetermined
	Recessional outwash and ice-contact stratified drift; sand and gravel deposited in front of or adjacent to glacier; subdivided as follows:
Qik	Kame and kame-field gravel
Qit	Kame-terrace gravel
Qil	Lacustrine sand
Qpv	Valley-train deposit; predominantly well-sorted sand and gravel
Qpo	Outwash-plain deposits; predominantly well-sorted sand and gravel
Qpd	Delta gravel
Qpl	Lacustrine sand
Qpa	Undifferentiated stream deposits
Qgt	Glacial drift; chiefly compact till, diamicton*
Qsa	Advance stratified glacial drift; outwash sand and gravel deposited in advance of the Vashon glacier
Qg	Glacial drift; undifferentiated
Qu	Pre-Vashon glacial units; undifferentiated
Qss	Salmon Springs Drift; chiefly oxidized sand and gravel
Qpy	Puyallup Formation; chiefly unoxidized sand and gravel and very compact mudflows of Mount Rainier provenance deposited in lowland during nonglacial period
Qst	Stuck Drift; chiefly very compact till deposited by the Puget lobe of the Cordilleran ice sheet and oxidized sand and gravel
Qad	Alderton Formation; chiefly unoxidized sand and gravel and very compact mudflows of Mount Rainier provenance deposited during nonglacial period
Qor	Orting Drift; deeply oxidized sand and gravel
	<i>*Diamicton: a nonsorted or poorly sorted, noncalcareous, terrigenous sediment that contains a wide range of particle sizes, such as a sediment with sand and (or) larger particles in a muddy matrix.</i>
	--- Contact, dashed where inferred

fronts west of Puyallup and north of Sumner (Dragovich and others, 1994).

We constructed upper surface contour and thickness (isopach) maps for the mudflow from the borehole data, including information from water well logs and geotechnical borings. (See Dragovich and others, 1994, for methods.) Figures 6 and 7 show the extent and geometry of the Osceola de-





**Figure 3.** Comparison of the modern shoreline of Puget Sound (B) with that immediately after deposition of the Osceola Mudflow (A). Dark shading on the drift plateau shows the area of mudflow exposures at the surface. In the valleys, the mudflow is found in the subsurface beneath more recent valley deposits. The location of the Sumner quadrangle is shown in B. Well 22/4 35H2 (in the Auburn quadrangle, directly north of the Sumner quadrangle) penetrated the Osceola deposit 265 feet below sea level; the Duwamish embayment was formerly an arm of Puget Sound. Compare this depth with data given in Figure 6. Modified from Luzier (1969) and Dragovich and others (1994).

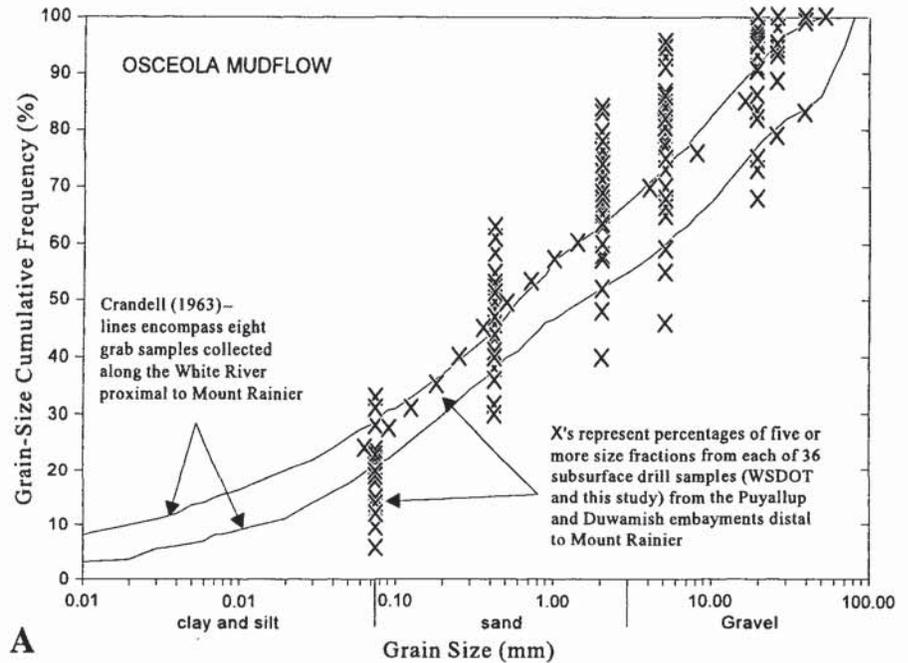
posit in the subsurface based on these subsurface data. The surface exposure of the Osceola farthest downstream is exposed in the cutbank of the Puyallup River near the Puyallup River Bridge at Sumner (Figs. 2 and 5). At this location the upper surface of the mudflow deposit is at an elevation of about 50 ft (15 m). To confirm the identification of the Osceola Mudflow in the subsurface, we submitted a small piece of wood from the Puyallup River Bridge deposit for radiocarbon dating. The uncalibrated radiocarbon age of  $4,700 \pm 60$  yr B.P. (sample number Beta-66269) confirms that this unit is the Osceola deposit. We also submitted twigs obtained from a presumed Osceola Mudflow drill sample obtained by the Washington Department of Transportation (WSDOT) below the State Route 167/32nd Street interchange north of Sumner. The uncalibrated radiocarbon age of  $5010 \pm 80$  yr B.P. (sample number Beta-65937) for this sample is well within the age envelope of the Osceola Mudflow determined in other studies and further confirms the identification of the Osceola Mudflow in the subsurface.

The contours of the upper surface of the Osceola deposit mimic pre-mudflow physiography (Fig. 6). At present sea level, a pre-Osceola Mudflow arm of Puget Sound would have extended south up the Duwamish valley to about the city of Sumner; a delta foreslope would lie just north of the quadrangle as noted in figure 3 of Dragovich and others (1994). Similarly, in the Puyallup valley an arm of the sound would have

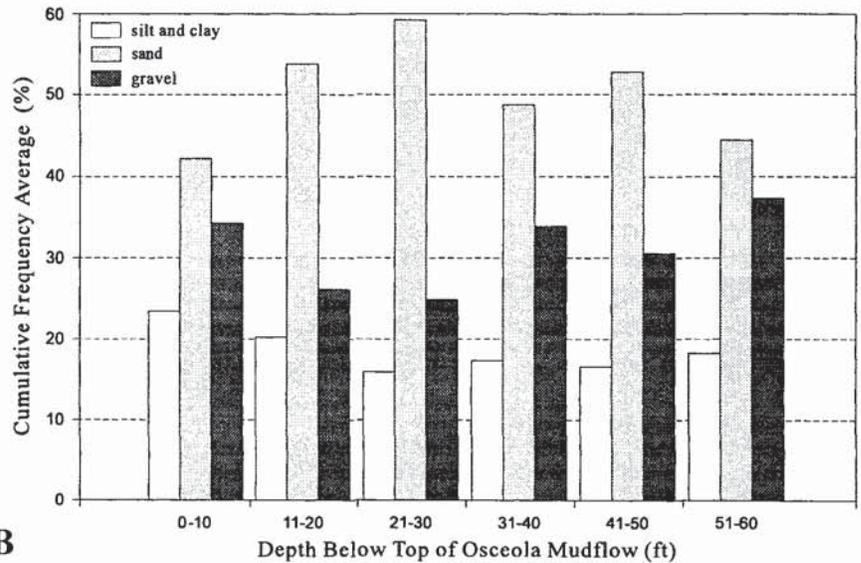
extended east to about the city of Puyallup. Geotechnical data from the Poverty Bay, Auburn, and Puyallup quadrangles (Fig. 1) indicate a significant increase in the slope of the upper surface of the Osceola Mudflow (from less than 2 to more than 7 percent) in the Auburn quadrangle within a 2-mi (3 km) stretch north of the northern boundary of the Sumner quadrangle (Dragovich and others, 1994). The mudflow apparently flowed over a delta in this area. Evidence for a similar delta can be seen in data from borings slightly west of the City of Puyallup. Figures 7 and 8, isopach maps, provide a three-dimensional perspective of the Osceola Mudflow and alluvium geometries, respectively. This stratigraphic information on the depth and thickness of the Osceola deposit is important in light of the low density of the mudflow deposit as indicated by low blow-count values, water-saturation of the unit in low areas, and resulting low bearing strength (discussed later in this report).

Deposits of younger lahars and lahar runouts from Mount Rainier further aggraded the lower valleys (Crandell, 1971; Scott and others, 1992; Vallance, 1994). Much of this aggradation may have begun about 2,300 radiocarbon years ago when lahars generated by the largest postglacial eruption of Mount Rainier deposited massive amounts of sand in the lower reaches of the Puyallup and White Rivers. Sandy deposits ('black sands') from this eruptive episode apparently liquefied during the Puget Sound earthquakes of 1949 and 1965 (Chle-

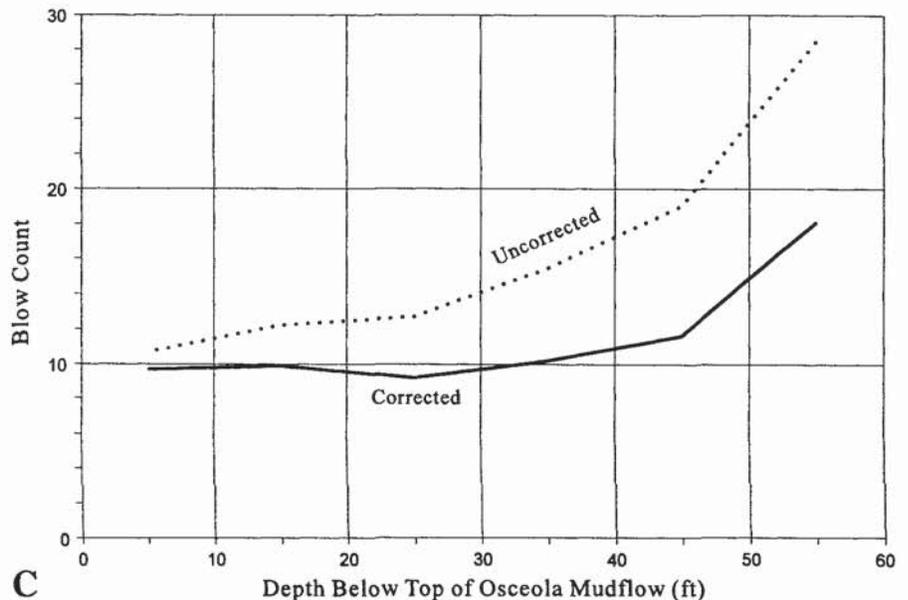
**Figure 4. A.** Grain-size cumulative frequency diagram for 36 subsurface samples of the Osceola Mudflow (Washington State Department of Transportation [WSDOT] data indicated by X's and 8 grab samples (Crandell, 1963; data envelope defined by the two lines). The WSDOT samples come from the Duwamish and Puyallup embayments (See Fig. 3), far downstream from the Crandell samples. J. Vallance (McGill University) and K. Scott (U.S. Geological Survey) have unpublished data that show that average grain size and sorting increase with distance from Mount Rainier due to stream bulking with better sorted sand and gravel that typically lack any finer materials. The reduction in gravel content may be an artifact of the sampling method (the split spoon drill samplers have a 1.5-in. [3.8 cm] aperture) and the reduced volume of downstream samples. The sampling bias is greater in the coarser fractions, suggesting that some downstream sorting in the subsurface deposit is real. **B.** Average cumulative frequency in percent by generalized size fractions versus depth below the top of the Osceola Mudflow based on gradation tests of 36 drill samples. The distribution of the data reflects overall normal grading of the gravel fraction. **C.** Blow counts versus depth below the top of the Osceola Mudflow. The rising blow counts with depth may be attributed to the corresponding increase in gravel. However, recalculating blow counts to accommodate increased effective stress with depth indicates only a slightly greater blow count at 41 to 50 ft (13–15 m) than at depths between 0 and 10 ft (3 m).



**A**



**B**



**C**

borad and Schuster, 1990; Palmer and others, 1991; Pringle and Palmer, 1992). For example, the Round Pass Mudflow probably reached the Puget Lowland about 2,600 radiocarbon years ago. Deposits of many other lahars have been documented upstream of the Sumner quadrangle in both the White and Puyallup River valleys, and many of these flows had downstream equivalents that inundated the lowlands. The stratigraphic position of most of these lahars in the Sumner quadrangle is poorly known. These deposits include the 2,300-yr-old volcanic sand deposits near the city of Puyallup (Palmer and others, 1991; Pringle and Palmer, 1992) and younger lahar deposits in the White River drainage (Scott and others, 1992).

The post-Osceola White River drains into the Duwamish valley at Auburn. Post-Osceola aggradation in this reach eventually filled an arm of Puget Sound (between the city of Sumner and Lake Washington) and constructed a fan of debris that redirected the lower Puyallup River into the Puyallup embayment. Similar sands are commonly found on top of the Osceola Mudflow. Figure 8 shows the amount of post-Osceola alluviation based on our analysis of the data. This map can be used to estimate the depth to the top of the Osceola Mudflow in the valley.

In the Puyallup River, a sandy lahar or loose, sandy alluvium is typically found be-

neath deposits of the 600-yr-old Electron Mudflow. Only silty or sandy alluvium is found on top of the Electron deposits in the Puget Lowland. Transport of sandy material into the lower Duwamish valley continued after the deposition of the Electron Mudflow because terrace-capping deposits of sandy (noncohesive) lahars or lahar runouts are found upstream of the Duwamish valley in the White and Puyallup valleys (Crandell, 1971; Scott and others, 1992). These deposits overlie the W tephra layer from Mount St. Helens erupted in A.D. 1480 and are younger than the Electron Mudflow.

Before 1906, the White River bifurcated as it reached the floor of the Duwamish valley (Willis and Smith, 1899). The White River flowed northward into the Green River, and the Stuck River flowed southward as a tributary of the Puyallup River. After a flood in 1906, most of the White River flow was directed into the Stuck River, and engineering projects permanently diverted the north-flowing White River into the Stuck River, which was renamed the White River (Luzier, 1969). Some of these abandoned channels are shown on Plate 1.

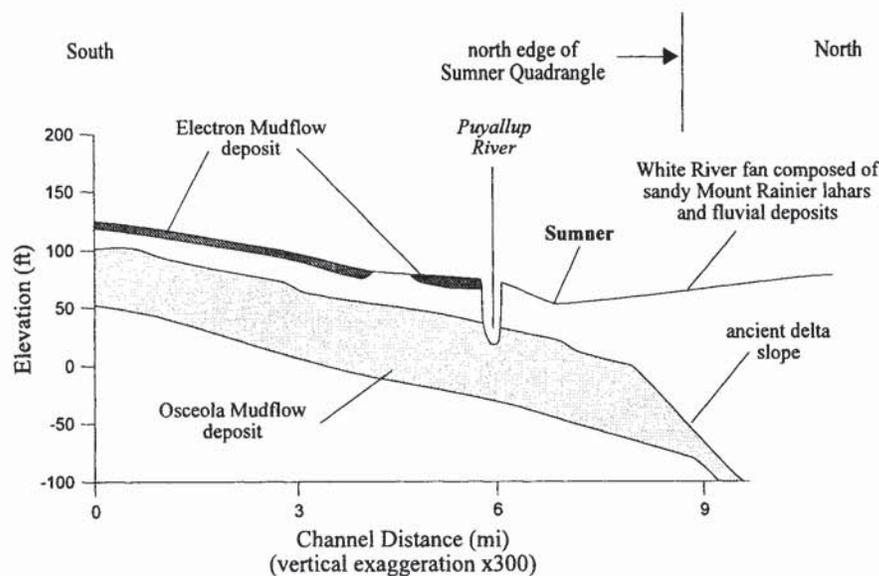
Mass-wasting deposits mapped by Crandell (1963) consist only of landslide debris, such as slump-earthflow or debris avalanche material. Colluvium that has accumulated in talus cones or piles as a result of surficial erosional processes, such as rilling or raveling, was not mapped. Hillslope toes are composed locally of a thick mantle of colluvial material resulting from upslope mass-wasting processes. Consequently, unmapped Category II low-density colluvial materials may overlie Category III high-density tills along the lowermost portions of many valley walls in the Sumner area.

Other post-Vashon deposits, such as Holocene lake sediments, also occur in the Sumner quadrangle. The lake deposits are chiefly thin peats, but they include some sand, silt, and clay formed in flood-plain depressions in the Duwamish valley. Many of the extensive peat deposits from low areas in the drift plain plateau or in valleys have been mined.

Modified land or fill is concentrated in valleys and includes fill adjacent to Lake Tapps and embankments for railroad lines, roadways, canals, and water impoundments.

## FLUVIAL HISTORY OF THE VALLEY

Fluvial deposits of gravel, sand, silt, and clay are found in the Puyallup, White, and Duwamish valleys and in the valleys cut by South Prairie and Fennel Creeks. Flood plains are built by fluvial deposits in two fundamental ways, by lateral accretion during normal river meandering and migration of the river and by vertical accretion during flooding. Point bars at the apex of river channels are the most important lateral accretion deposit area. Here, sand and gravel deposits are accreted to the point bar as the channel migrates across the flood plain and undercuts the opposite bank. Flood-plain deposits are dominantly a result of vertical accretion whereby sediment-loaded flood waters deposit sand, silt, or clay due to flood current deceleration over flood plains. Peats form in oxygen-deficient swampy set-

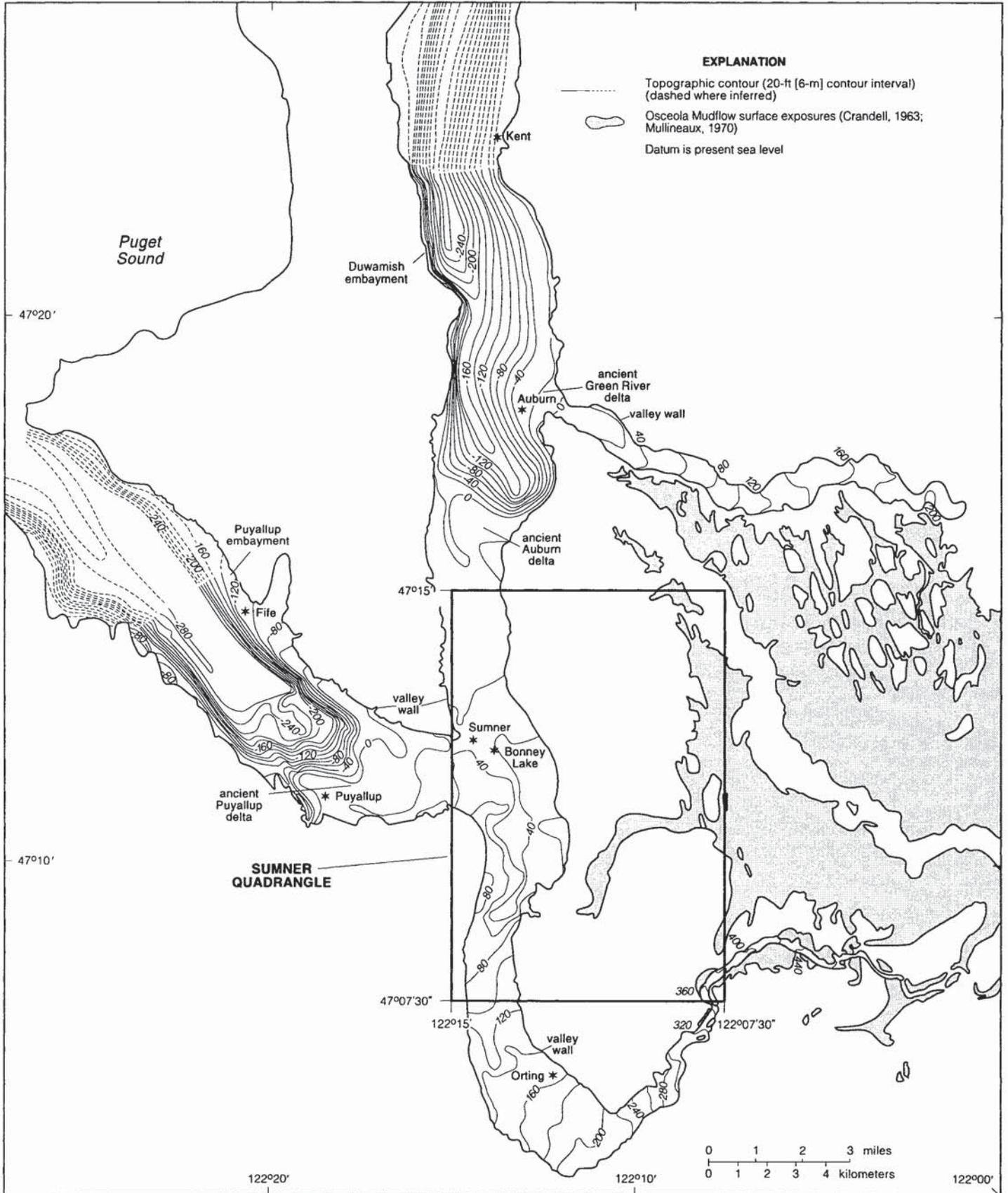


**Figure 5.** Diagrammatic profile of the Puyallup River valley in the Sumner quadrangle. The subsurface Osceola Mudflow is an important stratigraphic marker in the study area. In the Puyallup and Auburn quadrangles (Fig. 1), the mudflow traveled over ancient delta foreslopes (Fig. 3A) and flowed into former marine embayments of Puget Sound (Dragovich and others, 1994).

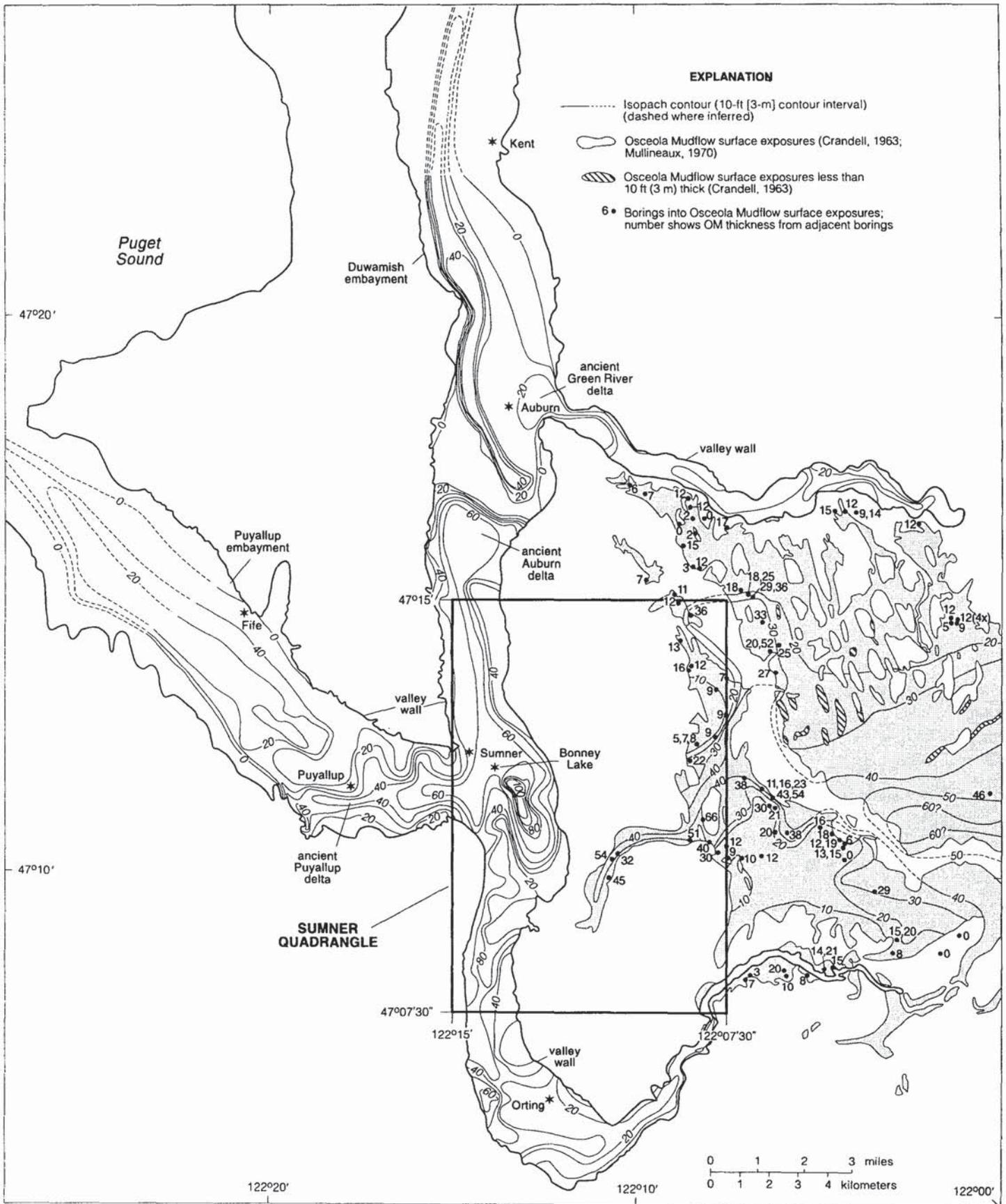
tings where vegetal decay is impeded. Channels migrate across flood plains slowly to rapidly, or may abandon portions of channels during migration, resulting in commonly complex fluvial stratigraphy. Discrimination between dominantly sandy channel deposits and dominantly fine or peaty flood-plain deposits is important for liquefaction susceptibility mapping. However, projections of alluvium to depth are complicated by complex facies changes caused by lateral channel shift or abandonment or bank collapse.

Other factors complicate any fluvial deposition model for the valley-fill sediments. Repeated deposition of lahars (mudflows) in a volcanogenic environment results in high sedimentation rates, and the history of 'normal' fluvial deposition is punctuated by catastrophic input of laharic sediments. Also, valley-fill sediments in the Puyallup and Duwamish embayments are composed of deltaic sediments formed as the ancient river deltas prograded down these valleys during the Holocene (Dragovich and others, 1994). Little is known about the distribution or characterization of valley-fill deltaic sediments in the Sumner quadrangle. The lack of detailed information highlights the need for site-specific liquefaction hazard characterization.

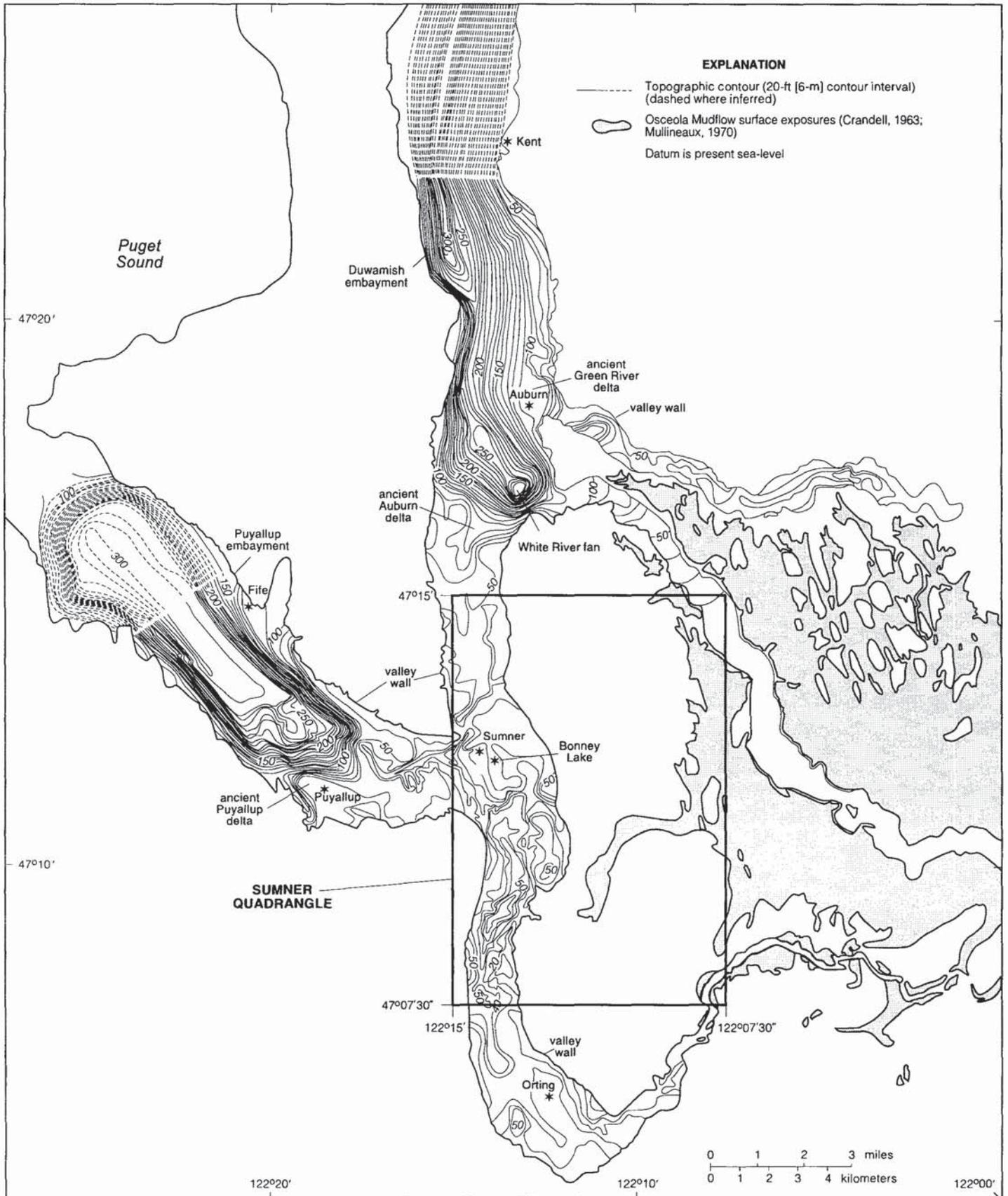
We reviewed several 1989 stereo aerial photographs (Washington Department of Natural Resources Flight Index Symbol SP-89; approximately 1:12,000 scale) to evaluate the previous geologic mapping, refine our geologic mapping, and map landslides, abandoned channels, and river bar and swale topography (Plate 1). In Plate 1, the hachured features mark the traces of clearly identifiable abandoned channels; these channels were abandoned by avulsion or by artificial channel adjustments such as those summarized by Palmer (1992, p. 4). The stippled pattern denotes the bar and swale topography formed by lateral accretion during channel migration when rivers undercut the cutbank opposite the point bar. The resulting collapsed sediment is carried downstream as bed load and is commonly deposited as a submerged bar on the same side of the river. The result is a cross-stratified deposit with a subdued



**Figure 6.** Contour map of the upper surface of the Osceola Mudflow deposit in and near the Sumner quadrangle. Rectangle shows the position of the Sumner quadrangle; stars indicate the approximate locations of the centers of various communities.



**Figure 7.** Isopach map showing the thickness of the Osceola Mudflow deposit in or near the Sumner quadrangle. Rectangle shows the position of the Sumner quadrangle; stars indicate the approximate locations of the centers of various communities.



**Figure 8.** Isopach map showing the thickness of post-Osceola valley fill in or near the Sumner quadrangle. Rectangle shows the position of the Sumner quadrangle; stars indicate the approximate locations of the centers of various communities.

relief of low ridges and intervening swales that may record many episodes of channel migration. The overall pattern of these features the Sumner quadrangle suggests that the Puyallup River channel changes position both by avulsion of long segments or by channel abandonment and by slower meander migration forming the characteristic bar and swale topography.

**LIQUEFACTION SUSCEPTIBILITY MAP OF THE SUMNER QUADRANGLE**

Geological mapping of the Sumner 7.5-minute quadrangle is provided mainly by Crandell (1963). Some of the landslides were mapped by Fiksdal (1979). We have generalized Crandell's surficial geologic map units into three major categories of liquefaction susceptibility (discussed in detail below). These categories deposits are:

- *Category I:* artificial fill and modified land, Holocene alluvium, and areas of outcrop of the Electron Mudflow in the Puyallup valley.
- *Category II:* Holocene lacustrine and mass-wasting deposits, including landslides and late Pleistocene sandy glaciolacustrine sediments.
- *Category III:* All Pleistocene glacial (including deposits of Vashon proglacial and ice-contact stratified drift but excluding late Pleistocene sandy glaciolucustrine deposits) and nonglacial deposits, and the Osceola Mudflow.

Table 1 summarizes the three liquefaction categories and the corresponding geologic map units. Plate 1 shows the distribution of these categories in the Sumner quadrangle.

**Table 1.** The three liquefaction categories used in this study and the corresponding geologic map units of Crandell (1963) and Fiksdal (1979)

Category	Geologic description	Map units Crandell (1963)	Map units Fiksdal (1979)
I	artificial fill and modified land; Holocene alluvium; Electron Mudflow	af afm Qa Qem	
II	Holocene lacustrine sediments; Holocene mass-wasting deposits; late Pleistocene (Vashon Stade) ice-contact and proglacial lacustrine sand deposits	Qlp Qms Qil,Qpl	OL (plate 5)
III	Vashon Stade (late Pleistocene) glacial deposits: ice-contact stratified drift; proglacial stratified drift; glacial drift, chiefly till; advanced stratified drift; glacial drift, undifferentiated; older Pleistocene glacial and nonglacial deposits; Holocene Osceola Mudflow	Qik,Qit Qpv,Qpo,Qpd,Qpa Qgt Qsa Qg Qss,Qpy,Qst,Qad, Qor, Qu Qom	

# Liquefaction Analysis of Soil Deposits Found in the Sumner Quadrangle

by Stephen P. Palmer

## METHODOLOGY USED TO EVALUATE LIQUEFACTION SUSCEPTIBILITY

My analysis of liquefaction susceptibility in the Sumner quadrangle closely follows the methodology of Grant and others (1992), Palmer (1992), Shannon & Wilson Inc. (1993), and Palmer and others (1994, 1995). I estimate the potential for soil liquefaction using the field evaluation methodology developed by Seed and Idriss (1971) and modified by Seed and others (1983, 1985). This field evaluation procedure uses Standard Penetration Test (SPT) N-values (ASTM D 1586-84)<sup>1</sup>, sample descriptions, grain-size analyses, and measured ground-water depths obtained from geotechnical borings to estimate the factor of safety for a hypothetical earthquake with a specified magnitude and peak ground acceleration (PGA).

The SPT N-values and other data are obtained from sampled depths in a geotechnical boring so that the thicknesses and depths of individual liquefiable soil units and the total thickness of liquefiable material in that boring can be estimated. The procedure used in this study characterizes the liquefaction susceptibility of various Quaternary deposits through the cumulative frequency histogram of the aggregate thickness of liquefiable material penetrated in the borings. This is equivalent to the thickness criteria used by Grant and others (1992) and Shannon & Wilson, Inc. (1993).

The Quaternary units in the study area are grouped into three categories (Categories I, II, and III) on the basis of their geological and engineering characteristics (Table 1). The liquefaction susceptibility of each category is quantified from borings drilled only in geologic units that are included in that category. The liquefaction category contacts shown in Plate 1 are primarily taken from the geologic mapping of Crandell (1963) and Fiksdal (1979). A number of areas of landsliding have been identified using air photo analysis and ground investigation by Dragovich and Pringle as part of this study and are included in Figure 2 and Plate 1.

This study is primarily concerned with evaluating liquefaction that would have potential to cause noticeable effects at the ground surface. A relationship presented by Ishihara (1985) suggests that for PGAs of 0.30 g or less, liquefaction that occurs at depths greater than approximately 40 ft (12 m) will probably not cause noticeable effects or damage at the surface. Thus, this study limits the evaluation of liquefaction to only the upper 40 ft (12 m) of the borings. Many of the borings used in this study are less than 40 ft (12 m) deep—the average depth of all borings is 41.7 ft (12.6 m). Also, restricting the evaluation to these shallow depths allows a more direct comparison to historic reports of liquefaction.

**Table 2.** Conversion of Unified Soil Classification System (USCS) soil class to fines fraction used as input to the liquefaction susceptibility analysis

USCS soil class	Fines fraction (percent)
SP	5
SM	25
SP-SM	15
SW-SM	35

The field evaluation methodology requires an estimate of the fines fraction (the fraction of a sample that passes a 200-mesh sieve). I used measured grain-size distribution data to provide this parameter. If measured data were not available, I estimated the fines fraction from the soil category assigned using the Unified Soil Classification System (ASTM D 2487-90)<sup>2</sup> and the conversions given in Table 2. I restricted the liquefaction analysis to sandy soils containing 40 percent or less fines. This is less conservative than Seed and others' (1983, 1985) method, as they allow liquefaction of sandy soils with as much as 50 percent fines. I also did not investigate the possibility of liquefaction of soils classified as silts (greater than 50 percent fines content) even though liquefaction of native silt soils has been observed in past earthquakes (for example, at Ying Kou City [Arulanandan and others, 1984, 1986] and San Fernando Juvenile Hall [Bennett, 1989]).

Cumulative frequency histograms for Category I, II, and III deposits were made for a hypothetical earthquake of magnitude 7.3 ( $M_w$  7.3) that produced a PGA of either 0.15 g or 0.30 g. This is consistent with the scenario earthquakes used by Grant and others (1992) and Shannon & Wilson, Inc., (1993) in evaluating liquefaction susceptibility in the Seattle and Tacoma areas. The scenario earthquake used in this study was intended to represent a major earthquake similar to the 1949 Olympia event. I consider the  $M_w$  7.3 scenario earthquake to be at an intermediate depth (30 to 37 mi or 48 to 60 km) located within the subducting Juan de Fuca plate; this is termed an intraplate earthquake. The two values of PGA used in the scenario earthquake are expected to bracket the range of damaging ground motions that would arise from a  $M_w$  7.3 intraplate event. The 0.30 g PGA corresponds closely to the PGA measured in downtown Olympia during the 1949 earthquake.

However, recent studies indicate that other earthquake sources have the potential to generate more severe ground motions than the scenario earthquake chosen for this study. The

<sup>1</sup> American Society for Testing and Materials (ASTM), 1991, D 1586-84, Standard method for penetration test and split-barrel sampling of soils. In Annual book of ASTM standards, v. 04.08, Soil and rock; dimension stone; geosynthetics, p. 232-236.

<sup>2</sup> American Society for Testing and Materials (ASTM), 1991, D 2487-90, Standard test method for classification of soils for engineering purposes. In Annual book of ASTM standards, v. 04.08, Soil and rock; dimension stone; geosynthetics, p. 309-319.

potential for great ( $M_w$  8 or larger) thrust earthquakes to occur on the Cascadia subduction zone has been recognized (Atwater, 1987; Weaver and Shedlock, 1991; Atwater and others, 1995). Also, evidence for a major earthquake ( $M_w$  7 to 7.5) on the Seattle fault about 1,000 years ago was recently presented (Bucknam and others, 1992; Atwater and Moore, 1992; Jacoby and others, 1992). The projected trace of this west-trending fault is located approximately 21 mi (34 km) north of the northern boundary of the study area. However, the Seattle fault is south-dipping, so that the main area of energy release during an earthquake on this fault could be closer to the study area.

Ground motion simulation studies for an  $M_w$  8.0 to 8.5 subduction zone earthquake were presented by Cohee and others (1991) and Wong and others (1993). These studies suggest that the PGAs in the Puget Sound region resulting from such an earthquake would be reasonably bounded by the 0.15 g to 0.30 g range of the scenario earthquake used in this study. However, the duration of strong ground shaking for a subduction zone event would be significantly longer than for the  $M_w$  7.3 event considered in this study. The longer duration of shaking would result in more numerous instances of liquefaction and more ground displacement and consequent damage. A major earthquake ( $M_w > 7.0$ ) on the Seattle fault could result in PGAs that might exceed 0.30 g in the northern portion of the study area and that could produce more numerous and severe occurrences of liquefaction than would be expected for the scenario event used in this study.

The evaluation of liquefaction susceptibility presented in this study is nonconservative because I did not consider liquefaction of sandy or silty soils containing more than 40 percent fines. Also, my choice of scenario earthquakes does not necessarily represent the most severe ground motions that can occur in the study area. However, my methodology provides a quantitative basis for assessing the relative liquefaction susceptibility of each of the three liquefaction hazard categories distinguished in the study area that is applicable regardless of the choice of earthquake sources. Furthermore, my results can be compared to those of Grant and others (1992) and Shannon & Wilson, Inc. (1993) to obtain a perspective on the relative liquefaction hazard regionally.

### Geotechnical Boring Data Used in Evaluation of Liquefaction Susceptibility

The geotechnical boring data used in this study were obtained primarily from the Washington State Department of Transportation (WSDOT). Some additional data were supplied by the Pierce County Department of Public Works, and the City of Sumner. A total of 153 borings were obtained from these agencies. The available borings are clustered around state highway and Pierce County road construction projects that required geotechnical evaluations.

Ninety-eight and 55 of the available borings were drilled in Category I and III deposits, respectively. There were no borings located in Category II deposits. Drilled total depths for these borings ranged from 9 to 161.5 ft (2.8 to 50 m), and the average depth of all borings used in this study is 53.9 ft (16.4 m). The average drilled depth in Category I deposits was 64.4 ft (19.8 m), and in Category III deposits the average was 35.1 ft (10.8 m). All boring logs included sample descriptions, SPT N-values, and a general description of drilling and sam-

pling procedures; most boring logs or reports recorded measured depth to ground water, accessory geotechnical data (such as grain-size analyses), and a site plan showing boring locations.

Seed and others (1984) note that variation in drilling methods and sampling procedures used in geotechnical borings can significantly affect the measured SPT N-values. They suggest that the ideal drilling and sampling practice for obtaining SPT N-values for evaluating liquefaction susceptibility is as follows:

- 4- to 5-in. (10.2-12.7 cm) -diameter rotary boring drilled using an upward-directed flow of bentonite mud (typically a tri-cone bit configuration);
- a sampling tube with 2.00-in. (5.08 cm) O.D. and 1.38-in. (3.50 cm) I.D. without a liner;
- AW drill rods for depths less than 50 ft (15.2 m), and N, BW, or NW rods for greater depths;
- 30 to 40 blows per minute delivered to the sampler;
- SPT N-value measured between 6 in. (15.2 cm) and 18 in. (45.7 cm.) penetration of the sampler at the bottom of the hole; and
- 2,520 in.-lb (2903 kg-cm) energy delivered to the sampler (60% of theoretical maximum).

The energy delivered to the sampler is typically not measured, but it has been shown to depend on the type of hammer and size of the drill rods used in the penetration testing. In the United States, the most commonly used hammer configuration is a rope and pulley system using a safety hammer (Seed and others, 1984). AW drill rods are often used in shallow geotechnical borings drilled in the Puget Sound region. Consequently, SPT N-values obtained from these borings would follow this detail of the recommended practice of Seed and others (1984). Use of a rope and pulley safety hammer system with AW rods would ideally result in a 60 percent transfer of energy to the sampler at depths less than 50 ft (15.2 m) (Seed and others, 1984), which would satisfy their recommended parameters.

The N-values reported in many borings drilled since the mid-1980s by the Washington State Department of Transportation were obtained using a variety of automatic trip hammers. Recent measurements performed on two WSDOT trip hammers indicated approximately 70 percent efficiency in energy transfer to the drill rods (American Society of Civil Engineers Seattle Section Geotechnical Group, 1995). However, many of the boring logs obtained from WSDOT files predate the use of the automatic trip hammer, and many recent WSDOT boring logs do not document the type of hammer (rope and cathead versus trip hammer) used in the SPT testing. Measurements of hammer efficiency made as part of the 1995 ASCE Seattle Section Geotechnical Group spring seminar on in-situ testing for seismic evaluation demonstrated that an assumption of approximately 60 percent efficiency is only appropriate for carefully conducted SPT testing using a rope and cathead safety hammer or an automatic trip hammer (American Society of Civil Engineers Seattle Section Geotechnical Group, 1995). As a minimum criterion, measurement of the SPT-N value in all borings used in this study explicitly adhered to ASTM D1586-84. I have treated SPT blow counts from all data sources as if the hammer efficiency were 60 percent. This may lead to a biased estimate of the calculated fac-

tors of safety, but as I have treated all borings in the same manner, this bias should have little effect in my evaluation of the relative susceptibility of the various liquefaction categories.

The most significant departure from the recommended procedures of Seed and others (1984) is the regular use of hollow-stem augers instead of rotary methods in drilling geotechnical borings in the Puget Sound region. A standard auger has an 8-in. (20.4 cm) O.D. and a 4-in. (10.2 cm) I.D. and drills a hole larger than the 4- to 5-in. (10.2 to 12.7 cm) optimal size. Water, rather than bentonite mud, is often used as the drilling fluid, if fluid is used at all during drilling. However, Seed and others (1988) have shown that the type of fluid (drilling mud or water) does not affect the SPT blow counts.

Shannon & Wilson, Inc. (1990) suggested that SPT N-values measured in borings drilled using hollow-stem augers are consistently lower than those measured in rotary-drilled borings. The certainty of this observation is obscured by the mixed use of safety- and donut-type hammers in their study. Shannon & Wilson, Inc., (1993) drilled paired rotary and hollow-stem auger borings with the same drill rig at three sites in the Puyallup valley and reported no significant bias in measuring SPT N-values for the different drilling methods. Only a small number of borings in this study's data set are known to have been drilled using rotary methods. For the majority of the available borings either the method of drilling was not reported or they were drilled using hollow-stem augers. Thus, this study ignores any bias introduced into SPT N-values measured in hollow-stem auger borings on pragmatic grounds: it would not be possible to perform a defensible evaluation of liquefaction susceptibility using only the sparse data set provided by rotary-drilled borings.

### Historic Liquefaction

The two largest earthquakes in recent historic times in the Puget Sound region are the 1949 surface wave magnitude ( $M_S$ ) 7.1 Olympia and the 1965  $M_S$  6.5 Seattle-Tacoma earthquakes. The study area was exposed to Mercalli Modified Intensity VIII and VII shaking in the 1949 and 1965 events, respectively (Murphy and Ulrich, 1951; Roberts and Ulrich, 1951; von Hake and Cloud, 1967). Sites of ground failures caused by liquefaction in the study area have been reported by Hopper (1981) and Chleborad and Schuster (1990). Four sites where liquefaction occurred are identified by the reference number used in Chleborad and Schuster (1990) and are shown in Plate 1. Table 3 reproduces the information given for the sites identified in Chleborad and Schuster (1990).

All of the historic liquefaction sites are located in the Puyallup and upper Duwamish valleys in Holocene alluvium (Category I deposits). Sites 57, 58, and 59 are located near the confluence of the Puyallup and White Rivers, and site 56 is slightly farther to the south. The reports for sites 56, 57, and 58 all clearly describe sand blows caused by the 1949 earthquake. The description of the sand as "clean and black" at site 58 is similar to that given for many of the liquefaction reports in the Puyallup valley cited in Shulene (1990), Chleborad and Schuster (1990), and Palmer and others (1991). The phenomena observed at site 59 during the 1949 and 1965 earthquakes are somewhat equivocal evidence of liquefaction. The ob-

**Table 3.** Descriptions of ground failures in the Sumner 7.5-minute quadrangle caused by the 1949 and 1965 Puget Sound earthquakes (modified from table 2 of Chleborad and Schuster, 1990; metric values and explanatory information in brackets). Location numbers correspond to ground-failure location numbers on Plate 1. All data have location accuracy A, indicating that available information allowed accurate relocation on the map

Loc. no.	Failure type (year of earthquake)	Reference municipality	Quotation and (or) comment
56	sand boils (1949)	Sumner, Wash.; Pierce County	Two sand boils, about 3 ft [0.9 m] in diameter and 5 or 6 in [13–15 cm] high, occurred at 8502 Riverside Rd. East in Sumner. Water from the sand boils had a salty taste (Francis Watson, personal commun., 1989).
57	sand boils (1949)	Sumner, Wash., Pierce County	Blows of water and sand occurred on my property about 1 mile [1.6 km] east of Sumner (Herman Nix, personal commun., 1989).
58	sand boils (1949)	Sumner, Wash., Pierce County	Many sand boils, about 6 in [15 cm] in diameter and 1 or 2 in [2.5–5 cm] high, were produced by the 1949 earthquake near the corner of Main and Van Tassel on the east side of Sumner. The sand was clean and black (Fred Weber, personal commun., 1989).
59	settlement (1965); misc. effects (1965); misc. effects (1949)	Sumner, Wash., Pierce County	"Broken water mains*** cement porch fell toward street*** school gymnasium walk sunk in and cracked. [In the 1949 earthquake] springs disappeared, water lowered" (Mrs. L. R. Bariekman, written commun., 1965).

served settlements could be the result of dynamic compaction of loose soils and not necessarily liquefaction-induced subsidence. Also, the report of the disappearance of springs and "lowering of water" is not a direct indication of soil liquefaction. The correspondence between the abandoned channels mapped on Plate 1 and three of the four liquefaction sites is noteworthy in that it suggests a higher susceptibility in the vicinity of these geomorphic features.

### LIQUEFACTION SUSCEPTIBILITY OF THE OSCEOLA MUDFLOW

Recent studies of historic liquefaction in the Puyallup valley (Palmer and others, 1991; Pringle and Palmer, 1992; Shannon & Wilson, Inc., 1993; Dragovich and others, 1994) have suggested that Holocene sedimentation has been dominated by lahars originating from Mount Rainier and that these deposits can be extremely susceptible to liquefaction. The Osceola Mudflow crops out widely in the Sumner quadrangle and is found in the subsurface of the upper Duwamish and Puyallup valleys (Dragovich and others, 1994). Because of the great extent of this deposit in the study area, it is important to make some determination of its susceptibility to liquefaction.

A significant factor inhibiting liquefaction is the gravel, silt, and clay content of a susceptible sandy soil (National Re-

search Council Committee on Earthquake Engineering, 1985). Crandell (1963, p. A46) describes the Osceola Mudflow as "...an unsorted and unstratified mixture of subrounded to subangular stones in a purplish-gray plastic clayey-sand matrix." Crandell's description of the Osceola Mudflow as a clayey sandy gravel suggests that this deposit may not be capable of liquefying during an earthquake. However, a quantitative approach to evaluating the soil properties of the Osceola Mudflow is needed in order to fairly assess the liquefaction susceptibility of this deposit.

#### Evidence of Past Liquefaction

During a geotechnical investigation conducted by WSDOT for the State Route 167/32nd Street Interchange, several undisturbed samples of the upper contact of the Osceola Mudflow and the overlying alluvial soils were obtained. An undisturbed sample S-2 was obtained in boring BH-1-93 between 40 and 42 ft (12.2 and 13.3 m) depth; the boring log of BH-1-93 is presented in Figure 9. As sample S-2 was pushed from the Shelby tube, it separated along a number of vertical and horizontal sand-filled partings between 40.0 and 40.6 ft (12.2 and 12.4 m). These partings occurred along apparent sand injections into the silty alluvium that directly overlies the Osceola Mudflow. The contact between these two units was observed in the bottom of this undisturbed sample at a depth of 41.5 ft (13.1 m).

The Osceola Mudflow is 46 ft (14.1 m) thick in boring BH-1-93, so it is unlikely that the observed dikes originated in the loose sands underlying the Osceola. Thus, the source for the sand dikes is either within the Osceola or in proximate liquefiable sands interbedded with the silty alluvium. The latter source seems unlikely from a geometric standpoint; the liquefied sand would need to have been injected as sills and move laterally and (or) downward into the silty alluvium immediately adjacent to boring BH-1-93 to form the dikes and sills observed in sample S-2. However, downward-directed injected sand has been observed in liquefaction features resulting from the 1964 Good Friday earthquake in Alaska (T. J. Walsh, DGER, oral commun., 1995). These features resulted from liquefaction at depths less than 3 ft (0.9 m) below ground surface.

The sand in the dikes and sills in sample S-2 is predominantly fine grained and crystal rich and contains numerous pumice fragments with only a minor component of other lithic grains. The fine sand fraction in a sample of the Osceola Mudflow obtained at a depth of 50.0 to 51.5 ft (15.2 and 15.6 m) in

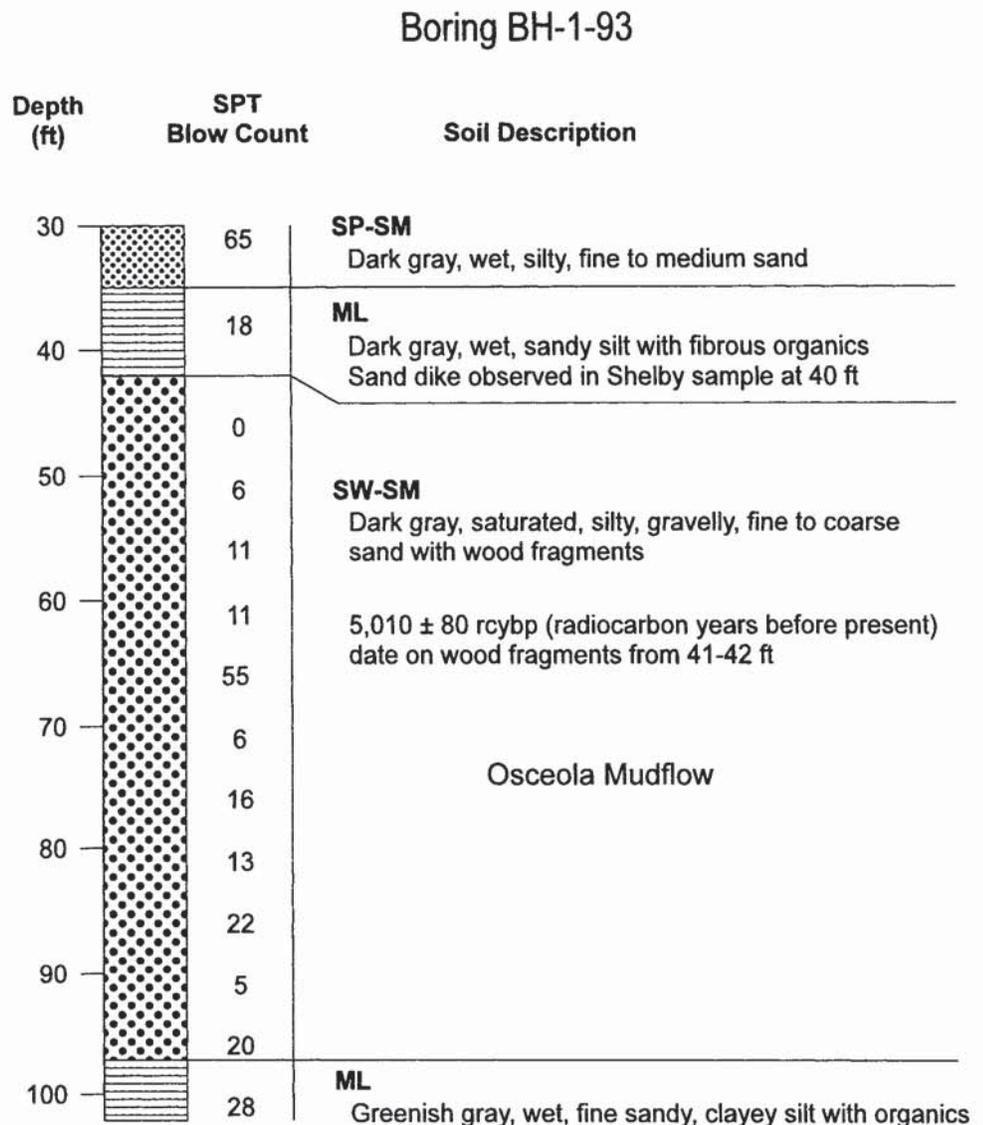


Figure 9. BH-1-93 boring log.

boring BH-1-93 contains a higher percentage of lithic grains, but it also is primarily composed of crystals and pumice.

Crandell (1963) indicates that in its outcrop area the upper 10 to 12 ft (3.0 to 3.6 m) of the oxidized (weathered) zone of the Osceola Mudflow is cemented with iron oxides and perhaps with secondary silica. He notes that this oxidized and cemented zone may be "... largely the result of seasonal alternations of moisture conditions that promote excessive oxidation, hydration, and dehydration." The presence of this cemented zone results in a high bearing capacity at the surface. Crandell (1963) reports that the mudflow beneath this cemented layer has practically no bearing strength when near its liquid limit. He points to examples from a deep excavation for a sawmill southwest of Buckley and from construction of the footings for the Rainier State School east of Buckley, which extended below the cemented layer into the unoxidized mudflow material.

Chleborad and Schuster (1990) provide comprehensive documentation of failures caused by the 1949 and 1965 Puget Sound earthquakes. Their compilation shows that liquefaction was not observed in areas of Osceola Mudflow outcrop. Ishihara (1985) presents a series of curves that demonstrate the

effect of a nonliquefiable surface layer on the potential for level-ground disturbance during liquefaction of the underlying soil column. For a PGA of 0.2 g, Ishihara's relations would predict that the 10- to 12-ft (3.0 to 3.6 m) -thick cemented surface layer of the Osceola Mudflow would preclude the development of liquefaction-related effects at ground surface. Although these relations are based on a small number of case studies, they do suggest that the lack of liquefaction effects observed during the 1949 and 1965 earthquakes does not necessarily imply lack of liquefaction of the Osceola Mudflow below the zone of cementation.

### Data on Relevant Soil Properties

Three considerations in evaluating the liquefaction susceptibility of the Osceola Mudflow in the subsurface of the Puyallup valley are:

- the relative density of the soils, as estimated from index measurements such as SPT blow counts and cone penetrometer tip (CPT) resistance;
- the amount of silt and gravel fraction present in this soil unit as estimated from gradation data;
- the plasticity of the silt and (or) clay fraction based on Atterberg limits.

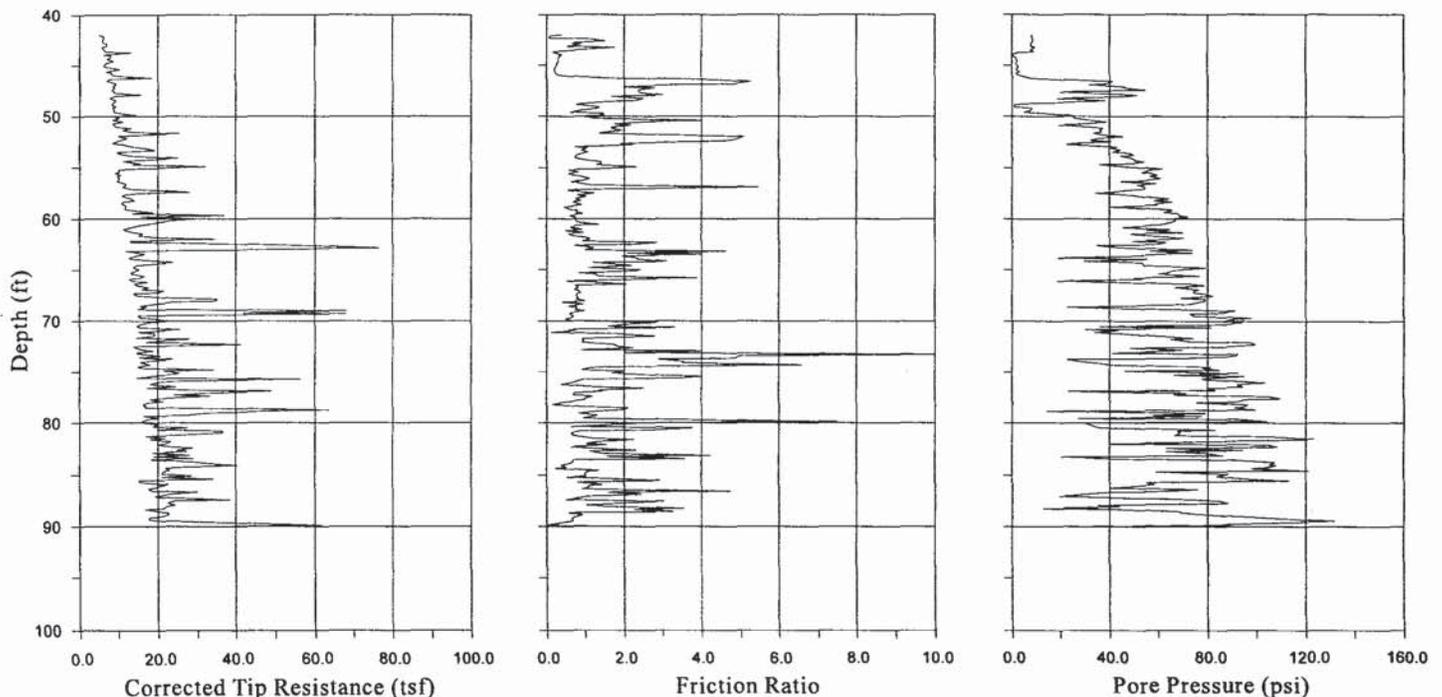
Figure 4C plots both the average raw blow counts ( $N_1$ ) and the corrected average blow counts ( $(N_1)_{60}$ ) against depth below the top of the Osceola Mudflow. The effective stress and energy normalization corrections of the  $N_1$  values are performed as suggested by Seed and others (1983, 1985) assuming a hammer efficiency of 60 percent. The average uncorrected blow counts show a general increase with depth, but average corrected blow counts are fairly constant with depth, and indicate that the mudflow deposit is a loose to medium dense soil.  $(N_1)_{60}$  values of 11 or less would allow liquefaction of a silty sand containing 35 percent fines at a cyclic stress

ratio of 0.2. This cyclic stress ratio is within the levels of ground motions characterized by the larger scenario earthquake used in this study and is likely within the range of ground motions expected at this site since the mid-Holocene.

Corrected tip resistances in CPT-1 (Fig. 10), located adjacent to boring BH-1-93, range from 5 to 30 bars (ignoring spikes), indicating loose soil conditions. Correction for excess pore pressure follows a method reviewed in Robertson and Campanella (1989) and uses a net area ratio of 0.6 for the Hoggentogger piezo-cone used by WSDOT. Figure 11 is a cross-plot of the corrected tip resistance versus friction ratio for the Osceola Mudflow penetrated in CPT-1. Points that plot inside the outlined region (as shown in Robertson and Campanella, 1989) can be considered susceptible to liquefaction. As can be seen from the figure, a large proportion of the data points falls in the region that indicates susceptibility to liquefaction.

Figure 4A presents gradation data obtained from surface outcrops of the Osceola Mudflow (Crandell, 1971) and from subsurface samples of this deposit in the Puyallup and upper Duwamish valleys. The figure shows the envelopes of the two gradation data sets. Inherent in the subsurface data are problems with undersampling of the coarse gravel fraction due to the size of the split-spoon sampler (1 $\frac{3}{8}$  in. I.D.) and the 'nugget' effect resulting from a single large clast dominating the weight percentage of a small volume of sample. The biases introduced from these two effects are opposing and may to an extent be self-canceling. However, it is not possible to accurately quantify these effects, and only some broad generalizations can be made from these gradation data:

- The particle-size distribution of the surface and subsurface samples are very similar.
- The fines content ranges between 5 and 30 percent and averages about 20 percent, and the gravel content ranges from 10 to 60 percent and averages about 25 percent.



**Figure 10.** Display of tip resistance (corrected for pore pressure), friction ratio, and pore pressure for the section of CPT-1 penetrating the Osceola Mudflow. Depth range of the mudflow was determined from sample descriptions in the adjacent boring BH-1-93.

- The subsurface samples may contain a smaller percentage of fines and a larger percentage of sand fraction than the surface samples.

Crandell's (1977) gradation data for eight surface samples indicate that the clay-size fraction ranges from 7 to 13 percent by weight. Scott and others (1992, p. 23) describe the Osceola Mudflow as "remarkably clayey; the composite deposit (matrix and coarse phases) contains 2 to 15 percent clay, with a mean of 7 percent (13 samples)." Clay percentages referred to by Scott and others (1992) are particle-size measures, not mineralogical percentages. Crandell (1971, p. 28) states that "Clay minerals make up from 60 to 80 percent of the clay-sized fraction of each sample. Montmorillonite and kaolinite predominate in various proportions in every sample of the deposit...."

Crandell (1971) also reports the Atterberg limits on the 0.420 mm and smaller (clay-size) fraction of the eight samples of the Osceola Mudflow. Liquid limits range from 28 to 42 percent with an average of 33 percent, and plasticity indices range from 2 to 15 percent with an average of 8.4 percent. These results indicate that the clay-size fraction of the Osceola Mudflow has low plasticity, falling in the range of ML to CL soils. Only one Atterberg test was available for a subsurface sample of the Osceola Mudflow. This test was performed on material passing the No. 40 sieve and indicated a liquid limit of 27.3 percent and a plasticity index of 5.8 percent. This sample has a fines content of 28.2 percent and a gravel content of 13.4 percent.

### Liquefaction of the 1980 Mount St. Helens Debris Avalanche Deposit

Jenkins and others (1994) report that geotechnical studies performed during construction of the Spirit Lake Memorial Highway (State Route 504) determined that the debris avalanche deposits from the 1980 eruption of Mount St. Helens are highly susceptible to liquefaction. To mitigate the liquefaction hazard, WSDOT found it necessary to densify the soils underlying the approach fill and bridge abutment footings at Bridge 12, which crosses South Coldwater Creek at the mouth of Coldwater Creek valley. Blast-induced densification was chosen on the basis of cost and feasibility of construction (Jenkins and others, 1994). Liquefaction resulting from the blasting was indicated from pore-pressure measurements and by the observation of sand boils and upward ground-water flow after detonation. A quarter-mile farther up Coldwater Creek valley from Bridge 12, vibrations from the passage of heavy construction equipment generated lateral spreading in debris avalanche deposits that had not been densified during the Bridge 12 project.

Fairchild (1985, 1987) discusses liquefaction of the debris avalanche deposits as a possible mode of generation of the North Fork Toutle River mudflow. This mudflow was initiated approximately 3 hours after the May 18, 1980, eruption in response to a long period of strong harmonic tremors. Fairchild (1985, 1987) reconstructed the timing and behavior of the debris flow and was able to determine its source area. Gradations from debris avalanche material collected from the source areas fell within the following ranges: gravel and coarser (26–36 percent), sand (44–59 percent), and fines (15–20 percent). These grain-size ranges are similar to those of subsurface samples of the Osceola Mudflow previously discussed. Grain-size data from Glicken (1986) show that the clay-size fraction of

the debris avalanche deposit ranges from 0.09 to 3.39 percent and averages 1.07 percent, which is significantly less than the ranges and averages reported by Crandell (1971) and Scott and others (1992) for outcrop samples of the Osceola Mudflow.

The Mount St. Helens debris avalanche deposits are poorly sorted (well graded in soils engineering terminology) and are similar to the Osceola Mudflow in terms of the gradation ranges. Recent experience has demonstrated that these debris-avalanche deposits are susceptible to liquefaction, even though they may contain significant percentages of fines and gravel. Because these gravelly deposits are matrix supported rather than clast supported, their susceptibility to liquefaction is primarily determined by the behavior of the sand and fine fractions.

### Case Studies of Liquefaction of Silty Soils

Liquefaction of fine-grained mine tailings (tailings slimes) is reviewed by Ishihara (1985). His review indicates that mine tailings susceptible to liquefaction exhibit the following characteristics:

- low relative density as a result of the method of emplacement;
- clay-size fractions ranging from 10 to 30 percent by weight, although this size fraction is essentially devoid of clay minerals;
- plasticity indices ranging from 1 to 11 percent—that is, these are low plasticity silts.

I reviewed two case studies of earthquake-induced liquefaction in native silty soils that are relevant to evaluation of the liquefaction susceptibility of the Osceola Mudflow. The first study is an analysis of lateral spreading at the San Fernando Valley Juvenile Hall during the 1971 Sylmar earthquake (Bennett, 1989). The second evaluates liquefaction in Ying Kou City, People's Republic of China, during the Haicheng earthquake of 1975 (Arulanandan and others, 1984, 1986).

The soils identified as having liquefied at the San Fernando Valley Juvenile Hall site are primarily silts with interbedded very silty sands (Unit B of Bennett, 1989). The fines content averages 62 percent and ranges from 38 to 83 percent; the clay-size fraction averages 10 percent and ranges from 3 to 20 percent. Uncorrected blow counts average 6.7 and range from 2 to 12;  $(N_1)_{60}$  values range from 3.7 to 10.6 (Bennett, 1989). These silts have plasticity indices ranging from 2 to 11 percent and liquid limits ranging from 25 to 35 percent. Consequently these soils are classified as low plasticity silts (ML and ML-CL soils). Liquidity indices for these soils are less than unity, indicating that these soils are not sensitive.

Soils that were identified by Arulanandan and others (1986) to have liquefied during the Haicheng earthquake range from silty sands to sandy silts and from clayey silts to silty clays. Laboratory testing and field evaluation indicate that these fine soils have:

- liquid limits ranging from 24 to 35 percent, and plasticity indices ranging from 6 to 14 percent;
- liquidity indices nearly equal to or greater than one, with a maximum of 2.3, indicating that these are sensitive soils;
- fines contents ranging from 40 to 90 percent;

- uncorrected SPT blow counts ranging from 0 to 14 in the upper 33 ft (10 m) of the soil column.

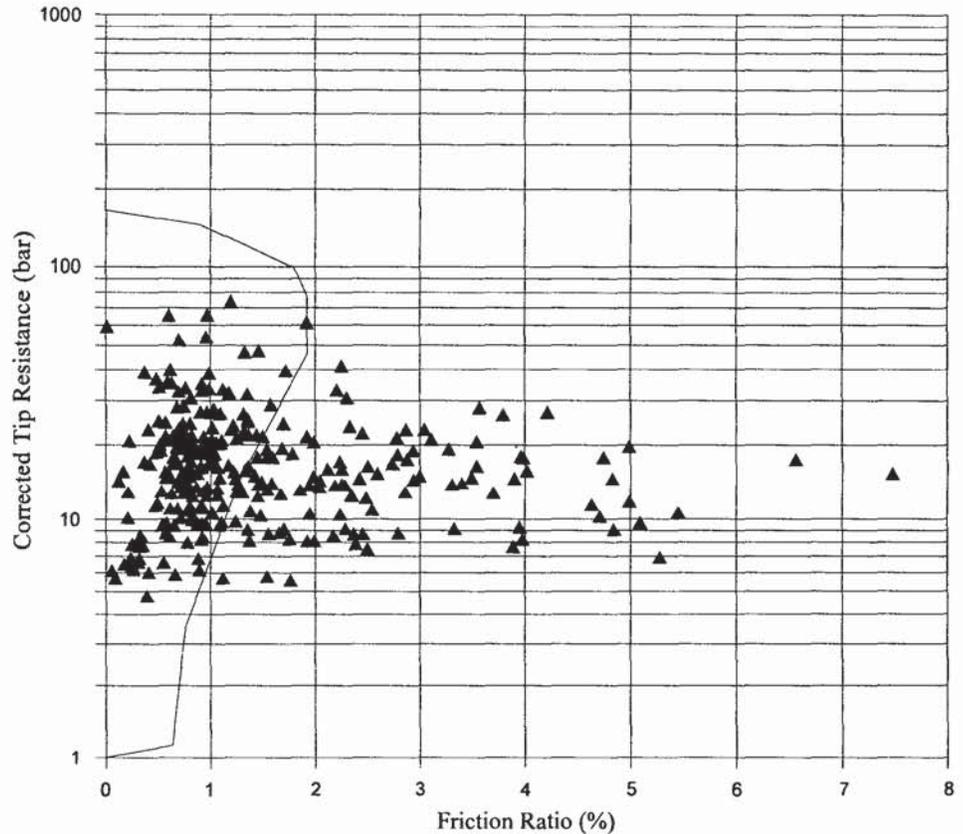
Figure 12 is a cross-plot of corrected tip resistance versus friction ratio for CPT soundings at liquefaction sites in Ying Kou City (data from Arulanandan and others, 1986). The CPT data used in this plot did not have simultaneous pore-pressure measurements. Arulanandan and others (1986) present data from a single piezo-cone sounding at one of the liquefaction sites; this sounding shows that excess pore pressures are generated during tip advance and that correction of the tip resistance data is needed. The correction scheme that I adopted is based on the evaluation of this piezo-cone sounding and is as follows:

- A correction of  $2.5 \text{ kg/cm}^2$  was used at intervals where the uncorrected tip resistance was less than  $15 \text{ kg/cm}^2$ ;
- A correction of  $1.0 \text{ kg/cm}^2$  was applied for tip resistances equal to or greater than  $15 \text{ kg/cm}^2$ .

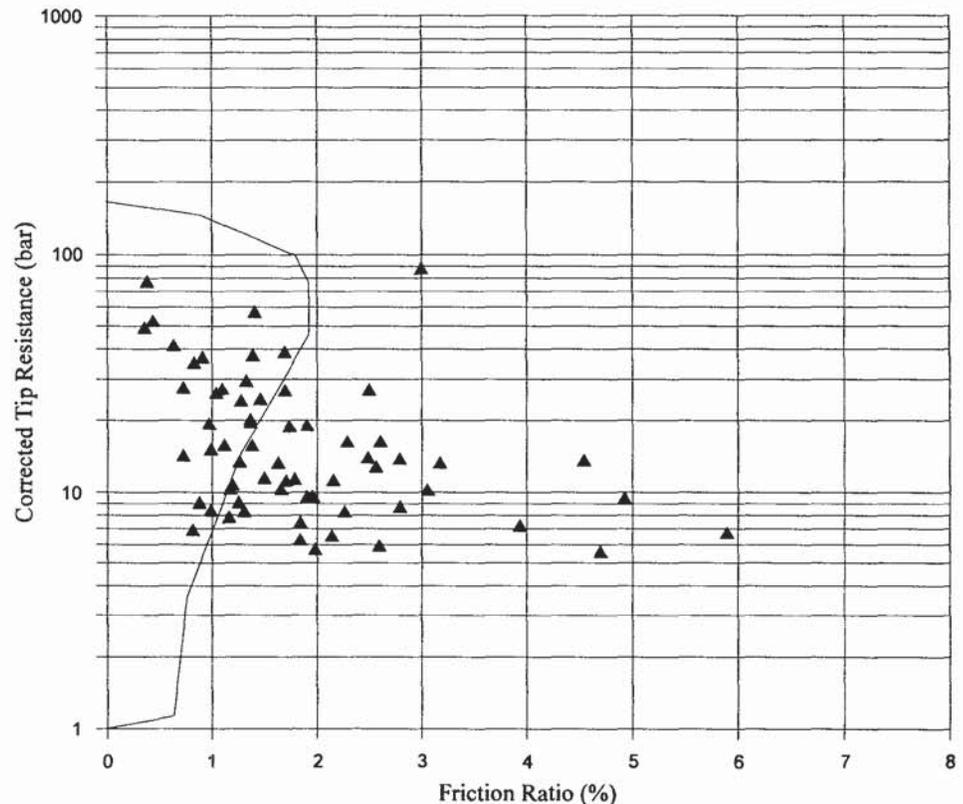
The CPT cross-plot shows that many of the data points fall within the region defined in Robertson and Campanella (1989) as being susceptible to liquefaction. The CPT cross-plots for the Osceola Mudflow and the Ying Kou City liquefaction sites (Figs. 11 and 12, respectively) are in fact quite similar. This similarity supports the hypothesis that the Osceola Mudflow may be susceptible to liquefaction. Arulanandan and others (1984) estimate that cyclic stress ratios of 0.1 to 0.15 were generated in Ying Kou City during the Haicheng earthquake, which is in the range of cyclic stress ratios generated within the Osceola Mudflow under the earthquake scenarios used in this study.

### Synthesis of Data and Interpretations for the Osceola Mudflow

The Osceola Mudflow found in the subsurface of the Puyallup and upper Duwamish valleys is typically a gravelly, silty sand having a USCS classification of SW-SM. Silt content typically ranges from 10 to 30 percent, and gravel content ranges from 10 to 60 percent. Atterberg limits on the clay-size fraction of the Osceola Mudflow indicate low plasticity, and a single test



**Figure 11.** Cross-plot of corrected tip resistance versus friction ratio for the section of the Osceola Mudflow penetrated by CPT-1. Note that many of the data points fall within the outlined region (Robertson and Campanella, 1989) delineating soils that are susceptible to liquefaction.



**Figure 12.** Cross-plot of tip resistance (corrected for pore pressure) versus friction ratio for sites in Ying Kou City where the Haicheng earthquake caused liquefaction. Data are from Arulanandan and others (1986). Many of the data points fall within the outlined region (Robertson and Campanella, 1989) delineating susceptibility to liquefaction. Note the similarity of this cross-plot to that for the Osceola Mudflow shown in Figure 11.

on material passing the No. 40 sieve likewise shows low plasticity. Corrected SPT blow counts and cone penetrometer tip resistance values indicate that the mudflow is a loose to medium dense soil where sampled in the sub-surface of the upper Duwamish and Puyallup valleys.

Vibration or blast-induced liquefaction of the debris avalanche deposits of the 1980 eruption of Mount St. Helens has been documented by Fairchild (1985, 1987) and Jenkins and others (1994). These deposits are typically poorly sorted silty gravelly sands and are similar to the Osceola Mudflow in terms of particle size and gradation. Both these deposits are matrix supported; consequently their liquefaction susceptibility is dominated by the sand and fines fractions. The Mount St. Helens debris avalanche deposits have a smaller clay-size fraction (average 1.07 percent) than the average for the Osceola Mudflow (7 percent). However, there is considerable range in clay content in the Osceola Mudflow samples (2 to 15 percent), and only about 75 percent of the clay-size fraction is actually composed of clay minerals. Thus it is necessary to examine the potential effects of the silt and clay fractions on liquefaction of the Osceola Mudflow.

I have reviewed two documented case histories of liquefaction occurring in silt soils. In both cases, the silt soils were of low plasticity, with plasticity indices and liquid limits falling in the same ranges as those documented for the clay-size fraction of the Osceola Mudflow. Based on the  $(N_1)_{60}$  values reported in these case studies, the soils that liquefied were typically loose to medium dense. Cross-plots of CPT corrected tip resistance and friction ratio obtained at liquefaction sites in Ying Kou City and CPT data from the Osceola Mudflow in the upper Puyallup valley suggest that the mudflow may be liquefiable. Finally, the sand dikes observed in WSDOT boring BH-1 provide equivocal evidence of liquefaction of the Osceola Mudflow based on their proximity to the upper contact of this stratigraphic unit.

From these data and analyses I have concluded that the fines fraction of the Osceola Mudflow is not sufficiently plastic to impede the generation of high pore pressures that could result in liquefaction of these soils under suitable earthquake ground-motions. There is no significant difference in plasticity between the fines fraction of the Osceola Mudflow and the silty soils that liquefied at the San Fernando County Juvenile Hall and in Ying Kou City. Likewise, the gradation and Atterberg limits data for the mudflow fall within the range of criteria presented by Seed and Idriss (1982) for silt soils that may be vulnerable to significant dynamic strength loss. These criteria are:

- percent finer than 0.005 mm <15 percent;

**Table 4.** Criteria used by Shannon & Wilson, Inc., (1993) for rating the hazard due to liquefaction based on analysis of geotechnical boring data in the Tacoma area

Percentage of borings in a geographic location with thickness of liquefied sediment $\geq$ : (a) 3.05 m (10 ft) for a 0.30 g event, and (b) 0.305 m (1 ft) for a 0.15 g event	Hazard rating
>50	High
25–50	Moderate
1–25	Low
<1	Very low

- liquid limit <35 percent;
- water content > 0.9 times the liquid limit.

Experimental and field studies have tended to examine only noncohesive granular soils and rarely have evaluated the response of a well-graded deposit such as the Osceola Mudflow. Therefore, the existing field-performance methods are potentially unsatisfactory in assessing the pore-pressure behavior and residual strength of a silty, well-graded soil such as the mudflow. Consequently, the dynamic strength behavior and liquefaction susceptibility of the Osceola Mudflow should be the subject of further investigation. This investigation should involve acquisition of gradation and plasticity data from additional borings drilled both in the alluvial valleys and in outcrop areas, and laboratory and field measurement of dynamic strength properties.

## RESULTS OF LIQUEFACTION ANALYSIS FOR THE SUMNER QUADRANGLE

Figure 13 is a cumulative frequency histogram for each scenario earthquake showing the percentage of the total borings located in Category I deposits that equal or exceed an aggregate thickness of liquefiable soils expressed as a percentage of the total boring depth. The aggregate thickness is the sum of the thicknesses of all soil units that would liquefy at the magnitude and for PGA value chosen for the scenario earthquake. Figure 13 shows the histograms for the two scenario earthquakes used in this study (a  $M_w$  7.3 earthquake that produces a PGA of 0.15 g or 0.30 g). The abscissa of the histograms measures the aggregate thickness of liquefiable material in a boring (expressed as a percentage of the depth of the boring). For borings drilled deeper than 40 ft (12.1 m), only the upper 40 ft (12.1 m) were analyzed for susceptibility to liquefaction. The ordinate delineates the percentage of the total number of borings that contain a percentage of liquefiable material greater than the abscissa value.

In constructing Figure 13, I have considered the Osceola Mudflow to be liquefiable and amenable to the factor of safety analysis of Seed and others (1983, 1985). In performing the factor of safety analysis, I assigned a 35 percent fines content to the soil unit (USCS soil class SW–SM), Table 1) composed of Osceola Mudflow. Figure 13 shows that 51 percent of the borings had at least 1 ft (0.3 m) of liquefiable material for the 0.15 g event and 36 percent had at least 10 ft (3.0 m) of liquefiable soils for the 0.30 g earthquake assuming a boring depth of 40 ft (13.2 m). Table 4 presents the thickness criteria used by Shannon & Wilson, Inc. (1993) to rank the relative liquefaction susceptibility of the various soil units in their study area. Using these criteria, Category I deposits have a moderate hazard rating as only 36 percent of the borings have 10 ft (3.0 m) of liquefied soils for the 0.30 g case.

**Table 5.** Relative liquefaction susceptibility and associated hazard rating from Youd and Perkins (1987)

Relative susceptibility	Hazard rating
1.0 to 10.0	High
0.1 to 1.0	Moderate
0.01 to 0.1	Low

A second method of ranking the liquefaction susceptibility of a soil deposit is presented by Youd and Perkins (1987). They calculate relative susceptibility to liquefaction using the following expression:

$$\text{Relative susceptibility} = [(A \times B \times C)/10] \times 100, \text{ where,}$$

A = percent of sandy soils expressed as a decimal fraction;

B = percent of these soils that are liquefiable if saturated, expressed as a decimal fraction;

C = percent of these soils that are saturated, expressed as a decimal fraction.

Their hazard rating scheme is based on the relative susceptibility and is summarized in Table 5.

Youd and Perkins (1987) evaluated and mapped the liquefaction susceptibility of soil deposits found in San Mateo County, California, using the field evaluation methodology of Seed and others (1983, 1985). In their liquefaction analysis they used a scenario earthquake with a magnitude of 6.5 that produces a PGA of 0.20 g (Youd and others, 1975). I calculated factors of safety for a number of soil profiles using both the scenario earthquake of Youd and Perkins (1987) and the magnitude 7.3 earthquake producing a PGA of 0.15 g used in this study. I found that for a variety of subsurface conditions in which liquefaction is marginal (that is, the factor of safety is near unity), this study's scenario earthquakes yielded factors of safety from 10 to 15 percent higher (see footnote 4) than those calculated using Youd and Perkins' (1987) scenario event. Because I have used a less severe earthquake to evaluate liquefaction susceptibility in the study area than that used by Youd and Perkins (1987), my results are not directly comparable.

Inspection of Youd and Perkins expression for computing relative susceptibility shows that this value can be obtained by integrating the cumulative frequency histogram (after conversion of percentages to their equivalent decimal values) and multiplying the result by 10. This calculation yields a relative susceptibility of 2.01 for Category I deposits in the Sumner quadrangle for the 0.15 g cumulative frequency histogram. This relative susceptibility falls well within the high hazard using the Youd and Perkins (1987) ranking criteria (Table 5).

I did not rigorously account for the differences in calculated factors of safety resulting from the different scenario earthquakes used in this study

and in Youd and Perkins (1987). However, it is clear that a more severe scenario earthquake will result in a greater amount of liquefaction (for example, compare the 0.15 g and 0.30 g cumulative frequency histograms shown in Fig. 13), and that Youd and Perkins (1987) used a more severe scenario event in their liquefaction susceptibility analysis. Consequently the relative susceptibility calculated from my analysis (2.01) somewhat underestimates the value I would have obtained if I had used earthquake magnitudes and PGAs comparable to those used by Youd and Perkins (1987).

Figure 14 is a cumulative frequency histogram constructed for Category I deposits, but in borings that penetrated the Osceola within the upper 40 ft (13.2 m), only the alluvial de-

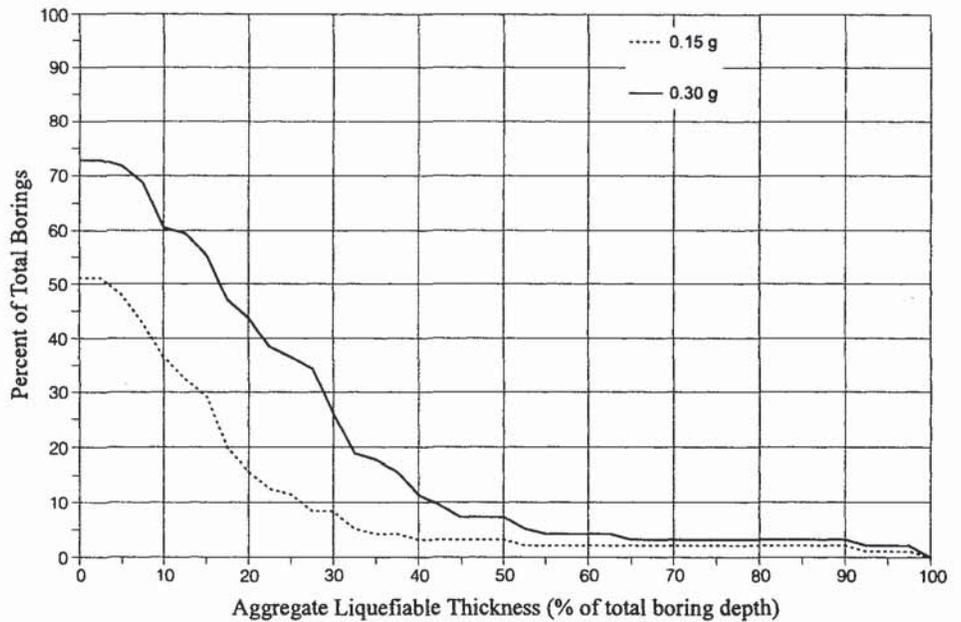


Figure 13. Cumulative frequency histogram for the upper 40 feet of Category I deposits in the Sumner quadrangle for a hypothetical  $M_w$  7.3 event. The Osceola Mudflow was considered liquefiable and included in computation of the aggregate thicknesses.

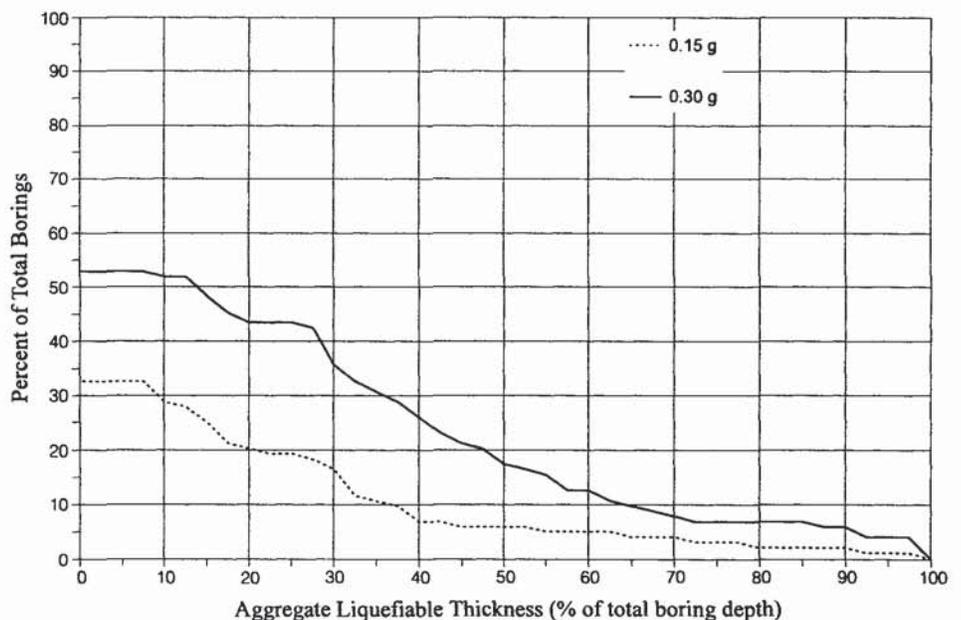


Figure 14. Cumulative frequency histogram for the upper 40 feet of Category I deposits in the Sumner quadrangle for a hypothetical  $M_w$  7.3 event. The Osceola Mudflow deposits were excluded in the computation of aggregate thicknesses.

posits that overlie the Osceola were analyzed. In this case the upper contact of the Osceola Mudflow was treated as the final depth of the boring. Figure 14 shows that 33 percent of the borings had at least 1 ft (0.3 m) of liquefiable material for the 0.15 g event, and that 52 percent had at least 10 ft (3.0 m) of liquefiable soils for the 0.30 g earthquake. Again, a boring depth of 40 ft (13.2 m) was assumed in estimating the thickness of liquefiable soils. Using the criteria of Shannon & Wilson, Inc., (1993) shown in Table 4, this subset of the Category I data again indicates ranking in the moderate hazard category. The relative susceptibility calculated using the methodology of Youd and Perkins (1987) for *non-Osceola* Category I deposits is 1.12, which corresponds to a high rating. This relative susceptibility is likewise somewhat underestimated because of the difference in scenario earthquakes between this study and that of Youd and Perkins (1987).

The four historical liquefaction sites in the Sumner quadrangle are in Category I deposits. Two methods of ranking liquefaction susceptibility indicate that Category I deposits have a hazard rating ranging from moderate to high. Past liquefaction of the Osceola Mudflow may be indicated by the sand dikes observed in sample S-2 in boring BH-1-93. Analysis of SPT blow counts, CPT soundings, gradations, and Atterberg limits and comparison of these data to case histories in which silt soils have liquefied indicate that the Osceola Mudflow is likely to develop excess pore pressure during dynamic loading and may be capable of liquefying during an earthquake. All of these lines of reasoning led to my decision to rank Category I deposits as having a high liquefaction susceptibility. *This ranking does not indicate that any specific locality within a Category I deposit will be underlain by liquefiable soils; the presence or absence of liquefiable material can only be determined by a site specific geotechnical investigation performed by a qualified practitioner.*

The abandoned channels and areas of bar and swale topography mapped in Plate 1 may represent areas of locally higher liquefaction susceptibility. As noted above, three of the four historic liquefaction sites in the Sumner quadrangle are in areas mapped as abandoned channels, and a fourth site is adjacent to an abandoned channel and a large area of bar and swale topography. The abandoned channels typically are low points in the local topography, and consequently the ground-water table would be at a shallower depth beneath these channels than beneath the adjacent flood plain. Both the abandoned channels and bar and swale topography will likely have a high proportion of sand-size sediments as a result of alluvial processes. Finally, some of these abandoned channels may have been filled during the development of the town of Sumner.

The outcrop area of the Electron Mudflow in the Puyallup valley is shown on Plate 1 and is considered a Category I deposit. The Electron Mudflow is underlain by alluvial deposits similar to those exposed farther downvalley near Sumner, which I have shown to have a high liquefaction susceptibility. Crandell (1963) observed that weathered sections of the Electron Mudflow have been weakly cemented by iron oxides, although the amount of cementation is not nearly as great as that of the weathered Osceola Mudflow. This cementation may inhibit both liquefaction of the Electron Mudflow and the surface expression of liquefaction of the underlying alluvial soils. However, sand boils and other indications of liquefaction were observed in the upper Puyallup valley near Orting during the

1949 earthquake. These liquefaction features occurred where the Electron Mudflow is likewise exposed in the floor of the valley. If these reports are reliable, the mudflow may not completely inhibit the surface manifestation of liquefaction of the underlying alluvial soils and may in fact itself be liquefiable. No geotechnical data were available in the Sumner quadrangle for the Electron Mudflow, so no quantitative liquefaction analysis could be performed. I include the Electron Mudflow as a Category I deposit in the Sumner quadrangle because it is undoubtedly underlain by very liquefiable alluvial soils and because of the accounts of liquefaction in the vicinity of Orting where the mudflow covers the valley floor.

Category II deposits include Holocene lacustrine and mass-wasting deposits and Vashon recessional (ice-contact and proglacial) lacustrine sand. There were no borings in this quadrangle that were drilled in areas mapped as Category II. The Holocene lacustrine deposits are primarily composed of peat, clay, and silt. They locally contain sandy layers as thick as 4 ft (1.2 m) (Crandell, 1963, p. 52). Crandell speculates that the sand in a peaty depression in the valley of Fennel Creek (sec. 35, T. 20 N., R. 5 E.) "probably was derived from outcrops of Vashon sand on the east side of the depression." Considering the topography and gradient of Fennel Creek upstream of the depression, it is likely that fluvial processes deposited the sands on top of the peat layer (P. T. Pringle, DGER, oral commun., 1995). Crandell also describes thick sand layers in peat deposits in the adjacent Buckley quadrangle; their origin is more difficult to explain because no major streams feed into those depressions. Because these Holocene lacustrine deposits are primarily composed of peat and silt, they would typically be considered as having a low liquefaction susceptibility. However, there was one reported site in a Holocene lacustrine deposit in the Big Soos Creek drainage north of the study area where liquefaction might have occurred during the 1949 earthquake (Chleborad and Schuster, 1990; Palmer and others, 1994).

Chleborad and Schuster (1990) report a number of earthquake-induced landslides and associated ground cracks along steep slopes in the Puyallup and Duwamish valleys. It is not clear, however, if these failures were caused primarily by liquefaction. Liquefaction-induced soil failures on steep slopes would be difficult to distinguish from landslides induced by the imposed ground accelerations, although a failure mechanism might be inferred by a thorough post-earthquake geotechnical investigation.

The Vashon (late Pleistocene) ice-contact and proglacial sandy lacustrine sediments found in the study area are similar in description, age, and geologic origin to Vashon glaciolacustrine units found in the Poverty Bay quadrangle that are assigned a moderate liquefaction susceptibility based on the analysis of geotechnical boring data (Palmer and others, 1995). Vashon recessional lacustrine deposits in the Sumner quadrangle, they have been included in Category II because of their similarity to the sandy glaciolacustrine units found in Poverty Bay quadrangle.

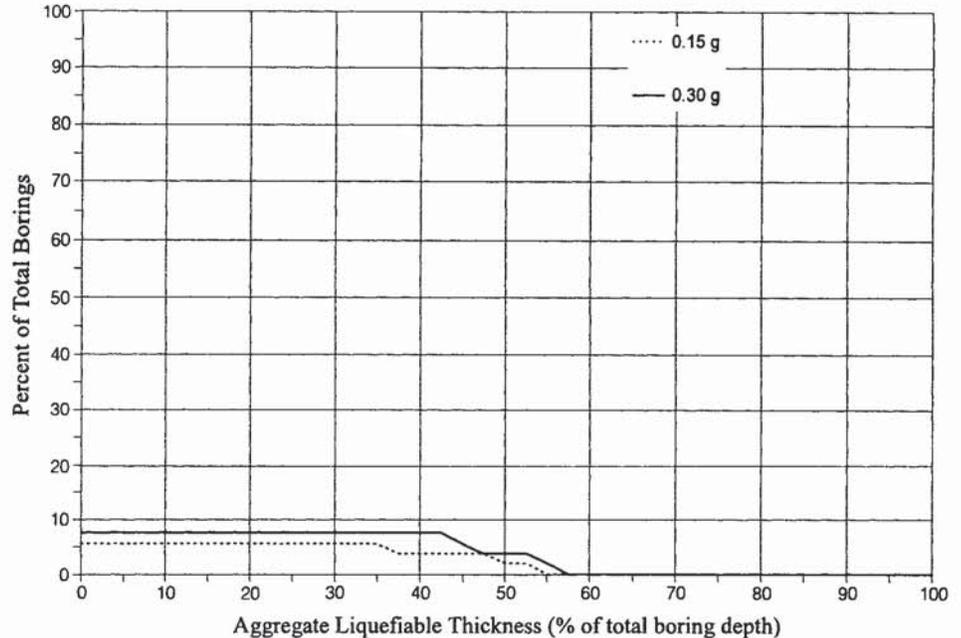
Soil types occurring in Holocene lacustrine and mass-wasting deposits are quite varied, ranging from nonliquefiable peat and organic silt to potentially liquefiable clean sand. Late Pleistocene ice-contact and proglacial sandy lacustrine sediments are likely to have a moderate susceptibility to liquefaction based on their similarity to other deposits found in the

Poverty Bay quadrangle and in the Olympia area (Palmer and others, 1995). The historic record of earthquake-induced ground failures observed in Category II deposits suggests that seismically induced liquefaction of these deposits is possible. Consequently I rank Category II deposits as having a low to moderate liquefaction susceptibility in order to reflect the variability in the geological and engineering characteristics of these deposits.

Figure 15 presents the cumulative frequency plot for Category III deposits based on 48 borings drilled in Vashon and older glacial and nonglacial deposits that have been overridden by at least one continental ice sheet. These deposits are typically quite dense and provide excellent foundation stability (Mullineaux, 1970). The relative susceptibility for Category III deposits is 0.007, which falls below the low liquefaction hazard rating of Youd and Perkins (1987). This category also has a very low ranking using the thickness criteria of Shannon & Wilson, Inc. (1993).

Data from seven borings drilled in Vashon recessional deposits (units Qit and Qpv in Fig. 2) were not used in constructing this figure because these deposits have not been compacted under the weight of an overriding ice sheet. Units Qit and Qpv have average uncorrected blow counts of 32 and 24, respectively. If representative, these average blow counts are sufficiently high to preclude liquefaction of these deposits under the levels of shaking represented by the choice of scenario earthquakes used in this study. Crandell (1963) describes both Qit (kame-terrace gravel) and Qpv (valley-train deposits) as a sand and pebble to cobble gravel containing scattered boulders. Thus, the high blow counts measured in the seven borings drilled in these units could be biased by the high gravel component of these sediments and are not necessarily representative of the relative density of these soils. Exposures of these Vashon recessional deposits have been identified on Plate 1 as sandy sections within these deposits may have some susceptibility to liquefaction because they have not been glacially compacted.

No boring data were available for the Osceola Mudflow in its outcrop area, where it is described as having fair to good foundation stability (Mullineaux, 1970). Crandell (1963) indicates that the upper 10 to 12 ft (3–4 m) of the weathered mudflow is oxidized and cemented and that it provides sufficient bearing capacity for light construction. However, below this weathered zone the mudflow becomes highly unstable when disturbed and near its liquid limit. Geotechnical analyses of unweathered sections of the Osceola Mudflow found in the Puyallup and Duwamish valleys indicate that these deposits might be susceptible to liquefaction. I have assigned this unit to Category III lacking more detailed information on the dynamic behavior of the unweathered portion of the Osceola Mudflow in its outcrop area.



**Figure 15.** Cumulative frequency plot for the upper 40 feet of the Category III deposits (excluding Vashon recessional deposits) in the Sumner quadrangle for a hypothetical  $M_w$  7.3 event.

No instances of liquefaction were observed in Category III deposits during the 1949 and 1965 Puget Sound earthquakes. The geologic descriptions, geotechnical analyses, and historical record indicate that Category III deposits have little susceptibility to liquefaction, and I have assigned them a low rating. However, unmapped areas of fill located within areas shown as Category III deposits could have a significantly higher liquefaction susceptibility. Thus, the presence or absence of liquefiable soils at a given location within the Category III map area can only be determined by a site-specific geotechnical investigation performed by a qualified practitioner.

## CONCLUSIONS

Table 6 summarizes this study's ranking of the liquefaction susceptibility in the Sumner quadrangle. In the study area, Holocene alluvial deposits in the Puyallup and White River valleys and Prairie Creek (Category I) are ranked as having a high liquefaction susceptibility and represent the areas with the greatest liquefaction hazard. Although the Electron Mudflow may not be a liquefiable soil, it is included as a Category I deposits because in the Sumner quadrangle it is undoubtedly underlain by very liquefiable Holocene alluvium. The lack of geotechnical borings penetrating Category II deposits (Holocene mass-wasting and lacustrine sediments and Vashon recessional lacustrine sand) precludes quantitative analysis of liquefaction susceptibility. The general description of the

**Table 6.** Ranking of the liquefaction susceptibility of the three liquefaction categories in the Sumner quadrangle

Category	Liquefaction susceptibility rating
I	High
II	Low to Moderate
III	Low

Holocene lacustrine sediments (Crandell, 1963) indicates that they are typically composed of silty and peaty soils having a low susceptibility to liquefaction. However, sand interbeds as much as 4 ft (1.2 m) thick have been observed in some lacustrine deposits. The presence of sand beds in these deposits and the possible occurrence of liquefaction in a Holocene lacustrine unit in the Big Soos Creek drainage to the north suggests that liquefaction of these sediments is possible. Some of the numerous ground failures (cracking, slumping, etc.) in mass-wasting deposits observed during the 1949 and 1965 earthquakes may have been the result of liquefaction, but no definitive evidence supporting liquefaction as the primary cause of these failures is available. Late Pleistocene sandy glaciolacustrine deposits in the Sumner quadrangle are quite similar to potentially liquefiable glaciolacustrine sediments found in the Poverty Bay quadrangle. I rank Category II deposits as having a low to moderate liquefaction susceptibility to reflect the variability in the engineering characteristics of the lacustrine and mass-wasting deposits and the historic reports of earthquake-induced liquefaction and other ground failures.

I have ranked Category III deposits, which include Vashon and older glacial and nonglacial deposits (except for Vashon recessional lacustrine sand) as having a low liquefaction susceptibility. Except for Vashon recessional deposits, Category III soils have been overridden by at least one continental ice sheet and are consequently quite dense. The liquefaction susceptibility for these glacially overridden deposits is ranked as low using the criterion of Youd and Perkins (1987) or very low using the thickness criteria of Shannon & Wilson, Inc. (1993). Vashon recessional deposits (other than lacustrine sands) are typically coarse gravel soils that would consequently have a low liquefaction susceptibility. However, more sandy sections of these recessional deposits might be more susceptible to liquefaction. Areas covered by the Vashon recessional deposits are delineated on Plate 1.

The mid-Holocene Osceola Mudflow is also included as a Category III deposit having a low liquefaction susceptibility. In outcrop, the Osceola Mudflow typically has a weathered, cemented surface layer not susceptible to liquefaction. Osceola Mudflow deposits found in the subsurface of the Puyallup and Duwamish valleys have not been exposed to the weathering processes that result in iron and silica cementation, and may in fact be susceptible to liquefaction. At question is the potential seismic behavior of the unweathered portion of the mudflow in its outcrop area. This area has been included in Category III because of the presence of the cemented surface layer and the lack of observed historic liquefaction and has been delineated on Plate 1. Further investigation of the dynamic behavior of the unweathered Osceola Mudflow is warranted.

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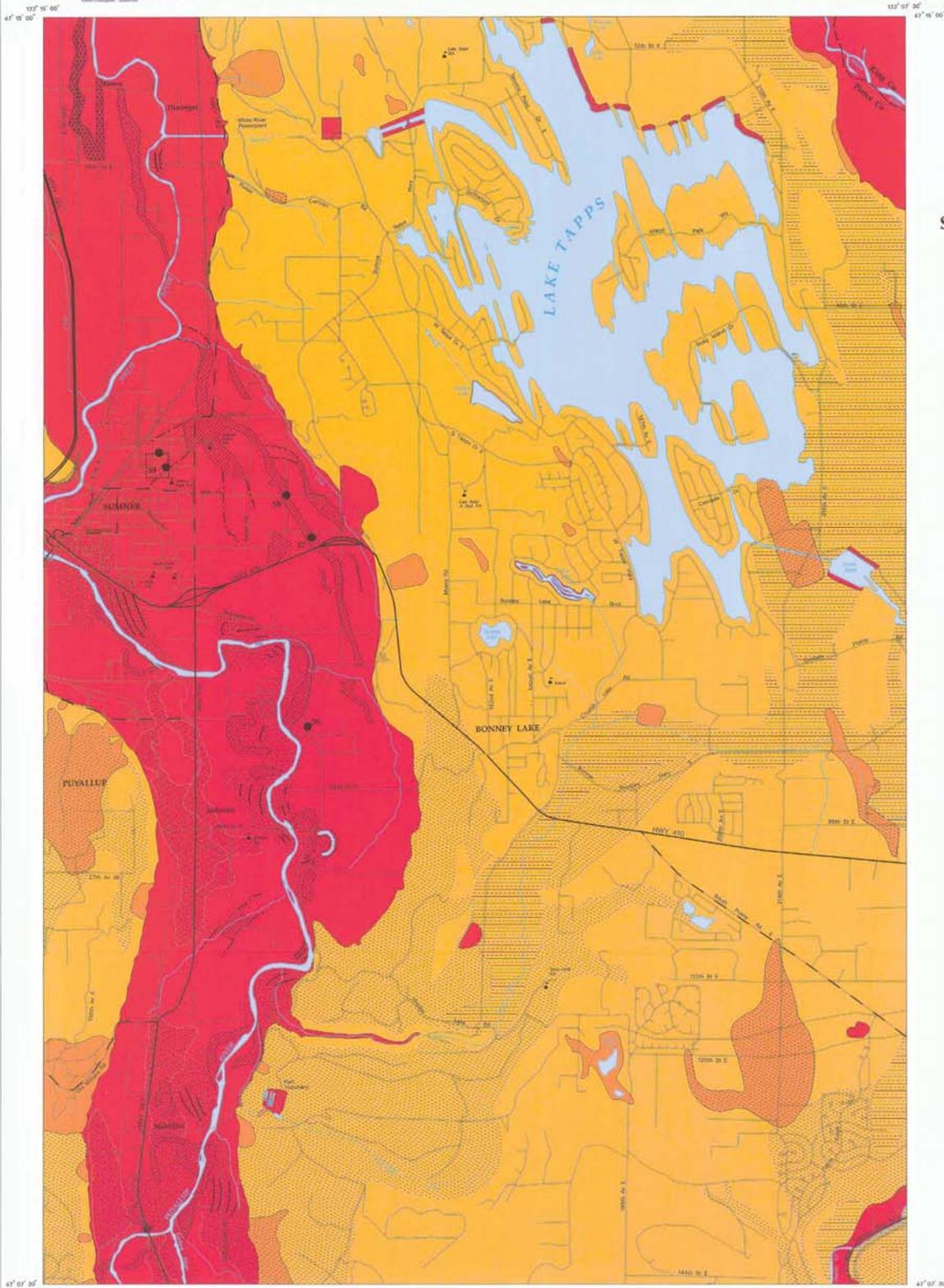
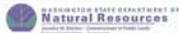
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- Youd, T. L.; Nichols, D. R.; Helley, E. J.; Lajoie, K. R., 1975, Liquefaction potential. *In* Borcherdt, R. D., editor, *Studies for seismic zonation of the San Francisco Bay Region*: U.S. Geological Survey Professional Paper 941-A, p. A68-A74. ■



## LIQUEFACTION SUSCEPTIBILITY FOR THE SUMNER QUADRANGLE, WASHINGTON

by  
 Joe D. Dragovich and Patrick T. Pringle  
 with a section on liquefaction analysis by Stephen P. Palmer

1995



### EXPLANATION

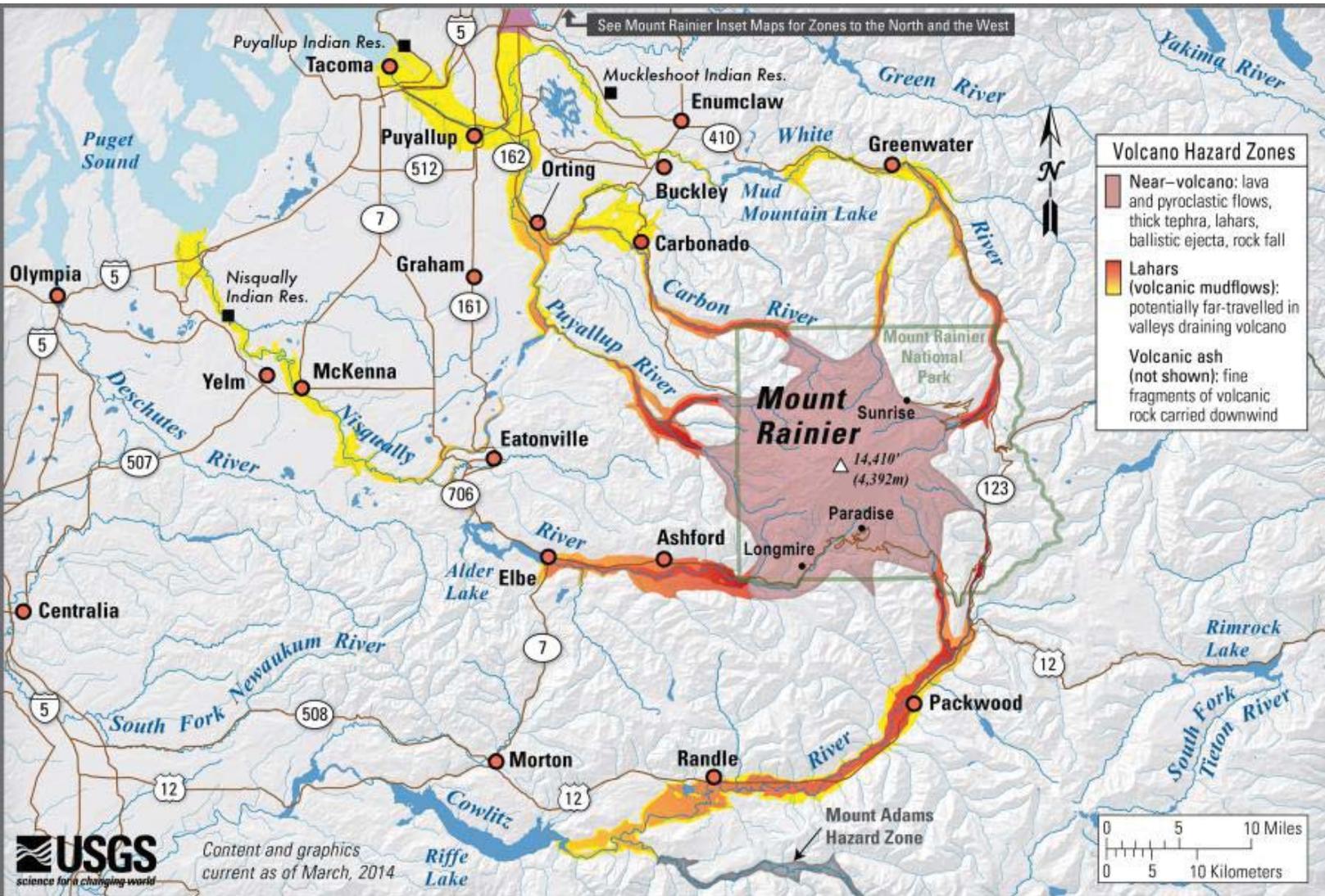
- CATEGORY I includes artificial fill and modified land and Holocene alluvium and the Electron Mudflow. LIQUEFACTION SUSCEPTIBILITY: HIGH
- CATEGORY II includes Holocene lacustrine and mass-wasting deposits and late Pleistocene sandy glaciolacustrine sediments. LIQUEFACTION SUSCEPTIBILITY: LOW TO MODERATE
- CATEGORY III includes all other Pleistocene glacial and nonglacial deposits and the Osceola Mudflow. LIQUEFACTION SUSCEPTIBILITY: LOW
- Major open water features.
- Contacts between liquefaction susceptibility categories. Geologic map units derived from Crandell (1963), Flisdal (1979), and interpretation of aerial photographs.
- Historic liquefaction sites identified by the corresponding reference number in Chleborad and Schuster (1990). Table 3 reproduces the quotations and comments given for the sites in Table 2 of Chleborad and Schuster (1990).
- Pre-1906 course of the White River as mapped by Willis and Smith (1899).
- Osceola Mudflow deposits.
- Electron Mudflow deposits.
- Proglacial and ice-contact stratified drift deposits.
- Abandoned channels of the Puyallup, White or Stuck Rivers that generally do not appear to contain intermittent streams or support riparian vegetation. Features are dashed where inferred or approximately located.
- Bar and swale topography, lines show bar crest lineaments.

This map is meant only as a general guide to delineate areas prone to liquefaction. This map is not a substitute for site-specific investigation to assess the potential for liquefaction for any development project. Because the data used in the liquefaction susceptibility assessment have been subdivided on the basis of regional geologic mapping, this map cannot be used to determine the presence or absence of liquefiable soils beneath any specific locality. This determination requires a site-specific geotechnical investigation performed by qualified practitioners.

This project was partially supported by the Federal Emergency Management Agency, and the Washington Division of Emergency Management.

Lambert Conformal projection  
 1927 North American Datum  
 Washington coordinate system, south zone  
 Base map information from the Washington Department of Natural Resources, Geographic Information System - 1995  
 Cartographic design and production by Carl F. T. Harms  
 Washington Division of Geology and Earth Resources

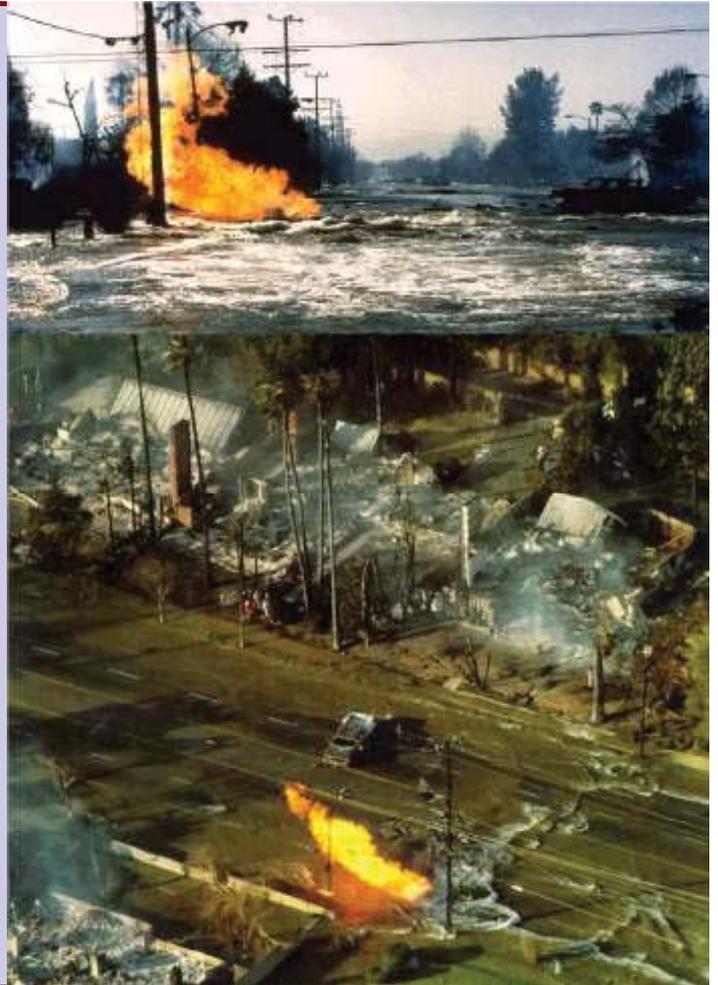






# **EERI Ad Hoc Committee Report on Soil Liquefaction During Earthquakes**

**T.D. O'Rourke**  
Thomas R. Briggs Professor  
Cornell University

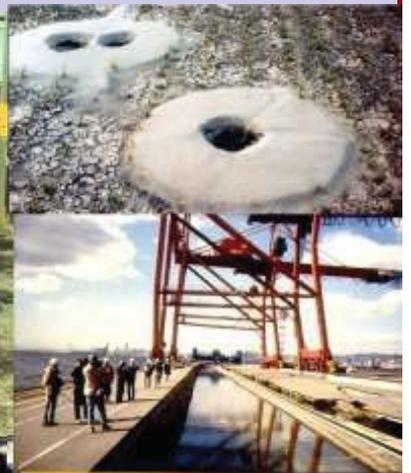


**Liquefaction During Earthquakes**



# LIQUEFACTION

- Transformation of Granular Soil from a Solid to Softened or Liquid-Like Material during Earthquake Ground Shaking



# IMPORTANCE OF LIQUEFACTION

- **Waterfront Structures**
- **Earth Dams & Embankments**
- **Nuclear Power Plants**
- **Levee Systems**
- **Foundations of Buildings & Facilities**
- **Underground Lifelines**



# BRIEF HISTORY OF LIQUFACTION ASSESSMENT

- *Ground Motions & Soil Liquefaction During Earthquakes*, EERI MNO-5 (Seed & Idriss, 1982)
- NRC Workshop (1985)
- NSF/NCEER Workshops (1996, 1998)
- Liquefaction & Undrained Strength Strength Assessment (Seed et al., 2003)
- *Liquefaction During Earthquakes*, EERI MNO-12 (Idriss & Boulanger, 2008)



# CONCERNS & CONTROVERSIES

- **Strong Objections Raised About *Liquefaction During Earthquakes* by R.B Seed, UC Berkeley**
- **Strong Differences of Opinion, Often Personalized & Polarized**
- **Important Ramifications for Critical Infrastructure and Cost of Infrastructure Projects**



# **AD HOC COMMITTEE ON SOIL LIQUEFACTION DURING EARTHQUAKES**

- **W.D. Finn, University of British Columbia (Emeritus)**
- **S.L. Kramer, University of Washington**
- **T.D. O'Rourke (Chair), Cornell University**
- **T.L. Youd, Brigham Young University (Emeritus)**



# COMMITTEE OBJECTIVES

- Review Technical Issues in Dispute with *Soil Liquefaction During Earthquakes*
- Advise EERI Board of Directors on Ways to Resolve Technical Issues
- Review & Advise on EERI Monograph Preparation & Review Process



# **KEY ASPECTS OF LIQUEFACTION ASSESSMENT**

- **Relies Heavily on Empirical Evidence through Case Histories and Interpretations of Past Performance**
- **Variable Soil & Groundwater Conditions**
- **Complex Processes are Simplified**
- **Variability and Uncertainties Associated with Assessment Procedures**

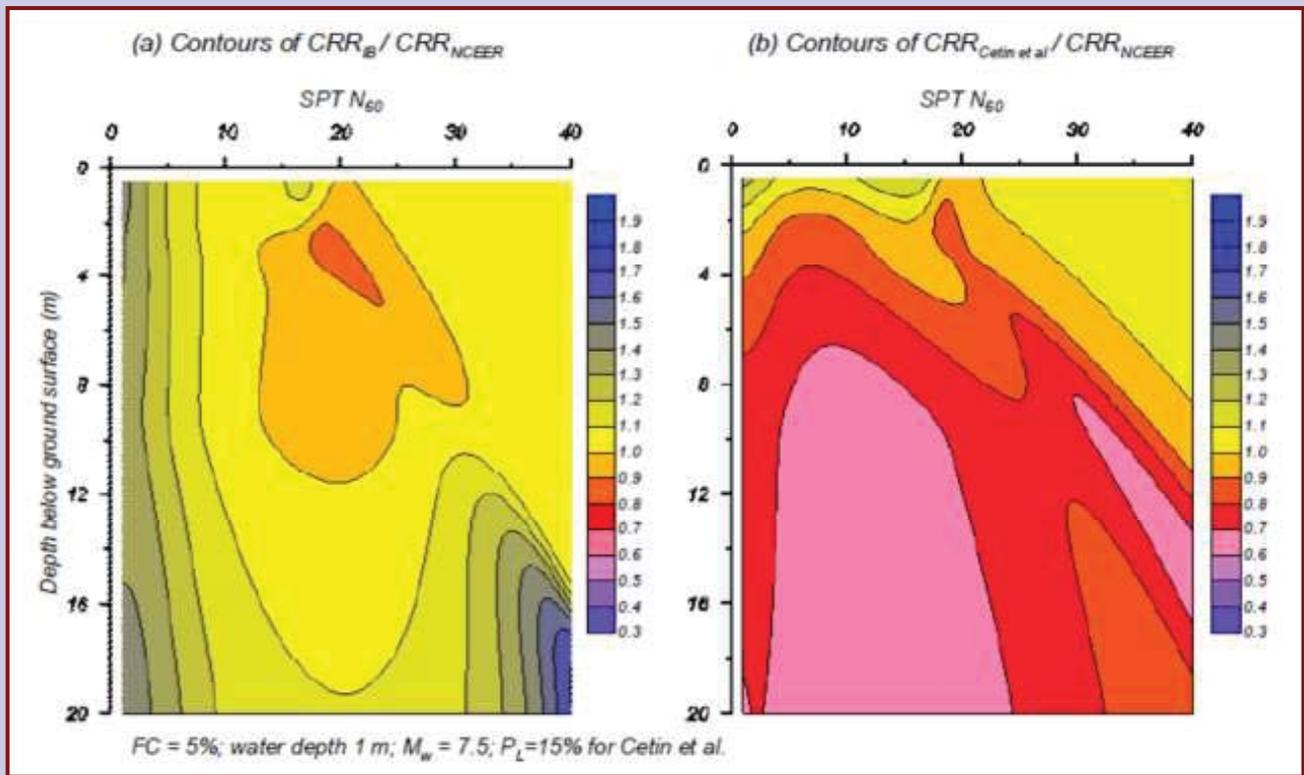


# **DIFFERENCES AMONG LIQUEFACTION ASSESSMENTS**

- **Considerable Differences, Especially for Earth Dams Where Depths of Liquefiable Zones May Be Considerable**
- **Differences May Affect Scores of \$ Millions on Yearly Basis**



# DIFFERENCES AMONG LIQUEFACTION ASSESSMENTS



# **TECHNICAL ISSUES IDENTIFIED BY COMMITTEE**

- **Liquefaction Triggering**
  - Definition of liquefaction, plasticity effects, depth-dependent factors ( $r_d$ ,  $K_0$ , etc.), silts, SPT/CPT procedures, dense soil behavior
- **Liquefaction Consequences**
  - Post liquefaction strength, lateral spread, & settlement
- **Liquefaction Modeling**
  - Data, documentation, & treatment of data



# COMMITTEE OBSERVATIONS

- **EERI Monograph Does Not Represent Consensus; It Represents Authors' Views**
- **Geotechnical Earthquake Community has Good Record in Convening for Consensus Views on Liquefaction**
- **1996/1998 NSF/NCEER Workshop Was Last General Consensus of Community (Youd, et al, 2001)**



# **THE WAY FORWARD**

## **(MAIN COMMITTEE RECOMMENDATIONS)**

- **Provide Forum for Discussion of Alternate Views, Consensus Development Where Possible, and Presentation & Comparison of Differing Approaches**
- **3<sup>rd</sup> Major Liquefaction Workshop and Report on Engineering Practices**
- **Workshop Organized through National Academies**



**Liquefaction During Earthquakes**



# **THE WAY FORWARD**

## **(OTHER COMMITTEE RECOMMENDATIONS)**

- **Organizing Committee Screened to Promote Constructive Interaction and Avoid Conflicts of Interest**
- **Interim Measure: Invitation to Publish 1 or 2 Papers in *Earthquake Spectra* to Seed & Coworkers and Idriss & Boulanger**
- **Reviewers Carefully Chosen and Discussions & Closure Published**



**Commenter:** Braeger, Andrew

**Source:** Webform

**Date:** 2/14/2024 10:03:08 AM

**Comment:**

There is urgent need for safe trail connections between the Foothills Trail, Riverwalk Trail, Sumner Link Trail, and Interurban Trail.

**Commenter:** Bristow, Libbie

**Source:** Webform

**Date:** 1/30/2024 9:59:32 AM

**Comment:**

Absolutely not! There are warehouses in that area that aren't even being used. NO MORE warehouses. They are a blight on our beautiful valley that you all keep giving away.

**Commenter:** Bristow, Libbie

**Source:** Email

**Date:** 3/15/2024

**Comment:**

There should not be any more warehouses in the Puyallup Valley. They are ruining the ground and the landscape, plus they remain empty.

**Commenter:** Bryan, Tana

**Source:** Webform

**Date:** 1/6/2024 10:53:52 PM

**Comment:**

No more warehouses in our beautiful valley any more! Ruining this area. Will be too much truck traffic.

**Commenter:** Bryan, Tana

**Source:** Webform

**Date:** 1/17/2024 11:24:07 AM

**Comment:**

No more warehouses!

**Commenter:** Bryan, Tana

**Source:** Email

**Date:** 2/13/2024

**Comment:**

None is the best solution to everything.

**Commenter:** Buchanan, Sherry

**Source:** Email

**Date:** 2/26/2024

**Comment:**

I recommend the "reduced intensity" alternative.

Sherry Buchanan

**Commenter:** Carr, Christine

**Source:** Email

**Date:** 2/13/2024

**Comment:**

I'm extremely opposed to adding 7 more warehouses on Shaw Road area farmland.

This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location.

**Commenter:** Case, Scott

**Source:** Email

**Date:** 12/15/2023 9:30:00 PM

**Comment:**

With the increase of the property becoming More warehouse expansion I would like to ensure semi trucks are not allowed to use Shaw Road until the City, Council have increased the roads south of Knutson Farm.

I suggest that the same be reviewed by the State and County for SR-162 the main road which has been single lane each way for over 70 years! Getting to, or out of Sumner using the backroad will take longer - how many semis and worker's vehicles are headed to the roads below South Hill Puyallup?

Thank you,

Scott Case

**Commenter:** Castillas, Sue

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Mitigation concerns

**Commenter:** Cecchetto, Caitlyn

**Source:** Webform

**Date:** 2/2/2024 4:47:23 PM

**Comment:**

My preferred option is to leave the site as is. This is an especially important area of Puyallup. The river, the wetlands, the riparian zone, the farmland, the passing roads, Van Lierop Park, and Foothills Trail will each be diminished or fully destroyed by this proposed action. The only proposed benefit this offers to the community is jobs, and Puyallup is currently in a period of historically low unemployment rates. The reality is that we are not hurting for more low paying jobs. I do not support harming the strengths of my city and increasing the burden on its infrastructure for such a negligible benefit to the community. The area as it currently is serves the community; the proposed action serves someone else. Since that's not an option though, I put my support behind Alternative 2. Should an option that further restricts warehouse development become available, I will support that instead. Once you make that land industrial, you can't put it back the way it was. This decision will last forever and it should be given the gravity it deserves.

**Commenter:** Chalk, Jimmy

**Source:** Email

**Date:** 2/13/2024

**Comment:**

We don't need it any more buildings until we can fix the existing traffic. we just don't have the infrastructure or commitment to fix it it's a nightmare to drive down that road imagine owning a home and dealing with it everyday

**Commenter:** Chisholm, Chris

**Source:** Webform

**Date:** 2/22/2024 9:34:00 AM

**Comment:**

I appreciate this EIS. However, the proposed action is an effort to go back on an agreement the landowner made after years of community meetings over a decade ago. Because the landowner and developer backtracked, the City of Puyallup has the right to recommend a No Action Alternative and send a message to the county that Puyallup stands united against warehouses on this farmland. Not only has the landowner moved the goal posts on how this land was supposed to be used, but then they cost taxpayers of Puyallup untold millions of dollars in staff time and legal fees over years of unfounded litigation. Having lost in court, they got their out-of-state developer to sponsor a Political Action Committee in 2019 to funnel hundreds of thousands of dollars into campaigns opposing city council members with institutional history of the original land use agreement. With just a couple thousand dollars, local citizens rallied against that development PAC and crushed the pro-warehouse candidates they supported. The people of Puyallup have spoken. We do not want warehouses on this farmland. For the above reasons, the Proposed Action is a nonstarter, and the Rail Line Alternative is also a nonstarter because it reduces little traffic and would create worse backups because of railroad crossings. The only alternative that the City of Puyallup should recommend to the county is No Action. There is no legal basis to object to the No Action Alternative because ultimately, the county will decide whether to permit any development anyway, and the city's recommendation should simply reflect an alternative the people of Puyallup have said we want. The city needs to stand up and speak clearly that we recommend no action. If the landowner and developer want to come up with their own plan that fits within the community's original land use agreement, let them do that work. We should not hand them the Reduced Density Alternative. Again, they need to make that plan themselves. If that plan comes in, that's when the City should start an EIS and base it on our latest comprehensive plan. If the landowner and developer continue to delay, they risk future comp plans that further restrict development. Our community has spoken clearly that we want to protect farmland and make sure sure that any development in the area in question is consistent with current realities. Those realities now include Farm 12 and its Step-By- Step program, as well as the new Van Lierop Park. The new reality also includes horrible traffic that has developed on Shaw Road over the past decade. Any proposal needs to improve that traffic, not make it worse. Our comprehensive plan should recommend zoning that's consistent with the status of our reality and what's expected in the next 20 years. Development should conform to the new vision and reality. Alternative 2 of this EIS just gives them a guess for what they should have done themselves. Instead, we must give them the No Action alternative so the county can decide what to do with recommendation from the city and people of Puyallup, sending a message that we want none of their proposed action.

**Commenter:** Chisholm, Chris

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Hi. Chris Chisholm, C-H-R-I-S C- H-I-S-H-O-L-M. I live near Clark -- Clark Street Park. And I also really want to appreciate staff for working on this so hard and so long. You know, we fought a long time -- including former council members, one who just spoke -- for years former Planning Commission members to even get to the point where we, the city, you know, fought to be able to do this EIS. I really appreciate that it's happening. That said, because these developers and the landowner moved the goal posts 10-plus years ago going back on the agreement that they made, the landowner made to follow the land use plan that was to develop and agreed upon in 2009, we're now at this point where we, the taxpayers and the city have to pay for this environmental impact statement. Incredible, you know, knowledge and skilled staff members put in countless hours and hired consultants to do alternatives like the whale proposal which is obviously a nonstarter because it's just -- reduces traffic very little and creates way worse traffic because of the road crossings. Appreciate, you know, that that was done because it kind of has to be done as far as this. But you know, not only did they move the goal post but then they also got this Running Bear, you know, multinational corporation based in Missouri to develop a plan but also start a political, a PAC to funnel hundreds of thousands of dollars in 2009 to support candidates to replace these council members who are protective of this land, to try to replace them with people that were pro warehouse, pro-development. I started a PAC to counter with \$2,500. Crushed those candidates. The people of Puyallup have spoken. We do not want warehouses. The only alternative that the City of Puyallup can recommend to the county is no action because, you know, yes, the city has to consider legal considerations. You know, let's realistically allow based on what the previous land use agreements were. But that's for the county to decide. The city needs to stand up and speak clearly that we recommend no action. That we recommend no action because they put together a proposal that is moving the goal post way outside beyond, twice as far beyond the bounds of our land use plan that was agreed upon in 2009, that they agreed upon, the landowners. So if they move the goal post -- you know, if they want to come up with their own plan that fits within our land use agreement, let them do that work. I appreciate that you did this work on Alternative 2 and everything. That sort of fits in with our land use plan that was agreed in 2009 but that was their job. They need to make a plan that fits in with it and then we do an EIS based on that plan and have an alternative that is going to be consistent with our new comp plan which does need to change to protect more farmland in that area, to protect -- to make sure that it's consistent with Farm 12, step-by- step with Van Lierop Park. That comp plan needs to take that area into consideration. Also, look at the horrible traffic on Shaw Road. That's what's been discussed. That's going to increase because of those new developments that are already going in there kitty-corner from this. And so the City of Puyallup has no choice but to recommend no action on their proposal. We can't just hand them a proposal Alternative 2 that fits sort of within our comp -- old comp plan that was agreed to in 2009. That's already 15 years ago. Things have changed. Shaw Road, oh, my gosh, complete change compared to 2009. We need to have a new comp plan that recommends zoning

to that area that's consistent with the current status of our reality now and what's expected in the next 20 years. And then development that they should propose within that area should conform to the new vision, new reality. So I appreciate all the work you did. I know it had to be done. The City of Puyallup cannot recommend to the county Alternative 2. That's just giving them work that they should have done themselves. The county then can decide with recommendation from the City of Puyallup, the people of Puyallup, knowing that we do not want any of this proposal that they're putting out there. The county, that's the next step. They can decide whether to approve or not. And they need to hear it clear from us. No.

**Commenter:** Ciocca, Anthony

**Source:** Webform

**Date:** 2/4/2024 4:22:46 PM

**Comment:**

All 3 options will worsen the already horrible traffic along Shaw Road, especially at the intersections of Pioneer and Main. What is the plan to mitigate traffic problems for citizens of Puyallup?

**Commenter:** Coleman, Mary

**Source:** Webform

**Date:** 12/15/2023 12:50:17 PM

**Comment:**

Please stop allowing warehouses to be built on the amazing Puyallup soil and farmland! Negatives impacts to our farming community, that feeds us all, are not worth it. Please do not allow this to happen.

**Commenter:** Colombo, Joseph

**Source:** Webform

**Date:** 1/11/2024 3:44:14 PM

**Comment:**

I am choosing the "No Action" option. No warehouses should be built on that parcel of land. Instead, affordable housing and/or more green space should be investigated.

**Commenter:** Colombo, Joe

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Joe Colombo, J-O-E C-O-L-O-M-B-O. First of all, I wanted to join everybody else in thanking the city for doing this work. I know it was a lot of work. I know how many hours must have gone into this. So thank you very much. I want to add my voice to the symphony of others who have been here this evening and say that the city really does need to propose the no action option. I drive 23rd and then hook up to Shaw Road and go down to Safeway, and that road is abysmal at the best of times. And if we add any sort of warehouses down at that area it's just going to be impossible. Somebody else mentioned what it's going to be like when the fair occurs. Oh, my goodness. I can't imagine what it's going to be like when the fair is in town to get around that intersection. Also, I also wanted to mention, just kind of add to what Chris said. What might have been, you know, seen as a possibility 10 years ago isn't what we need today. I know that you've already spoken and said that we can't have this be something like housing. Unfortunately, we desperately need housing. But we don't need the warehouses. It's just there's plenty elsewhere that can be used. We simply don't need them at this time. So I suggest the city give the no action option and move forward from there. Thank you for your time.

**Commenter:** Conley, Joe

**Source:** Email

**Date:** 1/26/2024

**Comment:**

Hello,

Been walking my dog at Farm 12 area and seen a pair of Bald eagles repeatedly feeding on the land where the proposed warehouse is to be built. (see attached video from today)

Got it on video today and just wondered if this was taken into consideration for the SEPA? Know the pair must live close. ." Bald eagles live within two and a half miles of the coast, bays, rivers, lakes, or other bodies of water, reflecting the availability of their main food source. They typically nest in large, mature, accessible trees, as well as cliffs and man-made structures."

Please consider this while making your decision on the development of this land.

Thank you,

Joe

**Commenter:** Conley, Joseph

**Source:** Webform

**Date:** 3/12/2024 1:54:03 PM

**Comment:**

Subject: Urgent Appeal to Preserve Farmland and Wildlife Habitat in Puyallup, WA Dear Members of the City Council, I am writing to you today with a deep sense of urgency and concern regarding the proposed construction of seven warehouses on precious farmland in Puyallup, WA. As a resident deeply invested in the well-being of our community and environment, I implore you to halt this development project immediately. The farmland in question not only serves as a vital source of sustenance for our community but also plays a critical role in supporting local wildlife, including the majestic bald eagles that grace our skies. These warehouses, if built, would irreversibly disrupt the delicate balance of our ecosystem, endangering the habitats of numerous species and potentially leading to irreversible ecological damage. Our region's bald eagles, in particular, are a symbol of strength, resilience, and freedom. They are not only a source of pride for our community but also a testament to the natural beauty and biodiversity that we must strive to protect. Allowing the construction of warehouses on their habitat would not only betray our duty to preserve our natural heritage but also undermine the values that make Puyallup a special place to live. Furthermore, the farmland in question is a vital asset for our community's food security and agricultural sustainability. With the growing importance of local food systems and the increasing need to protect arable land, it is essential that we prioritize the preservation of farmland for future generations. I understand the importance of economic development, but it should never come at the expense of our environment, wildlife, and long-term well-being. There are alternative locations for industrial development that would not compromise our natural heritage or jeopardize the future of our community. Therefore, I urge you to reconsider the approval of this development project and explore alternative solutions that prioritize the preservation of farmland and wildlife habitat in Puyallup. Our community's future depends on the decisions we make today, and I implore you to choose wisely. Thank you for your attention to this urgent matter. I trust that you will act in the best interests of our community and the environment we all share. Sincerely, Joe Conley

**Commenter:** Conley, Lauren

**Source:** Webform

**Date:** 3/12/2024

**Comment:**

Also have you taken into consideration the road into this proposed facility? The roads all around all backroads and have only enough space for residential traffic. Semi-trucks that are 50 ft or even less do not have a large diameter of turning radius and the road is not sufficient for these warehouses and will cause unnecessary traffic. The science logic warehouse is barely half full and the semi-trucks seem to already have difficulty exiting and entering the warehouse facility. Please do not ruin this beautiful nature area with warehouses that will go to waste.

**Commenter:** Cook, Charlene

**Source:** Webform

**Date:** 3/6/2024 7:15:00 AM

**Comment:**

I am opposed to the original proposal for the 7 warehouses. I live just off of Shaw Road and we are already having to plan trips around certain times of the day due to heavy traffic that makes it difficult to leave our development. More traffic, especially with apartments going in at the bottom of the hill on Pioneer and Shaw, will substantially increase the traffic congestion. With 2 elementary schools on Shaw Road, we are also concerned with how added congestion will affect the safe access to school for children and parents. A second concern is the impact on Farm 12/Step by Step. Their commitment to providing skills for low income mothers and lifting in them out of poverty through their teaching kitchen, restaurant, bakery, and event center will be greatly impacted by the warehouses. Putting a road west and right next to Farm 12 with large trucks traveling on it will make it extremely noisy. Large warehouses north and east of them will be unsightly and no matter how many buffers will substantially raise the noise level. People will not want to dine there much less hold important events if it is noisy, unsightly, and without the iconic view of Mt. Rainier. Those same problems also will impede the intended use of Van Lierop Park. I know a number of families who get family pictures in the park in the wildflowers with the mountain in the background. I am concerned about the impacts of the warehouses on the Puyallup River and the flora and fauna supported by the river. If there has already been problems with the storm water filters on the existing warehouses, how much more can we expect with even more warehouses and the huge of amounts of storm water runoff containing contamination from heavy truck traffic and materials being produced at the warehouses? Do we even need all that warehouse space? The existing warehouse and the new one on East Main to the west of Shaw Road are not being fully used. We have nearby areas that are designated as industrial sites (Sumner, Frederickson, Fife) that have empty space available. Why build something that will be a blight to our view and the entrance to our city when they may sit empty for years! If given a vote, I would urge you to choose not to place any more warehouses on this site. My second choice would be the preferred alternative proposed by the citizen's committee which suggests a smaller footprint of warehouses and leaves intact the areas close to the river, Farm 12, and Van Lierop Park.

**Commenter:** Coonley, Lauren

**Source:** Webform

**Date:** 3/12/2024

**Comment:**

I walk my dog every day at farm 12 and I have seen on multiple occasions a pair of American bald eagles. This is where the proposed ware house is to be built. Are you taking into consideration SEPA? Because they must have to live close that I see them constantly. "Bald eagles live within two and a half miles of the coast, bays, rivers, lakes, or other bodies of water, reflecting the availability of their main food source. They typically nest in large, mature, accessible trees, as well as cliffs and man-made structures."

**Commenter:** Crane, Sarah

**Source:** Email

**Date:** 2/13/2024

**Comment:**

We all of the city of Puyallup's best interest in mind. Let's work together and choose a better "reduced intensity" alternative as shown in the attached picture.

Thank you so much for your consideration as we work together to make Puyallup a great place for many years to come.

-Sarah Crane

**Commenter:** Crawford, Mark

**Source:** Email

**Date:** 3/14/2024

**Comment:**

I am opposed to the construction any additional warehouses in Puyallup. If we allow any more we should change the name of our town to \*Warehouseville" and ban all new residential housing. And don't forget, all citizens are opposed to the proposed tire recycling project across the street from Farm 12 and this horrible and ill conceived warehouse project.

A better "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location

Mark Crawford (former board president of Crystal Ridge)



**Commenter:** Cribbin, Vicki

**Source:** Webform

**Date:** 1/6/2024 10:08:36 PM

**Comment:**

So sad that beautiful valley soil that is great farm land would be paved over for industrial building. Once paved over the great valley soil will never be again. Shame on those that think only of profit and not what our beautiful city needs. Farm land, playgrounds for children.

**Commenter:** Crivello, Jayme

**Source:** Webform

**Date:** 12/15/2023 12:09:08 AM

**Comment:**

Strongly opposed to this. We want and need our green spaces. We don't want this eyesore and more traffic. Just stop building already.

**Commenter:** Crouchet-Klein, Kathleen

**Source:** Webform

**Date:** 1/23/2024 10:58:46 AM

**Comment:**

STOP this ridiculous ruination of the valley and surrounding areas. I was born and raised here and I am appalled and embarrassed by the lack of sensitive growth in this area. The contractors/big companies have bought this area and plundered it by permission of the City of Puyallup and Pierce County. I am opposed to any and all large businesses building here. Just take a look at how the shipping companies along 167 look. Some of the most fertile soil has had buildings and concrete put on it. Puyallup in the making!

**Commenter:** Cuenca, Phil

**Source:** Email

**Date:** 2/19/2024

**Comment:**

Please don't turn our nice city into a truckstop.

This area is already so busy.

What about that area where the cold storage burned down (there's already no view or nature there)?

Reduce to 3 warehouses is reasonable, right?

You have a beautiful river and view of Mt Rainier and you want to put warehouses on it?

You can really connect the Foothills to Riverwalk trail!

dog park? Something like Ft Steilacoom park?

Kids have no nature to enjoy these days. such a shame.

Phil Cuenca



**Commenter:** Cunningham, Andriana

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

I have been farming in Washington for the past four years. Three of those years has been in Puyallup. I cannot overstate how important it is to keep this land available for farming. It gets harder every year for new farmers to find land and be able to provide our community local and nutritious food. Turning this land into warehouses would also be turning our backs to the tradition of farming in Puyallup and the future of farmers in this area. My partner (who is an electrician in the IBEW) and I both understand the possibilities these warehouses bring in terms of employment and economic growth, however we both understand that sacrificing prime farmland for future generations is not a just tradeoff.

**Commenter:** Cutshall, Al

**Source:** Webform

**Date:** 1/22/2024 5:14:57 PM

**Comment:**

I totally agree with this trail improvment.

**Commenter:** Davis, Laura

**Source:** Webform

**Date:** 12/14/2023 8:01:08 PM

**Comment:**

The last thing our community needs is to lose the last bit of beautiful farm land. We absolutely do not need semis and traffic..what a horrible plan for our community

**Commenter:** Davis, Nancy

**Source:** Webform

**Date:** 2/26/2024 11:24:00 AM

**Comment:**

I am opposed to the 7 additional warehouses being built on the Knutson Farms property. We've lived off 23rd and Shaw Road since 2001 and have seen an unreasonable/unsustainable increase in traffic on Shaw Road, to the point we wait for long periods to turn off 24th Ave CT SE onto Shaw Road. Puyallup does not have the infrastructure to support a significant increase in vehicle traffic in this area. Between this proposal, the building of new apartments, new houses and retail between 122nd and E Main Street, along Shaw Road will cause additional serious degradation of the already terrible road conditions on Shaw Rd (i.e.: lengthy traffic backups, delays in turning onto/off of Shaw Rd, single lane each direction (North and Southbound) for literally thousands of vehicles currently (semi's, personal vehicles) utilizing this route. We saw a huge increase in vehicle traffic, including semi-trucks as soon as the overpass opened between Pioneer and Main Street. The negative results include longer wait times to access Shaw Road, as well as damage to Shaw Road, and what appears to be increased traffic accidents. Please STOP the planned expansion and focus on improving and widening Shaw Road from E Main St out to Military Road, to accommodate the increased daily traffic.

**Commenter:** Davis, Penny

**Source:** Email

**Date:** 3/6/2024

**Comment:**

Warehouse Proposed Project- quite awhile back there was a lengthy article in the News Tribune that more farmland was needed -Now a private owner of farmland is trying to pollute the area with selling to moneyed people who will pour asphalt & concrete in one of the most beautiful areas of farmland & Mount Rainier views. If the council is interested in maintaining the concern for climate reduction then addition of asphalt & concrete should never be part of the plan since proven to generate additional heat into the air as well as the pollution of diesel from large trucks. There are thousands of people in the area who will be impacted with the noise, heat, trucks, cars etc. With the building of additional dwelling units across from Safeway creating increased auto traffic we homeowners do not need additional challenges of the warehouses. Empty land should never indicate an area for concrete just because. Sumner has a different situation with their warehouses because they are extended into an outlying area. By the way there are also restaurants nearby the fields who happen to have outdoor seating. Again this seems to be a renewed battle with moneyed people who are not considering impacts of pollution, climate control, & citizens. Remember we also have train noise daily.

Penny Davis

**Commenter:** Davis, Penny

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Noise pollution concerns, Traffic impact, Community charity choir for Puyallup

**Commenter:** De Groot, David

**Source:** Webform

**Date:** 2/24/2024 10:05:00 PM

**Comment:**

EIS INPUT ON THE VAN LIEROP WAREHOUSE PROPOSAL Air pollution Typically, a long-haul truck idles about 1,800 hours per year, using about 1,500 gallons of diesel. The average heavy duty diesel engine burns around 8 gallons of fuel per hour. Burning of diesel fuel Carbon footprint Burning one gallon 10,180 grams CO<sub>2</sub>/ gallon Driving one mile (on average) 404 grams of CO<sub>2</sub> emitted On average, each idling truck produces about 21 tons of carbon dioxide (CO<sub>2</sub>) and 0.3 tons of nitrogen oxides (NO<sub>x</sub>) annually. Diesel exhaust also contains particulates, sulfur dioxide, carbon monoxide, hydrocarbons, and various other toxins. Water Pollution How much 6PPD do you contribute to the environment? For reference, a typical passenger car with four tires will contribute between 140 and 700 grams of 6PPD to the environment and 18-wheeler semi-truck will contribute 3600 and 18,000 g of 6PPD, over the course of a year (Tian et al. 2020). This contribution comes from general wear of the tire, as 6PPD makes up 2% of vehicle tires (Extance 2020). As stated herein, 6PPD isn't as large of a concern as its transformation product, 6PPD-quinone. 6PPD is relatively unstable, reacting with compounds commonly found in our air. The mechanism which forms 6PPD-quinone occurs when 6PPD is exposed to ozone. (B)oth 6PPD and 6PPD-quinone are very water soluble and will dissolve in available water. The solubilities are  $563 \pm 203$  ug/L and  $67 \pm 5$  ug/L, respectively (Tian et al. 2020). This means that all available of the 6PPD and 6PPD-quinone will be washed away during a sustained rain, into nearby streams, killing the coho salmon in them. THE EFFECT OF TIRE WEAR PARTICLES ON COHO SALMON POPULATIONS ON THE US WEST COAST. By Benjamin Liu-May May 13, 2022 <https://storymaps.arcgis.com/stories/bf113a76485f48779bf49a7cf8d896ae> TRUCK TIRE POLLUTION 6PPD and 6PPD-quinone <https://ecology.wa.gov/blog/january-2023/saving-washington-s-salmon-from-toxic-tire-dust> Other water pollution " Many large warehouse facilities have a place where drivers can wash down their rigs. Washing of trucks can release solvents, oil, grease and other contaminants harmful to the soil and waterways. Sound pollution " The noise of heavy trucks can carry for long distances, particularly in humid weather conditions. Trains can be heard far up onto South Hill. Adding to engine noise is the fact that many truckers ignore posted prohibitions against the use of compression breaks. Noise from the off-ramps at SR512/E. Pioneer have a potentially significant impact on the surrounding residential neighborhoods. Traffic Effects "Traffic is the most important factor influencing pavement performance. The performance of pavements is mostly influenced by the loading magnitude, configuration and the number of load repetitions by heavy vehicles." Pavement Deterioration and its Causes Sharad.S.Adlinge, Prof.A.K.Gupta IOSR Journal of Mechanical & Civil Engineering The number of heavy trucks as well as increased automobile traffic related to the warehouses is likely to have a significant accelerating effect on our Puyallup street system. I doubt that either the developer, the operator, or the trucks using our roads to access the warehouses intend to compensate the city on and ongoing bastis for the ongoing damage they will cause. Rail service is proposed by the Meeker line, which has a grade-level crossing at Shaw road. Unless an alternate connection to the BNSF line is created, there will be an unspecified number of (very slow) local train

crossings that will cause further delays on an already overtaxed arterial. In short, there are so many harms that this proposed project will inflict on the city of Puyallup, its residents, and the environment that there is no moral way this project can be allowed to proceed. It would be totally unconscionable if the financial interests of one corporation were to override the health and well-being of an entire city. David De Groot

**Commenter:** De Groot, David

**Source:** Webform

**Date:** 2/24/2024 10:06:00 PM

**Comment:**

EIS INPUT ON THE VAN LIEROP WAREHOUSE PROPOSAL Air pollution Typically, a long-haul truck idles about 1,800 hours per year, using about 1,500 gallons of diesel. The average heavy duty diesel engine burns around 8 gallons of fuel per hour. Burning of diesel fuel Carbon footprint Burning one gallon 10,180 grams CO<sub>2</sub>/ gallon Driving one mile (on average) 404 grams of CO<sub>2</sub> emitted On average, each idling truck produces about 21 tons of carbon dioxide (CO<sub>2</sub>) and 0.3 tons of nitrogen oxides (NO<sub>x</sub>) annually. Diesel exhaust also contains particulates, sulfur dioxide, carbon monoxide, hydrocarbons, and various other toxins. Water Pollution How much 6PPD do you contribute to the environment? For reference, a typical passenger car with four tires will contribute between 140 and 700 grams of 6PPD to the environment and 18-wheeler semi-truck will contribute 3600 and 18,000 g of 6PPD, over the course of a year (Tian et al. 2020). This contribution comes from general wear of the tire, as 6PPD makes up 2% of vehicle tires (Extance 2020). As stated herein, 6PPD isn't as large of a concern as its transformation product, 6PPD-quinone. 6PPD is relatively unstable, reacting with compounds commonly found in our air. The mechanism which forms 6PPD-quinone occurs when 6PPD is exposed to ozone. (B)oth 6PPD and 6PPD-quinone are very water soluble and will dissolve in available water. The solubilities are  $563 \pm 203$  ug/L and  $67 \pm 5$  ug/L, respectively (Tian et al. 2020). This means that all available of the 6PPD and 6PPD-quinone will be washed away during a sustained rain, into nearby streams, killing the coho salmon in them. THE EFFECT OF TIRE WEAR PARTICLES ON COHO SALMON POPULATIONS ON THE US WEST COAST. By Benjamin Liu-May May 13, 2022 <https://storymaps.arcgis.com/stories/bf113a76485f48779bf49a7cf8d896ae> TRUCK TIRE POLLUTION 6PPD and 6PPD-quinone <https://ecology.wa.gov/blog/january-2023/saving-washington-s-salmon-from-toxic-tire-dust> Other water pollution " Many large warehouse facilities have a place where drivers can wash down their rigs. Washing of trucks can release solvents, oil, grease and other contaminants harmful to the soil and waterways. Sound pollution " The noise of heavy trucks can carry for long distances, particularly in humid weather conditions. Trains can be heard far up onto South Hill. Adding to engine noise is the fact that many truckers ignore posted prohibitions against the use of compression breaks. Noise from the off-ramps at SR512/E. Pioneer have a potentially significant impact on the surrounding residential neighborhoods. Traffic Effects " "Traffic is the most important factor influencing pavement performance. The performance of pavements is mostly influenced by the loading magnitude, configuration and the number of load repetitions by heavy vehicles." Pavement Deterioration and its Causes Sharad.S.Adlinge, Prof.A.K.Gupta IOSR Journal of Mechanical & Civil Engineering The number of heavy trucks as well as increased automobile traffic related to the warehouses is likely to have a significant accelerating effect on our Puyallup street system. I doubt that either the developer, the operator, or the trucks using our roads to access the warehouses intend to compensate the city on and ongoing basts for the ongoing damage they will cause. Rail service is proposed by the Meeker line, which has a grade-level crossing at Shaw road. Unless an alternate connection to the BNSF line is created, there will be an unspecified number of (very slow) local train

crossings that will cause further delays on an already overtaxed arterial. In short, there are so many harms that this proposed project will inflict on the city of Puyallup, its residents, and the environment that there is no moral way this project can be allowed to proceed. It would be totally unconscionable if the financial interests of one corporation were to override the health and well-being of an entire city. David De Groot

**Commenter:** De Groot, David

**Source:** Webform

**Date:** 3/4/2024 8:47:00 PM

**Comment:**

There is no alternative given in the project alternatives that provides more benefit to our city and its citizens than the harm it will cause. in terms of (1) heavy vehicle trips, (2) the potential for flooding that built-up pads for buildings and paved areas will have on nearby natural and residential areas, and (3) the social effects of noise, air pollution, traffic congestion and degradation of our infrastructure all argue against this project. If one alternative must be accepted, the least damaging would be Alternative 2 - reduced project size.

**Commenter:** DeGroot, David

**Source:** Email

**Date:** 2/26/2024

**Comment:**

Air pollution

Typically, a long-haul truck idles about 1,800 hours per year, using about 1,500 gallons of diesel.

The average heavy duty diesel engine burns around. 8 gallons of fuel per hour.

Burning of diesel fuel Carbon footprint

Burning one gallon 10,180 grams CO<sub>2</sub>/ gallon

Driving one mile (on average) 404 grams of CO<sub>2</sub> emitted

On average, each idling truck produces about 21 tons of carbon dioxide (CO<sub>2</sub>) and 0.3 tons of nitrogen oxides (NO<sub>x</sub>) annually. Diesel exhaust also contains particulates, sulfur dioxide, carbon monoxide, hydrocarbons, and various other toxins.

Water Pollution

How much 6PPD do you contribute to the environment?

For reference, a typical passenger car with four tires will contribute between 140 and 700 grams of 6PPD to the environment and 18-wheeler semi-truck will contribute 3600 and 18,000 g of 6PPD, over the course of a year (Tian et al. 2020). This contribution comes from general wear of the tire, as 6PPD makes up 2% of vehicle tires (Extance 2020).

As stated herein, 6PPD isn't as large of a concern as its transformation product, 6PPD-quinone. 6PPD is relatively unstable, reacting with compounds commonly found in our air. The mechanism which forms 6PPD-quinone occurs when

6PPD is exposed to ozone. (B)oth 6PPD and 6PPD-quinone are very water soluble and will dissolve in available water. The solubilities are  $563 \pm 203$  ug/L and  $67 \pm 5$  ug/L, respectively (Tian et al. 2020). This means that all available of the 6PPD and 6PPD-quinone will be washed away during a sustained rain, into nearby streams, killing the coho salmon in them.

THE EFFECT OF TIRE WEAR PARTICLES ON COHO SALMON POPULATIONS ON THE US WEST COAST. By Benjamin Liu-May May 13, 2022

<https://storymaps.arcgis.com/stories/bf113a76485f48779bf49a7cf8d896ae>

TRUCK TIRE POLLUTION 6PPD and 6PPD-quinone

<https://ecology.wa.gov/blog/january-2023/saving-washington-s-salmon-from-toxic-tire-dust>

Other water pollution – Many large warehouse facilities have a place where drivers can wash down their rigs. Washing of trucks can release solvents, oil, grease and other contaminants harmful to the soil and waterways.

Sound pollution – The noise of heavy trucks can carry for long distances, particularly in humid weather conditions. Trains can be heard far up onto South Hill. Adding to engine noise is the fact that many truckers ignore posted prohibitions against the use of compression breaks. Noise from the off-ramps at SR512/E. Pioneer have a potentially significant impact on the surrounding residential neighborhoods.

Traffic Effects – “Traffic is the most important factor influencing pavement performance. The performance of pavements is mostly influenced by the loading magnitude, configuration and the number of load repetitions by heavy vehicles.”

Pavement Deterioration and its Causes Sharad.S.Adlinge, Prof.A.K.Gupta IOSR Journal of Mechanical & Civil Engineering

The number of heavy trucks as well as increased automobile traffic related to the warehouses is likely to have a significant accelerating effect on our Puyallup street system. I doubt that either the developer, the operator, or the trucks using our roads to access the warehouses intend to compensate the city on and ongoing basis for the ongoing damage they will cause.

Rail service is proposed by the Meeker line, which has a grade-level crossing at Shaw road. Unless an alternate connection to the BNSF line is created, there will be an unspecified number of (very slow) local train crossings that will cause further delays on an already overtaxed arterial.

In short, there are so many harms that this proposed project will inflict on the city of Puyallup, its residents, and the environment that there is no moral way this project can be allowed to proceed. It would be totally unconscionable if the financial interests of one corporation were to override the health and well-being of an entire city.

David De Groot

**Commenter:** DeGroot, David

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

David De Groot, D-A-V-I-D D-E G-R- O-O-T. One of the things that is so evident when you look at the map is that that area is an island. And what I mean by that is that it's bordered on two sides by railroad tracks, one side by a river, and another side by an elevated road. There is not really good access in or out. I think that's been established. What's more, there's not even a way of making good access in or out of that place. So the nature of the site is that it's inappropriate for the kind of development that's being proposed. Number one. Number two, this issue of the number of trucks coming in. Trucks bring with them some problems, one of the problems being a common rubber preserver that the tires, which is called 6PPD, which goes to 6PPD quinone, which is highly toxic to fish. And another one is that in large developments like this, truckers often hose down their trucks. And the hosing down of the trucks produces or releases oil, grease, sludge, various kinds of solvents. And the question is, what kind of surface, you know, this is going into presumably surface water elimination. And so the question is, is there a treatment for the kinds of toxic materials that would typically wind up coming off of a site like this? That's one thing. The other thing is in terms of air quality, the number of trucks coming in often do something called idling. And so they tend to not turn off diesel engines. They idle. And one diesel truck idling for a year puts out hundreds and hundreds of pounds of carbon and other kinds of toxins. And we're talking about hundreds of trucks. Now, of course, they're not idling all day. No truck is idling all day but no truck is there all the time. We're talking about many trucks that you have a full day of various trucks idling. And then you have a full year of many trucks idling. And we're talking about air pollution here. This is going to affect the community as well.

The other thing is noise pollution. I can tell you that under human conditions, the sound of trains running on the tracks, not the whistles being blown but just the sound of the wheels on the tracks can be heard far up onto South Hill. And it's a habit of truckers to joyfully ignore signs about not using compression brakes. And I think about the trucks coming off of 512 onto Pioneer. They're going to be using their compression brakes and that's all homes around there. That's all residential. And they're going to hear that. And the hopes, you know, much of the city is going to hear that. There's going to be noise of the trucks running. There's going to be noise of the Jake brakes or the compression brakes. And the idea that we have to keep in mind as well is that what we're going to hear about is the jobs that are created. Modern warehouses are going more and more to be mechanized and computerized. The number of jobs is shrinking. And as someone said earlier, if you drive through Sumer, for instance, or Kent, almost every warehouse, almost everyone has a sign outside saying either

available or for lease. They're not even full. And so the question is, if they're not full, they're doing us no good, anyone any good. And if they are full, they're doing us harm with noise and pollution. That place is simply not suitable for their construction. Thank you.

**Commenter:** DeGuiseppi, Elise

**Source:** Webform

**Date:** 3/4/2024 11:23:00 PM

**Comment:**

I strongly favor Alternative 2, because it has fewer warehouses and more open space and trails. I actually dread any further development in the city that brings warehouses, truck traffic and noise.

**Commenter:** DeRosier, DeAnn

**Source:** Webform

**Date:** 1/28/2024 5:20:38 PM

**Comment:**

Thank you for the presentation you uploaded. I was able to listen to it and all of the questions after. I would really like to see a no build here for many reasons, one of which is because there are already so many areas in Sumner and Puyallup being built up with warehouses. However, understanding that is not a likely outcome, the reduced site intensity alternative sounds better than having seven large warehouses on this site. I would really like to see it reduced by half, at least. I am concerned about the visual impact of seeing warehouses, and also wonder if you have looked at the impact it would have on both Mama Stortinis restaurant and Toscanos restaurant. The outdoor seating at Toscanos currently has an amazing view of Mt. Rainier which is part of the reason people enjoy that venue. It appears both restaurants would lose their view. I am not sure how much this would affect their businesses but needs to be looked at. You talked about the view from the park, but not from this area. I also drive along Main Street regularly and love seeing the mountain as I drive through that area. Maintaining the view of Mt. Rainier from the park is also important. That has become a very popular area for families and people to get out and walk and ride their bikes. I am not sure if you also know that geese have used these fields as a stop-over place on their migration, which would definitely be affected by paving over the majority of this area. It makes me sad to see more bird habitat and migration areas going away. The other problem with so much pavement going in, is there is nowhere for water to sink into the ground, replenishing groundwater and any underlying aquifers. Cutting this project in half would allow for leaving some of this habitat. I am happy that there will be a trail extension, though a concern I have is if this trail ends up being partially secluded, it could also draw homeless, as other parts of trails in the area have. I am happy to hear that rail line is probably not an option, since the purpose of the Shaw Road overpass was to avoid having to stop at more rail signals than necessary and adding rail crossings would definitely negatively impact traffic in the area. Traffic in the Shaw Road area is already bad in the evening and often backs up all the way over that overpass to Main Street. This will also be negatively impacted by the increase in truck traffic all these warehouses will create. I believe the wetlands, including wetland D, need to be preserved. We have lost so much agricultural land and bird and small animal habitat already to warehouse development in Sumner and Puyallup, I hate to see more of it go away. There is also the issue of the wetland being fed by groundwater, which if paved over, that wetland will disappear. Please don't let that happen. I am also worried about the impacts from so many more tires from vans and large trucks so close to the Puyallup River. There are already problems with salmon numbers going down, and I would hate to see those even further negatively impacted by this project due to tire byproduct contamination and other runoff problems. During heavy rain events, there could also be runoff from the paved property causing silt to enter the Puyallup River which would also negatively impact fish habitat. I am very concerned about the added truck traffic in the area, on Shaw Road as mentioned above, and especially on 80th, which is a narrow residential road. Those folks would be heavily impacted with noise from trucks, traffic going by regularly making it harder for them to come and go, as well as the road degradation caused by such

traffic. Tied into this, the thought of warehouses F and G right there next to peoples' homes, is awful, even with a tree or wall buffer. I don't believe anyone should have a warehouse in their backyard after choosing to make their home in a farm valley. It is sad to see our farmland disappearing. I have loved living in Puyallup because of the open spaces, pretty views and small town feel, and am tired of so many areas being sold off and made into warehouse communities, no longer nice areas with pretty views. Please do your best to make this smaller than the huge seven-warehouse design you showed, maintaining the wetlands, bird and small animal habitat, and the views of our majestic Mt. Rainier. Thank you for listening. DeAnn DeRosier

**Commenter:** Desteman, Monica

**Source:** Voicemail

**Date:** 2/10/2024

**Comment:**

Monica Destiny Mo n ICA d e s t e m a n. Hi I want to Voice my opinion on the consume Bond warehouses. I feel like they should not be put in place there because of the traffic and beautiful view from our park that's already put in place there. You don't want to look at warehouses surrounding a beautiful green area and Landscaping knowing interview. Crazy traffic flow in the area you might be. Thank you very much. Bye. Bye.

**Commenter:** Diaz, Grace

**Source:** Webform

**Date:** 2/27/2024 11:02:00 AM

**Comment:**

Please stop this madness! Farmland is crucial for our survival, and not storage. These fertile Lands need protection.

**Commenter:** Dildine, Ryan

**Source:** Email

**Date:** 2/26/2024

**Comment:**

Why build more warehouses on perfectly good farmland but I guess the city like the income from building this kind of stuff how about making it into a park instead of paving over it.

**Commenter:** Dittus, Lynette

**Source:** Email

**Date:** 3/13/2024

**Comment:**

Hello, I am a resident of Puyallup who lives close to the proposed warehouses in the valley. My kids and I often walk to their school at Shaw Road and it is already dangerous on that street with no sidewalks. Adding warehouses and the traffic that would bring on 142nd, along with the Foothills Trail traffic and the Farm 12/Fika traffic, would be dangerous. People already walk in front of traffic at Farm 12 because they park close to the trail instead of in the farm 12 parking lot. I have seen several people almost get hit. Adding huge trucks to that route that have the inability to stop fast would be very unwise. There are also bikers and walkers that constantly veer into 142nd just because they are not looking. There's also a proposed park that would lose a beautiful nature experience. Last but not least, Puyallup used to be known for its beautiful farming soil, when we moved in 15 years ago, those fields were full of tulips. It was gorgeous. My neighbor's house butts up to those fields. Taking some of the last Puyallup Valley soil and covering it with warehouses is very sad to Puyallup history. Even if an on-ramp was added from the freeway, our freeways can barely handle the traffic that's already there. It is already impossible to get off the freeway in a timely manner. This area is full to overflowing.

Thank you for your consideration, from a lifelong citizen of Puyallup.

Lynette Mix

**Commenter:** Dooley, Linda

**Source:** Webform

**Date:** 1/17/2024 6:25:25 PM

**Comment:**

I attended the meeting presentation on 1/17/24 at Pierce College. I studied the displays that were displayed. My comment is I don't see why we need some many warehouses on this site. I feel the developers and Puerce County should honor the original agreement that the City of Puyallup is showing on its display. We have lots of empty warehouse space in the area and I don't see why we need more in the area proposed. These warehouses would destroy all views of Mt Rainier from the valley. I also think that the acreage originally agreed upon as park land should be maintained. Warehouses could be built in the future if needed. Unless it can be shown that these warehouses will all be filled long term, then I don't think this should be granted. Once we lose the agricultural land, there's no going back. In addition, this area is in a high traffic area that already has significant problems.

**Commenter:** Downes, Nathaniel

**Source:** Webform

**Date:** 2/5/2024 9:44:03 AM

**Comment:**

This is a bad idea, pure and simple. We already have more warehouse space than can be utilized. This is also bad for revenue on a dollar per acre approach. If this land must be developed, use it for affordable mixed used housing. Study after study shows that gives the most benefit overall.  
<https://www.strongtowns.org/journal/2024/2/2/citizen-development-higher-value-per-acre>

**Commenter:** Downs, Timothy

**Source:** Email

**Date:** 2/14/2024

**Comment:**

The proposed warehousing development on the approximate 188-acre Knutson Farm property located in the city's Urban Growth Area will forever damage precious farmland, and eliminate alternative uses in a sensitive and potentially scenic area.

We have lived near Shaw Road and Forest Green Boulevard since the mid 1970's and this development has the potential to have more adverse impacts on the environment, and quality of life, than any project in the recent 50 year history of this area.

I hold a degree in Forest Resource Management from UW, and an MBA from SPU and can hold opposing points of view for land use. As such, obvious negative impacts include:

Loss of wildlife habitat, and lost potential for wild ecosystem development.

Adverse impacts in soil percolation as born out in Soil Science that will create excessive run off and destroy Puyallup River water and salmon habitat. Permanently.

Increased traffic, estimated to be 8,700 cars and trucks a day. The area is already the poster child for poor and unscientifically managed traffic, and the increase load will produce more pollution, traffic noise, traffic accidents, crime related to traffic density, road wear, and traffic accidents.

Shaw Road is already a complete traffic nightmare due to poor planning, and the additional load will unequivocally destroy the quality of life for local residents. This area has a long history of poor land use planning, that must end. I had an Economics Professor who wrote his Ph. D. dissertation in Economics at UW on the correlation between poor Pierce County land use planning, and an underperforming Pierce County economy. The work was in the 1980's at the University of Washington. That legacy continues, and please understand - it must end.

The net economic impact of warehouses in the area in my initial assessment is financially negative, excluding the positive impact for warehouse owners/investors who get the sweetheart deals on low cost land outside of the Seattle corridor. Artificial Intelligence will automate all warehouse jobs in just a few short years. We will be left with a concrete waste land, run by robots, in our back yard. Spectacular views of Mt. Rainier forever tarnished by concrete monstrosities.

I am extremely saddened by the visible decline in this once beautiful and scenic rural area, and development equals suburban blight. My first recommendation is a "no action" stance on any new development.

In lieu of no action, we support a reasonable alternative as summarized below -

A best case "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain,

protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location. Sincerely,

Gail and Timothy Downs, long-time residents of Puyallup since 1977.

**Commenter:** Durgin, Sam

**Source:** Webform

**Date:** 3/15/2024

**Comment:**

I am a resident of Puyallup living very near to this proposed site. I grew up in Pierce County and have lived in Puyallup for over 10 years now, and in that time I have seen Puyallup grow more and more congested with industrial zoning and warehouses. I understand that there are economic factors to consider, but I believe that it is also important to preserve some amount of literal as well as figurative breathing room for the residents of this area, and not look at every piece and parcel of open space or agricultural land as potential sites for giant warehouses or high density Residential buildings. East Main is already a highly trafficked area, especially where this proposed site would be. The addition of large volumes of trucks going in and out would turn certain streets into parking lots, Not to mention destroy the relatively bucolic quiet that residents of the areas have come to know and enjoy. I respectfully but firmly urge those involved to shelve this particular project and find a more suitable location away from such a densely populated residential area, that is already dealing with large amounts of traffic, commuters, and increasing noise from the growth of the area. Thank you.

**Commenter:** Eaton, Robert

**Source:** Webform

**Date:** 12/23/2023 4:18:03 AM

**Comment:**

Another city putting up massive warehouses. Fife, Sumner, auburn, Bonney lake. I don't understand the need for such logistics. Is there that much need for storage and freight in this area? These are beautiful fields that will never come back in all of these cities. It's a shame.

**Commenter:** Eckert, Carol

**Source:** Webform

**Date:** 12/19/2023 6:54:15 PM

**Comment:**

I am a retired science educator and I am once again dumbfounded that we are contemplating covering rich agricultural land and open space with warehouses. This is a prime wetland area for migrating birds and animals. Keystone species of plant and trees, like Garry Oaks, will be impacted due to construction and heavy traffic when finished. Please, someone, take a stand for our future. Say no to more buildings and save the open space and green land for our future.

**Commenter:** Edmonds, Craig

**Source:** Webform

**Date:** 2/26/2024 1:39:00 PM

**Comment:**

How could more warehouses be needed if the first warehouse, Lifescience Logistics is empty, and has been since it was built two years ago? We need more open space, not warehouses.

**Commenter:** Edson, Heather

**Source:** Webform

**Date:** 2/22/2024 3:06:00 PM

**Comment:**

I am against all these warehouses going into Puyallup. Our City is not set up for industrial warehouses; we are a farming community with beautiful, rich land that needs to be utilized. The traffic congestion is already far beyond tolerable, and then keep bringing in these trucks and more traffic makes things worse. They barely fit on the roads, difficulty maneuvering turns; as I watch them cruise on Main Street and downtown Meridian all day long. This is so appalling that our City Representatives would even be allowing these types of things to be pushed through. We should be focusing more on local small business, small local organic farming and supporting those businesses, not becoming a Fife where no one wants to live and is all industrial. Keep that for them and off 167!

**Commenter:** Eims, Penny

**Source:** Email

**Date:** 2/26/2024

**Comment:**

I am sending my preference for the land development in the Van Lierop Park area. Shaw Road and Main Street already have an unacceptable amount of traffic. After 3:30 pm it is literally dangerous to get out of our neighborhood (Deer Creek) to access Shaw Road, not to mention the backups in conjunction with the Shaw Road Elementary traffic and the pending traffic impact from the apartments being built across the street from the Shaw Road Safeway.

Aside from the traffic concerns are the issues with the open areas currently enjoyed by people living in the area and visiting Farm 12. Puyallup has grown too quickly, with not enough infrastructure support and the addition of multiple warehouses will only make things worse.

Please help preserve what is left of Puyallup's beauty - once this land is gone, it is gone forever.

Thank you,

Penny Eims

Penny Eims

AnimalVictory.org

<https://nationalanimalnews.com/>

**Commenter:** Elly, Dr.

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear Mr. Beale and City of Puyallup

We appreciate the opportunity to comment on the mega warehouse project proposed for Knudson Farms. Climate Pierce County is a coordinating council for several environmental organizations who share concerns on climate impacts in Pierce County. Many of the impacts are made worse by the choices we make in land use that then perpetuates a cycle of more impacts. This proposed project is one of those that make things worse by

- increasing GHG emissions through traffic which increases warming that leads to changed weather patterns and makes it harder for the city, the county and the state to reach its GHG emission reduction targets,
- increasing GHG emissions through removing land and vegetation that sequesters carbon by paving over with impervious surfaces,
- increasing air temperatures near a river from the heat island effect of large buildings which may also increase water temperatures in an already stressed salmon bearing river that needs to stay cool,
- increasing stormwater runoff that is already causing erosion with the potential of altering the river flow leading to more erosion,
- killing salmon with pollutants from tires in the stormwater which in turn flows into Puget Sound and does the opposite of restoring that to health
- paving over or drying up wetlands that depend on water infiltration, which cycles back to increased polluted stormwater because it is no longer being filtered
- disruption of biodiversity making use of the rich soil, wetlands, and vegetation now
- disruption of migratory birds travel
- the usual cycle of more traffic leading to the decision to widen roads and develop more
- the loss of growing capacity by taking out productive farm land with rich soil which reduces the opportunity to provide fresh local produce for our communities

Some Vision 2050 policies that are not being followed with this development:

MPP En -5 Locate development that minimizes the impact to natural features,

MPP En – 14 Identify and protect wildlife corridors both inside and outside the UGA

MPP En – 16 Preserve and enhance habitat to support healthy wildlife and accelerate the recovery of salmon, orca...

MPP En-17 Maintain and restore the hydrologic functions and water quality within the regions ecosystems and watersheds to recover the health of Puget Sound.

MPP CC – 4 Protect and restore natural resources that sequester and store carbon such as forests, farmlands, wetlands, estuaries and urban tree canopies.

MPP DP -37 Ensure that development in rural areas is rural in character and is focused into communities and activity areas.

MPP DP – 45 Avoid growth in rural areas that cannot be sufficiently served by roads, utilities, and services at rural levels of service.

By these policies the proposed project should not even be considered. It is misplaced in the middle of a rural area. The City of Puyallup intended to have the land be designated as agricultural land in 2003; in 2008 a deal was made that half could be developed and half would remain agricultural land. Pierce County did not follow through with the city's desires. The current Countywide Planning Policies has an entire section dedicated to a jurisdiction choosing to designate agricultural lands and what they must do to preserve it. This project does not align with those policies.

This project bordering on flood plain area, a fish and wildlife area because of the Puyallup River and Van Lierop park, wetlands, and recharge areas, comes under Countywide Planning Policies

Env 3.3 Provide protection for environmentally sensitive lands through the provisions of appropriate buffers when development is allowed and

Env 3.4 Adopt a “no net loss” approach.

Agricultural lands are considered natural resource lands and there too the Countywide Planning Policies in ENV 7.4 Adopting a “no net loss” approach where applicable.

Comprehensive Plan policies are built on Vision 2050 and the current Countywide Planning Policies. In both the City's and the County's updated comprehensive plans these policies should be reflected.

The Washington Growth Management Act has these goals:

- Maintain and enhance natural resource-based industries, including productive timber, agricultural and fisheries industries.
- Encourage the conservation of productive forest lands and productive agricultural lands, and discourage incompatible uses.
- Encourage the retention of open space and development of recreational opportunities, conserve fish and wildlife habitat, increase access to natural resource lands and water, and develop park.
- Protect the environment and enhance the state's high quality of life, including air and water quality, and the availability of water

Taken together, the State, the Puget Sound Region, and the County all support that this project is not appropriate for the area. The existing infrastructure does not support the project, there is no attention being paid to no net loss of agricultural resource land, there is no attention being paid to no net loss of wetlands, there is insufficient buffer to protect the river banks from erosion at the stormwater exit pipe, there is no filtration of the stormwater to prevent pollutants from getting into the river and into the sound and so forth. This project should not move forward without significant time spent reworking it and figuring out how to meet the policies that have been adopted.

Often the argument is made for jobs, but warehouse jobs are low density, meaning few workers for the amount space taken up. The economic value of the warehouses lies mostly in property taxes at the cost of air and water pollution, loss of natural resource lands, and loss of open space lands for enjoyment. A warehouse next to a park doesn't make sense. Multiple warehouses next to a treasured river is also not pleasant to see regardless of its harmful impacts. This project should be developed elsewhere, not in rural Pierce County in such a beautiful location.

Thank you for your time. Please adopt the No Action alternative and do not increase the number of warehouses at this location.

On behalf of Climate Pierce County,

**Commenter:** Engels, Tracy

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Thank you for passing this along.

Tracy

Pierce County  
Tacoma, WA  
Statement regarding Knutson Farm Industrial Proposal

March 15, 2024

Pierce County Planning;

First of all, the title is ridiculous. Farm Industrial? This is Puyallup after all, where we value our agricultural land, as should Pierce County. Doesn't it mention in several places that Pierce County (PC) vows to protect its remaining agricultural lands? Doesn't it mention in the Washington State RCW that PC is obligated to protect its remaining wetlands? You have managed to destroy the remaining wetlands on South Hill (don't even think of mentioning the scant amount of wetland that still exists on 144<sup>th</sup> and 86<sup>th</sup>) and shouldn't be allowed to destroy the remaining wetland in the Puyallup Valley. You are **legally required to protect it.**

As described in the DEIS, the proposal would result in many negative impacts to the Puyallup area such as: traffic congestion from 8,724 cars and trucks per day, 130 acres of destroyed farmland, stormwater runoff that would kill salmon, loss of wetlands, and visual and noise impacts to popular Van Lierop Park and Farm 12. The purpose of the DEIS is to identify alternatives that would mitigate these impacts. Such as no warehouses at all. There is an abundance of empty space in the warehouses that already exist in both Sumner and Pierce County and to put any more in place while there are millions of wasted square feet currently would be a travesty.

But I know that protecting wildlife, open space, farmland, wetlands, daffodil fields will not move you and that the only thing that will is the dollar sign in your heads, note that there are legal reasons you cannot approve this build. LEGAL. And the fines and mitigation you will have to pay will far exceed any money brought in by this build. We will fight, legally, and you will lose. Stop this now and save yourself millions of dollars.

Sincerely and with intent,

Tracy E. Engels

[REDACTED]  
[REDACTED]  
[REDACTED]

**Commenter:** Engels, Tracy

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Mitigation concerns

**Commenter:** England, Katie

**Source:** Webform

**Date:** 2/26/2024 4:49:00 PM

**Comment:**

We are overrun already with extreme traffic issues on/near Shaw Rd! Having lived here for over 25 years, the growth is very disappointing. We do not want any more warehouses in this area & by our schools & wonderful valley community. It personally affects my family on a daily basis. Please consider hearing the public's voices on this & come up with a better plan. Alternative 2 in the DEIS reduces the warehouses and associated traffic by 35%, protects 40 acres of farmland, and adds buffer walls around Van Lierop Park. Alternative 2 is a good start, but it still has unacceptable significant impacts. A better "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Thank you.

**Commenter:** Erickson, Lucas

**Source:** Webform

**Date:** 12/14/2023 7:56:18 PM

**Comment:**

Your plans to put warehouses everywhere are ruining our city and bringing excess traffic to an already horrendous traffic situation! You all should be ashamed of your greed.

**Commenter:** Erickson, Sarah

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Alternative 2 is not good enough.

Traffic is already a nightmare without additional workers and truck traffic. Traffic onto 410, up Shaw Road, and in and out of Orting needs to be resolved 1st before adding more traffic.

What happened to this area being for the community. There is not enough parking for the park and bike trail. Where is the park and activity fields?

Please don't turn our valley into a bunch of warehouses. Preserve the beauty of our city and our farmland.

**Commenter:** Fahnstrom, Barbara

**Source:** Webform

**Date:** 12/14/2023 9:34:43 PM

**Comment:**

No on ruining the beautiful property in the Puyallup valley with warehouses

**Commenter:** Field, Kim

**Source:** Webform

**Date:** 1/12/2024 8:45:53 AM

**Comment:**

I would like to know one changes wet lands to " open space" if a pussy willow grows in it then it's wet lands. Filling the wet land is not the right answer. It is only going to cause trouble somewhere else. The trail connections is a joke it in the heart of a flood plain it will erode faster than it can be maintained. We need to look at better option and keep it agriculture warehouses don't add beauty or maintain the vision of the community As well as van warehouses vs trucks warehouse the outcome is the same we do not have the traffic infrastructure in place to handle what we already have down there. I am not in support of this

**Commenter:** Field, Kim

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Hello!

A better "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location. Note: although the "no action" alternative with no new warehouses is preferred by most of us, it's probably not a realistic option unfortunately.

I am also concerned with the Foothills Trail, Van Lierop Park and the Riverwalk connection. 1: We need to make sure that Van Lierop Park trail/path is connected. 2: it must be aesthetically pleasing with either park settings for maintained green space. No one wants to run/walk along warehouses or roads. Finally 3: Very important! The current proposal has the Riverwalk connection in a natural flood plain. This needs to be addressed. As is it will cause erosion below the trail costly clean up and closures to the trail and finally this connection will need to be replaced sooner and cost community tax payer more or leave it unusable.

As a business owner who use the trail for said business, a recreational user of the trail and a resident of the community. I feel moving forward with any plan without these issues being discussed and rectified is not suitable.

Sincerely

Kim Field

Executive Director

All Things Fun Sports!

Resident of City of Puyallup District 2

**Commenter:** Fitzgerald, Chuck

**Source:** Email

**Date:** 2/13/2024

**Comment:**

I moved to the area in 1979 from the Midwest. I realize things change, but adding 7 warehouses on Shaw Road would be horrible in my opinion. Save some of our previous soil/please don't pour huge amounts of CONCRETE

Thanks

Chuck Fitzgerald

**Commenter:** Fitzgerald, Chuck

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Please reduce the warehouse building to a maximum of THREE

Chuck Fitzgerald

**Commenter:** Fleming, George

**Source:** Email

**Date:** 2/19/2024

**Comment:**

Dear City Council,

As ten year residents of the Puyallup Highlands community, My wife and I have observed traffic on Shaw Road increase from a lightly traveled roadway (except during high commute times) to a high volume and nearly constantly busy arterial. Our ability to safely merge onto Shaw Road northbound from Highlands Blvd is already affected by traffic volume, speed, lack of a traffic signal and by limited sight distance of approaching northbound vehicles.

Once we are able to merge onto northbound Shaw Road, we often encounter traffic backed up by motorists stopped waiting to turn left onto 12th Ave SE or into Shaw Elementary. We also have to keep an eye out for pedestrians (including elementary school-age children) walking along the shoulder between Highlands Blvd and 12th Ave SE to get to Shaw Elementary or to the Safeway shopping center at Shaw Road and E Pioneer.

What I've described are the "current conditions" of Shaw Road, and no longer limited to just peak commute hours. It seems obvious (at least to us) that these conditions will be greatly exacerbated by this industrial warehouse project.

It further seems to us that the commercial entity developing the Knutson industrial Warehouse Project be required to provide 'mitigation' for the cascading negative effects on traffic congestion and pedestrian safety on the roadways approaching the warehouse campus.

This might begin with the following:

Widen Shaw Road from 12th Ave SE north beyond Shaw Elementary to provide a two-way left turn lane to mitigate traffic hesitations and stoppages from left- during motorists.

Widen Shaw Road to four lanes between Highlands Blvd and 12th Ave SE to include left turn bays for both intersections and provide pedestrian sidewalks.

Consider a traffic signal at Shaw Road and Highlands Blvd. There are numerous school buses entering and exiting the Highlands and Crystal Ridge communities via Highlands Blvd and the lack of a traffic signal increases the potential for a serious and potentially tragic accident at this intersection.

We can only speculate, but it seems reasonable to believe that this project, if allowed to proceed as proposed will have a negative impact on local property values. Also, the preservation of precious farmland and the existing Van Lierop Park and Farm 12 should be important to this community.

We ask that the Council act to reduce the proposed 7 new warehouses to 3 and provide protections against further warehouse development at this site.

Thank you for your consideration.

George and Marialice Fleming

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]

**Commenter:** Fortner, Tirzah

**Source:** Webform

**Date:** 1/23/2024 6:47:52 AM

**Comment:**

Please do not allow this warehouse and rail line development. The wetland rules and building buffers need to be followed. Knutsen should consider moving into existing warehouse space in Sumner, instead of destroying the valley to build their own. There's plenty of existing warehouse space sitting vacant around the area.

**Commenter:** Fraser, Tom

**Source:** Email

**Date:** 3/14/2024

**Comment:**

I would like to retain this project in farm land. We have lost so much land to concrete warehouses. This valley was the most beautiful farm land. Now it has turned into greed.

**Commenter:** Fuchs, Ashley

**Source:** Webform

**Date:** 12/14/2023 9:38:26 PM

**Comment:**

DO NOT COVER THE REMAINING SMALL AMOUNT OF FARM LAND WITH CONCRETE. Puyallup valley has the best soil- don't do it please

**Commenter:** Geldien, Wendy

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Hi there -

As a second generation member of this community, we are upset with the continued selling of our beautiful farmland to large warehouse projects. These projects create too much traffic than this area can handle. We are at our limit and adding thousands more trucks to our streets an unacceptable burden to residents and our fragile ecosystem, never mind the eyesore and light pollution.

If the Knutson Farm project must happen, can we please better protect our community by reducing the intensity and lessening the scope, such as in the illustration below?

Thank you for your time and consideration.

Wendy

Community resident [image of preferred alternative: reduced intensity alternative]

**Commenter:** Gibson, Marie

**Source:** Webform

**Date:** 2/20/2024 5:27:00 PM

**Comment:**

I am very concerned about the proposed development. These warehouses would put a very big strain on traffic along Shaw Rd, Pioneer, Valley Av, and 80th. This is an area that already has heavy traffic. It also seems this would put a great strain on the environment so close to the Puyallup river. I am very unhappy about putting unsightly warehouses in this area.

**Commenter:** Girl, Banana

**Source:** Webform

**Date:** 12/17/2023 9:58:56 AM

**Comment:**

The amount of unused warehouses I drive by between Kent and Puyallup already says enough is enough. Stop encroaching on viable land that is used for generations of family livelihoods for greed and development that is not necessary. In the years I've lived in the valley, I watched warehouses be erected on both sides of East and West Valley from Auburn to Sumner, creating eyesores where once was open fields and views of the mountain. Same with the route past Carpinito Bro Farms in Kent, there are now countless warehouses that aren't even finished being built yet! Listen to your citizens, the people living and working in these areas, the humans who have to witness even more land being taken until soon there is nothing left that is untouched by the hands of the blue collar man. Use the warehouses you already have! Leave the land alone. Leave the Knutson family alone, before they face the same reality that Spooner Farms did back in 2018 along E Main in Puyallup.

**Commenter:** Gray, Roan

**Source:** Webform

**Date:** 1/8/2024 6:40:48 PM

**Comment:**

This is not right! I don't see any traffic or trucks at the warehouse that is already there. Is it empty?  
We need to preserve our lands! Plant more produce, or let it be!

**Commenter:** Green, Daniel

**Source:** Webform

**Date:** 1/30/2024 5:58:14 PM

**Comment:**

I do not think this project should be approved. The traffic will be ruinous and detrimental to the environment. How will they manage that many trucks in and out of the complex without adding an overwhelming amount of traffic and all of the negative associations.

**Commenter:** Grefalda, Emily

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Dear city administration,

My name is Emily. I have been a resident of Puyallup my entire life. I attended Riverside Elementary, Aylen Jr High and Puyallup High school. I bought a house in the SE part of Puyallup and my children attend school at Spinning Elementary. Spinning is located about 1.1 miles from the proposed project.

I appreciate the hard work of those who have been working on the Environmental Impact Statement and I fully support and agree that this proposed project has significant negative impacts on the environment and community of Puyallup. I strongly feel that no action should be taken and that the proposed alternatives do not adequately mitigate the potential risks. Even if the risks are minimized or reduced and the developer is held accountable for monitoring toxic stormwater, wetland damage or toxic air quality, there is no way that the development of this land into warehouses can be reversed or reverted back to its original state if it is found they have not properly accounted for these risks. Once this Industrial park has been constructed, there is no going back.

Particularly concerning to me is section 4.2 Surface water. The grading from high terrace towards low terrace approaching the Puyallup river during construction and the possibility of oil, fuel or other chemical leakage reaching the river is a red flag for me. Alternative 1 nor 2 eliminate this risk.

Section 4.8 Air Quality and Greenhouse Gases. The EIS states that the impacts are not expected to be significant but the reality is we do not know what exactly the type of manufacturing could be going on here. Im reminded of the two recent chemical fires on East Pioneer in August 2021 and 2023 at the cold storage facility that required residents to evacuate due to unsafe air quality.

Furthermore, as in section 4.10 Health and Safety. The construction happening over a natural gas pipeline could have devastating risks if not properly accounted for during construction and the use or storage of chemicals could easily go wrong in the process of construction or use of the warehouses. Neither of the two alternatives provide a lower impact that I consider acceptable.

This project is bad for Puyallup. The community has responded and the response is an overwhelming NO to this project.

Emily Grefalda

[REDACTED]

[REDACTED]

**Commenter:** Gustaves, Clay

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

The site maps for the proposed action and all alternatives should show the Williams Pipeline ROW as a site constraint in red. The Williams Pipeline ROW Contract states that the Grantor agrees not to build, create or construct or to permit to be built, created or constructed any obstruction, building, engineering works or other structures over or that would interfere with Grantees rights to construct, maintain, inspect, operate, protect, repair, replace, alter or remove interstate natural gas transmission pipelines. Parking lots would require engineering review and approval and our right of way contract would need to be amended with conditions for parking lot approval. I have not received any recent communication from the property owner or their authorized agents seeking approval for this proposed encroachment, and at this point in time it does NOT have Williams approval.

**Commenter:** Hackbarth, Sandra

**Source:** Webform

**Date:** 3/13/2024

**Comment:**

You do not have the roads (Pioneer and East Main) to support the 1,000 plus semi-trucks that will travel in and out daily.

**Commenter:** Hale, Debra

**Source:** Email

**Date:** 3/15/2024

**Comment:**

It's too bad that stopping the warehouse development altogether is not likely to happen.

The "reduced intensity alternative" seems the next best proposal.

Thank you for the information, and giving Puyallup residents an opportunity to have a voice in this.

Deb Hale

**Commenter:** Hamilton, Kat

**Source:** Email

**Date:** 3/2/2024

**Comment:**

Hello-

As someone who lives in this neighborhood, (down the street from Farm 12) we are asking that you consider the sustainable prosperity of our community and future generations and reconsider the building of these proposed warehouses.

First and foremost we DO NOT have the infrastructure to logistically support such a development. Roundabouts are not the answer and if anything, it will lead to more traffic, not to mention the fact that this is an emergency evacuation route WHEN there is a lahar. Please also consider the geographical placement and impact this will have, including the impact on the puyallup river, our health and the health of our children. These warehouses should not be built in residential neighborhoods, nor down the street from an elementary school. Studies have shown that air pollution increases in neighborhoods where warehouses are built and that warehouses do not improve an area economically.

We have been told that the plan below is the least impactful option for building warehouses. I would propose none but if nothing else, this would be the lesser evil. [image of reduced intensity plan] Thank you for your consideration.

Kat Hamilton

**Commenter:** Hampton, Monae

**Source:** Email

**Date:** 3/13/2024

**Comment:**

To whom it may concern,

Adding in 3 warehouses let alone 7 is a clear indication that the Knutson Farm Inc. warehouse proposal is unacceptable and the company has not in fact done their homework. Traffic in this area in particular is at an all time high and if 3 warehouses go up I suspect the Knutson Farm Inc. business isn't going to be very productive in regards to product moving in and out quickly because trucks are going to be in standstill traffic all hours of the day. Not only would this increase traffic for all residents and nearby businesses but the ROI Knutson Farm Inc. is expecting will in turn not be lucrative and the community will suffer for it.

Furthermore, these warehouses are going to negatively impact local residents, potentially driving them out of the community which then impacts local business and as many have already stated, the run off from these warehouses will also negatively impact the salmon. Pacific salmon enrich terrestrial and freshwater ecosystems and are culturally important around the world.

Please take into consideration the negative impact these warehouses will have on this community and have Knutson Farm Inc. move their warehouses in a location that is better suited for their business and surrounding communities. Don't let them destroy what little we have here.

Thanks,

Monae Munchrath

**Commenter:** Hansen, Robert "Doc"

**Source:** Webform

**Date:** 3/9/2024

**Comment:**

I first would like to thank the city for making a Determination of Significance (DS) on this very important issue. Having been in this position being the Environmental Official in several jurisdictions, I know the political pressures that staff are placed under to make these types of decisions and I know it took a lot of guts to reach this point. Thanks for the opportunity to comment on this project and discuss the significant impacts it might have on community's soils, geologic hazards, wetlands and wildlife habitats and traffic, all of which are very important issues, not only to the landowners adjacent to the project, but also to all land owners, residents and businesses throughout the City of Puyallup and lands adjacent to the City. I would also like to thank the city planning and engineering efforts critiquing this massive study. As a professional in this field for over 40 years, I find this DEIS very informative and thorough in its evaluation and critique of the professional studies submitted by the applicant, i.e. (Soils and Geotechnical studies, Wetlands analysis, Wildlife analysis and Traffic Study). I agree with most of the mitigative measures that will be adopted if the project goes forward. I think it is clear after the analysis that outside of no development, only Option 2 should be considered because Option 1 as proposed by the applicant can not adequately mitigate many of the potential or existing significant impacts identified by the studies, particularly those related to surface water flow, erosion, wildlife and traffic. The river and wetland buffers currently proposed by the applicant are insufficient to mitigate wildlife and stormwater impacts, flooding, or significant traffic from such a development outlined in Option 1. The potential chiseling of the water banks of the Puyallup River can potentially move the buffers inward toward the proposed warehouses creating flooding and property damage consequences. Secondly, I strongly recommend that you consider changing the language of mitigative measures so that they become mandatory. As you know as professional planners when developing a comprehensive plan, policies are written so that flexibility is permitted under unforeseen circumstances. Therefore, policies are written with the modal verb "should." However, when mandating an action resulting from detailed analysis, the verb is always "shall." Throughout the document, in most cases, mitigative measures identified are labeled as "should" rather than "shall." Some examples: • ER-3: Develop Geotechnical Assessment from a WA Licensed Geotechnical Engineer. A geotechnical engineer licensed in Washington State would be retained to develop a geotechnical assessment to determine the presence of geologic hazards, including active landslide hazards, seismic hazards, and shoreline erosion hazard areas, in accordance with Title 18E.80.030 PCC, Title 18E.90.030 PCC, and Title 18E.110.030 PCC. The geotechnical engineer should also review and approve all grading, erosion, and drainage control plans prior to construction to assist in reducing liquefaction and landslide risks from and to the Project. The licensed engineer of record should determine the appropriate foundation, footing, and structural design to conform to the International Building Code standards for seismic and landslide hazards and establish buffers to site the Project away from shoreline erosion/ channel migration hazard zones in accordance with best practices. • ER-4: Prepare and Implement SWPPP for Erosion and Sedimentation Hazards.

Consistent with the Clean Water Act (CWA) and NPDES permit program, and the PCSWDM, the Applicant should implement a Construction SWPPP that will satisfy the requirements of the NPDES General Permit for Stormwater Discharges Associated with Construction Activities. The Construction SWPPP should include measures for temporary erosion and sedimentation control and identify a regular inspection and maintenance schedule for all erosion control structures. The SWPPP should include descriptions of all BMPs to be implemented during construction to minimize erosion and sediment entering surface waters. Erosion and sedimentation control measures should be implemented at the beginning of the construction process and maintained throughout all phases of construction. Measures may include, but are not limited to, installation of a stabilized construction entrance, a wheel wash, silt fences, seeding, mulching, and dust control, and all other BMPs as recommended by a KNUTSON FARMS INDUSTRIAL PARK PROJECT ENVIRONMENTAL IMPACT STATEMENT ENVIRONMENTAL ANALYSIS DECEMBER 2023 4-18 licensed civil engineer. Additional erosion control supplies, including sandbags and channeling materials, should be stored on-site for emergency use. The Project site should be monitored for erosion on a weekly basis and after large rainfall events, and corrective action should be taken as needed. Soil stockpiles should be stabilized and protected from erosion, and soils should also be stabilized before a holiday or weekend if needed, based on forecasts of precipitation. • ER-5: Prepare Emergency Site Management Plans for large scale weather events for Erosion and Sedimentation Hazards. Due to the presence of active floodway, floodplain, and known severe CMZ areas that present a risk of large-scale geological impacts to the site, the Applicant should prepare emergency site management plans that would be implemented in the event of large-scale weather events that may cause flooding on or directly adjacent to the Project site. The Applicant should consult with Pierce County Surface Water Management, Emergency Management, and Planning Departments on the site emergency management planning pursuant to approval during site development approval permitting processes. (Pages 111, 112) Sub SW-1. • Evaluate the outfall prior to Hearing Examiner hearing and prior to County and Hearing Examiner approval and final KFIP permitting and take corrective action as needed to meet PCC 18E Performance Standards over time and to be consistent with the Pierce County Comprehensive Plan policies listed in Section 4.2.2 and with the standard for subdivision approval. This mitigation should include: KNUTSON FARMS INDUSTRIAL PARK PROJECT ENVIRONMENTAL IMPACT STATEMENT ENVIRONMENTAL ANALYSIS DECEMBER 2023 4-104 ■ The new monitoring plan prepared by an engineer should consider recent flooding and sediment loads (discussed in Section 4.2.4), high energy river flows, and should provide a clear record of design and purpose of each component of the outfall. The monitoring plan should explain the range of expected impacts of river flood hydraulics during standard and extreme (10 to 100-year storms) flood events, sediment deposition within the outfall, and both current and future stormwater discharge volumes and rates. The plan should provide specific guidance about how much sediment deposition, erosion or loss of planted vegetation is allowed or expected as part of "normal" outfall facility function and should provide maintenance recommendations for repair when the outfall functions are failing to meet defined performance standards. ■ The definition of "failure" must be provided, as well as contingency plans designed to address indications of current failure or imminent failure. – To ensure that any redesign or repair is adequate, the Project proponent should monitor the structure at least annually in perpetuity, and ideally after each overbank flood event, to ensure that the structure is still safe, intact, and functioning as designed. Regular monitoring would ensure that responses to indications of degradation would be timely and

would not wait for serious or catastrophic failures. – To provide information critical to assessment of outfall function, KFIP should carry out a new scour analysis using current cross sections of the river, since the previous cross section surveys discussed in Section 4.2.3 are now more than 10 years old. The new scour analysis should include assessment of impacts of both current and future flow volumes from upland basins—both Viking (current) and all future indicated basin runoff in the Viking and KFIP contributing basins. The new scour analysis should provide updated feedback as to the type, minimum size, orientation, and extent (along the riverbank) of any proposed riverbank protection or stabilization materials. – SW-3. Hydrogeologist/Geotechnical engineer assessment of steep slopes and location of proposed infiltration facilities. • As part of permit review and consistent with PCC 18E.80 (Landslide Hazard Areas), a geotechnical engineer or equivalent should evaluate the steep, sandy slopes below the currently proposed infiltration trench locations to determine whether the sandy floodplain terrace slopes would withstand hydraulic loading pressures from the proposed infiltration facilities. – Hydroperiod monitoring should take place over at least one wet season and include initial infiltration testing in proposed infiltration areas, and installation of long-term monitoring wells with water level dataloggers in constructed infiltration areas and in wetland areas to determine groundwater levels and document that hydrology timing and volumes are adequate to maintain and preserve historic wetland conditions. – Monitoring should also evaluate and define the purpose of each infiltration trench within the context that most of the currently proposed infiltration trench locations are not sited hydrologically upslope from Wetlands A, B and C, and none are proposed near Wetland D. Therefore, the currently proposed infiltration facilities may not provide hydrology at the right locations to effectively support the on-site wetlands but may provide other floodplain benefits. "A geotechnical engineer licensed in Washington State would be retained to develop a geotechnical assessment" but "The geotechnical engineer should also review and approve all grading, erosion, and drainage control plans prior to construction" and "the Applicant should implement a Construction SWPPP that will satisfy the requirements of the NPDES" and "The Construction SWPPP should include measures for temporary erosion and sedimentation control and identify a regular inspection and maintenance" and "The Project site should be monitored for erosion on a weekly basis and after large rainfall events, and corrective action should be taken as needed." Many more examples exist. I understand that often the mitigative measures are sometimes meant to be suggestive to decision makers, and the word "shall" can be interpreted just as suggestive and displays the serious nature of the mitigation. I strongly recommend that staff and the Council consider doing a word search of "should" in mitigative measures and replace it with the mandatory "shall" where appropriate. Such mandated language will prevent future potential conflictive and confrontational consequences. Thank you for your consideration Doc Hansen

**Commenter:** Hansen, Doc

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Concerns of mitigation measures

**Commenter:** Hartmann, Carey

**Source:** Email

**Date:** 2/24/2024

**Comment:**

I say NO, this is the LAST thing we need in this area. Some of the best farmland in our state, it's CRIMINAL to bury it. Traffic is terrible already, we need less congestion NOT MORE ! Warehouses are an eyesore, and this land and view was already damaged by one warehouse, that's one more than there should be at this location. Any official that votes to approve this will be ousted if I have anything to say 😡

Carey and Susan Hartmann

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**Commenter:** Hassett, Leslie

**Source:** Email

**Date:** 1/18/2024

**Comment:**

Hello, I'm an interested citizen as to what happens with this project. I was a resident of WA for over 10 years. I moved out of state by necessity for employment reasons. I'm eyeing Puyallup as a potential retirement city and I've identified some neighborhoods of interest, some are close to the proposed project. I hope to move within the next year or two. I'm interested in Puyallup because of the intentional emphasis on resources for seniors and the active community of seniors and interesting things for all ages. Puyallup is less expensive than Seattle but within a short distance. My main concern with this project is noise. I read in the EIS where the noise issue was listed as an impact and proposed mitigation strategies. Here's my question: What measures are the engineers and others involved with approving or denying using to determine if mitigation measures actually sufficiently mitigate noise for people with sensory sensitivities? I am autistic and sounds and noise affect me much more than most other people. I'm not alone in the autistic community. Although there would be restrictions on time of day of traffic in and out of the site with none allowed at night, still, daytime noise could make going outside unbearable and actually eliminate that area as a possible residence. Is the city and engineers using a noise measuring process that accounts for sensory issues of autistic people? I'm not just speaking for myself. I wonder if there are autistic people already living nearby who may not know about the project proposal or who do not wish to speak up and divulge their identity. I hope that measuring of noise mitigation will take into account this special population. Thank you for your consideration.

Leslie Hassett

**Commenter:** Hassur, Mike

**Source:** Email

**Date:** 3/6/2024

**Comment:**

My wife and I are adamantly opposed to adding 7 new warehouses in this area. The traffic in this area of Shaw Road is already bad enough. We support the "reduced intensity alternative" shown in the attached map.

Thank you.

Mike Hassur and Kathy Uruski [image of reduced intensity plan]



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses

**Commenter:** Hassur, Michael

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Preferred Option for Knutson Farm Development... [image of reduced intensity alternative]



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses

**Commenter:** Healy, Robert

**Source:** Webform

**Date:** 3/13/2024

**Comment:**

I appreciate the City of Puyallup taking over the role of lead SEPA agency for this project and the burden that entails. Of the alternatives presented, alternative 2 is the best. However, I do have concerns: 1) I am concerned that the traffic impacts to Sumner could be underestimated since the Sumner 167 entrance is closer than the Puyallup entrance. 2) The impacts of a warehouse in the line of sight from the Park to Mount Rainer, is simply unacceptable. 3) The proposed open space and trail should be better thought out and grounded with the City of Sumner's future plans for a trail system along the Puyallup River. Overall, the developer seems to be missing an opportunity, to develop the property while maximizing public and ecosystem benefits. They could partner with the Puyallup Tribe or Port of Tacoma to create valuable fish habitat, a trail system with the City of Sumner, and additional public use space, while still building the warehouse complex that support good paying jobs. I urge the developer to engage in those conversations and make this a better project.

**Commenter:** Helgeson, Trevor

**Source:** Webform

**Date:** 1/20/2024 11:46:06 AM

**Comment:**

We don't want or need this. There is plenty of warehouse space around in places like Tacoma, Kent, and Tukwila. We need that land more than the warehouse space.

**Commenter:** Hembrow, Juanita

**Source:** Email

**Date:** 2/17/2024

**Comment:**

Greetings!

I'm writing as a concerned citizen of downtown Puyallup. My family has lived here since 1976, after moving from California. I grew up in the Santa Clara valley where I saw miles of orchards plowed over to make way for housing developments and expressways. Some call this "Progress," but at what cost? By the time we left, urban sprawl had engulfed the entire bay area, forming one huge, amorphous glob, without visible city limits. There was no individual town character, water was not safe to drink, and the air was not safe to breathe. My husband and I have always hoped that growth in the Puyallup valley would be different, and wiser.

By comparison to cities in California, Puyallup was a small town when we arrived, with a "small town" character and feel. We'd found a wholesome community where we could raise our children. This is still their "hometown," even with all the changes we've seen over the years. It is still a pleasant town, and has so far managed to retain a somewhat "small town" atmosphere. This is what draws new people, and retains its longtime residents. But it is painful to see Puyallup's famously rich "valley soil" become covered over by asphalt and traffic jams!

Nothing and nowhere stays the same forever, but Change needs to be carefully, thoughtfully governed in order to preserve a city's best qualities and character, while also protecting its health and environment.

Instead of growing miles of daffodils that this area was long known for, there are now powerful outsiders and business concerns who want to grow miles of warehouses and asphalt. By its very nature, large commercial interests are seldom concerned with preserving a city's healthy surroundings and its endearing attributes. We, Puyallup's citizens, have to make sure we preserve what we value here.

Will Big Business be building (and financing) the infrastructure needed for truck transport to and from their warehouses, or will they be asking taxpayers to help cover those costs? Traffic flow in our downtown has increasingly been impacted by very long trains, hauling all kinds of dangerous cargo, cutting the town in half for longer periods of time. So how can our roads possibly be expected to survive additional traffic from heavy commercial loads hauled over their surfaces?

If Big Business is permitted to build warehouses here, they should at the very least be required to mitigate any and all negative impacts to our community, as well as contribute toward the preservation and betterment of the city. They should help foot the bill for parks, theaters, and other "beautification" projects. And hopefully, it goes without saying, they need to be held financially liable for any spills, contamination or other damages they contribute to our environment. They should not be allowed to reap profits by converting fertile farmlands to commercial interests

without mitigating their impacts and contributing a good share of those profits toward city improvements.

Then, and only then, might they claim their impacts to be "in the name of Progress."

Thank you for (hopefully) taking our concerns to heart,

Juanita Hembrow (and family)

**Commenter:** Henderson, Hunter

**Source:** Webform

**Date:** 12/18/2023 12:45:55 PM

**Comment:**

Once again some of the most premier farm land in the county is being developed into something that won't feed our people. These low quality jobs and finite jobs through the supply chain are not enough to merit the destruction of ecosystem and prime farm land. We need homes. We need parks. We do not need more 6PPD-Q entering our systems and poison our bodies We do not need more emissions and trucks on the road. Just because something can happen doesn't mean it should. Black people in the USA could not vote till 1964, was that right? Was that justice and equality for all? No. But it was the law. Once again, just because something is happening doesn't mean it should. Stop the warehouse.

**Commenter:** Henley McKinnon, Lauren

**Source:** Webform

**Date:** 3/5/2024 3:05:00 PM

**Comment:**

Subject: Concerns Regarding Knutson Farms Environmental Impact Statement Dear City of Puyallup, I hope this message finds you well. I am writing to express my concerns regarding the proposed warehousing development on the Knutson Farm property, particularly in relation to the plots marked as E, F, and G. I am an avid supporter of the public spaces and trails in the area, and I believe that the large scope of the project, especially in these specific areas, may have significant negative impacts on the open space views, public access, and the connectivity of existing trails in Pierce County. Over the last two decades, the community and the county have invested considerable time and effort to develop a network of bike paths and river walks, fostering a connection between rails and trails. The areas marked as E, F, and G have become crucial in this endeavor, providing a unique opportunity to maintain and enhance beautiful open space access along this remaining corridor. The existing public park views and trail spaces are a testament to the dedication of Pierce County and the public in creating recreational spaces that have become an integral part of our community. It is disheartening to think that the proposed development could potentially obstruct these views and limit public access, undermining the progress made in the past 20 years. I urge the City to reconsider the full development of the entire Knutson Farm plot, especially in light of the incoming housing developments and the growing need for recreational spaces. The changing needs and weather patterns in the river basin should also be taken into account when planning for the future of this area. Preserving open space, creating recreational areas, or establishing open space preserves could offer more sustainable and community-oriented alternatives. Moreover, the presence of existing tenants in the A and B spaces raises questions about the urgency of developing the entire plot. Without a clear understanding of the necessity for such expansive development, it seems premature to sacrifice the potential for additional recreational spaces or open space preserves that could benefit the community at large. I appreciate the City's commitment to gathering input and conducting an objective analysis through the Environmental Impact Statement. I hope that these concerns will be thoroughly considered during the review process. Preserving the beauty and accessibility of our open spaces should be a priority, and I trust that the City will make informed decisions that align with the long-term interests of the community. Thank you for your time and consideration. Sincerely, Lauren Henley McKinnon

**Commenter:** HenleyMcKinnon, Lauren

**Source:** Webform

**Date:** 3/5/2024 3:58:00 PM

**Comment:**

Subject: Concerns Regarding Knutson Farms Environmental Impact Statement Dear City of Puyallup, I hope this message finds you well. I am writing to express my concerns regarding the proposed warehousing development on the Knutson Farm property, particularly in relation to the plots marked as E, F, and G. I am an avid supporter of the public spaces and trails in the area, and I believe that the large scope of the project, especially in these specific areas, may have significant negative impacts on the open space views, public access, and the connectivity of existing trails in Pierce County. Over the last two decades, the community and the county have invested considerable time and effort to develop a network of bike paths and river walks, fostering a connection between rails and trails. The areas marked as E, F, and G have become crucial in this endeavor, providing a unique opportunity to maintain and enhance beautiful open space access along this remaining corridor. The existing public park views and trail spaces are a testament to the dedication of Pierce County and the public in creating recreational spaces that have become an integral part of our community. It is disheartening to think that the proposed development could potentially obstruct these views and limit public access, undermining the progress made in the past 20 years. I urge the City to reconsider the full development of the entire Knutson Farm plot, especially in light of the incoming housing developments and the growing need for recreational spaces. The changing needs and weather patterns in the river basin should also be taken into account when planning for the future of this area. Preserving open space, creating recreational areas, or establishing open space preserves could offer more sustainable and community-oriented alternatives. Moreover, the LACK of existing or fully rented out tenants in the A and B spaces raises questions about the urgency of developing the entire plot. Without a clear understanding of the necessity for such expansive development, it seems premature to sacrifice the potential for additional recreational spaces or open space preserves that could benefit the community at large. I appreciate the City's commitment to gathering input and conducting an objective analysis for additional uses of these spaces. I hope that these concerns will be thoroughly considered during the review process. Preserving the beauty and accessibility of our open spaces should be a priority, and I trust that the City will make informed decisions that align with the long-term interests of the community. Thank you for your time and consideration. Sincerely, Lauren Henley McKinnon (revised)

**Commenter:** Hernandez, Leticia

**Source:** Webform

**Date:** 2/1/2024 10:20:55 AM

**Comment:**

Leave this great soil land as is. This is not a safe location for the community traveling these roads to be faced with semi overloads. The roads are designed for neighborhood not commercial traffic. We have tons of accidents at the Shaw & Pioneer intersection & congestion. I can't imagine car/ semi accident on this busy intersection.

**Commenter:** Hernandez, Heather

**Source:** Webform

**Date:** 2/21/2024 7:12:00 PM

**Comment:**

The Knutson Farm warehouse project must either be stopped or significantly reduced. Constructing 2.65 million square feet of warehouses over fragile farmlands and ecosystems is a grave mistake. This valley holds some of the most fertile land in the country, and to pave over it and construct warehouses is absolutely shameful. In addition to this, the added 8,724 cars and trucks added to this already congested arterial is setting up our community for disaster and irreversible damage from pollution to the valley floor. I'd like to request the following changes: Reduce new warehouses from 7 to 3 Reduce to 1 million square feet Reduce added trucks and cars to this already congested area Connect the Foothills Trail the the Riverwalk Trail Your community is counting on you, Puyallup! Protect our precious farmlands and salmon.

**Commenter:** Hobbs, Amy

**Source:** Webform

**Date:** 1/25/2024 4:34:38 PM

**Comment:**

My family and I do not want this here. My family has lived in the Sumner and Puyallup valley since the 40's. Four generations of us, and this project is not giving back to the area what it's taking away. Stop this project.

**Commenter:** Hoebelheinrich, Carolyn

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Please do not allow more warehouses in our valley! There are already many warehouses locally that are not fully utilized. We do not need more to destroy our farmland, further negatively impact the river ecosystem, obstruct our views, and add to the traffic on our already overburdened road infrastructure. I was born and raised in this valley and am saddened by the erosion of the small town feeling/community we used to have here. More commercialization is not the way to go for our town and greater valley area.

**Commenter:** Hoff, Gary

**Source:** Email

**Date:** 3/9/2024

**Comment:**

I support the reduced intensity alternative.

Thank you.

Gary Hoff

**Commenter:** Holmes, Holly

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear city administration,

My name is Holly. I have been a resident of Puyallup my entire life. I attended Riverside Elementary, Aylen Jr High and Puyallup High school. I now own a home in Puyallup and have kids of my own in the Puyallup School District.

I appreciate the hard work of those who have been working on the Environmental Impact Statement and I fully support and agree that this proposed project has significant negative impacts on the environment and community of Puyallup. I strongly feel that no action should be taken and that the proposed alternatives do not adequately mitigate the potential risks. Even if the risks are minimized or reduced and the developer is held accountable for monitoring toxic stormwater, wetland damage or toxic air quality, there is no way that the development of this land into warehouses can be reversed or reverted back to its original state if it is found they have not properly accounted for these risks. Once this Industrial park has been constructed, there is no going back.

Particularly concerning to me is section 4.2 Surface water. The grading from high terrace towards low terrace approaching the Puyallup river during construction and the possibility of oil, fuel or other chemical leakage reaching the river is a red flag for me. Alternative 1 nor 2 eliminate this risk.

Section 4.8 Air Quality and Greenhouse Gases. The EIS states that the impacts are not expected to be significant but the reality is we do not know what exactly the type of manufacturing could be going on here. Im reminded of the two recent chemical fires on East Pioneer in August 2021 and 2023 at the cold storage facility that required residents to evacuate due to unsafe air quality.

Section 4.8 Air Quality and Greenhouse Gases. The EIS states that the impacts are not expected to be significant but the reality is we do not know what exactly the type of manufacturing could be going on here. Im reminded of the two recent chemical fires on East Pioneer in August 2021 and 2023 at the cold storage facility that required residents to evacuate due to unsafe air quality.

Furthermore, as in section 4.10 Health and Safety. The construction happening over a natural gas pipeline could have devastating risks if not properly accounted for during construction and the use or storage of chemicals could easily go wrong in the process of construction or use of the warehouses. Neither of the two alternatives provide a lower impact that I consider acceptable.

This project is bad for Puyallup. The community has responded and the response is an overwhelming NO to this project.

Holly Holmes

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**Commenter:** Hopkins, John

**Source:** Email

**Date:** 1/18/2024

**Comment:**

At this time I am commenting as an individual . However I wear many hats ....

Supervisor Pierce Conservation District.

Chair Friends of the Riverwalk.

Incoming board member for Foothills Rails to Trails Coalition .

Also former Mayor of Puyallup, during one of the most contentious times in the life of this project.

To be clear, I don't like this project in the least. It's simply the wrong location for warehouses.

However Alternative 2 does the least damage . So I'll hold my nose and accept it if additional mitigation measures are enacted .

My first comment is to commend the EIS for its thorough study ...well done !!

Reasons for my general opposition...

The project is next to the Puyallup River . This is a sensitive environmental area for obvious reasons .

I believe that the River has actually flowed, or at least occasionally flooded the "green area " on the map ...the contours are a clue . Any development should be set back from that zone, and buffered with riparian planting . I would actually propose further set backs . I would also propose working with the Pierce Conservation district. Water quality and riparian habitat are major areas of their expertise.

A footnote is that the observed storm water treatment system from the existing warehouse is woefully inadequate. It should never have been allowed.

Regarding traffic....The large trucks especially will cause contamination with debris from tires and brakes that is particularly harmful to fish . The large trucks also have a major impact on roads and traffic ...just ask Sumner how their warehouses have affected their roads. Even the smaller local roads like Inter Ave will be adversely affected.

Regarding trails....The link from the Riverwalk trail to the Foothills trail is of paramount importance .

I consider the blocking of the passage under the bridge to be an illegal closing of a public footpath .

How the path is routed is a matter of taste, engineering and maintenance . Personally I would prefer a route at a higher elevation than the riverbank, owing to the potential flooding concern. Others will prefer adjacent to the river.

The Van Lierop park must be connected and the view of the mountain protected . The park was designed to showcase that view, and any obstruction would be a serious impact. I will also note that a surprising outcome of the park is major increase in bird habitat . I see photographers there all the time. This needs to be preserved.

Back to my previous comment on this report . The impacts have been well defined and mitigation measures identified . My recommendation is for a modified alternative #2 . Input from Friends of the Riverwalk and from Foothills Rails to Trails should be ongoing through the project. The Pierce Conservation District and the Tribe should also be vital partners. The City already is engaging and this is completely appropriate, as the entire project is in the urban growth area, and should already have been annexed into the city .

Once again, I am commenting as an individual however I would expect that further comments would be forthcoming from previously mentioned organizations.

Thanks,

John Hopkins.

Puyallup.



**Commenter:** Hopkins, John

**Source:** Email

**Date:** 3/13/2024

**Comment:**

FROM....FRIENDS OF THE RIVERWALK

AS A SUB SET OF THE FOOTHILLS RAILS TO TRAILS COALITION WE HAVE REVIEWED, SUPPORT AND AGREE WITH THEIR COMMENT LETTER IN ITS ENTIRETY.

WE EMPHASIZE THAT THE MITIGATED REDUCED IMPACT PROPOSAL IS THE PRAGMATIC SOLUTION.

WE ALSO THINK THAT THE TRAIL ROUTING ALONG THE RAISED BANK IS THE LIKELY PRACTICAL OUTCOME, AND WILL ALSO BE EASIER TO MAINTAIN. THIS FURTHER NECESSITATES MITIGATION BY MEANS OF WAREHOUSE SETBACKS OR SIZE REDUCTIONS.

WE APPRECIATE SUCH A COMPREHENSIVE STUDY, AND LOOK FOR POSITIVE OUTCOMES .

THANK YOU,

JOHN HOPKINS

**Commenter:** Hopkins, John

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Comments regarding trails, Concerns of trails

**Commenter:** Hopp, Vicky

**Source:** Email

**Date:** 2/25/2024

**Comment:**

Hello.

I am a 43 year resident of Puyallup and am greatly concerned with this development known as Knutson Farms. The increased traffic of this proposed development is not acceptable in this location.

Respectfully,

Vicky Hopp

**Commenter:** Huckee, Tricia

**Source:** Email

**Date:** 3/15/2024

**Comment:**

[Note this comment was provided twice under separate emails]

As a longtime resident of the area I would like to request that the warehouse project be reduced (if not stopped completely). I would like to request the following changes:

- Reduce New Warehouses from 7 to 3 (1 Million square feet)
- Reduce the Amount of Traffic
- Save Precious Farmland
- Protect Surroundings & View at Van Lierop Park and Farm 12
- Protect Salmon, Wildlife, and Wetlands
- Connect the Foothills Trail to Riverwalk Trail

Thank you for being open to these changes,

Tricia Huckee

**Commenter:** Hunt, James

**Source:** Email

**Date:** 2/24/2024

**Comment:**

Hello,

I am writing to ask you to Please stop Puyallup from becoming ware house city. What was once some of the most beautiful fields in the world are being paved over perhaps never again to be seen. Not only does natural beauty suffer, but so does wildlife and vegetation and wetlands. I was under the impression that beaver were in that area.

Again - please do your best to help to retain the natural beauty of Puyallup and the wonderful lifestyle that we enjoy. Warehouses, trucks, traffic, concrete - please stop this as much as possible.

Jim Hunt

resident since 1979 - when Van Lierop's and the area had flowers.

**Commenter:** Hunt, Bart

**Source:** Email

**Date:** 2/24/2024

**Comment:**

I have been trying to find good produce in this area since I am not a farmer. We need areas to help give us produce instead of Turing our valley into more storage warehouses. We have seen this in Kent and we need to preserve the land we can still use. Please don't build more warehouses.

**Commenter:** Hunter, Joanna

**Source:** Webform

**Date:** 1/29/2024 8:03:10 AM

**Comment:**

Farm land! Cover it all up and where do we get our food? Buy it from another state? Puyallup is morphing into an unrecognizable metropolis, losing its country charm. No more warehouses!!!

**Commenter:** Jacobson, Charles

**Source:** Email

**Date:** 3/1/2024

**Comment:**

As a citizen of Puyallup and frequent Shaw Road over pass user plus bicycle trail user I want to support the "reduced intensity" alternative to this entire project.

I for one can for see a mess of traffic over that overpass which would mitigate all of the effort to improve traffic flow on Shaw and on E Main to Hiway 410. I worry about increased Puyallup River pollution esp with tire debris that is toxic to salmon.

the noise and increased truck fumes will not add to the health of the environment as well. Finally we need to secure farm land for the future not cover it with cement. cjacobson

**Commenter:** Jasionkowski, Kim

**Source:** Webform

**Date:** 1/22/2024 2:08:16 PM

**Comment:**

We don't need any more warehouses in downtown Puyallup. You don't improve the roads to accommodate all the trucks going to and from the warehouses. Traffic is already horrible and now you want to make it worse. It's just build, build, build. I hope the City of Puyallup can do something to stop this. I know it's the county allowing this to happen (not surprised).

**Johnson Jewelers**

**Commenter:** Johnson, Amy

**Source:** Email

**Date:** 3/15/2024

**Comment:**

I prefer the "reduced intensity" alternative as shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location.

Amy M. Johnson, CG, GG

Johnson Jewelers

[REDACTED]

[REDACTED]

[REDACTED]



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses

**Commenter:** Johnson, Brett

**Source:** Webform

**Date:** 3/15/2024

**Comment:**

Greetings City Planners and Councilmembers, I do not believe the Knutson Farms project was permitted by the county in the spirit of protecting rural lands. These lands have been farmed for more over a century. Before that, they were wetlands, forests, and other natural areas. They should have been protected as Agricultural Resource lands, yet Pierce County decided to rezone most of the area as "Employment Center". In light of this, I without reservation support the "No Action" Alternative. With that said, I realize these projects may be difficult or impossible to stop outright. If a compromise must be reached, I would support a further-reduced version of the "Reduced-Intensity" Alternative. The areas in the Northeast are designated by the City of Puyallup as "Rural Buffer Residential", and should remain in that classification. Zero warehouses should be built in those areas. Please maintain the City's zoning for open space along the river. I would hope that some of it might be restored as habitat for wildlife in the future instead of being paved for parking lots. In the southern part of the proposed project, Building F would obstruct views from Van Lierop Park, and plans for Building G are drawn well within wetland D and its surrounding buffer. Both buildings should be excluded from the plan in favor of something far less intrusive. Rather than filling in the wetland, the city should protect it and encourage plantings etc. to make it more functional, not less. Additionally, a connecting trail should be included in the plan from Van Lierop Park to the proposed trail system near the Puyallup River. Currently, the proposal shows a parking lot in the area where a trail would make sense. Again, the entire southern portion of the project, including buildings F, G, and surrounding parking lots, should be removed in their entirety. In conclusion, the Reduced Intensity Alternative would need to be reduced further for this project to have any measure of balance with its surroundings. Four warehouses would still be possible to build on the north-central portion of the property. Expanding Open Space along the Puyallup River and removing Buildings F and G in favor of less intrusive uses would represent a significant improvement. Thank you for your time and consideration. I hope the City of Puyallup will settle on an alternative that makes the most sense for the future of our local community and the natural environment.

**Commenter:** Johnson, Brett

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Thanks. So it's Brett, B-R-E-T-T, Johnson, J-O-H-N-S-O-N.

I'm a Puyallup kid at heart. I grew up, was born and raised off of Shaw Road. I'm actually going to visit my folks right after this. I live in Tacoma. I'm here on behalf, actually, of the Tacoma Audubon Conservation community. And we are opposed generally to these kinds of warehouse projects. They're taking away our open space, covering up our rich Puyallup Valley soil. I don't know if you've ever felt that soil before but it's unique. It's special. And we need to protect as much of that as we can. And we're also concerned about habitat. You know, along the river, of course, and the wetlands, but also the potential for restoring habitat in the future. And if we have a bunch of warehouses sitting around it's really hard to do that. So I'm thinking not just in the near future but in the far future, you know, what do we want this place to look like? What legacy are we going to be leaving our kids, our community, and our future society? So there's a lot of considerations here beyond just dollars and cents and the legal ins and outs. And we've got to think bigger picture. But if no action is not possible with the way that the county does things, and they've been doing this for decades and it's been really hard to watch as I've grown up and seen Puyallup change this much. We need to stand up and do something. But if we have to accept an alternative, we're going to have to start with Alternative 2 and pare it down as much as possible. And try to get as much land set aside for open space as possible. Properly remove warehouses F and G on the southside because of wetlands, because of the view from the park. Reorient the rest of it so we can maximize the legacy for our future. Thank you.

**Commenter:** Johnston, Sondra

**Source:** Webform

**Date:** 12/16/2023 11:54:25 AM

**Comment:**

Any planning to install 4- or 5-lane roads for public use in the area?

**Commenter:** Johnstone, Fred

**Source:** Webform

**Date:** 1/25/2024 11:32:24 AM

**Comment:**

While I hate to see farmland permanently destroyed, it sounds like the warehouse complex is inevitable. I am a very active foothills trails user and would love to see that connect with the river trail. Hopefully this will inspire the city to actually create a bicycle corridor through downtown towards Tacoma. I know there is one planned as part of the 167 project but that could take years. I live off of Canyon Road and otherwise have to cycle on the sidewalk on the Shaw road overpass over to the river Trail.

**Commenter:** Josten, Elizabeth

**Source:** Email

**Date:** 2/14/2024

**Comment:**

Hello,

I am a long-time Puyallup resident having lived here since 1963. I want the city council to know that allowing 7 more warehouses in the Shaw Road area is a terrible plan! Puyallup should be preserving farmland and empty spaces that can absorb water and not contribute to water runoff and potential flooding. Also, the amount of traffic associated with 7 warehouses is far too much for the infrastructure! Please, limit the new warehouses to the three proposed in the "reduced intensity" plan recommended by the Citizen Group Protect Puyallup.

Thank you for your consideration.

Sincerely,

Elizabeth Josten



**Commenter:** Judah, Liesl

**Source:** Webform

**Date:** 12/16/2023 7:53:00 AM

**Comment:**

There is zero chance that the additional 1400 heavy vehicles per day wouldn't have a negative impact on the quality of life for those living here. There are multiple elementary schools near by- the fumes from that traffic adding to the already congested Shaw road. The new bridge in sumner over 410 is helpful for traffic but would prove useless against another 1400. We don't need another warehouse much less 7

**Commenter:** Kajca, Peter

**Source:** Webform

**Date:** 1/24/2024 10:04:25 AM

**Comment:**

this plan is just bad start to finish. it will make our roads way more over crowded. will create more pollution. our roads are not built for this and will not be fixed for many years if this passes, and at what cost. plus we just don't need more wear-houses around this area, we have plenty, and they are not at 100% full. we can do so much better than these two proposals. but the big thing is this will not make puyallup a place people will want to come to, no it will make it a place to stay away from!

**Commenter:** Kajca, Laurie

**Source:** Webform

**Date:** 1/25/2024 9:26:44 AM

**Comment:**

I support the NO ACTION alternative none of the proposed warehouse facilities would be build. I do not support alternative 1 Rail alternative or alternative 2 reduced intensity alternative. Neither alternative will reduce the environmental impact to the puyallup river and traffic and noise reduction and quality of life to my community. I do not feel that we need seven more warehouses when the existing warehouses in the area and surrounding communities are sitting empty or half full.

**Commenter:** Kajca, Peter

**Source:** Webform

**Date:** 1/26/2024 9:33:20 AM

**Comment:**

i think i might have made a mistake with my last comments. I want no action proposal as my comments to you. i think this would be a big mistake if any of the other proposals went forward. puyallup and this great of this area does not need more wear houses. the roads will never be able to handle the extra traffic, and the noise from more trucks , or more trains would be just miserable for the whole area. doing any of these wear houses would not make puyallup a place to come to , but stay away from.

**Commenter:** Kajca, Laurie

**Source:** Email

**Date:** 2/26/2024

**Comment:**

As described in the DEIS, the proposal would result in many negative impacts to the Puyallup area such as: traffic congestion from 8,724 cars and trucks per day, 130 acres of destroyed farmland, stormwater runoff that would kill salmon, loss of wetlands, and visual and noise impacts to popular Van Leroy Park and Farm 12. The purpose of the DEIS is to identify alternatives that would mitigate these impacts.

Alternative 2 in the DEIS reduces the warehouses and associated traffic by 35%, protects 40 acres of farmland, and adds buffer walls around Van Leroy Park. Alternative 2 is a good start, but it still has unacceptable significant impacts.

A better "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location. Note: although the "no action" alternative with no new warehouses is preferred by most of us, it's probably not a realistic option unfortunately. My husband and I previously submitted our comments that we support the NO ACTION alternative. We also realize that the No Action is probably not going to happen. Therefore we strongly support the Reduced Intensity be the Preferred Alternative in the final EIS decision.

Peter and Laurie Kajca

**Commenter:** Kajca, Peter

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Traffic impact concerns

**Commenter:** Kempff, Paul

**Source:** Email

**Date:** 3/15/2024

**Comment:**

To whom it concerns:

I am opposed to the current proposed warehouse development on the City of Puyallup Knutson Farms Property. I believe the development proposed in Alternative 1 and in Alternative 2 would significantly diminish the health of our community for the following reasons:

1. The increase in traffic would contribute significantly to congestion in an area already prone to congestion. The interchanges with Hwy 410, Hwy 167 and Hwy 410 currently are choke points for current traffic.
2. The estimated increase in the number of trucks in this area would have a greater impact than the same number of cars. It is likely that there would be an increased possibility of more accidents.
3. It is a shame to continually pave over and build on arable land. Farming is, arguably, more sustainable than the constant rush to build more warehouses to market, transport and sell more stuff which often has a short use life and is often not reusable.
4. The impacts to the environment would be detrimental: loss of floodplain and wetlands; increased runoff which contains toxins and thus negatively affects the health of salmon; increased poor air quality from truck exhaust; less ground to absorb water particularly in the increasingly heavy rain events.
5. The likely increase in truck and auto traffic would increase the risk of injury to pedestrians and bicycle riders. I know by personal experience how congestion contributes to driver impatience and aggressive driving.
6. Adding the number of warehouses proposed would negatively impact the people's enjoyment of Van Lierop Park and of Farm 12 with increased noise, traffic and diminishing the current expansive views both have.

As it is likely that some form of warehouse development will occur, I would recommend the proposed alternative referred to as the "reduced intensity" alternative which better addresses the concerns addressed above.

Thank you for taking my comments.

Thank you for being.

paul

paul kempff





**Commenter:** Kenyon, Steven

**Source:** Webform

**Date:** 12/20/2023 10:00:09 AM

**Comment:**

Re. Knutson Farms project I see the studies on traffic and potential projects to help fix them. But the impact goes beyond those locations. Drivers learn where to go to avoid those trouble spots. Pushing traffic further away and impacting routes that are not in your study. You need to expand the study out further. Thank you.

**Commenter:** Kibiger, Lynae

**Source:** Webform

**Date:** 1/4/2024 10:05:11 AM

**Comment:**

The natural environment on this land is home to several agricultural farms key that feed our community. The warehouse project should be limited or canceled. The massive build would spoil the land and contribute to pollution of the Puyallup River from roadway run off. The 2 lane road is inadequate for semi traffic and access to the farms and Orting. As assessment of need should be part of the decision for the city, with already built and empty multiple warehouses along Pioneer. As a Puyallup resident I am not in support of this project.

**Commenter:** Kibiger, Lynae

**Source:** Email

**Date:** 3/12/2024

**Comment:**

This large proposal of warehouses is not good for our community or environment.

Lynae Kibiger



**Commenter:** Kirk, Bethany

**Source:** Webform

**Date:** 12/14/2023 8:01:22 PM

**Comment:**

I strongly oppose this. How can the roads/infrastructure even begin to accommodate the truck traffic associated with a warehouse complex of this size? The roads in and around this area are already too congested. I honestly can't believe this is even being considered.

**Commenter:** Kirk, Bethany

**Source:** Email

**Date:** 1/7/2024 8:35:00 AM

**Comment:**

I want to attend this meeting. Please share the invite/link so I can share my (entirely negative) feedback about this (outrageously irresponsible) plan to turn the valley into a warehouse hellscape, with all the associated increased truck-traffic, noise, pollution, and general chaos.

VERY CONCERNED CITIZEN,

Bethany Kirk

**Commenter:** Kirk, Bethany

**Source:** Email

**Date:** 1/8/2024 9:22:00 AM

**Comment:**

Thank you, Chris. I have a few more questions:

1. When/where will public comments/ discussion occur? Other than via submitting comments for the DEIS (which I did).

2. I've heard there is another meeting on the 17th at Pierce College. Is that correct? If so, please provide details for that meeting.

2. What is the process for this project? In particular, I am unclear about the following:

a. Who are the persons making decisions and/or leading this work, for the city and for the county?

RESPONSE: Pierce County is responsible for final site de

b. What does the city do with/about the DEIS comments at the conclusion of comment period, and what bearing do they have on a final outcome?

c. What are the next few steps in the process?

d. Who makes the final decision and when is that anticipated to occur?

3. I've heard that it is a county project, but because it is in the city's planned annexation area we did the DEIS. Should I - and other concerned citizens - be contacting someone at the county about this project/process, as well?

4. Is there a public-friendly, online source for all this information? Can I sign up for updates and alerts to stay informed as this project moves forward? Like everyone else, I have a full-time job and a myriad of other responsibilities, and do not have any more time to sleuth around the internet trying to find all this info.

Appreciate the help.

Thanks,

Bethany Kirk

**Commenter:** Kirk, Bethany

**Source:** Webform

**Date:** 3/12/2024

**Comment:**

My first preference is no action, I.e., no new warehouses built on the property. My second choice would be Alternative 2 - Reduced Intensity Alternative. The downtown Puyallup area simply cannot accommodate the increase in traffic the planned complex would bring. It cannot even accommodate the increase in traffic resulting from Alternative 2. It already takes me 15 minutes to drive the 2.5 miles from my house to Farm 12 mid-week, mid-mornings - during the slowest of traffic times. Add several thousand more vehicles a day to these roads and residents will be locked in our neighborhoods, choking on exhaust smoke from all the trucks. This will ruin the quality of life for thousands of families.

**Commenter:** Kohli, Mary

**Source:** Email

**Date:** 2/26/2024

**Comment:**

The preferred alternative is my choice. I personally don't want any warehouses in Puyallup. The traffic is already ridiculous. [image of reduced intensity alternative]

**Commenter:** Kunze, Amalia

**Source:** Webform

**Date:** 2/11/2024 9:23:53 AM

**Comment:**

I strongly oppose the proposed warehouses at the Knutson farm site. An archeological study for one should be done before anything further is done with the land. The warehouses will increase congestion, noise and environmental pollution and create a terrible eyesore. I think a mixed development of residence, smaller store fronts and restaurants and a larger park footprint would be more attractive, less congested and more environmentally friendly, and will revitalize the area.

**Commenter:** Lane, Staley

**Source:** Webform

**Date:** 1/30/2024 2:02:33 PM

**Comment:**

You claim that there is climate change and then pave a chunk of natural land? Won't be much of an environmental impact. Tell that to the salamanders, frogs, insects and deer that inhabit the area. Not to mention the waste water run off into the river. Say it like it is, it is greed. You don't care about the environmental impact because you don't live here. You're rich and we have no say in this. Because you don't care. Puyallup will be another dump just like seattle. Full of smog and people crying about extinction and such when it could have been thwarted by not paving over farm land. Disgusting. Hippocrits. Hope you have the day you deserve.

**Commenter:** Lee, Keun-Hae

**Source:** Webform

**Date:** 1/29/2024 2:03:45 PM

**Comment:**

I oppose the approval of the warehouse construction permit. 1) increased traffic in an already congested area of Puyallup. This area is already in need of a traffic solution that allows residents to move freely and quickly to neighborhoods along Shaw road. Additional warehouse traffic will further add pressure to this congested traffic zone. 2) Puyallup is in desperate need of additional housing and recreation areas. Additional housing would benefit local restaurants and businesses already in this corridor in a way that additional warehouses would not. 3) Quality of life will be negatively impacted. The city is in desperate need of additional recreational spaces for residents in this area. Expanded parking and plans for additional recreational areas in this area are already in development. They will be negatively impacted by the warehouses. The county is not interested in the needs of Puyallup citizens. Please put community over quick profit.

**Commenter:** Leisner, Andrew

**Source:** Webform

**Date:** 2/26/2024 6:09:00 AM

**Comment:**

As a Valley resident (Fife & Puyallup) for the last 36 years, I've watched farm after farm be converted into warehouses. Warehouses that cover the natural, fertile, valley soil and replace it with concrete and asphalt. The history of these towns is in farming and with every warehouse we allow, we give up on that history and cave to the commercial greed of landlords who can't fill their warehouse spaces. I ride my bike through Van Lierop park regularly on my way to the Foothills Trail to ride through additional farm lands for miles. When I ride past the current warehouse, I see it mostly dormant with little activity. There are no trucks at the dock doors, no cars in the parking lots and yet we want to add more empty space to our beautiful valley. What will happen to the view of the mountain we love from the swing bench? Our scenery makes Puyallup what it is. What will happen to the ability to commute rather free of big rigs on Shaw and Pioneer? The roads are not designed for that much heavy traffic and they create risk to the pedestrians using the sidewalks and crosswalks. As DOT tightens drive time regulations on truckers, their sense of urgency to make their deliveries increases and their respect for local laws decreases. Adding this much warehouse space in this area is unacceptable and mitigation work needs to be done to stop the sprawl of commercial greed.

**Commenter:** Levy, Judith

**Source:** Email

**Date:** 2/28/2024

**Comment:**

I am writing to say I feel this proposal is TOO large and should be scaled back. This would add too much truck traffic which is damaging to the roads. It would also decrease the area for rainwater to be absorbed and would result in increased water flowing into the Puyallup River with the possibility of flooding.

Thank you,

Judith Levy

**Commenter:** Linden, Krista

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Thank you for the opportunity to comment on the Knutson Farm EIS and the impact this development will have on our community. I am the Founder and CEO of Step By Step, Farm 12, Fika, and our soon- to-be Early Learning Center, all of which is located on the Van Lierop homestead property. Our facility is adjacent to the City of Puyallup's Van Lierop Park, as well. I appreciate the city's efforts to mitigate the impacts the warehouse development will have on the city, its citizens, and those who do business in the city. When I was desiring to buy the Van Lierop property in 2014/2015, I attended a lot of public meetings and I know what the city and its citizens want to see happen on this land, the gateway to the city. Thankfully, we were able to purchase the land and with the support of city council, city staff, and the community we have worked together to create something that everyone can be proud of. Farm 12 and its enterprises (along with Van Lierop Park) are known and loved around the region. While I support a landowner being able to do what they are allowed to do on their property, I also understand that we cannot underestimate the very profound impacts this development will have on our city forever, not just on our infrastructure, but the vision we've had for this area. We can't undo this once it's done, but we have the opportunity to very thoughtfully plan it out, now. The proposed warehouse complex, even at the reduced scale presented in the EIS, is going to dramatically change our community, and not in the way we hoped. When I saw the increased traffic count during the public forum, it seemed impossible that that many vehicles could even come close to being manageable. It is a significant concern to us. Between the park, Farm 12, Fika, Step By Step and the Early Learning Center, we have a lot of children on and around our site and they will soon be crossing 33rd street. Having so many cars and trucks creates a huge safety concern for us. After reviewing the EIS, I would like to suggest that 33rd street be vacated between Step By Step and Van Lierop Park. I also would like to see the warehouses concentrated to the north and keep the view corridor free, allowing for open space/farmland on the southeast side of Van Lierop Park. I also recommend the EIS add specific language about the impacts to the Farm 12/Fika/Step By Step site, as I describe above. Thank you for your all the time and resources the city has committed to this development. It is clear that the desire is there to be thoughtful and proactive when it comes to how we want the gateway to our city to present itself now and into the future. Sincerely, Krista Linden

**Commenter:** Lindholm, Michael

**Source:** Webform

**Date:** 1/30/2024 6:29:30 AM

**Comment:**

Seriously tho, nobody wants these buildings built! Traffic is already bad enough on the entire road of shaw.

**Commenter:** Loney, Kelly

**Source:** Email

**Date:** 2/21/2024

**Comment:**

These are my comments on the proposed warehouse project at Van Lierops / Knutson Farms.

The proposed mitigation in Alternative 2 of the DEIS is a good start. Alternative 2: 1) reduces the number of warehouses; 2) preserves some open space/farmland; 3) limits truck traffic; 4) provides some traffic mitigation; 5) protects Van Lierop Park from sound and visual impacts; and 6) reduces impacts to the Puyallup River and wetlands.

However, the proposed mitigation in Alternative 2 is not sufficient. Reducing warehouse square footage and traffic volume by 35% is not enough. Even at that scale, the project is too massive for this area. At least 50% reduction is needed. Fewer warehouses, less traffic, and more farmland protection should be incorporated into the City of Puyallup's preferred alternative in the Final EIS to Protect Puyallup.

We need to protect our rich farmland, our views of Mount Rainier from Van Lierops park, our views OF PUYALLUP, our property values, our greenspaces - especially with all the apartments going in right next door to VanLierops on Shaw Rd - and a trail - as planned originally, I thought - from Van Lierops park to the river.

Thank you, please do the right thing for Puyallup.

--

Kelly Loney

[REDACTED]

[REDACTED]

**Commenter:** Loney, Kelley

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concern

**Commenter:** Luppino, Janae

**Source:** Webform

**Date:** 12/14/2023 6:59:06 PM

**Comment:**

Summer orting highway is already terrible for traffic this plan looks to connect further an already maxed out road.

**Commenter:** Maahs, Kathy

**Source:** Webform

**Date:** 1/29/2024 10:43:15 AM

**Comment:**

Please don't let more of our farm land be bull dozed to support big warehouses. Once coved that farm land will be useless to future families. At the very least down scale the project and save as much farm land as you can. Even if it means buying the land . Thank You Kathy M

**Commenter:** Macadangdang, Craig

**Source:** Webform

**Date:** 2/28/2024 9:21:00 AM

**Comment:**

RE: Warehouse Proposal on Shaw Road Farmland Update I favor the adoption of the "Reduced Intensity" Alternative. Craig Macadangdang City of Puyallup Resident A better "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Citizen Group Protect Puyallup recommends this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year.

**Commenter:** Mahoney, Roberto

**Source:** Webform

**Date:** 2/5/2024 7:48:24 PM

**Comment:**

Hello, I grew up in Puyallup and lived there for 25 years. Growing up in a diverse community with farms, great green spaces, and many active people. The foothills trail is an excellent trail. Next to a section of that was Van Leriop daffodil field. Unfortunately that area became a warehouse area. Then with this area is a proposed warehouse area now. Why does this have to be the MO of the city now? Too many city and county council people who are getting rich by making land deals and receiving kick backs perhaps? Why can't the land be converted to a park like Hyde Park in London? There is absolutely nothing wrong with not developing farm land to house warehouses. Leave it zoned in such a way that it becomes one of the better parks in Puyallup.

**Commenter:** Mamic, Helen

**Source:** Email

**Date:** 3/7/2024

**Comment:**

The city should never have rezoned Knutson property in the first place....wasn't Knutson on the city council when this happened?

Money, money, money...that's all the city of Puyallup wants.

**Commenter:** Marckmann, Cydne

**Source:** Email

**Date:** 3/13/2024

**Comment:**

Please stop the warehouse growth projected for the Knutson Farm land parcel and limit it to 3 warehouses. The traffic on Shaw Road is already miserable and this will make it exponentially worse. There is no infrastructure to support this decision-in our roads, our water supply, our sewage. In addition, converting this water absorbing soil to concrete will impact the run-off into our creeks which are vital to our salmon ecosystem.

Think of everyone for generations this decision will impact and not just the pockets of those who are contributing to the city council's campaign coffers.

Cydne Marckmann, DNP, ARNP, FAANP

**Commenter:** Martineau, Nicole

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Hello,

I'm writing to oppose the addition of 7 more warehouses on Shaw Road area farmland near Van Leroy Park and Farm 12. It's my hope that we can encourage those involved to choose an option that preserves more historic farmland, reduces traffic congestion and provide the best option to continue a common sense and thoughtful trail connection. This project is far from congruent or representative of the community I grew up in. I understand that change can be good, but this is the kind of change we can do without. Thank you for your consideration. [attached image of reduced intensity alternative]



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses

**Commenter:** Maston, Teresa

**Source:** Webform

**Date:** 1/3/2024 9:00:42 PM

**Comment:**

As a home owner and someone who pays taxes in this area I would like for this project to not take place. I feel that the flooding will have sever impacts on the environment and surrounding areas as well as effect the wild life in our community. I also am not interested in the light pollution that will impact me.

**Commenter:** McDonald, Mary

**Source:** Email

**Date:** 2/28/2024

**Comment:**

Good morning,

My husband and I have been Puyallup residents for 43 years; our home located approximately 1 mile east of Van Lierop Farms. We have considerable concern regarding the Knutson Farm Industrial proposal to add 7 more warehouses on Shaw Road area farmland near Van Lierop Park and Farm 12. Over the years we have witnessed significant congestion on Shaw Road which negatively impacts quality of life for all. Whether it is school dismissing at Shaw Road Elementary with buses and parents attempting to reenter traffic or folks just trying to navigate after grocery shopping at Safeway or commuting home. What is the threshold or breaking point regarding numbers of cars and traffic for this community?

An additional concern is the loss of wetlands should this project go unquestioned. We live on the base of the 21st St SE hillside. When we purchased our home in 1980, the land above us was designated wetlands. Development of homes above us has resulted in the displacement of water necessitating mitigation with large trenches in many yards. Has a thorough study been conducted regarding the impact of this proposal to the existing wetlands? Is it sound to build large concrete warehouses over fertile wetland?

Now, to the issue of storm water runoff with resulting impact to our salmon. It is believed that a recently born orca has died in our Puget Sound waters. These creatures depend upon the salmon population for their food source. Storm water runoff increases the toxic pollutants released into marine habitats. Does it make sense to make efforts to save the orca while conducting business that threatens their survival?

I strongly urge a reconsideration of the proposed magnitude of this project. It is time to live our values and promote quality of life.

**Commenter:** McKean, Paul

**Source:** Email

**Date:** 2/15/2024

**Comment:**

Good Evening,

I only have a few comments regarding this project. I'm totally against this project, every vacant plot of land in Puyallup doesn't need any more warehouses or apartment complexes. I realize this project is too far along to be stopped so it needs to be limited if possible. I know a lot of work has been put into the impact study, but I think the traffic part is wrong, if you build warehouses or buildings this will bring a lot more traffic than the study accounts for. All you have to do is look at Shaw Road during rush hour, it's a mess. Since Puyallup is becoming home to many warehouses just look at 512 and Canyon during late afternoon a total mess, I never drive on Meridian during later afternoons or on weekends, it's parking lot. Changing the timing of the lights on Shaw or making improvements is a band-aid approach, during the Fair, a bigger mess! The study talks about no Amazon centers, sounds nice on paper but what happens in a few years when business models change? Once money has been spent on the project changes will come, and I know the leadership in Puyallup will cave like they always do, every time the Fair asks the city says yes, no matter the impact on the citizens.

v/r,

Paul McKean

**Commenter:** McNamara, Karen

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Yes. I'm Karen McNamara. It's

spelled K-A-R-E-N, and my last name is McNamara. It's

capital M, small c, capital N-A-M-A-R-A. I live in the Deer

Creek subdivision, which is just off of 16, just as you

start up Shaw Road. And right now I can barely get out of

my subdivision on a daily basis. They really -- the times that I actually can get out is when the traffic is backed clear down to Pioneer Crossing and some soul is kind enough

to stop and allow the traffic. And they wait and let you and somebody from Shaw Road coming down, zooming down Shaw

Road while the light is stopped and get out. But you take your life in your hand whenever you travel out that way.

Last Friday night, my husband and I left our

subdivision by the method I just described, and drove down to The Old Canner in Sumner, and the traffic going back south on Shaw Road was back clear up to Toscanos. And that was at 5:30 at night.

Well, first of all I should say I appreciate so much the fact of how much hard work that you all are doing to mitigate this because I know that the county has gone back on their agreement and that's not right. So I want to say I appreciate how hard the city has been working to try and mitigate this. But just like the speaker before me, this is absolutely unacceptable. They do not need these warehouses. The warehouses they built on East Main are not full. The warehouse that's off this, whatever you call that, Martin or

Fifth or whatever where they built that little extension off the top overpass over the land is not full. You drive down and you take 167 to go north right by Sumner and those warehouses are not full. And once this farmland is gone, it will be gone. It will be gone. We will never get this

back. It's our food. It's our water. It's our noise. It's our traffic. This is unacceptable because somebody wants to store a bunch more stuff. And I'm concerned about the light industrial. What exactly does that mean? We had that fire down off 15th where an entire, huge section of the city was told you can't

go outside because they had chemicals in that building that were causing huge environmental problems.

And the other thing I noticed in your impact is this is a major corridor for the fair. And when the fair -- I don't see a single slide that talks about the traffic impact when the fair happens. Because right now as somebody who lives along that corridor, try to get home from anywhere north during the fair, now that it's gone to a full month

down in the fall, and two months in the spring, and God knows how many more weeks they're going to add onto that, where are you going to put all those cars? Where's all that stuff going to come from? There's not enough time. I mean, it took forever to get a left turn lane off East Main to get onto the freeway. And now you're talking about proposing to widen that.

And I'm worried about the storm drain runoff. Go park there during fishing season. Everyone parks on East

Main to get down to the river to fish. What's that stormwater going to do to the fish that are coming down the river? And what's going to happen if, God forbid, Mount Rainier blows up and we have a lahar and it comes balling down the valley? Because right now according to the thing, when Mount Rainier spit a few years ago and the lahar came through, they said that my subdivision would probably not be impacted because of the way the land flows right there. That finger sticks out by Farm 12, but that my backyard would be filled to 20 feet because I live at the base of the subdivision. I am concerned about the traffic that's coming down Shaw Road. You're going to cut down trees. You already have a huge water reservoir off Shaw Road and it's full half the time. The valley floods. The water drains off that hill like crazy. You have school buses that cross there and you have two huge school complexes. We have the development of Pioneer Crossing. And yeah, it's nice to have Safeway right there and MOD Pizza. But the reality is you're also allowing in the city to put a huge apartment complex directly to the west across the street from there and you want to add a McDonald's to that for God's sakes. You cannot get out on Shaw Road as it is. The county should not be allowed to do this kind of project. They absolutely should not be. We are the ones that live here. These people building Knutson, on Knutson land, they're not going to live here. They're not the ones who are taking the time to drive home every night in the traffic. They're not the ones who raise their families in this community. And I appreciate so much that you want to keep the view thing. But realistically, your lovely picture that you always cut out for Puyallup, it takes a picture of a mountain with the daffodils, that's all gone. It's going to be gone. So whatever you can do, I appreciate the hard work that you're doing but we need to have more public input on this. Go home and tell all your friends and neighbors because this is not acceptable at all. The alternative should be no project, period. Thank you.

**Commenter:** Metzger, Bud

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

KF warehouse development concerns, Environmental concerns associated, Traffic impact

**Commenter:** Meysenburg, Shelly

**Source:** Webform

**Date:** 1/6/2024 9:36:28 PM

**Comment:**

Please please please stop ruining our city!!! That's all I need to say!

**Commenter:** Michael, Jordan

**Source:** Webform

**Date:** 1/17/2024 10:52:21 AM

**Comment:**

After learning about this Knutson Farms EIS project today 17 Jan 24, as an active resident of Puyallup I do not support this project and development of additional warehousing across this acreage of land. I'm sorry but the board members considering the approval of this really needs to consider importance of the land, the area, the property and business owners around this area and disapprove to move forward with this. First, puyallup is home and holds historic value of protecting tribal lands, terrain and other land agreements in the entire state in favor of protecting our environment and ecosystem in the Puget Sound. Expanding of warehousing development would go against those values and other environmental department agencies in the area. Two, the land is predominantly farmland and encourage some small business stakeholders and local crops that drive some of the popular farmers markets in the area hence the name "Knutson Farms EIS." The mere fact the name of the project alone drives me to believe you are driving away space and land to maintain these lands, businesses, and stakeholders for the local economy and a large reason why this area is so popular away from city limits and that is my second argument against the project. And lastly, for the peer pleasure of local residents and better opportunities we DO NOT need more warehousing in this area. After looking at a GIS geography model within 100 miles center of this acreage you desire for this project we already have hundreds of acreage north in Sumner, along the Highway all the way to Auburn of warehousing space. What's the immediate need for more? It's not necessary and the simple enjoyments of these farm areas including Knutson Farms, and Farm12 small business owners do not want to foster an environment of warehousing storage space, excess traffic of semis and supply distribution to an overly crowded area already for traffic. Also, if I recall there was a park project ongoing right next door to Farm12 to expand the use of the outdoor space for public safety and recreation? What ever happened to that? Ultimately, this is not the project this area needs right now, and we need to reconsider this on all levels. Maintain the farmlands the way they are or a possible solution would be to invest resources that equip and protect the farmlands, the space for recreational and pleasure of the environment many people use in this area to include the trails, the Orting greenbelt trail and the great small business products by Farm12 and Knutson farms that grow this community to what Puyallup stands for. Thank you for your time. I hope my comment is received, respectfully, and is considered in the veto of progressing this project.

**Commenter:** Michael, Jordan

**Source:** Webform

**Date:** 1/17/2024 10:56:04 AM

**Comment:**

After learning about this Knutson Farms EIS project today 17 Jan 24, as an active resident of Puyallup I do not support this project and development of additional warehousing across this acreage of land. I'm sorry but the board members considering the approval of this really needs to consider importance of the land, the area, the property and business owners around this area and disapprove to move forward with this. First, puyallup is home and holds historic value of protecting tribal lands, terrain and other land agreements in the entire state in favor of protecting our environment and ecosystem in the Puget Sound. Expanding of warehousing development would go against those values and other environmental department agencies in the area. Two, the land is predominantly farmland and encourage some small business stakeholders and local crops that drive some of the popular farmers markets in the area hence the name "Knutson Farms EIS." The mere fact the name of the project alone drives me to believe you are driving away space and land to maintain these lands, businesses, and stakeholders for the local economy and a large reason why this area is so popular away from city limits and that is my second argument against the project. And lastly, for the peer pleasure of local residents and better opportunities we DO NOT need more warehousing in this area. After looking at a GIS geography model within 100 miles center of this acreage you desire for this project we already have hundreds of acreage north in Sumner, along the Highway all the way to Auburn of warehousing space. What's the immediate need for more? It's not necessary and the simple enjoyments of these farm areas including Knutson Farms, and Farm12 small business owners do not want to foster an environment of warehousing storage space, excess traffic of semis and supply distribution to an overly crowded area already for traffic. Also, if I recall there was a park project ongoing right next door to Farm12 to expand the use of the outdoor space for public safety and recreation? What ever happened to that? Ultimately, this is not the project this area needs right now, and we need to reconsider this on all levels. Maintain the farmlands the way they are or a possible solution would be to invest resources that equip and protect the farmlands, the space for recreational and pleasure of the environment many people use in this area to include the trails, the Orting greenbelt trail and the great small business products by Farm12 and Knutson farms that grow this community to what Puyallup stands for. Thank you for your time. I hope my comment is received, respectfully, and is considered in the veto of progressing this project.

**Commenter:** Michael, Jordan

**Source:** Email

**Date:** 1/17/2024 7:54:00 PM

**Comment:**

Project Members and Board Members,

After learning about this Knutson Farms EIS project today 17 Jan 24, as an active resident of Puyallup I do not support this project and development of additional warehousing across this acreage of land. I'm sorry but the board members considering the approval of this really needs to consider importance of the land, the area, the property and business owners around this area and disapprove to move forward with this. First, puyallup is home and holds historic value of protecting tribal lands, terrain and other land agreements in the entire state in favor of protecting our environment and ecosystem in the Puget Sound. Expanding of warehousing development would go against those values and other environmental department agencies in the area. Two, the land is predominantly farmland and encourage some small business stakeholders and local crops that drive some of the popular farmers markets in the area hence the name "Knutson Farms EIS." The mere fact the name of the project alone drives me to believe you are driving away space and land to maintain these lands, businesses, and stakeholders for the local economy and a large reason why this area is so popular away from city limits and that is my second argument against the project. And lastly, for the peer pleasure of local residents and better opportunities we DO NOT need more warehousing in this area. After looking at a GIS geography model within 100 miles center of this acreage you desire for this project we already have hundreds of acreage north in Sumner, along the Highway all the way to Auburn of warehousing space. What's the immediate need for more? It's not necessary and the simple enjoyments of these farm areas including Knutson Farms, and Farm12 small business owners do not want to foster an environment of warehousing storage space, excess traffic of semis and supply distribution to an overly crowded area already for traffic. Also, if I recall there was a park project ongoing right next door to Farm12 to expand the use of the outdoor space for public safety and recreation? What ever happened to that? Ultimately, this is not the project this area needs right now, and we need to reconsider this on all levels. Maintain the farmlands the way they are or a possible solution would be to invest resources that equip and protect the farmlands, the space for recreational and pleasure of the environment many people use in this area to include the trails, the Orting greenbelt trail and the great small business products by Farm12 and Knutson farms that grow this community to what Puyallup stands for. Thank you for your time. I hope my comment is received, respectfully, and is considered in the veto of progressing this project.

Respectfully

Puyallup Resident - Jordan

**Commenter:** Miller, Pat

**Source:** Email

**Date:** 3/2/2024

**Comment:**

I have lived in this SE section of Puyallup my entire life (67 yrs). I live on 21st Street SE and have seen an unprecedented amount of development over the last several years.

I deal with traffic every day on my street from Cascade Christian school, and now there is a proposal to add multiple portable buildings to the site to house grade school children.

Not sure how many children this would add, but each child has a parent drop them off in the morning, and pick up every day. The additional traffic on my street is going to be significant.

Along with that, development has started on the project on the corner of Shaw and Pioneer (I understand it will be a large apartment complex). Not to mention the future plans

for the old food processing plant that burned (corner of 15th Street and Pioneer). These coupled with the Knutson Farm development means a whole lot of increased traffic

on the existing infrastructure. This small corner of Puyallup is being bombarded with developments and it is at the expense of the residents. The Puyallup valley has always been

a place I would never leave, but I question city government approving all the industrial developments in this area.

That being said, I suggest the city adopt the proposal with the least amount of impact, the "Reduced Intensity" proposal.

Thank you for the opportunity to comment. Patricia Miller

**Commenter:** Miller, Dayna

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Please reduce intensity by limiting warehouses being built on our farmlands!!

Dayna Miller

[REDACTED]

[REDACTED]

**Commenter:** Mix, Brendon

**Source:** Email

**Date:** 3/13/2024

**Comment:**

Hello,

I am writing regarding the proposed to seven warehouses on the Knutson Farm property. As someone who commute every day to SeaTac, I am already stuck in traffic for over an hour each direction. Adding that amount of large vehicle traffic would be a nightmare, even with an extra on ramp, Which has been proposed as the solution. Although that would help with the small roads in the area, it would do nothing for the traffic on I-5 and 410. These are already stopped to Auburn and beyond at all heavy traffic times. It has come to the point where even in non-traffic times these roads are completely stopped. I would not want to move from this area, as I love it here, but a decision like this could add a lot of time to my commute each day. Don't make the people that live here lose precious family time for these warehouses.

Thank you,

Brendon Mix

**Commenter:** Mocorro Powell, Ashley

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Ashley Mocorro Powell. It's L-E-Y for Ashley and it's M-O-C-O-R-R-O space, no hyphen, P-O-W-E-L-L. So I'm a lifetime resident. I was born and raised in unincorporated Pierce County and also in the city of Puyallup at Good Samaritan. Thanks to all of you I'm a local public school student. I graduated with some time through our local institutions, including this one that we're in today, Pierce College Puyallup and split time at the Fort Steilacoom campus. So thank you for having this meeting here today for public comment. And I'm a University of Washington Seattle alumni and became a scientist as a part of that process through their Department of Biology. So I'm going to name some items that I won't go into extensive comment for. Maybe we'll send it in written form to you about some of the gaps that I know that you guys couldn't maybe address because of the EIS, but just things to consider. I'm just going to say what it is. Pierce County and the City of Puyallup know, people might have argued this back in the day, is an environmental justice community. We have similarities across many other West Coast and East Coast port communities that face a lot of challenges of rail trails and roadway and highway systems that disproportionately impact our communities around health, recreation, education, access to quality of life, and also access to livable and equitable sources to food. I really wish that our local food bank and other people were here today to be able to speak to the pressures on that system. I served as a Washington Department of Agriculture reviewer during our COVID timeframe and shutdowns, reviews of what was happening during that time across the state and for food access needs. And what was happening in Pierce County was really inspiring to me about how food was getting out. So this land is really important to me because the U.S. Geological Survey lists it as a rare type of soil, right, that comes from Mount Rainier, and the lahars that we mentioned and all these different geological formations. And it's really unique for agricultural purposes which is what our seal is for the City of Puyallup. I know the county isn't here today to talk about it but I hope they'll help you pay to redesign that if they end up building these warehouses in our community. But we have to think about the factors that we use different databases that exist in the sciences that the state recognizes for environmental justice and help them pass at the Department of Health, including that Washington Health Disparities map and several others that are in use across the state, that if you were to overlay these areas that you're talking about, there are massive impacts to the communities in the surrounding area, which we've already heard today. So I thank people for bringing that public comment. Other facts that were brought today into this consideration for the EIS is noise pollution. I've worked and had the honor of working with communities across the globe, as well as within environmental justice communities from Cancer Alley into the Gulf areas, as well as Appalachia, mountaintop and mountain mining communities, and hearing about industrial areas that are being put into communities and next to schools which includes a minimum of two schools that will be impacted, right, in this development area, that these industrial areas will create long-time noise. And when you look at different agencies, whether it's Department of Health or different advocacy organizations or like advocate organizations, they list that, you know, this impacts our

mental health, our well-being, our spirit. It impacts health disease and air quality. So we've already talked about traffic today so I won't go into that a lot but I appreciate all the work that was done into the different mitigations. But I live in two different areas in both the City of Puyallup -- I live behind Fruitland Elementary basically area closer to South Hill and also downtown Puyallup right next to the railroad. And I can tell you that if you were to do the rail area that will be a significant noise pollution and impact for all those communities that live right along that rail line that were built many, many generations before I was probably around but that I pay rent now to listen to on a regular basis and can't actually grow food at my community in the soil plots that I have with my tenant, not because he doesn't allow it because when I put stuff out there I noticed that there was a weird soot that was on all of my like benches and things that made me hyper paranoid about what was coming off of the trains and that goes by. And I am a commuter and use those public services. But I just wanted to bring that up is that there are a lot of different impacts here that weren't spoken about today in regards to disproportional health impacts. So, and also, I mentioned it also last week on the informational meeting that climate is a big concern for me as a scientist. The Washington Health State Assessment for Climate Change and Risk Management has come out in the last two months and I don't see that considered. And I know it's part of the EIS process but I would like to see that more thoughtfully thought of in regards to just different concerns that we're seeing in our community already around Key Island effects as some people have mentioned about with like increase of concrete, stormwater pollution and runoff which is a major issue for the Puyallup River. As American Rivers, a nonprofit organization listed in 2020, the Puyallup River is one of the most -- top 10 rivers in the country. In the country as an endangered river because of the impact of pollution that it already faces. So there's a lot of different scholars and things that I'd highly recommend the city and the county look to at Washington State University's Puyallup Extension, University of Washington Tacoma, Puget Sound Institute that are doing a lot of work around air quality, stormwater runoff, and all the different tire runoffs that are happening and how it kills Coho salmon at different stages of their life. I just really would like you to think about these things and I definitely would say I'm in opposition of this as far as no development and look actually at creating ways for community to continue to use it for recreation, for Farm 12, and that community space that's been built because of Krista Linden and so many others that centered around her vision with the Van Lierop family and the city to build those parks that continue to gather people there to make it see where we can continue to grow food, as well as to gather and to celebrate so many lifetimes and moments that I see when I go down there every single time. Like people getting married, having birthday parties, and things like that. So I'll end there but I just wanted to share my thank you for having this public comment.

**Commenter:** Mohlman, Bryan

**Source:** Webform

**Date:** 2/29/2024 8:57:00 AM

**Comment:**

It would be a horrible decision to install the 7 warehouse building project into this area of Pierce County. Every year we lose more and more green space. Impermeable surface increases and habitat is lost. This choice favors potential economic incentive over the long term sustainability of life in the area. Please don't turn this open area into warehouse space. Make an effort to reforest it and provide necessary green space and habitat to this much needed river side property.

**Commenter:** Moreno, Cherie

**Source:** Webform

**Date:** 1/17/2024 12:49:44 PM

**Comment:**

This cannot be built as described in any form. This is unsafe for the families in this area and will add 2000+ people to an area that is confined and has limited throughputs. There are plenty of other places that are not sandwiched between houses, Puyallup river, railroads and small residential streets. This area is already overloaded and the proposed upgrades to the area won't even address the upgrades needed for current traffic issues. Traffic is backed up for hours on Shaw and on 162 during rush hours. The bridges over the river are constantly overwhelmed and backed up. The upgrades to the traffic ave and 410 on-ramps isn't enough for our current traffic needs. This would increase the volume of people in this small secluded area and will make this unsafe for the families in this area. How are we supposed to flee to higher ground when the emergency alarm sounds? all our roads are already backed up and we probably won't make it to safety let alone off of 80th street with this increase. This disregard to the families, the beautiful farm land and the wildlife that live here shows that Knutson farms does not care about this area and should not be allowed in it. I say no, finding a different area around Knutson farms would be best if they need to build warehouses. There is plenty of area over there without the constraints that this location has. Or use the current warehouses that are sitting empty through the area. This proposal does a poor job of taking into account the needs of the area. When did they do the traffic impact? I feel like it was done during the day. That's the only time I saw anyone out there performing these assessments. Mid day. How does that accurately show the needs for this area? Please do not approve this in any way. The impacts from this build will affect more than just traffic. It will affect the people and wildlife who live in Puyallup negatively for decades.

**Commenter:** Morita, Jay-me

**Source:** Email

**Date:** 3/2/2024

**Comment:**

Hello,

I am writing to propose my support for the preferred alternative to the warehouse development near Van Lierop park. It would be much more beneficial for the community to reduce the number of new warehouses to 3 so that the integrity of the park, the foothills trail, farmland, and area near Farm 12 is maintained.

Farm 12 is a very popular venue for the community, which I am personally fond of, and hosts many events such as weddings and large parties. A series of industrial warehouses would be a tremendous eyesore for patrons attending events there.

Thank you for taking the time to listen to the concerns of the community.

V/R,

Jay-me Morita

**Commenter:** Mosley, Jackson

**Source:** Webform

**Date:** 1/3/2024 8:36:00 PM

**Comment:**

As a former resident of puyallup and someone who still works within the city, I think it's a shame to ruin the beauty and open areas with warehouses. So much farmland has already been taken over by warehouses, many of which are empty. We shouldn't give up any more. Once those are constructed there is never any going back. Don't ruin the good that you do have just for the sake of money.

**Commenter:** Myrick, Bob

**Source:** Email

**Date:** 2/13/2024

**Comment:**

I prefer the Reduced Intensity Alternative put forward by an interested Puyallup group. Alternative 2 would be my second choice.

I'm very disappointed that Knutson Farms has kept the connection to the Riverwalk Trail closed as a bargaining chip. Stupid idea and choice as it just make us users mad.

**Commenter:** Nagai, Laura

**Source:** Email

**Date:** 3/9/2024

**Comment:**

To whom it my concern,

When I originally purchased my home in 2010 on 142nd St E (2 blocks away from the proposed warehouse invasion) I was drawn to the quiet, peaceful, openness of the valley. While Farm 12 has been a welcome addition to the neighborhood, the increased vehicle and pedestrian traffic it has brought has already been felt. With 7 warehouses I can only imagine how the traffic will increase on these tight country roads. Without sidewalks it is always a challenge to walk down 80th. With large trucks, there is no way I would feel comfortable walking with my child to the park. This is not land for warehouses. They do not fit into the neighborhood. Please save the park, mt. rainier view and the neighborhood feel we have. This would not be progress for the city. No amount of money could bring this land back. So please, do not allow the full 7 warehouse park!! It would be a travesty!

Thank you,

Laura Kase Nagai

**Commenter:** Neermann, Juli

**Source:** Webform

**Date:** 3/12/2024

**Comment:**

The only alternatives should be farming, green space, or maintaining the original van leirop park plans. The county should not allow this space to be developed. The impact further into South Hill will definitely be felt. There is already too much traffic in the area from additional housing, let alone additional trucking. Will trucks be prevented from using Shaw? They aren't now, so they probably won't be after development. There are plenty of open spaces in the Fredrickson, Fife, Sumner valley areas. What are the occupancy rates in those areas? Keep it there; the canyon road project is planned for those areas. Do we really need the business/tax dollars that much to allow this? There are so many trees in the County that have been taken down for new housing developments, where are they being planted for replacement growth. How is this the sustainable decision? Allowing this business development is going to contribute to unhealthy environmental issues.

**Commenter:** Neighbors, Mariah

**Source:** Webform

**Date:** 3/11/2024

**Comment:**

This is a very disappointing idea. It would gravely impact our beautiful community. Families choose to live here, in part, because of the access to great parks and outdoor activities (such as trails and the riverwalk). Another reason families choose to live here is because the traffic patterns allow for relatively easy access to freeways. If these warehouses were to be built all of this would change, causes a huge reduction in the quality of life for current, and potential, residents. We want to encourage the "family lifestyle" Puyallup provides, not discourage it. We want to keep our town a DESIREABLE place to live, not turn it into Fife.

**Commenter:** Neshyba Nara, Veronika

**Source:** Webform

**Date:** 1/7/2024 12:07:54 AM

**Comment:**

As a new resident to the area, I'm extremely disappointed to learn about this project. I enjoy Puyallup's clean air and farm valley vibe, and I am constantly thinking about ways to improve my community for the sake of future generations. Giant warehouses do not improve anything for future generations aside from a dubious boost in the economy. The river valley is an important ecological feature of our landscape, and should be rehabilitated as much as possible to improve air quality and maintain our mild local climate. We're dreaming of parks, walkable cities, gardens, beautiful architecture, co-op grocery stores, museums, art stores, coffee shops - not huge complexes with thousands of idling cars, leeching toxins into the river and filling our air with fumes. We deserve to picture a better Puyallup.

**Commenter:** Neshyba Nara, Veronika

**Source:** Webform

**Date:** 1/7/2024 12:20:33 AM

**Comment:**

As a new resident to the area, I'm extremely disappointed to learn about this project. I enjoy Puyallup's clean air and farm valley vibe, and I am constantly thinking about ways to improve my community for the sake of future generations. Giant warehouses do not improve anything for future generations aside from a dubious boost in the economy. The river valley is an important ecological feature of our landscape, and should be rehabilitated as much as possible to improve air quality and maintain our mild local climate. We're dreaming of parks, walkable cities, gardens, beautiful architecture, co-op grocery stores, museums, art stores, coffee shops - not huge complexes with thousands of idling cars, leeching toxins into the river and filling our air with fumes. We deserve to picture a better Puyallup.

**Commenter:** Neville, Nick

**Source:** Email

**Date:** 3/6/2024

**Comment:**

Hello,

We live near the intersection of Shaw and Pioneer. We are concerned about the proposed development of the warehouse. We believe the reduced intensity alternative will assure both economic growth while preserving the culture of our area.

Thank you,

Nick Neville

**Commenter:** Newell, RaeDeen

**Source:** Webform

**Date:** 1/23/2024 12:41:49 PM

**Comment:**

I currently live in a complex in Sumner. When we first moved in during 2019, a new complex was starting to be built on the other side of our fence. As construction continued, the rain water needed somewhere to go, which meant into the complex's yard, making it unusable from October to May every single year. One can't assume the displaced water that normally goes into these fields every season from rain will flow into the rivers. In flooding, that water has to go somewhere and we will find that it starts to destroy the property of those around these warehouses. Also, there are more than enough empty warehouses between Puyallup and Sumner that Knutson can use without putting up new ones.

**Commenter:** Nguyen, Theda

**Source:** Webform

**Date:** 1/12/2024 4:16:01 PM

**Comment:**

I don't believe warehouses should be built in the area off of Shaw Rd. What a beautiful space that is already at its max when Farm 12 and Step by Step host their amazing events. Having warehouses around would take away everything from this treasured place. The traffic would also get worse once the daycare and school is built - how much better to make the space for our growing community of people and children rather than industry.

**Commenter:** Nickey, Karen

**Source:** Email

**Date:** 3/11/2024

**Comment:**

Hello All:

First, I have read the captioned proposal. I live about two miles away from the proposed warehouses off of Shaw and 15th so I know a little something about the area. Following are some critical points for not proceeding with this project as is.

1) This will just make an existing bad problem worse. That is, Shaw Road is already heavily congested Monday through Friday from during rush hour traffic in the morning and for a very long time in the afternoon from about 2:00pm to 6:00pm. East Main has the same traffic congestion. Adding more cars to this area, which will happen with this project, would be devastating to traffic. Furthermore, I'm sure some of you have noticed that Hwy 167 north bound each week day morning is consistently noted as red on the traffic map...meaning barely moving. Why make that situation worse.

2) Killing of the salmon in the adjacent river, due to water run-off of toxins, is not acceptable and negatively impacts future salmon population growth. Is this your goal?

3) What might help make this acceptable to the public ?

a) Reduce the number of warehouses to three for the long term.

b) Have the builders create a walking trail or improved walking trail along the river and or rail road tracks. Build an accessible play area park (with bathrooms) close the Farm 12 restaurant and flower park area. In other words, make it more pedestraian friendly.

3) Require a good amount of horticulture and trees around the buildings so it won't look as ugly as the present warehouses that are along Shaw.

There are more reasons not to build as is proposed but I'm sure others are contacting you as well on those matters. These matters concern me most of all and should you as well. Remember, you represent the people of Puyallup and should not vote based on your personal beliefs.

Thank you for your consideration.

Karen Nickey

**Commenter:** Nilsby, Juanita

**Source:** Email

**Date:** 2/29/2024

**Comment:**

My preference is that no additional warehouses are added to the Shaw Road corridor. Since that's unlikely, I fully support the Reduced Intensity Alternative presented by Citizen Group Protect Puyallup. "reduced intensity" alternative: This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Juanita Nilsby

**Commenter:** O'Brien, Tom

**Source:** Email

**Date:** 3/3/2024

**Comment:**

I'm commenting on the DEIS to the city of Puyallup. Currently, I don't see much traffic using the existing warehouse on Shaw Road. With all the obvious environmental repercussions from such a proposed huge project, my opinion is the "reduced intensity" option. There doesn't appear to be a reason for such a large, negatively impactful project now or in the near future. I realize there are also some benefits from such structures like jobs and tax revenues, but to me the choice of

"reduced intensity" seems to be a reasonable compromise.

Tom & Barb O'Brien

**Commenter:** Otsuka, Edward

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Yeah. My name is Edward. Last name

is Otsuka, O-T-S-U-K-A. And I'm new here to Puyallup. I remember about 30-35 years ago the beautiful land, the farmland, the flowers. You guys call them, what, daffodils? But it was a very, very beautiful city. I see what they did down in Seattle. I see what they've done in Bellevue, Redmond. It may be a nice city but I call it the franchise city. But I've seen the destruction of the warehouses. Just go ahead and take a look at Fife with all those warehouses and all the traffic, all the pollution. Just take a good look in real-time. Tukwila, Southcenter. That place is one big, paved garbage heap. And that's what you're going to turn this city into a garbage heap of warehouses. Just take a look at Sumner. That's the newest development. It is not an attractive city to even think about coming to. And I'm sure all the beautiful things that we used to see with the flowers, the farm, the mountain. Now we're going to see this ugly, stinking view of a bunch of warehouses that has no value, takes away all of your natural resources, limited natural resources. It's going to increase the amount of pollution -- air pollution, noise pollution, water pollution, and environmental eye- sore pollution. If you, this city, do not fight against this and put a stop to it, you are going to turn your city into a garbage heap. I guarantee it. I've seen it plenty of times in other cities. And the whole purpose of moving to Puyallup, the beauty, all the natural resources are going to be destroyed. You start with that one warehouse right there by Farm 12 which is a stinking eyesore. All right? That one should be torn down. Just like that woman spoke. I bet you it was illegally -- somewhere, somehow the paperwork was messed with so that they could legally put that warehouse there. And I bet you if they find the truth about it that thing should be torn down and that place back and restored to its natural habitat. So I'm just saying that it's really time to stop playing this game of politics and put an end to any idea of destroying the last resources that you guys have that makes this city so great. Thank you for your time.

**Commenter:** P, Audrey

**Source:** Email

**Date:** 3/12/2024

**Comment:**

My initial reaction to this proposed project was to say NO to any building, especially concrete pavement, and truck traffic, on or near this parcel. I am very much in favor of protecting farmland and natural resources like open space, native plants, wetlands, wildlife and Puyallup River. The quality of life the people of Puyallup and Pierce County have chosen by living here requires these things to be protected. I also feel the voices of the Native Indians in the area need to be heard and acted on within any decision on this project. This project would have a major impact on the surrounding air, land, wetlands, adjacent river, noise and people. An increase in traffic, especially truck traffic, along a narrow neighborhood road (80th) which is often busy with pedestrians and cyclists and families accessing the View Park, Trailhead and Farm 12, would increase safety risks in the area. As a commuter from Puyallup to/from Buckley for 40 years I know how traffic/ accidents or flooding/Ice can push traffic onto Hwy 162 – then 80th St to Pioneer and Shaw Rd, to avoid the merges of Hwy 167/Hwy 410/Hwy 512. Adding truck traffic on Hwy 162 and 80th St even with a roundabout would restrict further the people that live there as well as businesses and Parks/Trails access.

Given the history of this project and how long it's been 'in the works', I fear that opposition to any development may result in an even worse request by the owner, down the road.

Thus, I vote for Alternative #2 – Reduced Intensity Alternative. I appreciate the city of Puyallup pursuing an EIS and the work put into this EIS and the proposals suggested. I would like to see all mitigations proposed in this alternative to be implemented, with a few added modifications (based on my concerns expressed above).

See Photo/Map pg 26 (Alternative #2)

1. Make access to this industrial park facility from ONLY the Wing Road, entrance and exit, for trucks. Local delivery trucks to FARM 12 would use Pioneer to/from 33rd St. In this case there would be no need to add a traffic circle on Hwy 162 at 80th St.
2. On 80th St. There should be cross walks with flashers for trail user, park users and Farm 12 business. Ideally only one crossing for all to use or no more than 2 spaced for the benefit of users.
3. Delete the construction of warehouse G and F, for these reasons; Protect view of Mtn. Rainier from View Park, protect public from any potential hazards from the Wilson Pipeline (no building or paving over it), provide space for a connection trail(s) between East Puyallup Foothills Trail Head and trail extension along the river and connect to Puyallup River Walk plus to View Park, Protect Wetland D (as noted in Appendix C) and its buffer area with low growing native plants to support wetland eco system with native species. By building a winding trail/path around/between wetland D and pipeline, or a raised bridge path across a small section of it to form trail connections, then this section would provide an open space for all to enjoy and protect the environment.

4. Reduce the size of warehouses E and A, to fully protect the Puyallup River Flood zone and Riparian buffer, protect and naturalize the wetlands A, B, C and their associated buffers, and to build the Foothills Trail connection to Puyallup River Walk Trail at the highest possible location from the above natural areas. This would help prevent future damage, trail closures, and major maintenance costs. The trail should not be near any warehouse space for the best outdoor recreation experience for the public.
5. Plant native plants in all wetlands/buffers to help with any soil erosion and flooding damage. Ensure the Puyallup Riverbank is stable with native plants and not prone to erosion especially at the outflow area.

Thanks for the opportunity to submit comments for this project,

Audrey Pitigliano

[REDACTED]

[REDACTED]

**Commenter:** Pacheco, Cheryl

**Source:** Email

**Date:** 1/29/2024

**Comment:**

To whom it may concern. What well be in these warehouses? Will it cause massive problems? Yes, those of us who choose to grow organically do not want those in. We are a area of produce in this area. Why do you all feel a need to intrude on our lands that we work so hard on? I bet you not to put those stupid warehouses in.

If you were building homes for the homeless, I could live with that. But that's not your intentions anyway. All about the greed that is what's killing us!

Thank you for taking the time to be concerned.

Cheryl P.

**Commenter:** Paine, Lesley Blair

**Source:** Email

**Date:** 2/2/2024

**Comment:**

I support the no action alternative, but in my experience, some action will be taken. In that case, I support Alternative 2, with the hope that a safe connection of the trail will be developed with environmentally positive materials. I would like to comment on the section headed "Effects to Aesthetics". Included in this plan appears to be a 12' sound wall. The reason I am opposed to the entire development is that the aesthetics from the East Main Avenue side has already been deteriorating. The view we had until a couple of years ago of Mount Rainier and the foothills was truly unique in all the world. I have travelled extensively, and we moved from Louisiana to Sumner, in large part, because of that view. Soon after, the first warehouse arrived, but I continued to take time at both the new Van Lierop Park, photographing the lupines and the sparrows, and on Inter Avenue, photographing or sitting quietly. Now I photoshop or crop out one warehouse. If a 12' fence and many warehouses go up, the view will be gone forever. People talk about the need for mental health, and we complain that we don't know what to do about it. As a pastor, I know that part of the answer is in connection to nature, getting outside, breathing fresh air. This is how I literally ground myself. Others do the same, out on the trails, sharing photos and experiences. Please find some way to protect it. Attached are my photos of this unique area. [photos attached]

**Commenter:** Palmer, Connie

**Source:** Webform

**Date:** 1/23/2024 4:01:05 PM

**Comment:**

Before any construction should be allowed, the roads MOST DEFINITELY need to be improved. Not after...BEFORE. I grew up on 80th, Van Lierop bulb farm was my back yard, parents still live there. The roads & congestion are extremely dangerous. Lights need to be installed before any construction should be allowed.

**Commenter:** Palmer, John

**Source:** Email

**Date:** 1/5/2024 12:47:00 PM

**Comment:**

Hi Steve,

In my first citizen comment email, I have a couple of suggestions for you and staff's consideration regarding the Knutson Farm Project Website and City FB posts.

First, the below image is on the website. I think the "proposed open space" is misleading. The area in green is almost entirely floodplain that is not developable. I suggest the label be revised to say "floodplain area/proposed open space"

Second, in the FB posts, more recently the posts say the DEIS assesses the impacts. But, no mention of mitigation. I suggest the post also indicate that the DEIS includes proposed mitigation measures to address the impacts. And the City is interested in your comments on the impacts and mitigation.

Thanks,

John Palmer

**Commenter:** Palmer, John

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear City of Puyallup,

Please find attached my comments regarding the Preferred Alternative.

John Palmer

████████████████████

████████████████████

**Commenter:** Palmer, John

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear City of Puyallup,

Please find attached my comments regarding Land Use and Farmland.

John Palmer

[REDACTED]

[REDACTED]

**Commenter:** Palmer, John

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear City of Puyallup,

Please find attached my comments regarding Transportation.

John Palmer

[Redacted]

[Redacted]

**Commenter:** Palmer, John

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Dear City of Puyallup,

Please find attached my comments. I have five separate comment letters:

- Preferred Alternative
- Land Use and Farmland (with Ladenburg Letter Attachment)
- Transportation
- Surface and Ground Water
- Trail

John Palmer

[REDACTED]

[REDACTED]

[REDACTED]

**Commenter:** Palmer, John

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Please include in the DEIS comment record the public comments at the March 12, 2024, City Council meeting regarding the Draft EIS.

**Commenter:** Palmer, John

**Source:** Webform

**Date:** 3/15/2024

**Comment:**

The Preferred Alternative and associated mitigation measures in the FEIS must be clearly defined and specific and not vague to avoid the potential for misapplication when the Pierce County and the City of Puyallup proceed with permit and application actions (i.e., approval/disapprovals) to implement the FEIS and Preferred Alternative. Further, the FEIS must state that implementation of FEIS and requirement of mitigation measures occurs when the County and the City take actions on the proposal as outlined in SEPA. The FEIS must set the expectation that County and City permit/application actions will implement the FEIS in order to avoid or mitigate the significant impacts identified in the FEIS. The FEIS should not include statements that the Preferred Alternative is only advisory in nature to unnecessarily create potential ambiguity in implementation. When addressing SEPA substantive authority to implement the FEIS, the FEIS should stick to objective language as summarized in Ecology's SEPA Handbook. See below. Substantive authority is an essential part of SEPA. It allows decision-makers to use the environmental analysis required under SEPA to condition or deny proposals. One of the most important aspects of the SEPA process is the consideration of environmental impacts and possible mitigation measures during agency decision-making. SEPA substantive authority gives all levels of government the ability to condition or deny a proposal based on environmental impacts. Mitigation must be included as permit conditions to be enforceable. The exception is when a proponent alters the permit application(s) to include the needed changes or conditions. Identification of mitigation in a DNS or EIS alone is not sufficient to allow enforcement. Before requiring mitigation measures under SEPA substantive authority, agencies are to first consider whether local, state, or federal requirements and enforcement would mitigate the identified significant adverse impacts. Decision-makers should judge whether possible mitigation measures are likely to protect or enhance environmental quality. Mitigation measures must be related to a specific adverse impact clearly identified in an environmental document on the proposal, and must be reasonable and capable of being accomplished. When using SEPA substantive authority, the decision-maker must: 1. Cite the agency SEPA policy that is the basis for conditioning or denying the proposal; 2. Document the decision in writing; and 3. Make available to the public a document that states the decision, and any mitigation measures will be required. This document may be the permit, license, or approval; or it may be combined with other agency documents; or 4. the decision document may reference relevant portions of environmental documents. To deny a proposal under SEPA, an agency must find that: 1. The proposal would be likely to result in a significant adverse environmental impact identified in a final EIS or final supplemental EIS; and 2. Reasonable mitigation measures are not sufficient to mitigate the identified impact to a non-significant level. SEPA supplements the existing authority of all agencies. To exercise SEPA substantive authority each agency must adopt SEPA policies that will be the basis for conditioning or denying proposals. These policies must be readily available to the public for the benefit of applicants and concerned citizens. (See adoption procedures in WAC 197-11-902.

**Commenter:** Palmer, John

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

So the first thing I want to say is thank you City of Puyallup staff for putting this EIS together, this draft EIS on behalf of the citizens of Puyallup. It is really important. And the reason why this is happening is because the developer completely ignored our land use plan that we adopted in 2009. And that plan called for more of a balanced development and a fair amount of farmland protection. That went through a lot of process with the landowners. With the landowners' agreement. So what's happening here is the land -- the

developers are trying to basically do an end run around our plan. And that has caused so much friction in this community. This community does not want this project if you've understood that. That is just clear. So now we're trying to deal with it. So doing the EIS is the tool now to mitigate this project. So, thank you, thank you, thank you for putting this together. It's vital and the public input is vital. The other big picture I want to say is that this area is not intended for -- it's not appropriate for a warehouse industrial complex. It's just not. It's not in any plan. You couldn't find it in any plan. Unlike Sumner,

which is part of Puget Sound planning. I mean, that is an industrial area. Frederickson. Fife even. And even North Puyallup has zoning for warehouses. And it has infrastructure to support it -- good roads, et cetera. This is like not the case here. We have one road, Shaw Road, which is a heavily traveled commuter access road that serves Pierce County. And as it's been said tonight, it's very busy. And it's going to get way busier. And also, it's historic farmland. I

mean, generations have farmed -- have worked on this land. It's a fabric of Puyallup. And so you've got those qualities and then you have this proposal. And it just is completely incompatible. So that's -- that's just a big problem.

The third thing I want to say is we, in this room, paid for Shaw Road. Shaw Road was roughly \$20 million. But the citizens of Puyallup paid about \$7 million of that. So it's kind of astonishing that the developer would ignore our land use plan and expect to use the road that these citizens paid for.

So we do have a right, and it's through this EIS that we mitigate this project, in part because it's our road and they're getting -- we are, the City of Puyallup is providing the permit to access Shaw Road. So very much the City of Puyallup has a big role to pay here in addition to the EIS. So thank you. Thank you for this. In terms of the alternatives presented, I think the, you know, proposed alternative -- or not the proposed - the proposal, I mean, is a nonstarter for a lot of the reasons I just went over. But I think the EIS itself does go into all the variety of impacts associated with the proposal. So it's a nonstarter. The rail, good attempt to look at something else to ease traffic but at this time it's a nonstarter, too. It

doesn't really easy much traffic, and frankly, those rail lines would be pretty obtrusive to that area. So we're basically left with Alternative 2, which isn't really -- it's a pretty good start. It reduces by 35 percent the warehouses and traffic, and it protects the Van Lierop Park and provides some farmland protection. So I applaud at least the start of Alternative 2 or the reduced intensity.

By the way, the slide says Alternative 4 but the EIS itself calls it Alternative 2. I just wanted to make that clarification.

However, it is still pretty intensive. Even the reduced intensity alternative is still pretty intense. Six thousand vehicles per day on to Shaw Road. I'd have to take a much closer look at the modeling on that but it almost

doesn't kind of pass the laugh test in terms of how much cars and trucks. And thousands of those are trucks on Shaw Road. So that seems still way too high. It seems like we need more reduction there. So more comments to come on that. The other comment I have has to do with the location of the farmland. First of all, it's absolutely

critical that there's farmland preservation set-aside as mitigation for this project. Critical. Given the background on this, it just has an important component to mitigate for the warehouses that do go in.

The way it is now, it's based on the -- it's in the north area of the portion of this area based on our land use map which we built in 2009, which I was a part of on the Planning Commission. But exactly where that farm is

protected, farmland is protected isn't critical. So it is possible to move that around. And it may be better to focus on the farmland protection over the southern part of the

area where warehouses D and F I think, the lower two ones, and focus the warehouses more on the northern part and protect more of the southern part.

You have Farm 12, Step by Step, Van Lierop Park be more compatible with that area on the southern end. So I think that's something to look at. So you can't -- there could be a variation of Alternative 2 in terms of where

that's protected. But it's absolutely essential that we

have farmland protection. So good work so far. I do think we need another

public meeting. Probably the beginning of March or something like that. Maybe you don't have to go through the whole presentation but I do think that people -- this was put out over the holidays.

People are just starting to look at it. But it's that big and so it's going to take a while to digest it. So I think another public hearing would be

good. So thank you.

**Commenter:** Palmer, John

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Traffic concerns, Warehouse development concerns, Stormwater and wetland concerns

March 14, 2024

To: City of Puyallup

From: John Palmer

RE: Comments on Knutson Farm Inc. DEIS – **Farmland Protection**

**It is vital that Preferred Alternative in the Final EIS include at least approximately 45 acres of protected farmland outside of the floodplain.** There are many reasons that justify this. The DEIS touches on some of these reasons and includes Alternative 2, which protects about 45 acres of non-floodplain farmland consistent the City of Puyallup’s Comprehensive Plan for this area. It is not necessary that the protected farmland be exactly where it is identified in Puyallup’s Comp Plan. The farmland should be protected where it is most beneficial (See Palmer comments on Preferred Alternative). The farmland should be protected with a covenant as mitigation for any warehouses. The multiple reasons for this farmland protection are summarized below along with my recommended additions for the Final EIS.

- Loss of Prime Agricultural Land is a Significant Impact. The discussion of farmland loss is included in the Land Use Section of the DEIS, but is not discussed as a stand-alone significant impact, which it is. The proposal includes the destruction of 126 acres of prime farmland. This is significant. It is one of most significant impacts of the proposal.

*I recommend* the loss of 126 acres of prime farmland be identified and discussed as a significant impact in the FEIS. Discuss the quality of the ARL soils in the project area. Discuss the limited amount of ARL soils in the Puyallup Valley. Discuss the loss of Puyallup Valley farmland over the past 100 years due to urbanization. The Earth Resources Section maybe a good place for this expanded discussion. Surprisingly, loss of farmland is barely mentioned in this section. Lastly, by clearly labeling the loss of farmland as a significant impact, the preservation of farmland in Alternative 2 (and the Preferred Alternative) can be represented as mitigation for the loss of farmland.

- History to Protect Farmland in the Project Area to meet GMA Requirements. As described in the DEIS on pages 4-220-21, as part of Pierce County’s process to comply with the State’s Growth Management Act (GMA), 365 acres in this area was proposed by the County to be protected as agricultural resource land (ARL). This resulted in a compromise to protect 160 acres of farmland in this area as reflected City Resolution 1903. Subsequently, the city adopted its Comp Plan amendment in 2009 consistent with Res. 1903, which includes about 45 acres Ag/Open space of non-floodplain land where the proposed project is located. The DEIS Alternative 2 includes farmland protection for these 45 acres, which overlaps with several of the proposed warehouses. This background discussion in the EIS is important to include. It demonstrates multiple public processes and intention to protect farmland in the proposed project area to comply with the GMA.

I *recommend* the FEIS also reference County Executive John Ladenburg's December 12, 2008 letter to City urging the adherence to Res. 1903 prior to the City's 2009 Comp. Plan amendments demonstrating the County's ongoing commitment to protect farmland in this area and comply with the GMA (attached).

I *recommend* mentioning in the FEIS that the City's 2009 Comp Plan map was agreed to by the Planning Commission and the land owners/representatives, including the representative that owned the project area at the time, on December 18, 2008, after months of negotiation. That map was subsequently adopted by the Puyallup Council in 2009.

I *recommend* the FEIS show the ultimate farmland that was zoned as ARL in the Puyallup Valley as part the County's 2004 amendment process, which I believe is approximately 3,000 acres. The 365 acres subject to Res. 1903 represented about 10% of the area intended to be protected as ARL in 2004 (a significant piece). A map of the ARL areas in the Puyallup Valley shows that land south of 167 contains large areas of ARL, while lands north of 167 area have warehouse development. The proposed project is south of 167.

Including the above history and information in the FEIS provides important regional context for the significance of protecting farmland in the project area and the agreements made with the County, City, and landowners to do so.

- Consistent with City and County Land Use Plans and Policies. As summarized in the DEIS Chapter 4.5 and Table 4-22, the proposed action is dramatically inconsistent with many Pierce County and City of Puyallup land use polices (inconsistent with 65 policies). With respect to farmland protection, the proposed action is inconsistent with the City of Puyallup's Comp Plan and pre-annexation zoning, which designates approximately 45 acres of Agriculture/Open space where warehouses/parking/stormwater facilities are proposed. Although Pierce County's EC zoning for the proposed project area allows warehouses there are important County polices noted in Table 4-22 that call for full consideration of a city's land use plans in future annexations areas, protection of the rural character and agricultural identity of this area, and master planning that protects open space. These policies, some of which are highlighted below, strongly support the protection of at least 45 acres of farmland as part of this project.

Goal LU-4: Facilitate the transformation of unincorporated urban areas into cities and towns through annexation.

LU-4.2.6: Ensure that the County's land use designations and associated development regulations are consistent with a city or town's land use plans within its respective Potential Annexation Areas

GOAL AM LU-1 Ensure the Alderton-McMillin community remains rural in character over the next 20 years.

GOAL AM D-1. Promote commercial and industrial development that is visually attractive, and compatible with the residential character and agricultural identity of the community while being respectful to the natural environment.

Policy LU-47.9. Encourage master planning for industrial areas, including such features as open space, landscaping, integrated signage, traffic control, and overall management and maintenance through covenants or other property management techniques

It is also important to note that warehouse industrial complexes of the size of the proposal are not targeted in this location in any Pierce County, regional, or city plans. Warehouse complexes of this size are targeted in areas such as Sumner, Fredrickson, and Fife. I *recommended* adding this fact to the EIS.

In summary, although warehouses are allowed under the County's EC zone, it does not mean 7 huge warehouses are allowed. The preferred alternative in the Final EIS must reduce the number of warehouses to be consistent with the many policies that supplement the underlying zoning and must protect at least 45 acres of farmland outside the floodplain consistent Puyallup's plan and the State GMA.

- Compliance with the Pierce County Code for Project Impervious Cover. As noted in the DEIS, the impervious limit is 60% per PCC 18E.50.040 and Table 18E.040(A). The project proposed impervious surfaces of warehouses and parking for 108 acres out of the total of 126 acres of developable land, which is 86% impervious. This far exceeding the 60% limit. The floodplain should not count toward the impervious limitation. To meet the 60% impervious limitation, the amount of warehouses/parking would be limited to no more than 75 acres with 50 acres pervious. This aligns with protecting at least 45 acres of farmland.
- High Public Value for Protecting Farmland in this Area. The citizens of Puyallup and the surrounding area place an extremely high value on protecting farmland, especially in this location. The scoping comments are included in the DEIS with over 300 citizen comments opposed to this project, primarily to protect farmland and avoid further traffic congestion. The City of Puyallup recently completed an Environmental and Sustainability Action Plan (ESAP) and preserving agricultural lands was identified as the #1 priority issue in the community survey. A citizen change.org petition to stop or reduce the number of warehouses and protect farmland is signed by over 5,500 people. Many citizens in the area grew up working picking berries on the Van Lierop and Knutson farms and hold strong connection to this agricultural land. The daffodils historically grown on this land are a hallmark of the City of Puyallup and are included on the city seal. The Van Lierop Park and Farm 12 are enormously popular with citizens in the area, in large part to the connection to the open space, rural character, and farm land in the immediate vicinity.

The DEIS addresses the noise and visual impacts that the proposal would have on the VLP. However, what is missing is the significant impact that the loss of farmland would have on the people of Puyallup and the surrounding area as noted above. This is a very tangible significant impact. Economist can measure this value.

I *recommend* the broader significant impact on the loss of farmland from this proposal be included and summarized in the FEIS. And that the public intensity for farmland preservation be discussed, with specific reference to the scoping comments, comments on the DEIS, City Facebook post comments, ESAP survey, the citizens petition, and an economic

assessment of the public value. Perhaps this is best included in the Cultural Resources section of EIS. Importantly, this must be included somewhere. SEPA RCW [43.21C.030](#) states:

(b) Identify and develop methods and procedures, in consultation with the department of ecology and the ecological commission, which will **insure that presently unquantified environmental amenities and values will be given appropriate consideration in decision** making along with economic and technical considerations;

Currently, the DEIS falls short in characterizing the huge importance and value of protecting farmland in this area. Although, it may be hard to quantify, it must be included in the FEIS as it represents the one of the most significant impacts of the proposal.

- Mitigates the Massive Scale of the Proposal. The DEIS addresses aesthetics and primarily addresses visual and noise impacts to Van Lierop Park. However, the DEIS fails to address the macro scale impact of the massiveness of the proposal. I *recommend* the sheer size of 7 warehouses in this unique location be identified as a significant impact in the aesthetics section. The impact will be felt by the many citizens visiting the Farm 12, VLP, Foothills Trail area. The massive scale will completely change the character and feel of this rural area. Thus, aesthetic impacts are not just the view of Mt. Rainier. This is especially important to address given the current rural setting and the Alderton-McMillin Plan's goals to maintain this character. Protecting at least 45 acres of farmland serves to mitigate the massive scale of the proposal and helps to retain some rural character, especially if the preserved farmland is adjacent to the VLP, Farm 12, and the Foothills Trailhead.
- Mitigates the Proposal's Significant Impact from Stormwater Runoff and to Wetlands. As summarized in the DEIS, the proposal would cause significant impacts to fish from stormwater runoff and to adjacent wetlands. Reducing the warehouse footprint and preserving at least 45 acres of farmland serves to mitigate these significant impacts. Farmland protected adjacent to VLP (where warehouses E, F, and G are proposed) serve to protect and provide water to Wetlands A, B, C, and D. Preserved farmland reduces the impervious surfaces and associated stormwater runoff from the warehouse and can provide stormwater dispersion from the reduced number of warehouses.
- Mitigation for the Proposal's Significant Traffic Impacts. As summarized in the DEIS under Alternative 2, protected farmland of a least 45 acres serves to mitigate traffic impacts by reducing the number of warehouse and vehicle trips.
- Mitigation for the Proposal's Significant Impact to Van Lierop Park and Farm 12. As noted above regarding mitigation for the massive scale of the proposal, preserved farmland of at least 45 acres adjacent to VLP serves to mitigate the significant visual and noise impacts to VLP identified in the DEIS. Preserving farmland where warehouses E, F, and G are proposed would mitigate impacts to VLP (and Farm 12 and Foothills Trailhead) far better than creating a buffer wall around VLP (See Palmer Comment – Preferred Alternative).

March 14, 2024

To: City of Puyallup

From: John Palmer

RE: Comments on Knutson Farm Inc. DEIS – **Preferred Alternative (Reduced Intensity)**

Thank you, City of Puyallup, for preparing this DEIS. The comments herein focus on the recommended Preferred Alternative to be included in the Final EIS.

As described throughout the DEIS, the Proposed Action would cause significant impacts, including impacts to traffic, farmland, wetlands, ESA listed species in the Puyallup River, aesthetics, and compatibility with the community and is inconsistent with multiple County and City land use policies. Further, as shown in the DEIS, it's not possible to mitigate the significant impacts with 2.6M Sq. Ft. of warehouses. The proposal is simply too large for this location. Further, the Rail Alternative, with the intent to mitigate traffic impacts, fails to do so. Thus, the only viable alternative of those presented in the DEIS is the Alternative 2 – Reduced Density

Alternative 2 (Reduced Intensity) is a good start and attempt to mitigate the array of negative impacts associated with the proposal. However, the **Alternative 2 Variation** shown in Figure 1 would more effectively mitigate the negative impacts and would be more financially beneficial to the developer compared to Alternative 2 in the DEIS.



DEIS ALTERNATIVE 2 - "Reduced Intensity" - Variation  
1.7 Million sq ft of Warehouses

Figure 1 – Alternative 2 Variation

The **Alternative 2 Variation** shown in Figure 1 has the same amount of warehouse square footage (1.7M sq. ft.) as Alternative 2 and roughly the same amount of protected farmland/open space in the upper terrace (approx. 45 acres). The main difference is that the warehouses are focused on the northern part of the project area and the protected farmland is focused on the southeastern part of the project area.

As the DEIS explains, Alternative 2 reduces the scale of the warehouse from the proposed action from 2.6M sq. ft. to 1.7M sq. ft. This reduction serves to: 1) protect farmland consistent the City of Puyallup Comprehensive Plan and mitigate the massive scale of the project, 2) reduce the visual and noise impacts at Van Lierop Park and Farm 12, 3) reduce impacts to wetlands, 4) reduce stormwater impacts to the Puyallup River, and 5) reduce traffic impacts. The **Alternative 2 Variation** mitigates these impacts in a better way as summarize below:

- From a land use perspective, **Alternative 2 Variation** establishes an accessible, contiguous, connected, and consolidated land area for continued farming that is connected to the flood plain area (also used for farming). This helps support the long-term viability of farming in this area and is much better than the segmented and narrow band of farmland protected in Alternative 2.
- Although the Puyallup Comprehensive Plan calls for approximately 45 acres of farmland/open space protection in the “high terrace” part of the project area as reflected in Alternative 2 of the DEIS, the exact location of protected farmland is not critical. What’s critical is that a large area of farmland (45+ acres) in the “high terrace” area above the floodplain of the project be protected as mitigation for the project to 1) minimize the massive scale of the project, 2) meet the intent of Puyallup’s Comp Plan, 3) meet the 60% impervious limit in Pierce County’s code, and 4) meet Pierce County and the City’s intent to protect farmland in this area as reflected in City of Puyallup Resolution 1903.
- **Alternative 2 Variation** reduces the significant impacts to Wetlands A, B, C, and D much better than Alternative 2. By preserving farmland in the “high terrace” area adjacent to these wetlands, they will all continue to have ground water re-charge. As discussed in the DEIS, Alternative 2 provides some protection for Wetland D, but does not necessarily provide re-charge to Wetlands A, B, and C.
- **Alternative 2 Variation** reduces the significant noise and visual impacts to Van Lierop Park and Farm 12 much better than Alternative 2. Alternative 2 protects the “Mt. Rainier view” and provides a buffer wall surrounding the park. These mitigations are better than nothing, but having a warehouse(s) in the location of proposed warehouses F and G still significantly impacts the ambiance and rural character of VLP, Farm 12, and the Foothills Trailhead. **Alternative 2 Variation** fully protects the Mt. Rainier view, eliminates the need for a barrier wall on the east boundary, and protects the open space feel and ambiance of the area, which is enjoyed by many people as indicated by the popular use of Farm 12, VLP, and the Foothills Trailhead.
- **Alternative 2 Variation** concentrates the warehouses rather than distribute them across the project area as in Alternative 2. This serves to minimize the significant impacts to adjacent uses by reducing the amount of exterior facing walls and focusing traffic inside

the warehouse complex. Focusing the warehouses in the northern project area near the railroad line is also consistent with the City of Puyallup's Comp Plan for light manufacturing (ML land use) in the northern part of the project area.

- **Alternative 2 Variation** may reduce the need for traffic mitigation compared to Alternative 2. By eliminating traffic access to the Warehouse F and G area, the traffic use on 80<sup>th</sup> Street E may be reduced. This could obviate the need for a roundabout on Highway 162. Further, road and frontage improvements along parcels where warehouse F and G are proposed would no longer be needed. Removing these mitigations would be a cost savings for the developer for the same amount of built warehouses (1.7M sq. ft.).
- **Alternative 2 Variation** is also likely to be preferred by the developer compared to Alternative 2 due to less mitigation costs as noted above and more compact area of warehouses.
- **Alternative 2 Variation** is more consistent the policies in Pierce County's Alderton-McMillin land use plan due to the reasons summarized above. Specifically:
  - GOAL AM LU-1 Ensure the Alderton-McMillin community remains rural in character over the next 20 years.
  - GOAL AM D-1. Promote commercial and industrial development that is visually attractive, and compatible with the residential character and agricultural identity of the community while being respectful to the natural environment.

The above points summarize why **Alternative 2 Variation** as shown in Figure 1 is better than Alternative 2 presented in the DEIS if the city was selecting between these two for the Preferred Alternative.

However, **Alternative 2 Variation** still includes 1.7M sq. ft. of warehouses and associated traffic and stormwater impacts as summarizes in the DEIS. I provide separate comments on traffic and stormwater impacts and mitigation (See Palmer comments on Traffic and Stormwater). For instance, the traffic analysis shows that Shaw Road and East Main St. are very busy traffic corridors that fail v/c at numerous locations under the No Action Alternative and are further congested due to the traffic associated with 1.7M sq. ft. of warehouses. And the developer has proven it cannot responsibly manage stormwater runoff from one warehouse let alone 4-7 new warehouses. Further, Puyallup citizens and the public in generally oppose this proposal as reflected in the EIS scoping comments and the Change.org petition signed by over 5,500 citizens. Puyallup citizens highly value farmland preservation, particularly in this historic location, which is consistent with the Growth Management Act requirements to protect farmland. Considering the above, the project should be further scaled back to three warehouses with a total of 1.0M sq. ft. of warehouses as shown in Figure 2.

Thus, the Preferred Alternative in the Final EIS should be Reduced Warehouse Intensity with a maximum of 1.0M sq. ft. as reflected in Figure 2.

Lastly, the Final EIS must indicate that to appropriately minimize the significant impacts identified in the EIS the developer must re-submit a new site plan to the County and City consistent with the Preferred Alternative. The site plan must integrate the warehouse layout, parking, stormwater facilities (including low impact development methods), farmland protected areas in the upper terrace, landscaping areas, and buffers. Any subsequent permit/application approvals by Pierce County and the City of Puyallup must ensure the re-submitted site plan is consistent with the Preferred Alternative in the FEIS.



**PREFERRED ALTERNATIVE - “Reduced Intensity”**  
1.0 Million sq ft of Warehouses

Figure 2 – Reduced Warehouse Intensity to 1.0M Sq. Ft.

March 14, 2024

To: City of Puyallup

From: John Palmer

RE: Comments on Knutson Farm Inc. DEIS – **Surface and Ground Water**

Thank you, City of Puyallup, for preparing this DEIS. The comments herein focus on the surface and groundwater section of the DEIS.

The DEIS does a good job identifying and characterizing the significant impacts to surface and ground water that would occur as a result of the proposal. The DEIS characterizes the serious problems associated with the stormwater outfall associated with the existing warehouse owned by the developer, which is intended to be used by the proposed 7 new warehouses. The DEIS characterizes the toxic stormwater runoff that would occur due to inadequate treatment to remove 6PPD-q and other pollutants. The DEIS characterizes the destruction of wetlands that would occur due the proposal. These are serious significant impacts that violate federal laws including the Endangered Species Act and the Clean Water Act. The Preferred Alternative and mitigation measures in the Final EIS must avoid these serious impacts.

The following are my comments and recommendations to be included in the Final EIS.

- The Stormwater outfall must obtain a CWA 404 permit that will include Endangered Species Act compliance with the outfall and discharge. The developer clearly attempted to avoid the need for a 404 permit with the existing outfall by designing it above the highwater mark. That approach has failed miserably as described in the DEIS. The bank erosion problems and the feeble developer attempts to put boulders in the river are a clear violation of the CWA and ESA. The fact that this level of impact has occurred with one warehouse calls into serious question the feasibility of using this outfall for 7 more warehouses.

The City must immediately contact the US Army Corp of Engineers and National Marine Fisheries Services (NMFS) on the need for a CWA 404 permit. It is abundantly clear that this outfall needs a CWA 404 permit. The Final EIS must specify the need for the CWA 404 permit and outline the steps the developer must follow to obtain the permit. The 404 permit is a federal action and must undergo ESA Section 7 consultation with NMFS and the USFWS. The ESA consultation will evaluate not only the bank issues but will address the predicted stormwater discharges and impacts of ESA listed salmonids and critical habitat. The ESA consultation will result in Biological Opinions and assurance that the 404 permit and associated proposed project stormwater discharges meet the ESA.

It is critical that the permitting agencies (City and County) have ESA take coverage for this project. As described in the DEIS, the proposed action will result in “take” of ESA salmon species and violate Section 9 of the ESA. Absent ESA authorization of take through ESA Section 7 consultation on the CWA 404 permit, the City and County put themselves at high

legal risk for violation of the ESA when issuing any permits associated with this project. The City should not bear such risk. It is the responsibility the developer to do so.

- The EIS must prescribe the elements of a new stormwater plan. The DEIS mitigation measures call for the submittal of new plans from the developer to manage stormwater and protect wetlands. The Final EIS must provide prescriptive guidelines for such plans. Absent such prescription, the developer will fail to deliver adequate plans as proven by the absence of such plans thus far and the horrible mis-management of the stormwater outfall. The current vagueness of proper stormwater management and wetland protections is a serious problem with the proposal as discussed in the DEIS. The proposed trenches to treat stormwater and recharge wetlands are likely to fail as the stormwater outfall has as noted in the DEIS. The trench proposal lacks any technical rigor. Further, the location of the trenches are not located in areas that will recharge Wetlands A, B, and C. These problems must be corrected by prescriptive mitigations in the final EIS.

The City must immediately reach out and consult with experts on the prescriptions of such a plan, which must include the Washington Stormwater Center, Department of Ecology, Puyallup Tribe, and the NMFS. The science is clear to mitigate for 6PPD-q, stormwater must be treated through soils. The science is clear that enhanced treatment is needed to remove metals. Stormwater regulations also require low impact development (LID) techniques where feasible. The agreement with the Puyallup Tribe also calls for infiltration where feasible and enhanced treatment of what is not infiltrated. This means the project must use of biotreatment and LID methods throughout the site to manage, infiltrate and treat stormwater from all impervious surfaces.

The stormwater prescriptions will include use of pervious surfaces on some locations and extensive use of bioretention and bio infiltration throughout the project area. This will require land. It is not acceptable to assume the warehouses/parking are a given, then claim its infeasible to manage/infiltrate stormwater due to the lack of land. Sufficient land will need to be used for these stormwater facilities. After consulting with the experts, the prescriptive guidelines must specific how much land is needed for stormwater facilities per square foot of impervious surface.

Importantly, the stormwater plan will be essential to avoid “take” of ESA listed salmonid species and avoid violating Section 9 of the ESA.

- The EIS must include a discussion of other stormwater pollutants, including copper, zinc, and PAHs. The DEIS includes a good discussion of 6PPD-q. However, it does not include any discussion on other important pollutants in stormwater runoff, such as copper, zinc, and PAHs. These pollutants will be in the stormwater runoff from this project. They are known to be harmful to salmon species. The Final EIS must include a discussion of the impacts from these pollutants as well. Scientific literature is readily available. (e.g. [An overview of sensory effects on juvenile salmonids exposed to dissolved copper : applying a benchmark concentration approach to evaluate sublethal neurobehavioral toxicity \(noaa.gov\)](#)). For copper and zinc removal, enhanced treatment methods must be used.

- Farmland and greenspace protection are necessary for the protection of wetlands and minimization of stormwater runoff impacts. The best stormwater and wetland management method is preservation of open land. The proposal includes 108 acres of impervious surfaces on the 126 acres of “upper terrace” land above the floodplain (86%). This level impervious surfaces is simply too high making it impossible to adequately avoid significant stormwater and wetland impacts. Protected farmland can play an integral and important role in minimizing the stormwater and wetland impacts. If farmland is protected in the location of proposed warehouses E, F, and G, that serves to maintain the recharge of Wetlands A, B, and C and protect Wetland D. Further, farmland can be used for dispersed runoff from warehouse roofs. See Palmer comments on the Preferred Alternative.
- Without the above measures, the stormwater impacts will undue over a decade of stormwater retrofits in Puget Sound. If the above recommendations are not included in the FEIS, the proposal would result in a high level of toxic pollutant discharge into the Puyallup River. The toxic pollutant runoff from 108 acres of impervious surfaces with inadequate treatment would be very significant. In fact, such a project, would undue the benefits of stormwater retrofit projects installed over the last decade to improve water quality, protect salmon, and restore the Puget Sound. Each year, local governments in the Puget Sound implement projects to treat stormwater from impervious surfaces built in the past with no stormwater controls. Dept. of Ecology issues \$30-50 million per year for such efforts. A project to retrofit 1-5 acres runs into the millions. This includes projects done by the City of Puyallup and projects downstream to clean up Commencement Bay. I recommend the Final EIS summarize the stormwater retrofit efforts in Puget Sound over the past decade to put the proposed project with inadequate stormwater treatment in this context.

March 14, 2024

To: City of Puyallup

From: John Palmer

RE: Comments on Knutson Farm Inc. DEIS – **Transportation**

Thank you, City of Puyallup, for preparing this DEIS. The comments herein focus on the transportation analysis and mitigation in the DEIS.

Traffic impacts are a big community concern for this proposal. Shaw Road has become a major commuter corridor since the Shaw Road Overpass was built in around 2010. I have multiple concerns with the traffic study and transportation section of the DEIS. Below are my comments and recommendations:

- The Shaw Road Overpass cost \$25M, which was financed with federal, state, and local grants along with \$8.4M in City of Puyallup funds. The Knutson Industrial Warehouse proposal is completely dependent on access to Shaw Road for the project to be viable. Without the City's investment in the overpass, including the contribution of significant tax dollars from the citizens of Puyallup, there would no potential for this project. This important context is not provided in the DEIS.
  - *Recommendation:* Include the above facts related to the financing on the Shaw Road Overpass in the EIS. These facts buttress the City's authority and discretion to provide access the Shaw Road and to appropriately limit that access if the project creates negative impacts to the very citizens who paid for the Overpass.
- The traffic volumes for the existing condition are flawed and too low. The traffic counts on Aug 3, 2021 (Intersections 1-27) were during the pandemic when commuter traffic on Shaw Road was significantly lower than we have today. Subsequent traffic counts on June 23, 2022 for additional locations was still influenced by the pandemic due to the slow process of workers going back to on-site work. The DEIS states the traffic counts were scaled up to account for the pandemic, but does not say by how much. The DEIS also states the traffic counts were scaled down due the notion that summer (Aug 3) traffic is higher than other times of the year. This assumption is wrong. Due to vacations, Shaw Road is significantly less travelled in the summer months, particularly in August (personal observation from daily use). The DEIS states the adjusted traffic counts were calibrated to the May 4, 2022 East Town Crossing Traffic Study, which was still influenced by the pandemic, and the adjustment were only made at three intersections – what about the others?

The traffic study must be based on a good baseline of existing conditions. The approach used to characterize existing condition is seriously flawed and is biased low, which has the effect

of underestimating whether or not the added traffic from the project exceeds the traffic performance standards at various intersections and road segments.

- *Recommendation:* New traffic counts must be taken in 2024 to reflect current conditions. The traffic counts need to be taken during the Tuesday – Thursday, which reflect higher traffic due to many people working at home on Mondays and Fridays. The traffic count must be taken on multiple days to ensure the day used to represent the existing traffic is not biased low. The traffic counts must also be taken on a normal school day at Shaw Road school, not an early release or off day, as traffic queuing on Shaw Road is affected by traffic in and out the school.
- Because the traffic counts and the traffic model are biased low, the estimated travel time for the Traffic Ave/Slate Street to Shaw Road/39<sup>th</sup> Ave segment is too low (Figure 7 in Traffic Report - Appendix E). This southbound PM commute is the worst congested segment in the area under current conditions. As discussed above, the traffic counts for the DEIS traffic study are biased low and don't reflect current conditions. Further, the model is biased low as reflected in the Table 9 Travel Time Calibration of Appendix E (observed 9.3 minutes versus modelled 7.3 minutes for this most important segment). Fortunately, travel time for this segment is one performance measure that can be easily measured by the public. I timed this segment three times during the peak PM period (15.1, 13.2, and 17.3 minutes). These results are double the time that is used in traffic study to represent existing conditions. Thus, the actual existing condition is significantly worse than that used in the DEIS traffic study to represent existing conditions indicating that all the performance metrics (travel time, v/c, queue length, LOS) for both the existing conditions and all the scenarios are significantly biased low and do not reflect the actual and future traffic impacts.
  - *Recommendation:* Obtain new traffic counts in 2024 as noted above and re-do traffic analysis.
- The Shaw Road segment from E. Pioneer to 5<sup>th</sup> Avenue SE is very congested, near LOS F under existing conditions. However, due to the flawed biased low traffic counts, the congestion in this segment somehow is not captured in the metrics. This is the most congested segment during PM commute. The queue length commonly extends 1,000 feet to 5<sup>th</sup> Ave. SE and can often extend the nearly 2,000 feet to E. Main during the PM commute under existing conditions. It's not even listed as surpassing 1,000 ft. And why is this segment missing in the v/c tables? It is inconceivable that the project traffic will not make congestion on the Shaw Road Overpass much worse (from E. Pioneer to E. Main).
  - *Recommendation:* Obtain new traffic counts in 2024 as noted above and re-do traffic analysis. Specifically address this key road segment in the analysis for LOS, queue length, and v/c ratio.

- The 10% non-truck vehicle distribution of traffic onto Shaw Road south of Pioneer is too low. The basis for the project trip distributions is not described in the DEIS or Appendix. More than 10% of the non-truck vehicles are likely to be using Shaw Road southbound in the afternoon as workers commute to home to the south Puyallup/South Hill area. The assumption that 40% of project PM non-truck vehicles will be going North to 167 and 25% will be going to Pioneer/SR 512 is relatively too high to the 10% assumption on Shaw Road. This has important implications to Shaw Road, which exceeds performance standards under existing and the No Action alternative.
  - *Recommendation:* Distribute 15 or 20% of the non-truck project PM trips to southbound Shaw Road and re-run the traffic analysis. Explain distribution rationale in EIS.
  
- The DEIS lacks a narrative description of how the project traffic will impact the already congested Shaw Road and East Main during peak AM and PM traffic. The DEIS includes a lot of metrics for the traffic analysis. However, what's missing is a simple narrative of the traffic impacts under the different scenarios. It's quite obvious that adding the project traffic from 5<sup>th</sup> Avenue SE onto Shaw Road will dramatically affect traffic on Shaw Road and East Main. Adding the light on Shaw Road to facilitate access to and from 5<sup>th</sup> Ave SE will clearly result in more backed-up traffic northbound on Shaw Road (south of the new light) in the morning commute and more backed-up traffic southbound on E. Main and Shaw Road (north of Shaw Road/E. Pioneer intersection) in the afternoon commute. These obvious results are not discussed in the traffic report. They need to be. The analyses that best addresses this obvious traffic impact are the Travel Time comparison in Table 4-54 and the v/c ratio tables for scenarios A and D.
  - *Recommendation:* Provide a better narrative description of the traffic impacts drawing upon key metrics like travel time and v/c ratios to explain the impacts on the two key travel timed segments. And explain why travel time is reduced from scenario A to C and from scenario C to E. It is not clear why these reductions are occurring. Explain what mitigations are resulting in the travel time reductions from A to C to E. And why scenario E travel times, with 5,844 added project trips per day, are similar to the No Action. Absent these explanations there is no confidence in the traffic study results.
  
- The developer needs to pay for infrastructure improvements for Shaw Road and E. Pioneer. It is necessary to: 1) widen Shaw Road from Shaw Road School to 39<sup>th</sup> Ave. to reduce added traffic congestion that will be caused by the project traffic and associated new Shaw Road light to allow project traffic to access Shaw Road, 2) widen E. Pioneer from Shaw Road to 167 to upgrade the road for higher levels of truck traffic that will be caused by project on this narrow laned road, and 3) require the developer to build a separated bike trail along E. Pioneer from Shaw Road to 21 St. SE since this road will no longer be safe for bike travel.

- *Recommendation:* Require the developer to pay a proportionate share for the Shaw Road and E. Pioneer road upgrades noted above. Require that Shaw Road segment from Shaw Road school to 16<sup>th</sup> Ave SE be widened *prior to* the occupation of any new warehouse. This is necessary to mitigate the traffic congestion on the Shaw Road Overpass to allow more PM peak traffic to move south prior to merging into one lane southbound on Shaw Road. The proportionate share tables need to be revised based on the revised model. Require the developer to build the above noted bike path.
- Figure 4-61 needs to be fixed. The # of trucks is inaccurate for each of the routes.
- In summary, the traffic study needs to be revised and re-modeled to reflect the above biases/flaws. It is vital to get this right. If the traffic impacts from the project are not accurately portrayed and mitigated, Puyallup residents and the surrounding communities will be stuck with very significant irreversible negative impacts. But even with traffic study as presented, it is clear that the proposed action (A), rail (B), mitigated proposed action (C) pose unacceptable significant traffic impact and, at a minimum, the reduced density alternative (E) is needed to appropriately mitigate the traffic impacts.

March 14, 2024

To: City of Puyallup

From: John Palmer

RE: Comments on Knutson Farm Inc. DEIS – **Trail Alignment**

Thank you, City of Puyallup, for preparing this DEIS. The comments herein focus on the location of the Foothills-Riverwalk Trail segment.

I recommend the paved Trail go along the rim (as proposed by developer) and connect to Van Lierop Park. The trail along the rim is better than dropping the trail down to and along the river levy for several reasons. First, the view from the rim is spectacular. The Mt. Rainier View, overlooking the floodplain, is spectacular (see photo). There will be some eyesore from the adjacent warehouses, but there will be some buffering between the trail and the warehouses from stormwater facilities that will mitigate that and the user will be looking away at the spectacular view. Second, there will be significantly more maintenance associated with having the paved trail cross the floodplain and along the levy. This area floods, which will require clean up. And due to its remoteness, it will require more police patrol for homeless encampments. The unpaved path will still exist along the river for that option, but the paved trail should be along the rim.

Connecting to the Van Lierop Park has multiple benefits. It connects the regional Foothills Trail system to the Park, providing an excellent stop-over location for trail users. There is already a safe road crossing of 80<sup>th</sup> ST E that connects to the Foothills Trail. The masterplan for VLP has interior trails, so trail users passing thru the Park will not cause an impact to the Park. Lastly, it's not a good to have the trail adjacent to Wetland D and have a second crossing at 80<sup>th</sup> ST E.



**Commenter:** Palo, Nannette

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

KF development concerns, County project concerns and how it effects CoP

**Commenter:** Palumbo, Dawn

**Source:** Email

**Date:** 1/12/2024 8:46:00 AM

**Comment:**

We want a "No Action" option

The traffic this will create will be detrimental to the area.

**Commenter:** Pantastico, Hoa

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Here are my comments on the proposed warehouse project:

Table 4-22 of the DEIS sums things up well. The proposed Project would be inconsistent with the policies and goals of the County and City. There is no shortage of developable land that would be suitable for this type of industrial project. If development is done, it should support rather than harm city/county residents (e.g., air pollution, local ecology (water and land), traffic, regional park system, visual interest).

There is, according to the DEIS, a “clear lack of master planning of the overall Project, despite the substantial size of the Project site and total building square footage; the Project lacks a coherent plan for open spaces within the development envelope, minimal landscaping set asides, no signage plan presently, traffic impacts that are significant without a clear presentation of controls to adequately mitigate an unknown set of end user(s) and a lack of details regarding overall site management and maintenance approaches to meet this policy”. Many use factors “are wholly under-considered and not addressed by the proposed development’s plan”. IF any project moves forward, an updated project plan must:

show that there is no other viable project site and

develop a comprehensive plan that respects the site’s critical areas, abides by environmental & use constraints, and adequately mitigates potential harms to the local environment and people

Thank you,

Hoa Pantastico

**Commenter:** Paradis, Sara

**Source:** Webform

**Date:** 1/13/2024 6:54:25 AM

**Comment:**

Why do you continue to fill our beautiful valley with ugly warehouses that congest our streets with terrible traffic of semis? Leave that space open for business to thrive or homes to take advantage of the view. All that's left of Puyallup and Sumner are disgusting warehouses. Please think before you allow more. People who live here are fed up.

**Commenter:** Parshall, Chris

**Source:** Webform

**Date:** 1/20/2024 12:41:46 PM

**Comment:**

We support the development, but we request as a part of the development that Puyallup city sewer be extended down to the residents off 80th Street. We also request that Farm 12 be required to provide more parking off of 80th street. And sidewalks and lighting be installed. The current parking on 80th is a hazard to cars and pedestrian traffic.

**Commenter:** Patterson, Christine

**Source:** Email

**Date:** 3/12/2024

**Comment:**

Hello,

I live at the top of Shaw road. I used to commute to Seattle by driving to Sumner, parking and jumping on the Sounder train. The train ride between Sumner and Seattle was 40 minutes. More often than not, I spent less time on the train ride than the time it took to get my car from Sumner up Shaw Road to my home. Traffic is so congested in this area. Now that I am retired I avoid leaving my house between 3:00-6:00 PM to run even local errands. You have already approved a huge housing development across from Safeway which is going to make this situation even worse.

Adding seven huge warehouses with the related semi truck traffic is horrifying. It will make living in this area untenable. Is the warehouse that was built a couple of years ago in that area even fully occupied?

I moved to this area because it was a small town with a nice rural feel. It is not progress to pave over farm land and destroy the wildlife habitats. Once that happens we will never get it back.

I am so disappointed that the elected officials in Puyallup are going forward with this development proposal. The quality of life for Puyallup residents will suffer. I know that my vote in the next election will be based on who supports this project and who doesn't.

Thank you

Christine Patterson

**Commenter:** Paulsen, Camille

**Source:**

**Date:** 3/15/2024

**Comment:**

If there was a contest among cities across the country to win the jackpot of a large historic piece of land with some of the richest soil in the world, bordered by a glacially fed river and an absolutely stunning view of Mt. Rainier, what do you think those cities would propose? Probably something that would bring the community together to show off this jewel in the crown of their city. Maybe a beautiful hotel/resort surrounded by gardens that would grow food for their restaurants, employ hundreds of people, bring tourism to the area and be the gateway to Mt. Rainier? Maybe a vineyard with summer concerts like Woodinville? A thriving field of community gardens that would connect people to nature and produce food for themselves, their neighbors and those in need? Any of those ideas would win the contest for best use of this incredible land that is our heritage and our future. So what is Puyallup and Pierce County's proposal? Let's pave the entire place over with industrial automated butt-ugly warehouses that employ very few people, yet increase truck traffic, pollution and dangerous runoff to a nearby salmon-rich river. And lower every tax-paying citizen's property values and quality of life in the process. In boating terms, Puyallup's proposal would come in DFL. We are sitting on a trifecta of incredibly rich farmland, a pure glacial river and an unbelievable view that cities around the country would kill for... and this is the best we can do??? Puyallup has scenic beauty in spades but zero vision or common sense when it comes to how to utilize it for the good of the community. Skagit Valley figured this out a long time ago. Puyallup is cementing its reputation as a backwater version of Kent and Fife: warehouse central with the traffic nightmares to match. Every person on the council who supports this incredibly short-sighted eyesore of a greedy land-grab warehouse development should be ashamed of themselves. They should also be made to live next to this monstrosity and deal with the traffic, noise and lower quality of life. And they need to ask themselves one question: are they truly proud of the legacy they are leaving for future generations?

Respectfully submitted,

Camille Paulsen

**Commenter:** Paulsen, Dirk

**Source:** Email

**Date:** 2/26/2024

**Comment:**

Dear Sirs,

The Puyallup area has experienced unchecked, lightly regulated growth in the last few years. There seems to be absolutely no cohesive plan to develop the area in regards to leaving open spaces for the enjoyment of the residents that pay the taxes. This latest proposal to allow more warehouse space is another example of developers using fertile farmland to plunk down concrete slabs and add more congestion and wear and tear to an infrastructure already overburdened by previous rampant building.

As much as I would like that no development take place, I am well aware that the area in question will experience some development. Therefore, if forced to choose between the two alternatives I would opt for the Reduced Intensity plan ( Alternative #2). Thank you,

Dirk Paulsen

**Commenter:** Pederson, Carrie

**Source:** Webform

**Date:** 2/20/2024 8:45:00 PM

**Comment:**

We DO NOT need more warehouses that remain empty!!! You are allowing developers ruin everything!!!! We need to preserve farmland!! There are way toooo many available/empty/abandoned warehouses everywhere-use those first!! You are destroying everything that is good

**Commenter:** Peranzi, John

**Source:** Webform

**Date:** 1/10/2024 11:07:55 AM

**Comment:**

i support the rapid development of the proposed industrial site. the first priority should be the bike trail which Knutson deliberately cut off years ago. when the trail is completed then complete the warehouse site. this trail segment will finally create a safe connections between river walk and foothills trail. this meets the goal of all. john peranzi [REDACTED] foothills trials board member

**Commenter:** Perrin, Christine

**Source:** Email

**Date:** 3/15/2024

**Comment:**

I grew up in Puyallup and have lived just up Shaw road for the last 22 years. The traffic on Shaw road has become worse every year and I'm very opposed to adding the warehouses and added traffic that goes with it. We used to see the fields turn yellow with daffodils in the spring, now can see warehouses. I would prefer no new warehouses adding to the congestion the area, and negatively impacting the environment, but if that isn't an option the plan with the least impact is my preference.

Christine Perrin

**Commenter:** Perry, Ron

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Traffic mitigation concerns/impact

**Commenter:** Peters, Michael

**Source:** Webform

**Date:** 12/19/2023 8:08:34 AM

**Comment:**

Thank you for this opportunity to comment. I heartily endorse this project from a standpoint of continuing to create a lack of community and encourage a sprawling mess of a city where tax revenue is the most important resource. The Sound Transit overpass at Traffic avenue and 410 is now an even more complicated traffic nuisance and I can only imagine what a continuous cycle of freight would look like in that area. I have lived in my residence near the Woodbine cemetery for twenty years now. Aside from that cemetery, I don't feel that I am considered by Puyallup to be in the city. I feel more that I am in the tax base area that surrounds the Pioneer Park and City Hall area - the only part of the city that has any community value. I felt fear from the moment the Shaw Road extension was finished and the two "road closed" signs popped up on the wings as you proceed northbound. I remember saying to my son when that opened and we looked at the sunrise behind majestic Mt. Rainier, "one day son, this will all be warehouses." Upon further learning from other city residents that the permitting was done years ago for such a project and conveniently hidden until it was too late. I grew up in the Puyallup area. I remember the days before Fairview Ave. I remember \$.10 scones. I remember Hi-Ho shopping center and the razing of the original Sparks Stadium. The parks that existed then still exist now. I remember the beautiful and historic Woodland school, much like Maplewood school, but now just a bland elementary with a field where history once stood. The only development the city has made in the time I have lived here is to create an improved community space at Pioneer Park. While nice during the day, I prefer not to interact with the evening transient population. No other development has happened courtesy of the city. The Shaw road 'upgrade' is a suspension-punishing disaster that solved nothing, not to mention 39th by Costco. Puyallup has annexed, sure... and extended the tax base, but to what benefit? I am in the city limits, but as property taxes and fuel costs increase, I cannot walk to a transit station continuously on a sidewalk and have a safe crossing. Walking to the transit center near the 512 westbound onramp involves dirt paths, ditches, narrow, overdeveloped and unlit roads (yes, 13th), and generally taking your life in your hands to cross the street. I think the city could do better to demonstrate some commitment to the community than to approve a project that will exist so my Amazon delivery can get here even quicker. The Van Leirop Park is more of an appeasement and a seasonal weed patch, outside of the Lupine bloom, and an insult to the people of this community. I accept that I could participate in community and council meetings, if my job that I commute to would afford me that ability. Instead of taking up a person's pet project and isolating more wealth, here are some proposals: Create sidewalks in the city that don't require a developer and a code variance that allows 20 houses on an acre next to one house on five acres. Encourage some consistency in neighborhoods. Adhere to published zoning and cease to allow developers to bring short-term financial benefit that creates a long-term problem that will fixed and funded by the taxes on established neighborhoods. Create and share a master plan for 9th from 512 to 15th... a mess more than just during the fair, and a pedestrian and bicycling nightmare. Imagine Tukwila's "Starfire" or Yakima's "SOZO Sports Complex" as a community recreation and gathering area at the

Knutson site - something that brings commerce to the area and injects regional money into local businesses on a weekly basis. The current warehouse at the site could be converted into indoor tennis, soccer, etc., facilities that could be rented or contracted out. The infrastructure for food and lodging already exist in close proximity to the site. I bicycle a lot around Puyallup and the neighboring communities. There is A Lot of "For Lease" warehouse space just up the road. Auburn, Kent, Sumner, Pacific, and Algona all have ample "For Lease" warehouse spaces as of the past week. All of these locations are in less-complicated areas as well. While I know demand for our 'now' economy is growing, please be more forward thinking and put something forth at this site that enriches the community and that you would want your name attached to. It's unfortunate that a local name like Knutson will be tied to this project and forever looked down upon. Thank you for the opportunity to comment, Michael Peters

**Commenter:** Peters, Becky

**Source:** Webform

**Date:** 1/23/2024 9:26:35 AM

**Comment:**

No! Stop allowing our farm lands to be destroyed for warehouses. Furthermore why grow food in a warehouse when it grows just fine in the ground like it has for the many many years they have owned this property. The Puyallup/Sumner valley area was once known for it's agriculture. Remember all the strawberry fields that used to be in Puyallup and we traded them all for car lots, apartments and more traffic and crime.

**Commenter:** Peterson, Andrew

**Source:** Webform

**Date:** 1/30/2024 5:03:18 PM

**Comment:**

Regarding 4.9 Transportation: As another corridor for traffic commuting north, intersections along 5th ST NE/Milwaukee Ave (Puyallup) should be included in the traffic study area. I live on this street and am very aware that when traffic is heavier on other routes north, commuters and trucks will use 5th. This is a big impact because it is more residential in nature than Meridian or E Main/Traffic Ave. Regarding 4.10 Health and Safety: The traffic impacts stated above include a walking route to Stewart Elementary including a street crossing. Shaw Road Elementary is on Shaw Road close to the project site. Spinning Elementary is on Pioneer near the project site. The latter two schools listed here are along the routes where increased traffic is expected. Increasing traffic could increase danger at school areas and should be included in the Health and Safety portion of the EIS (or Transportation portion if more applicable). Regarding 4.10 Health and Safety: Another increased safety risk due to increased traffic is the crossing of 8th Ave SE by pedestrians, bikers, etc. There is Farm 12/Fika, Van Lierop Park, parking, and a future connection to the Riverwalk trail on the north side of 8th. There are bathrooms, parking, and the Foothills Trail trailhead on the south side of 8th. People currently cross the street all the time to get to the attractions on the other side because it is a low speed road without much traffic. Increased traffic for warehousing in this area would put people at risk who continue to cross the street. Regarding 4.6 Aesthetics: Warehousing in general is undesirable in a community where people live because it provides little benefit to the local resident (besides taxes, perhaps) and it adds negative impact in many ways (listed in the EIS). Potential homebuyers would prefer not to live by these warehouses or would pay less to do so, thereby driving down value in the neighborhood. I am unaware how far this effect reaches. (This could also potentially be included in cumulative impact?) Regarding 5.4 Cumulative Impacts Analysis: It appears 4.6 Recreation and 4.7 Aesthetics are reversed and listed as 4.6 Aesthetics and 4.7 Recreation in this section of the EIS. Therefore the links are also linking incorrectly.

**Commenter:** Peterson, Cynthia

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Thank you for allowing me to submit my response to this project. I do not know all the negative effects on the environment this industrial park will have on the Puyallup Valley if it is permitted to be built. I have been reading articles and listening to podcasts that discuss how farms are being destroyed in this country. In Washington State alone, I listened to a podcast ("Saving Family Farming") that Washington State is losing 14 farms every week. This agricultural land must be preserved and not covered with concrete. Traffic in this area is terrible already without adding the vehicles and semi-trucks of seven industrial warehouses to it. Shaw Road and Orting Highway are both often backed up for miles. There is also construction happening at the intersection of Pioneer and Shaw Rd that will certainly add to congestion. Farm 12 is also known to many in our community, even outside of Puyallup, it is a staple to this community. I would like to see no more warehouses added to this area, however if warehouses are permitted, there should be a limit of three or four that sit behind the park and do not cause an eyesore to the Mt. Rainier View and cannot be visible from 80th St E. I think that if the developers will connect the Foothills Trail to the Riverwalk Trail and can attractively hide warehouses behind the park it could have benefits to the wellness of the community. Certainly the warehouses along 80th St E cannot be built because it would be terrible to have a trail surrounded by warehouses that also blocks the Mt. Rainier view.

**Commenter:** Piacenti, Jordan

**Source:** Webform

**Date:** 2/6/2024 1:58:20 PM

**Comment:**

We should not be sacrificing more of our farm land to development. Farm land lying unused is available to fulfill future food production needs. The breakdowns of supply chains during the pandemic showed us that we need to rely more on locally sourced food & less on imports. The beauty of our city is a risk if we continue to fill our beautiful land with unsightly warehouses.

**Commenter:** Pickard, E. J.

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Firstly, Thank you to the City of Puyallup staff and everyone who helped put this EIS together. if it wasn't for your work this developer would've gone unchecked. this development does not benefit the city of Puyallup, it doesn't benefit the citizens of Puyallup, in fact, it is detrimental to anyone who lives within Puyallup, South Hill, Graham, Sumner, Bonney Lake, and Orting, Anyone who relies on Shaw Road, Pioneer Road, E Main, 162, Or the 410 E Main/Traffic Ave interchange. the city's current transportation infrastructure is inadequate for the population it currently serves, hundreds if not thousands of people rely on Shaw Road to get up the hill to South Hill/Graham, the thousands of people that live in the Sunrise community, and this doesn't account for the projected growth and the thousands of planned units in planning or under construction. the city doesn't have the budget to maintain its current infrastructure, the majority of our downtown intersections aren't signalized, our roads are in various states of disrepair, and our stormwater system is currently being overhauled because it can't support our current growth. the developer intends to add almost ten thousand vehicles onto our roads mainly the Shaw Road overpass coming from the 410 interchange. to anyone that's familiar it can take you as much as 20-30 minutes to get off 410 to the light at the bottom of pioneer and Shaw, when school lets out at Shaw Road Elementary and shaw is flooded with buses and parent pickups you can easily wait 10+ light cycles to get across the overpass to start going up the hill. It's madness as it is. our roads were never designed or intended to support high-intensity industrial uses, it has never been in our city plans, as it was never wanted. the deis makes it seem like we can throw in a round about and retime some lights and everything will be fine, what happens in 30 years when the city cant afford to maintain the infrastructure? and let's make that designation now, the roadway infrastructure that would be used by this potential development, our roads, streetlights, sidewalks is all city-owned and controlled, the taxpayers paid millions of dollars for the Shaw Road overpass connection to Main and for the developer to have any access to OUR roads the city must issue them a permit, without a permit from the city the developer can't build a driveway to serve the facility. on top of that the previous landowner signed a land use agreement with the city predating the developer and agreeing with the city's urban growth area and zoning regulations which the developer blatantly ignores, the agreement limiting development on the land and having a larger rural buffer. along with our road infrastructure being entirely incapable of handling the current traffic during peak times, especially during the fair or Puyallup night market, our river flood control infrastructure is barely holding on, I can't count the number of times the river has gotten all the way up to the point of almost going across the levy, and the historical, horrific floods that have inundated the valley and caused massive property damage and loss of life. the river is a constant threat to those in the valley. from countless years of mismanagement and design practices that have had a significant negative impact on the local ecology and biodiversity, the river is nothing like it used to be. it's almost at capacity in terms of peak stormwater discharge, the majority of the riparian zone is non-native invasive plant species that out-compete native plants. there is not enough transparency on 6PPD and its potential effects

on the salmon, something that not only impacts the citizens who fish but also the Puyallup and Nisqually tribes who have historical rights to the waters up and downstream. the tribes have had to fight to bring back the salmon population from a point of near extinction due to the continued mismanagement of the land and water. along with those the developer also owns the Viking Warehouse an almost 450,000 sqft industrial facility, the stormwater discharge from the single site alone, despite persistent mitigation efforts has continued to cause channelized erosion to the Puyallup river bank, undercutting vegetation, eroding the soil, resulting in the loss of almost 10' of river bank and knocking down trees in the process. What's going to happen to that erosion if they have seven buildings instead of one? 70' of river bank erosion? what chemicals will that expose us and our wildlife to? we don't know. we do not know current 6ppd levels, the potential impacts, or how much it would increase, or the potential toxicity from such an intense new development.

beyond the supportive auto infrastructure and stormwater infrastructure, the land the project site is on is historically some of the most important land in the state. the land is comprised of mainly puyallup fine sandy loam, Sultan silt loam, Briscot loam, and Pilchuck fine sand, the rare combination a result of years of flooding and historic lahar flows, making the soil some of the richest most fertile farmland in the entire world. The land along with most in the valley has a long-standing agriculture history pre-dating the Knutson family going back well over a hundred years, the land has seen booms and busts of hops, rhubarb, daffodils, and all the things that make Puyallup, Puyallup. it's one of our last connections as a town to our pioneering roots, it's irreplaceable, and the damage that would be caused to our soil would be irreversible. it took thousands of years to naturally cultivate the soil fertility and the developer wants to cover it with asphalt and fill dirt. this would permanently alter the land in a way we would never be able to come back from. the daffodil represents puyallup, it's in every logo, it's on every street corner, we have festivals in its honor, our young women compete to be the daffodil queen, but there's no longer any commercial production in the city, and the land that once used to be a golden carpet of daffodils is nothing more than mismanaged farmland in a desperate state a limbo. the proposed development would also significantly impact farm12 and van lierop park, a project the city is heavily invested in. The proposition to mitigate the negative impacts is to build a 12' concrete sound wall with a 30' landscape buffer around the entire proposed facility, this is greenwashing. the same thing happened with forestry in the 80's when they tore down forests and hid it behind a small strip of trees a "green veil" so people couldn't see what damage was being done to the land essentially it would turn the almost 200 acres of farmland into something that more resembles a prison or military complex the current facility has military-grade fencing with security sensors and cameras all around a full-time guard on duty, it's built like a prison. if there was a full concrete wall surrounding the property it would be like a glorified labor camp. and by the time the landscaping matures the landscape buffer being so close to the park, the mature landscaping would tower over the van lierop loop, entirely blocking the view of Mount Rainier we've all had the privilege of enjoying for so many decades. it could look pretty, with all the trees and shrubs especially when they're mature you won't have any clue what's behind the concrete walls, it doesn't change the irreversible destruction no matter how well they hide it and cover it up. the developer has no interest in the land or the repercussions of their development. they don't care how it's run or managed they don't care what their tenants dump into our river they don't care enough to show up to the meetings or send an agent, they care about one thing, money. they are out of state and would have very little oversight of day-to-day operations and we would be forced to rely on companies to properly

manage stormwater, chemical compliance, and best practices, to not have any negative environmental impacts, and when the company only cares about their bottom line and has no personal interest in the building or land, nothing is stopping them from deferring all maintenance till the end of the lease and being careless in their actions. what happens when no one cares and everyone points their finger at someone else to take responsibility? the post-pandemic trend of industrial development is not the first industrial development boom the state has seen, and it won't be the first bust either. we're currently observing a massive increase in industrial product construction and subsequently availability, countless new starts along 167, in north Puyallup, Sumner, Frederickson, the common theme? almost every single facility has a for lease or for sale sign, half of it is empty, there's not enough demand to fill the current product on the market so what makes the developer think it's a good idea to add 2.7mm sqft where it's not wanted or welcome this developer has a lengthy history of disregarding the city and the citizens, they went behind our backs, tried to work with the county, and took us to court (lost) tried to buy out our city council (failed), and continue to push and push, they have burned their bridge, the city can not trust them, they refuse to hold up previously acknowledged agreements and continue to ask for more and more and more, they are destroying our land, they're destroying our water, they are ruining our soil fertility and causing significant impacts to our quality of life. the citizens of Puyallup do not want or need this. to echo some of the public comments from January, we desperately need housing, We can't accommodate our projected growth as it is so we need to start planning for that. the only reason the developer is pursuing industrial is because of outdated almost 20-year-old land use plans that are not representative of Puyallup today. they stand to make a fortune off of it while everyone around them suffers, and they don't care. they'd rather throw lawsuits and money at us than work towards a common goal so the city needs to make it clear they are not welcome in our city. to wrap up, the only alternative the city can recommend is no alternative. the price is too high, the risk is not worth the reward, and the detrimental impacts to us, to our children, to our children's children. we cant afford to make this mistake. and we will never be able to recover from it if we do. the city needs to stop paying for an alternative that kind of fits and conforms to the old land use plan, stop working for free, the developer should they choose to continue the pursuit needs to do the work themselves. we need more time as a community to assess the impacts, long term and short term, for that reason I'm adding my voice in saying we need either an extension of the comment period or another round of comment period, I didn't see a single sign go up on the property to notify of the new events, and I don't think the majority of Puyallup residents have any idea this is still being fought over. **WE DO NOT WANT THIS. WE THE PEOPLE HAVE SPOKEN AND WILL CONTINUE TO SPEAK, NO ACTION. NO ALTERNATIVE. NO DEVELOPMENT.** save our farmland, save our water, save our air, protect the earth. don't bow down to a foreign corporation with no vested interest in our community. for your kid's sake and mine. don't destroy our history, don't destroy our culture, or it may never return.

**Commenter:** Pierce, Art

**Source:** Webform

**Date:** 12/14/2023 8:33:43 PM

**Comment:**

Like we don't have enough warehouses in our valley today! Save some of the last remaining farm lands instead of destroying some of the last great agricultural land in our area. What about the wildlife and drainage and agricultural benefits that the land provides today instead of blacktop ping it? It is a shame that our once flourishing farms are 90% covered by blacktop now. We don't need anymore and I ask you take the loss of precious farmland into consideration.

**Commenter:** Pierce, Art

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Please STOP destroying what little farmland remains in our area. And STOP destroying what little forests we have as well. Let another city destroy their farmland/trees. This is some of the best soil in the world. Why would any of you want to bury it under concrete/asphalt? Whoever you are that makes these decisions to destroy every acre of land and trees, just stop it. Do we really need more \$ at the expense of our once vast land/trees? No we really don't. Please tell whomever the construction company is to go somewhere else. Thanks for reading.

**Commenter:** Pigeon, Kayley

**Source:** Webform

**Date:** 2/7/2024 10:50:05 PM

**Comment:**

The Puyallup river was named the number one endangered river in the country in 2021 by American Rivers. There should be consultation and leadership taken from the Puyallup tribe on how to go about this sustainably. This river needs protection and needs community to step up to connect wildlife corridors in urban environments. This ideally would be a green space that is protected and used for ecological and economic importance. The other warehouses should have never been granted permission in this area to begin with. No need to continue the capitalistic infrastructure that continues to deplete and corrupt not only the environment but also our communities. Thank you.

**Commenter:** Pitigliano, Audrey

**Source:** Webform

**Date:** 3/11/2024

**Comment:**

I will send comments by email. Thanks

**Commenter:** Porter, Kaaren

**Source:** Webform

**Date:** 2/21/2024 1:33:00 PM

**Comment:**

Absolutely a horrible idea.

**Commenter:** Powell, Elaine

**Source:** Webform

**Date:** 2/19/2024 2:23:00 PM

**Comment:**

Please, please do not allow this huge industrial warehouse project to proceed as is. This area cannot possibly handle such an increase in traffic volume, especially large trucks, and the merging into and out of the present heavy traffic area. Let alone the pollution created that would cause a huge reduction in quality of life for the surround citizens of Puyallup. A non profit restaurant/facility vital to this community would be surrounded by these warehouses and resultant heavy traffic. Much has been invested by the citizens and would erode the invested quality of life enjoyed by many. Let alone the reduction of nearby property values. There is a Puyallup Public Elementary School and a church/school just 2 blocks away. There would be more pollution, safety issues and reduction of quality of life for those children, staff and others attending each of these community gathering areas. Do not destroy historic prime farmland by letting this monstrous project proceed. Keep a good quality of life for all Puyallup citizens! PLEASE. Thank you, Elaine & Robert Powell

**Commenter:** Powell, Elaine

**Source:** Email

**Date:** 3/14/2024

**Comment:**

I previously submitted comments in regards to the Draft EIS for this project but failed to state my preferred alternative, which is the Reduced Intensity alternative.

Thank you,

Elaine Powell

**Commenter:** Prociw, Tracey

**Source:** Webform

**Date:** 2/21/2024 3:35:00 PM

**Comment:**

It is imperative that such actions are prohibited. It is essential to safeguard greenspaces for the benefit of both human inhabitants and the diverse wildlife inhabiting this area. Moreover, the escalating traffic congestion, potential obstruction of scenic vistas, and the transformation into an urban landscape devoid of natural elements must be staunchly opposed. Residents have chosen this locale for its natural splendor, not to endure the adverse consequences akin to an industrialized zone.

**Commenter:** Ragan, Michael

**Source:** Webform

**Date:** 2/12/2024 8:37:15 PM

**Comment:**

My recommendation is for the No Action Option. This proposal would have an overall long term negative impact on the people of Puyallup. Air quality, traffic, noise, and whatever else is brought to these warehouses will not improve the quality of life that brought people to Puyallup in the first place. If we want Puyallup to continue to be a great place, we need to stop this project from impacting our daily lives forever. If this cannot be stopped, it should only be allowed at the smallest possible scale. Thanks for listening.

**Commenter:** Reaves, Renae

**Source:** Webform

**Date:** 1/13/2024 9:44:17 AM

**Comment:**

There isn't a need for more warehouses. I work in the construction industry, specifically with landlords and brokers, and there is plenty of empty warehouse spaces available all throughout the area. If this land is to be developed, please consider making it into housing. We all know there isn't enough housing in the area. Honestly, enough of Puyallup's old farm lands have be turned into warehouses and if it keeps going, the area will look like Fife or Tacoma, all industrial complexes with no natural beauty left. Puyallup is the last big city as one makes their way to Rainier, we should keep that gateway as beautiful as the mountain.

**Commenter:** Reeves, Lisa

**Source:** Webform

**Date:** 2/21/2024 3:34:00 PM

**Comment:**

This project is coming whether I want it or not. I prefer the alternative of the least impact of the three warehouses. Shaw Road and Main cannot support the traffic as is for the additional trucks. Currently, I cannot depart my neighborhood during work and school commute times and drive a work-a-round to get to my destination. I predict the cut-through traffic in my neighborhood from Shaw to 23rd during these times will only increase. Our Puyallup neighbors living closer will endure noise and diesel fumes from the trucks. I like the suggestions for more recreational and entertainment opportunities including breweries and wineries for this beautiful land. The views of the mountain are stunning from this area and can be a tourist destination.

**Commenter:** Reeves, Lisa

**Source:** Email

**Date:** 2/28/2024

**Comment:**

I am a thirty plus year resident of the City of Puyallup living near Shaw Road. I moved here because of the great schools and neighborhoods, beautiful parks and trails and scenery and open space. We have had tremendous growth as a thriving small city with all of its conveniences, but it has come at a cost with the increasing traffic and struggling infrastructure to support it, farmlands disappearing to make room for retail warehouses and the impact on the wildlife we have left. We may be getting to the tipping point of becoming the next warehouse city like those around us. These warehouses are coming with the truck traffic on roads that cannot support them, the commuters that will cut through our neighborhoods to get around it, the noise, the runoff pollution from more concrete being poured, the wildlife displaced or disappearing and our quality of life impacted. Although it's kind of a done deal, I strongly recommend the reduced intensity alternative of 1 million square feet to preserve the surrounding land. Lisa Reeves

**Commenter:** Rhodes, Scott

**Source:** Webform

**Date:** 1/17/2024 10:28:17 AM

**Comment:**

As a former citizen of Puyallup who's family still lives there, and a 2015 graduate of Puyallup Highschool. I strongly disagree with the use of our lands for this purpose. As stewards of the land, that we acknowledge is Puyallup Tribal land before it was ever claimed and developed by us, it would be wildly irresponsible to allow this development to occur on such fertile land. Please do not allow warehouses to be built on our farmland.

**Commenter:** Richeson, Daryl

**Source:** Email

**Date:** 2/26/2024

**Comment:**

This is the best of the worse options

People seem to forget what puyallup

Should be , the roads In That area are already over welded with traffic and this will just add to the problems

Please at least keep the site to this area alone

**Commenter:** Richeson, Daryl

**Source:** Email

**Date:** 2/26/2024

**Comment:**

We live up above the planned area

This is what we moved here for many years ago , this owl is super cool and visits our home . This is the wrong place for the site What kind of owl is this ?

[image of barred owl]

**Commenter:** Richeson, Jaime

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

The impact of the additional traffic caused by the project will be detrimental to residents and businesses. Traffic in the area is already very dense with little or no alternative routes. Causing delays and congestion. It's hurts businesses because consumers will avoid the area. With the addition of upcoming multifamily complexes in the works, more cars will be in the area and the addition of semi trucks ruining the roads while diminishing the quality of life of the long time residents of Puyallup Valley. Not to mention reducing property values. You're not putting industrial sites in a broad vacant area, you're cramming industrial into a residential/ wetland. In addition to ruining the habitats of the owls, eagles hawks and importantly, humans.

**Commenter:** Riley, Kathleen

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Well, I am coming in down to the wire with comments. First of all, I want to say that I appreciate how well you presented the alternative plans during your Zoom presentation.

In general, my preference is for NO FURTHER WAREHOUSE DEVELOPMENT OF THE SITE, whatever alternative number that is. I meant to get a count of the number of EMPTY warehouses that I see currently in Sumner and elsewhere, AND THERE ARE PLENTY OF THEM. Does the EIS take those numbers into account? It should!

Therefore, I don't understand the need for more warehouses, especially on agricultural land. I worked at WSU-Puyallup for 30+ years on flower bulbs and Christmas tree crops. Our valley soil is some of the best in the country, and the site is well-suited for the current rhubarb production. (I had to laugh about the "wetland" designation on the eastern edge of the site; we used to do daffodil basal rot research there because of the fine conditions for disease development). This land could be a great place for a community garden area. The Brown Property community garden is space is very limited, most of the growing has to be done in raised beds due to the glacial soil and there isn't very good sun exposure in some areas. The Sumner Community Garden, where I have a plot, is very successful and could be a good model for Puyallup. What about an arboretum, including an exhibit of native plants; a dog park; leasing some land to first-time farmers and veteran's programs? This could be Puyallup's "last gasp" at protecting some of its agricultural heritage.

I have been contacted by John Palmer concerning his 'reduced intensity' alternative, and he believes that the "no action" alternative is not "reasonable". Why not? Even his plan does not take into account the following problem with truck traffic having to move onto Shaw Road. One day recently I was traveling north from the light at Pioneer Way and a semi was turning left from that exit street (orange in John's picture) and had to use the center lane in order to wait for southbound traffic to pass. THE SEMI COULD BARELY FIT BETWEEN THE LINES, so whoever planned the bridge did not take into account the space or turning radius needed for large trucks! In order to get into that center lane, the driver had to pull into both south and northbound lanes, disrupting traffic in both directions.

The railroad alternative is not at all feasible. Just trying to bring the Meeker Southern up to "code" would be a multimillion dollar proposition, I'm sure!

There was also a mention of the intersection of 80th East and Orting Highway being made into a roundabout. That would be very close to the river, so how would damage to the river be mitigated, e.g., salmon, etc? That street is definitely residential and is already taking a beating. Other traffic disruptions include the entrance to 410 from Traffic Avenue as you go into Sumner. The left turn lane is already a mess if there are more than 6 cars wanting to turn onto the highway, which

prevents cars from using one of the lanes into Sumner. The mess that would be compounded on Shaw Road is obvious. It is already necessary to time any travel to miss the school/work traffic.

DEIS Preferred Alternative.jpg

Per John Palmer's email, I agree that:

"Alternative 2 in the DEIS reduces the warehouses and associated traffic by 35%, protects 40 acres of farmland, and adds buffer walls around Van Lierop Park. Alternative 2 is a good start, BUT IT STILL HAS UNACCEPTABLE SIGNIFICANT IMPACTS. "

I appreciate his efforts, and if that is what it takes to get some kind of compromise, then so be it. But for myself, I vote for NO FURTHER DEVELOPMENT. This land is just too valuable to waste!

Sincerely,

Kathy Riley



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses

**Commenter:** Rink, Stacia

**Source:** Email

**Date:** 3/15/2024

**Comment:**

To Whom It May Concern:

My name is Stacia Rink and I am a current resident of the city of Puyallup. I live three houses off of Shaw Road, and only one mile from the proposed Knutson Farm 7 Warehouse project. I am writing to ask the city of Puyallup to DENY the Knutson Farm 7 Warehouse proposal using the city's authority under the State Environmental Policy Act. The environmental impact statement (EIS) for this 7 warehouse project clearly reveals monumental NEGATIVE impacts to the Puyallup community and the environment. A responsible response by the city would be to deny any further warehouse development in this precious open-space area of the former Knutson Farm, preventing further degradation of the traffic congestion that currently exists in this at-capacity highway corridor, and halting the addition of excessive noise, polluted water runoff, and aesthetic pollution of the area and adjacent community. No amount of mitigation will undo the mess that this project will make. Please do not let it go forward.

The EIS reveals that the proposed Knutson Farm 7 warehouse project would substantially increase the volume of traffic. The proximity of the project site to the urban growth area is clearly revealed on page 3 of Appendix E, Full Traffic Impact Assessment from Knutson Farms DEIS dated 12-14-23. Urban growth continues in Pierce County and Shaw road is the road used by many to get from HWY 167 to their homes south of Puyallup. Adding traffic to the study area will only serve to further constrain what is already a heavily traveled corridor. As a Shaw road community member, I do not want more traffic added to this two-lane country road. While four lanes have been installed proximal to the project site, this only serves to bottleneck the traffic as it merges to go south on Shaw road. Adding semitrucks to this already challenged traffic corridor would be illogical and detrimental to all who use this corridor. Please note that all traffic counts taken were performed in summer months during the COVID-19 pandemic. The traffic should be remeasured post-COVID-19 and in the fall starting in October when all students are commuting to local colleges, and folks are commuting to work to get an accurate representation of current traffic counts.

The proposed Knutson Farm 7 warehouse project would destroy the prime farmland that once grew beautiful daffodils and tulips, and would negatively impact the thriving Farm 12 business and trailheads via degraded aesthetics, noise pollution, and traffic congestion. People outside of our community have discovered this gem that is Farm 12, and they enjoy the open-space, trails and views of the farmland. The trail system is widely popular as well, and keeping the farmland as open space would complement these activities in the adjacent area. In addition, the EIS cumulative

impacts statement (section 4.4) clearly indicates that the Knutson Farm 7 warehouse project poses a significant hazard to salmon from the polluted runoff that would enter into the river. We previously advocated for the farmland to be converted into recreational space. The city should reconsider this idea to the benefit of city of Puyallup and local residents.

As a 28-year resident of the city of Puyallup and a life-long resident of Pierce County, I respectfully request that the city of Puyallup acknowledge the monumental negative impacts that this project would impose on the traffic, environment, Puyallup residents and adjacent community members, and then act to DENY the Knutson Farm 7 warehouse project. I thank you serving the Puyallup community.

Sincerely,

Stacia Rink

**Commenter:** Rink, Stacia

**Source:** Email

**Date:** 3/15/2024

**Comment:**

To Whom It May Concern:

I want to address the idea of an alternative to the draft Knutson Warehouse EIS. If DENYING the project is not an option, then I would advocate for the reduced intensity alternative to help mitigate the monumental impacts by the warehouse project (e.g., traffic, farmland loss, wetland destruction, stormwater runoff killing salmon, visual/noise/rural character impacts to Farm 12, Van Lierop Park, and Foothill Trail Trailhead). [image of reduced intensity alternative]

**Commenter:** Roberts, Stephanie

**Source:** Webform

**Date:** 1/12/2024 1:11:21 PM

**Comment:**

Traffic in Puyallup at the end of Shaw Rd & E Pioneer is already a monstrous inconvenience to surrounding homeowners, particularly those trying to get to and from work. Why do the people who live in Puyallup have absolutely NO control about their community and what is built upon it? This project, in addition to the apartment building being built in the middle of downtown Puyallup is the last straw for some of us who have lived here a long time. Shame on the greedy people who continue to overbuild on our precious land!

**Commenter:** Rometsch, Paul

**Source:** Email

**Date:** 1/12/2024 2:42:00 PM

**Comment:**

Hi Chris,

Great presentation last night, we're blessed to have you on the city staff. Your presentation was thorough and clear. My wife was particularly impressed with your ability to answer questions in detail on such a wide scope of topics.

While doing unrelated research as to why parking lots are slippery on bikes since a friend of mine fell (injury-free) last night in a parking lot (I may or may not have fallen injury-free a few months ago after a rain), I concluded it is the sealant. Not only does it make parking lots slippery for bikes but parking lot sealant is quite the nasty pollutant, with Washington pioneering a coal-tar sealant ban. Given the scale of the pavement in the design with the wear and tear of the proposed land use, were impacts from lot sealant run-off considered in the EIS?

Thanks,

Paul Rometsch

**Commenter:** Rometsch, Paul

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Paul Rometsch, P-A-U-L R-O-M-E-T-

S-C-H. I live [REDACTED] And the site comes in context because I actually don't really drive much. My bike is my main mode of transportation. I know many of the staff already know this. And so my initial concerns with that is that -- with the project is that cyclists will have to not just encounter

these trucks once but twice at the Shaw and Pioneer intersection, but also as they cross -- the truck's cross over the footless trail at 8th I think it is by Farm 12. And so, you know, this complex, it's significant. You know,

and you're running into -- right now it's nearly nothing and then now all of a sudden, you know, thousands of trucks a day. So that's a critical safety factor. Elements should be added to help mitigate that.

In addition, transportation around that whole

corridor. As you've heard tonight, as you've heard through

any form of feedback when the city performs anything about transportation, Shaw Road is a problem. But also, East Pioneer. I've heard the comprehensive plan transportation survey element of that. East Pioneer also showed up pretty red for nondrivers. So, you know, what elements, how will thousands of trucks along East Pioneer, you know, increase

the difficulty for nondrivers? That intersection is pretty critical for nondrivers; right? There is a Safeway there. People need

to buy groceries. So if you -- with no transit access unless you figured out the Puyallup runner by now, or even if you just live nearby or you're at the school or something, you can't -- you're going to have to, you know,

encounter some pretty crazy traffic as it stands just to get food. So that's just, you know, one of the things -- one of

the things I'm concerned about is, you know, is the transportation and it's beyond cars.

Secondarily, even with the reduced alternative, there's a significant amount of land we're losing or just paving over; right? What comes with pavement is heat. We

get a lot of heat. We're losing -- well, it's not natural habitat as it stands. It is habitat sustaining. And so, you know, how are we -- how are we mitigating that? You know, we can't really make new agricultural land as we all heard tonight. So like what elements can the project influence to help,

you know, mitigate that. You know, something we haven't talked about a bunch is runoff from the proposed land use. So truck tires

wear pretty heavily because, you know, that's a lot of weight on those tires. Also, with all the pavement comes parking lot sealant. You know, Washington state did ban some of the more toxic sealants. However, it still needs to

be replaced every three to five years, you know, 10 years if you're particularly stingy. But that runs off into the water. So with all that pavement there, you know, that's going to be a significant impact to the Puyallup River and, you know, and their salmon. So it's very important that, you know, we consider all these mitigating factors for that. So even with their (indiscernible) intensity, you're still going to have a lot of pavement, 1.7 million square feet. That's a lot. So that's another element to keep in mind. And yeah, so just from a transportation safety perspective, as well as an environmental safety perspective, this project has a lot of question marks and, you know, that doesn't even get into the cultural and, you know, land use fit. You know, whether that fits the vision or not, hopefully, you know, the city had 20 years ago when they first started creating the agricultural set-aside. But there are elements that need significant improvement even with the alternative to create something that will actually have, you know, a minor impact on the natural environment. Thank you.

**Commenter:** Rometsch, Paul

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Traffic mitigation concerns from KF project

**Commenter:** Romwro, Laura

**Source:** Webform

**Date:** 1/6/2024 9:41:49 PM

**Comment:**

Giving this land to warehouses is wrong on every level. We NEED to keep our farmlands sustainable. Additionally, adding more warehouses only depletes the desirability to live in the area. PROTECT our Puyallup/Sumner farmlands!

**Commenter:** Rose, Any

**Source:** Webform

**Date:** 1/6/2024 8:52:10 PM

**Comment:**

I do not agree with adding more warehouses, semi trucks going through our down town, destroying our land. Not to mention the traffic impacts. We need to preserve our natural land, not build more on it.

**Commenter:** Ross, Markee

**Source:** Webform

**Date:** 2/2/2024 10:00:27 PM

**Comment:**

Please don't develop this land! There is no need to lose more farmland. Neighboring cities like Fife have vacant and available for lease warehouse spaces. We don't need to build more warehouses in our communities.

**Commenter:** Ross, Jean

**Source:** Email

**Date:** 1/2/2024

**Comment:**

There shouldn't be any public meetings. It's no one's business but the Knutsons.

**Commenter:** Ross, Jean

**Source:** Email

**Date:** 1/2/2024 10:46:00 PM

**Comment:**

There shouldn't be any public meetings. It isn't anybody's business what they do with their property. Especially the City of Puyallup overstepping it's boundaries to be a nuisance to the property owners and overall community

**Commenter:** Ryan, Kyle

**Source:** Webform

**Date:** 12/17/2023 8:30:56 AM

**Comment:**

Every single street that you are proposing to build up, expand, and put MORE trucks on to is an absolute traffic nightmare twice a day. Our infrastructure should be expanded only to facilitate the growth of population we have accumulated, not to make room for corporations to choke us all out. No warehouses, no trucks; keep the land as it is. Go somewhere else. Elementary schools, grocery stores, and homes line Shaw. What are you trying to do to the people of Puyallup? The Puyallup RIVER is ON your proposed warehouse site. Are you really ok with destroying the natural beauty of this place? Do you think we really want 7 massive concrete warehouses and package distribution trucks ranging from mid sized vans to tractor trailers coming in and from every which direction? No, the people of Puyallup do not.

**Commenter:** S, Ben

**Source:** Webform

**Date:** 1/7/2024 7:57:35 AM

**Comment:**

I do not want this development built at this location as it will have a significant negative impact on my homes value and the environment around it.

**Commenter:** Saarela, Linda

**Source:** Webform

**Date:** 1/25/2024 3:37:03 PM

**Comment:**

I sit virtually every morning and evening and watch the traffic come off HWY 410 onto East Main and Intel Ave, then up Shaw Rd and visa versa. I find the promise of retiming signals as a means of improving traffic flow when 1000 to 1500 heavy vehicle trips are added to existing road systems unsatisfactory. I am confused why road improvements are not mandated with such a large project. Traffic off Hwy 410 and up Shaw is only going to increase as growth "on the hill" continues. The trucks tied to the complex would seem to reap the benefit of being closer to the highway while residents out Shaw and Meridian will suffer way beyond the current challenges, which are horrendous. I adamantly oppose the proposed action. I am in favor of Alternative 2 with road improvements and train track option for the flow of goods. Both are needed and necessary. It seems like the proposals benefit the developer with little concern for the impact on tax payers. I hope I am wrong.

**Commenter:** Saarela, Linda

**Source:** Email

**Date:** 2/13/2024

**Comment:**

I previously commented through this site that the proposed options for the Knutson Farm Warehouse Development were unsatisfactory.

I am aware there is a better "reduced intensity" alternative. See the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse.

I insist this compromise alternative be the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this massive proposal that is incompatible with this location. I am shocked the study believes adjusting the signals will take care of the traffic issues. I sit each morning and evening and watch the traffic proceed down Shaw to HWY 410 bumper to bumper for multiple hours. I am thankful I do not live further out as the entire South Hill is significantly impacted by this development in terms of traffic. Again, I insist this compromise alternative be the preferred alternative!

Linda Saarela

Executive Coach and Master Trainer

██████████

████████████████████

**Commenter:** Saarela, Linda

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Traffic impact concerns, Warehouse development concerns

**Commenter:** Sanders, Monica

**Source:** Webform

**Date:** 12/14/2023 5:56:19 PM

**Comment:**

I understand the need for growth and jobs, but this is getting ridiculous. Pretty soon we will be known as the town inside a warehouse complex.

**Commenter:** Sanderson, Cindy

**Source:** Webform

**Date:** 1/16/2024 9:56:33 AM

**Comment:**

OPPOSE PROJECT IF THERE IS NO TRAFFIC CONTROL AT 4TH OR 5TH AVE NE, PUYALLUP, RIVERSIDE DEVELOPMENT. I HAVE BEEN INFORMED TRAFFIC CONTROL IS NOT UNDER CONSIDERATION NOW. 2005 - 2011, SIX YEARS TO INSTALL TRAFFIC CONTROL. FUNDS PAID SHOULD GO TO THE PROMISED/PAID USE. SPENDING OUR DEVELOPMENT FEES ON OTHER PROJECTS NEEDS TO STOP HERE. SEE EMAIL: From: Erica Unruh <EUnruh@ci.puyallup.wa.us> Sent: Wednesday, February 12, 2020 12:44 PM To: Cindy <cindylsanderson@msn.com> Cc: Julie Door <jdoor@ci.puyallup.wa.us>; Steve Kirkelie <SKirkelie@ci.puyallup.wa.us> Subject: RE: Traffic Light at East Main & 5th Ave NE Good Afternoon Ms. Sanderson, I have looked into your concerns regarding the intersection of E Main and 5th Ave NE and having a traffic signal installed. Thank you for your patience as I took the time to gather information. It took a little longer than anticipated, due to the number of years back we needed to research. In working with multiple departments involved in this, the following information was gathered: When mitigation fees were collected by the City back in 2005 by businesses in that location, these fees were collected as traffic impact fees intended to go towards a new traffic signal at 5th Ave NE & East Main along with widening of East Main. Because the mitigation fees collected were not sufficient to pay for a traffic signal and the widening, only the widening was done in 2011. Currently, the East Main & 5th Ave NE proposed signal is within our adopted Transportation Improvement Plan (TIP), and the current funding that has been identified is potentially a part of the mitigation for the Knutson Warehouse development that is currently going through an Environmental Impact Statement (EIS) process with the City. If that remains part of their mitigation, then the developer would install the signal as part of their off-site improvements that need to be completed before they are issued occupancy. Timing on the EIS has just recently gotten started. Please let me know if you have any additional questions. Thank you, Erica Unruh Community Relations Coordinator City of Puyallup | City Manager's Office

**Commenter:** Sawyer, K

**Source:** Webform

**Date:** 1/12/2024 12:11:10 PM

**Comment:**

Knutsen Farms warehouse proposal should be defeated. SAVE what's left of our Valley farm lands.

**Commenter:** Schneider, Robin

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Hello, I would like to say that I am against any warehouse development at this location. These warehouses are not needed when considering the development at Frederickson, Sumner, Auburn and Kent. This property is some of the last farmland in the valley and needs to be preserved for future agriculture use. Thank you.

Sincerely,

Robin Schneider

**Commenter:** Schumock, Phil

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Hello,

I am a long time resident living in the Crystal Ridge development just uphill from the Knutson Farm site. After reading the information concerning the proposed development on the Knutson farmland, I am very much in favor of the “alternative” reduced density proposal as shown below in the graphic. As noted in the DEIS, the impacts of the more dense options are truly unacceptable for the community. As a successful business leader in Tacoma, I understand the need to allow development of assets, but our society has long imposed guardrails to such development where it benefits the greater community. In this case, the owners of the property are being provided, with the reduced intensity option, plenty of return on their asset while balancing the needs of the community.

Sincerely,

Phil Schumock

**Commenter:** Seely, Art

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Traffic concerns, Weather concerns and stormwater concerns

**Commenter:** Selby, John

**Source:** Email

**Date:** 2/24/2024

**Comment:**

As a board member of the Foothills Trail Coalition I participated in a discussion of the Knutson Farm EIS last night in our board meeting. I would like to submit two personal comments.

1) I would much prefer that the trail follow the levee. I know that the concern with that is that it likely would require much maintenance due to flooding over time. However, there is a simple solution to avoiding flooding of the trail: raise the levee above the likely flood level of the river. Large pipes under the trail would allow flooding of the area south of the trail for flood control.

2) I think two pedestrian crossings of 80th street E should be avoided due to the likely interference with traffic. I suggest that the existing one be used to access Van Lierop Park. Then the trail should be routed along the north side of 80th street E to the eastern edge of the property to connect with the trail toward the river.

By the way, years ago I met with Mr. Knutson and learned that his preferred trail route was the same as mine, i.e. go along the eastern edge of the property then follow the levee to connect with the Riverwalk.

Thank you,

John S. Selby

Board Member, Foothills Rails-to-Trails Coalition



**Commenter:** Shein, Stephanie

**Source:** Email

**Date:** 2/24/2024

**Comment:**

Please No Warehouses

**Commenter:** Sheldon, Kathryn

**Source:** Email

**Date:** 1/13/2024 6:09:00 PM

**Comment:**

Dear Chris,

Again , thank you to all that have been working so diligently on this project. Regarding for my concerns about increased traffic , not directly related to the Knutson project but to planned projects in Pierce County for road improvement, housing , and commercial development that will create more traffic for Shaw Road.

Road Improvements related to Shaw Road. I think that sometime this year, they will begin one of several projects planned for 122nd Ave E.

Changes at 136th St. E and 122nd Ave E

Changes at 144th St. E. and 122nd Ave E

Sunrise Parkway E and 122nd Ave E

Plans include additional center turn lanes, bike lane (?) and traffic signal changes , addition , and other road widening.

Sunrise Master Plan Community is adding many more houses, hundreds and new apartment complexes and some commercial attractions at the intersection of 122nd Ave E. and Sunrise Blvd. There are an additional 72 units of apartments planned on 110th Ave E across from the Clover Park and fire station entrance. Also across the road is a new warehousing /fulfillment center , land started clearing about 3 weeks ago and I am sorry I don't know the number of warehouses and footage. This is on the same road where International Auto Auction junk yard is located.

The point is that on the applications typically the applicants provide that the access is going to and from will be via Meridian and Pierce County Planning accepts this. Some have never even visited the South Hill area. One planner actually works from home in Portland, (that may have changed recently) but that is our new reality . Next, reality is that we as residents find that the drivers will find alternative ways to get to 110th Ave E. One of the ways is 122nd Ave E from Shaw or use 512 to Meridian and then cut across via 128th St E and 136th St E. They do use other routes as well. The point is that with the widening of 122nd Ave. E the widening will encourage more individuals with commercial vehicles to use Shaw Rd to access 122nd Ave E to 152nd St E and then left onto 110th Ave E. Expect to see even more tow trucks. This information could be important to your future planning considerations for this project and others in that area.

Sincerely,

Kathryn Sheldon

**Commenter:** Sheldon, Kathryn

**Source:** Email

**Date:** 1/23/2024

**Comment:**

Greetings,

Thank you for all that the teams for this project have been doing. I wish that Pierce County had acted this diligently for the project. In my heart of hearts , this property would remain as farmland. I don't think based on the legalities and logistics involved that that will be the case. The current proposal is a vast improvement and a balanced use of the property, but again, hard to endorse it.

Though the traffic has been greatly reduced and there is a process in place to restrict the truck and car traffic , plus increase of buffer zones, I don't recall seeing a better plan for improving the traffic into and out of this park for avoiding the intersection of Shaw Road and E. Pioneer. The left truck turn lane on Shaw Road into the business park where the Viking Warehouse is inadequate. It is too short to accommodate more than 2 trucks and drivers will experience even more back ups on Main going north to turn onto Shaw, than what they do now at peak and this will create a new "peak" . Due to the steepness of the overpass, when slick road conditions exist, the semis and box delivery vans are at risk for accidents and slide outs. I don't know if it is possible to divert a portion of the traffic into the park from Main by creating a railroad crossing across the tracks some where between 4th and 33rd st. E. , off of Main that would be truck traffic only ? I also don't recall if there is going to be a widening of E. Pioneer as it approaches the Shaw intersection with additional lanes ? Less idle time means less air pollution.

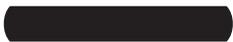
Still concerned about how to best filter the stormwater into swales or something to collect the diesel particulate matter and tire dust that creates 6 PPD quinone and thus reducing the particles from being in the air and in the water that does direct outflow into the Puyallup river which is salmon bearing , including for Chinook.

Next the warehouses in the business park, would be ideal to create ones that are for small business owners that need small warehouse space and dock doors that are on the ground, not dock height for semi trailers. Perhaps we can focus on providing for those small businesses needs since everyone seems guilty of only building super warehouses. My husband for example , the business is located in Tacoma because they can not get what they need here in Puyallup or Pierce County. They service and repair fire trucks , a lot of them and need parking space, space to pull all sizes of fire apparatus inside for repair and safer storage. They too have been hit by thefts at their business , nothing is sacrosanct .

Thank you

Sincerely,

Kathryn Sheldon



**Commenter:** Sheldon, Kathryn

**Source:** Email

**Date:** 2/1/2024

**Comment:**

Hello,

One more comment.....

At the on line meeting, I brought up the small, triangular shaped piece of land that is in corner of the proposed warehouse E. The open space is small but as I recall ,but I think part of the acreage that is potentially available for housing or agriculture use. If for agriculture use, the tip of the triangle constricts to the point and is adjacent to the pedestrian trail next to E's parking lot. The point is that without direct access or access to the remainder of the open use acreage, that piece is not functional due to the potential challenges of moving equipment in and out of the property. My suggestion is to check at the narrowest point and see if it is wide enough to move average size tractor or some other related equipment into and out of the space. Whether it would be necessary to move the walking path that goes along side of E's parking lot in closer to accommodate equipment , if that is possible and if not, then what other use for that small piece of land. I don't know the footage of it, thus I've no suggestions for other useage , other than restore it to native habitat.

Sincerely,

Kathryn Sheldon



**Commenter:** Sidor, Kyle

**Source:** Email

**Date:** 2/26/2024

**Comment:**

As a lifelong Puyallup resident it has been disappointing to see the nearsightedness of development approval. Infrastructure should be considered and implemented BEFORE development occurs. The Shaw Road traffic is already terrible and this warehouse development would only cause additional congestion. I would like to see either “no action” or “reduced intensity” option moving forward.

**Commenter:** Simons, Anneliese

**Source:** Email

**Date:** 12/16/2023 7:25:00 AM

**Comment:**

Pierce County doesn't need more of our precious land paved over for a warehouse. We are losing too much of our watershed to industrial usage that creates more traffic, congestion and pollution that will affect generations to come.

As a resident of Pierce County, I oppose the warehouse project at Knutson Farms.

Anneliese Simons (b) (6)

**Commenter:** Skibba, Allison

**Source:** Webform

**Date:** 12/14/2023 7:39:02 PM

**Comment:**

No thank you - too much semi traffic

**Commenter:** Slama, Mike

**Source:** Webform

**Date:** 3/15/2024

**Comment:**

My general feeling is that the development of intense warehouse activity in the proposed location is inappropriate, driven mainly by the already burdened traffic infrastructure to get from the warehouses to the freeways. My preference would be to develop the area in line with the City's "Comprehensive Plan Future Land Use Map" as this plan is more consistent with the City's needs and would provide a reduced traffic burden on the already stressed Shaw Road corridor. The City's future zoning would be preferable even over the "No Action" alternative as it would allow a more balanced development of the area and lead to annexation out to Puyallup's Urban Growth Boundary. Since the City of Puyallup's future zoning option is not listed, it appears that it is not available for some reason. (Why not?) If the City of Puyallup's future zoning option is not available, the "Reduced Intensity" Alternative 2 would be preferable due to: 1) lessened traffic burden 2) the increased trail network 3) the modifications to the sight-lines from Van Lierop park Both 2 & 3 would serve as irreplaceable gifts to future generations. In any alternative chosen, it would be a perfect time to include the recommendations made in the "Pierce County Comprehensive Levee Setback Feasibility Study Update" And specifically, the option listed as: "P10 Sumner Setback (Knutson Left Bank)", including Phases A and B, the "Sumner Setback". P10 is not currently prioritized, but access to this area along the river will never be easier. This option could easily provide an opportunity for wonderful salmon spawning habitat restoration.

**Commenter:** Smith, Preston

**Source:** Email

**Date:** 2/16/2024

**Comment:**

Please consider my following comments on the Draft Environmental Impact Statement (DEIS) for the Knutson Farm Industrial proposal to add 7 more warehouses on Shaw Road area farmland near Van Lierop Park and Farm 12.

As I understand from the DEIS, the proposal would result in increased traffic congestion along the Shaw Road corridor, remove 130 acres of farmland, add significant stormwater runoff that could potentially kill salmon, and make the Van Lierop Park and Farm 12 undesirable locations to recreate and eat. Of the proposed alternatives in the DEIS to mitigate some of these impacts, I would like to add my support for a third alternative.

Please consider a "reduced intensity" alternative. Something like in the image below. This alternative reduces the number of new warehouses to 3 (1M sq. ft.), which would also result in reduced traffic impacts, reduced farmland removal, and keep the area surrounding Van Lierop Park and Farm 12 a desirable location to continue visiting. I think it makes more sense to concentrate the new warehouses in one area near the existing warehouse and I recommend this alternative be listed as the Preferred Alternative in the Final EIS issued by the City of Puyallup later this year to appropriately mitigate the impacts of this proposal that really is incompatible with this location. Please feel free to reach me at this email address or at 253-820-9519 if further clarification would help.

Thank you for considering.

-Preston Smith



**Commenter:** Smith, Scott

**Source:** Email

**Date:** 2/17/2024

**Comment:**

Hello,

As a resident in the Puyallup Highlands we will be directly impacted by this building. Traffic before the warehouse complex construction was bad, pierce county had a traffic study done and the results clearly stated that traffic was already congested up Shaw road, throughout east main and any smaller roads in the general area. The study confirmed that building the complex would make it worse. I'm shocked that pierce county would continue the project after those results.

This will not benefit the residents of the surrounding area at all. Home values will drop, pollution will increase and impact salmon coming up the Puyallup river and overall be devastating to the farmland and natural in our area.

Thank you,

Scott J. Smith

**Commenter:** Sosa, Shelly

**Source:** Webform

**Date:** 2/21/2024 8:49:00 AM

**Comment:**

I am totally against this development of a bunch of huge warehouses - what is wrong with our City and the people who run it???? What happened to the baseball fields and parks that this was originally slotted for - isn't the money from the Fair enough???? Now we have to have huge warehouses everywhere - NO

**Commenter:** Spadafore, Angela

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

Van Lierop Farm has so much history in the valley it cannot be surrounded by warehouses. A preferred alternative that ensures only buildings to the north of the park should be drafted in the Final EIS, if a "no build" is not possible. The warehouses F and G cannot be built as they interfere with the view of Mt. Rainier and destroy the beauty of this farmland. I often visit the daffodil trail with my sister in this area and we have been looking forward to frequenting Van Lierop Park when it is completed. This park is so unique to offer the perfect view of Mt. Rainier that it cannot be allowed to be diminished. Thank you.

**Commenter:** Spencer, Danielle

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

I know you are going to build more warehouses but only build a couple more we don't need more traffic and trucks trying to use Shaw road. Also the noise to this area would be terrible as well as people who would let want to live next to it and take away value from homes in the area. Not to mention make the van leirop parks smaller. Please look into only building four at the most.

**Commenter:** Stamon, Kris

**Source:** Email

**Date:** 3/8/2024

**Comment:**

Dear City Staff

Please find attached public comments to the Draft EIS for the Knutson Farms project. I hope these are able to be considered in the Final EIS.

Thank you

Kris Stamon

Puyallup resident

Public Comment respectfully submitted by Kris Stamon. Puyallup citizen. March 7, 2024

- Includes this 4-page comment narrative plus 3 drawing exhibits.

# SPECIFIC COMMENTS TO THE DEIS DATA

## Missing EIS Authors / Contributors:

There are other impacts to the site that do not appear to be included in the DEIS, these include:

### I. Agricultural Land Protection Review

The amount of protected agricultural land required to be designated by Pierce County zoning and/or the City of Puyallup's GMA does not appear to capture the land in this local region. NOTE, THAT THIS IS NOT PARCEL SPECIFIC, BUT QUESTIONS THE PROPERTIES IN THIS APPLICATION AND THE OTHER SURROUNDING PROPERTIES IN A 2 MILE RADIUS. The historic nature of the land contributing to the community and the existence of prime farming soils should trigger a re-review of the agricultural protection of the area.

These or other relevant resources related to Agricultural Land Protection should be consulted:

- Washington State Conservation Commission, Farmland Protection and Land Access
- American Farmland Trust
- Washington Farmland Trust

I also question the compliance with RCW 36.70A, relating to adequately defining the amount of land for conservation in this region. Looking at the massive development of warehouse and industrial uses on previous agricultural land since 2009 from Kent to Puyallup provides a clear and stark picture that previous/current allocations of protected agricultural land are not satisfactory. This issue is much bigger than this specific project. We cannot just assume the current planning regulations are the baseline! These are built upon poor planning implementation from 20 years ago, most likely put into place by individuals with monetary conflicts of interest.

### II. Local Indian Tribes

The Puyallup Tribe of Indians will be impacted by downriver stormwater and pollution impacts. This tribe should be consulted with their findings of impacts included in the EIS.

### III. Surrounding Jurisdictions

The traffic impacts, and possibly others, would seemingly create further mitigation that the DEIS is not capturing. Sumner, Orting, Alderton, Edgewood, and South Hill should all be consulted on the impacts that this proposal may cause to their infrastructure and communities.

## TRAFFIC

### I. Traffic Counts, pg: 4-306

Traffic counts were collected in 8/2021 and 6/2022. These times do not accurately portray the traffic in this area and must be re-counted. Otherwise, the impacts shown in this study are not accurate.

The following data is built on faulty/questionable assumptions:

- Traffic counts adjusted based on the derived Covid-19 adjustment.
- Traffic counts occurred in the summer. My experience on these roads over the past 20 years is that the summer is the lightest time of the year. Regional activities during the fall through spring that contribute to REALLY BAD existing volumes on a daily basis include:
  - o School: Shaw Road ES produces congestion in addition to parents and buses commuting to the other surrounding schools. Congestion during these times is currently horrible.
  - o Commuters: There is a noticeable increase in traffic congestion during the non-vacation season (not summer).
  - o Alternative bypass route: Shaw Road and E Pioneer have become bypass routes to access South Hill and Graham. This is caused by over congestion of the other north-south corridors (Hi 162, Meridian, 94<sup>th</sup> Ave E, Woodland Ave E, etc.). As a result, Shaw Road from the intersection at E Main Ave to 39<sup>th</sup> Ave S and further is often a continuous line of cars at all times of the day and evening.
  - o The Fair: When large events occur at the fairgrounds, not just the main fair, Shaw Road is used even more heavily as a bypass route. Again, this does not occur on a limited timeframe due to the continual large events the fairground hosts year-round. These events should also be captured in the traffic data.

The recalibrated traffic counts on May 4, 2022, from 4:00-5:00 pm do not capture the impactful items listed above. The analysis should be updated to allow for re-measuring of existing traffic counts ideally over a diverse duration of time in lieu of a single date.

## **II. Queue Lengths at Intersections & Travel Times, pg: 4-309**

Due to the overwhelming volume of traffic on Shaw Rd, I would challenge the accuracy of a simulation model portraying an accuracy of these two items. On a daily occurrence, there is a steady line of bumper-to-bumper traffic traveling 5-10 mph from E Main to 39<sup>th</sup> Ave. This is stop and go, waiting multiple rounds of traffic lights to get through intersections. The analysis should be updated to utilize actual observation and monitoring for the queue length and travel time data.

## **III. Background Traffic Growth, pg: 4-310**

Trips generated by specific future projects should be re-evaluated. I believe there are other pending developments in the region that should be included. The analysis should be updated to include more future development projects that will affect the projected data.

## **IV. LOS and Delay, Queue Lengths, pg: 4-316**

These analysis results of existing conditions do not appear accurate based on actual observations of traffic, (noted above).

## **V. ADDITIONAL OBSERVATIONS:**

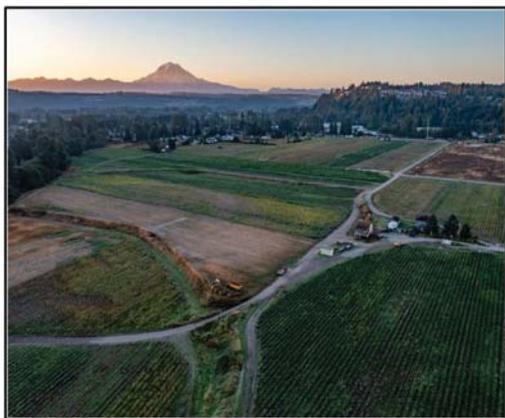
- a. 4-321: The note that adding traffic volume by the project has a counterintuitive reduction at several intersections seems like a red flag. I believe this means that a scenario of southbound PM commuting traffic along Fryar Ave/Traffic Avenue will just stack up further to the north and that congestion will just last longer because the commuter will get slowed down by further upstream traffic lights and stop signs. This is an unacceptable impact caused by this project.
- b. 4-332: Figure 4-67, it should clarify that the widening of the road needs to maintain the sidewalk and street trees.
- c. Speed of large truck movements: are these embedded in the model analysis? I observed a semi-truck last week at the intersection of Shaw Rd and Pioneer. The truck was traveling southbound on

Shaw and making a right turn onto Pioneer. Due to the size of the truck and maybe the weight of the load, the truck had to swing out to the adjacent lane and almost took the entirety of the green light to complete the turn. All of the traffic behind this truck missed the entire light sequence.

- d. Commensurate volume to capacity: I feel that this project is taking up any and all capacity of the existing and improved infrastructure in the region, which will result in future projects triggering traffic impacts that are not necessarily commensurate to their scale.
- e. Quality of pedestrian commuters and recreation: This area is currently a pleasant place to walk and bike. This will increase in the future when Shaw Rd improves to add sidewalks and extend bicycle lanes. The addition of large semi-truck traffic will significantly reduce the experiential quality. More improvements should be required to offset the safety, noise, and pollution directly impacting pedestrians on the adjacent sidewalks and bicycle paths. As proposed, I would change my use of these facilities by avoiding them... not what we want in Puyallup.
- f. Lastly, the scale and type of this project does not fit with the surrounding infrastructure. The mitigating impacts proposed do not appear to be significant enough.

## PROPOSED ACTION TO REVISE ALTERNATIVES

### PREFERRED ALTERNATIVE – NO ACTION



This picture really does capture the reason that this land should be protected from irresponsible development.

Native agricultural land containing **prime soils must be valued and protected** as a sensitive resource to our region! The loss of these areas in the valley by development is an irreversible act that **must be avoided**. This entire property is identified as prime agricultural land per the Washington State Department of Agriculture.

**THE PREFERRED ACTION MUST BE NO ACTION!**

### ALTERNATIVE – FURTHER REDUCED INTENSITY

If the law prevents No Action from occurring, I strongly recommend the City to revise the Alternative Plan to **further** reduce the intensity of industrial development. The reasoning for this is stated above and hopefully the requested new data will be collected and included in the Final EIS that will further support this case.

OPTION A: Not Preferred: Maintain the 1.7M warehouse area as included in the DEIS, but redistribute the open space as shown and eliminate access onto 80<sup>th</sup> Street E.

OPTION B: Preferred: Reduce warehouse area to 1.0M (max). This option achieves the following:

#### 1. Increase the Amount of Preserved Agricultural Land

The 43 acres identified in the Puyallup UGA and included in Alt 2, seems inadequate. I am hopeful that staff can identify additional supporting resources and data that can increase this quantity of land.

## 2. Create Contiguity of Open Space to be Impactful

The preserved agricultural land, wetlands, floodplain, view corridor, utility easements, and the other buffer areas should be required to be located together and contiguous so this open space can achieve:

- Agricultural land is located adjacent to other neighboring agricultural land and has accessibility to be farmed.
- Open space is used as a buffer between Industrial Uses and the neighboring residential, Van Lierop Park, and Farm 12.
- Open space around the park will help with impacts by noise, visual, and pollution from the industrial use.
- The view corridor from the park to Mt Rainier is as important as the view from the park to farmland and the river corridor.
- Open space can be used for the trail connector. The proposed connecting trail through the middle of the development is not acceptable.

## 3. Orient Industrial Complex Along the Railway

The existing railway along the north edge of the property is a preferred element to associate with the industrial use. Therefore, concentrating any warehouses and truck traffic along this edge of the site is preferred.

## 4. Reduce Traffic Impacts

The quantity of warehouse area should be further restricted to prevent the undue destruction of our regional infrastructure. This use is not compatible with the site and surrounding region. But if some warehouses must be constructed, then:

- The warehouse total area should be reduced to a maximum of 1 million square feet.
- There should be no vehicular access allowed from this development onto either 134<sup>th</sup> Avenue or 80<sup>th</sup> Street E. While the impacts to intersections may be minor, these two roads are narrow rural roads that would need to be widened to support any traffic load from this project. Additionally, 80<sup>th</sup> Street E is lined by single family residential, added traffic and trucks will deteriorate their quality of life and significantly reduce their property values.

The proposed site approach would also concentrate all truck access to only the Shaw Rd and 5<sup>th</sup> Ave connector. This should have a positive impact to the surrounding road network by focusing trucks to travel from this point to the most immediate highway access points.

The City of Puyallup should be commended as caring enough about our community to fight for the right to author the EIS. The city staff have done an outstanding job producing a thorough report, it is unfortunate that such focus must be allocated to irresponsible developments which detracts time and resources that could be used for other community benefits.

Thank you,



Kris Stamon



AS-SUBMITTED WAREHOUSE DEVELOPMENT  
2.65 Million sq ft of Warehouses



DEIS ALTERNATIVE 2 - "Reduced Intensity" - Variation

OPTION A

1.7 Million sq ft of Warehouses



**PREFERRED ALTERNATIVE - "Reduced Intensity"**

1.0 Million sq ft of Warehouses

**OPTION B**  
 **PREFERRED OPTION!**

**Commenter:** Stamon, Kris

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Concerned with warehouse development, Traffic impact concerns

**Commenter:** Stanley, Stephanie

**Source:** Webform

**Date:** 1/12/2024 10:19:37 PM

**Comment:**

The traffic is already so bad with no end in sight and this will 4x the traffic including more semis and noise disrupting the area as well as making the intersection to enter 410 even worse than it is!  
Please use common sense and realize this isn't a good fit for that small, congested area

**Commenter:** Stueve, Bridget

**Source:** Webform

**Date:** 1/4/2024 8:44:19 PM

**Comment:**

Please reconsider this plan. Please look into nixing warehouse E,F,G and making that into an expanded part of van lierop park. This gives more open space. Doesn't block all of the beautiful views from the park and frankly the roads cannot handle the traffic from Farm12 let alone any additional traffic. This will impact not just Puyallup but the local school nearby, the traffic in and out of Orting and create a huge environmental burden on our farmland and runoff into the Puyallup River nearby.

**Commenter:** Sundahl Jr., David

**Source:** Email

**Date:** 3/14/2024

**Comment:**

We want the reduced intensity option.

They're already putting something like 190 apartment units on Shaw and Pioneer, with additional commercial space.

With those units built and if all warehouses built, this whole area is going to see much worse traffic, and we lack the infrastructure to accommodate it!

David and Jessica Sundahl [attached image of reduced intensity alternative]



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses

**Commenter:** Sutton, Nathan

**Source:** Webform

**Date:** 3/11/2024

**Comment:**

Dear City of Puyallup leadership, Thank you for the opportunity to provide input on the Knutson Farms DEIS. I would like to log a number of concerns about the proposed warehouse construction project, in addition to the foremost concern about the risk of pollution to the Puyallup River. 1) Traffic congestion: Increased traffic would further complicate drop-off and pick-up at Shaw Road Elementary School. We don't have a reliable bussing option for our children, and traffic is already heavy around the school. Moreover, increased warehouse traffic would be compounded by traffic related to the proposed apartment complex at Shaw and E. Pioneer. 2) Impact to Farm 12/Step by Step: Farm 12/Step by Step has become an important cultural landmark in Puyallup, which also provides a critical public service. The construction of warehouses immediately adjacent to the location would adversely affect the experience of workers, clients, and patrons, which the leadership has otherwise carefully cultivated. 3) Even the "reduced intensity" proposal, while preferable to the original, would obscure the pristine southeasterly views along E. Main. I would lament that permanent loss as a bicyclist who regularly uses that route. Of course, occupants of the buildings on the west side of E. Main would also lose their view. 4) A solution to a problem that doesn't exist? I haven't been able to find evidence that warehouse space is in high demand. Thank you, Nate Sutton

**Commenter:** Talbott, Robin

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

I live in Ocean shores, I grew up in Edgewood. My daughter and her growing family live on 80th St. I visit them regularly in my little camper. 80th seem to be like a freeway now, not sure what it will be with all the trucks going through. I have 3 precious granddaughters, I worry about now. There's a lot of land in already established business parks. Use that first. Empty warehouse's in all of them!  
Thank you!

**Commenter:** Talbott, Chris

**Source:** Webform

**Date:** 3/16/2024

**Comment:**

I would love to see the Foothills Trail and the Riverwalk Trail connected. I occasionally ride my bike here and that is very important to me. I think that it could be reasonable to allow 3 or 4 warehouses in this location if traffic revisions take place. It would be sad to see the area completely covered in warehouses, but I do support more local jobs. Alternative 2 would be favored over the proposed action. I think it should be a priority to maintain the Mt. Rainier view from the park as well. I also worry about pedestrian and bicyclist traffic along 80th and the trail if warehouses are built along that road. Thank you and God Bless.

**Commenter:** Taylor, Tom

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Please consider the reduced intensity alternative pictured below for the Knutson development project. The recent DEIS brought to light significant concerns regarding increased pollution and traffic congestion that would result from full warehouse development. It is not evident that proposed mitigation measures are sufficient for either concern. The area is currently gridlocked at rush hour even without the warehouses and other planned development projects. Of course, you are aware of the water runoff toxicity issues impacting salmon survivability. That concern will likely force increasingly stringent mitigation measures in the future. Puyallup will have a big mess on their hands post development if those issues are not mitigated now.

It is also clear that the community is against all development in that area as evidenced by public comments and the petition with over 5,500 signatures. I realize that may be an unattainable bar to meet, but the DEIS provides the city with information and license to require appropriate mitigation of the noted problems at this time. In fact, that is the city's obligation to its citizens.

Thank you for your efforts.

Tom Taylor

RODESCO Resident

**Commenter:** Taylor, Tom

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Mitigation concern

**Commenter:** Teter, Sandra

**Source:** Webform

**Date:** 2/28/2024 11:56:00 AM

**Comment:**

Our land here is precious. Our communities want to preserve what we have. do not sell out to the companies that want to build and reduce the beauty of our environment. So much has already been taken away!!! No more

**Commenter:** Teter, Eber

**Source:** Webform

**Date:** 2/28/2024 12:00:00 PM

**Comment:**

Knutson Farms: This community needs your investment in our beauty and environment, not your profits!!!! You try to project a wholesome image of your company and if you do this, it is a LIE

**Commenter:** Thompson, Fay

**Source:** Webform

**Date:** 2/3/2024 12:48:35 PM

**Comment:**

We have enough warehouses surrounding Puyallup. All the farm land is gone. Please stop building so many warehouses here. Try another location.

**Commenter:** Treakle, Terri

**Source:** Email

**Date:** 12/15/2024 2:33:00 AM

**Comment:**

This is a horrible idea. It's going to ruin Puyallup Valley like it's already been ruined with all the warehouses that have been built. Puyallup used to be a beautiful area and now there's warehouses all over and used to be prime farmland. It will ruin Van Lerops park and obstruct a beautiful view. It will ruin more wildlife homes and just is a horrible idea. I hate that they are ruining Puyallup Valley.

**Commenter:** Treakle, Terri

**Source:** Email

**Date:** 2/13/2024

**Comment:**

You know no matter what we say they're going to do exactly what they want anyways but I'm here to tell you that it's going to impact our environment horribly. there's already enough traffic in Puyallup it's gonna make it 1000 times worse not only that is going to destroy wetlands salmon runs and your not going to be happy until this whole valley is freaking warehouses and new housing. We need to leave the Puyallup Valley alone. They've already destroyed all the farmland that it used to be when I moved here on in the name of progress. It's disgusting that they want to take away all the beauty of natural land and develop warehouses on it. I hate this idea. The very reason I moved here was for all the beauty and not a concrete jungle but you want be happy u til it's all just like California and every other big city.

**Commenter:** Trott, John

**Source:** Webform

**Date:** 2/27/2024 9:40:00 AM

**Comment:**

What happened and the beginning of the trail were the red gate is that said it was up for future development of soccer and baseball fields,I guess that has changed

**Commenter:** Trott, John

**Source:** Email

**Date:** 12/23/2023 1:12:00 PM

**Comment:**

It's a flood plain shouldn't open space count for anything make it in to a park or lease it out as a farm

**Commenter:** Turner, Cass

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Can you please just NOT do this? We don't need anymore warehouses in this neighborhood. Let them put them in North Sumner, with all the rest. We have the ones off Shaw and now some on Main St East. Ridiculous. Have you been trying to get up Shaw Road from the freeway at Rush hour? It's impossible. If ANYTHING, why don't you widen Shaw Road to handle the LOCAL traffic. We don't need anymore trucks taking up space in this very small, congested area

Besides, I live near the current warehouses on Shaw and I have NEVER SEEN a truck go in or out of there. That tells me they are EMPTY and you don't even need the ones you have. Why put in more?????

Why not do this?: This is much more preferred. (picture below)

thank you

Cass Turner, resident 1/2 mile away [image of preferred alternative: reduced intensity alternative]

**Commenter:** Turner, Cass

**Source:** Email

**Date:** 2/27/2024

**Comment:**

I don't understand why we need so many warehouses. Have you looked at the north end of Sumner lately, or even the Kent Valley? This is horrible. There are plenty of warehouses all over the south end of King County and North Pierce. I live by and have driven by the new Knutson warehouse and it appears to be empty. It's been open for months, and I've NEVER seen a truck or car there.

Furthermore, Pierce County keeps adding more housing, more apartments (and now warehouses,) yet they rarely expand the roads. They may have added the bridge on Shaw Road over the train tracks, but traffic backs up all the way to Hwy 410 during rush hour in the afternoon. What will the impact of all those trucks be on Main Street and Shaw Road? It will be a disaster! And so some traffic diverts to Hwy 162 and the County is planning on building a road up to Tehaleh with no plans to widen Hwy 162. When will Puyallup or Pierce County start looking at its infrastructure instead of tax dollars? Traffic here is a nightmare. Please don't make it worse by eliminating open space in favor of warehouses.

Cass Turner, resident

**Commenter:** Turner, Jennifer

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Good morning,

I'd like to express my preference for zero more warehouses at the Van Lierop Farm site in Puyallup, but since that's an unlikely outcome, my second preference would be for the Reduced Intensity with no more than 3 additional warehouses, increased salmon and wetland protection, and preserved agricultural space. An image for my preferred alternative is attached. Thank you for considering citizen preferences.

Jennifer Turner

**Commenter:** Turpin, Theresa

**Source:** Email

**Date:** 1/24/2024

**Comment:**

Thank you for the opportunity to review the EIS for the Knutson Farm warehouse proposal. I am not a resident of Puyallup, however, I am an avid cyclist and user of the Foothills Trail and the Riverwalk Trail. Before retiring, I did work both as a private consultant and as a public sector employee. I did provide assistance in Puyallup, Sumner, and on the Foothills Trail itself.

First, some background on the recreation and trail along the building site - this was proposed in 2016 as mitigation for the project, before there were other proposals connecting Riverwalk to Foothills. The trail was written into the binding site plan, the developer had to pay for and construct the trail along the development and once the trail was completed, deed it to Pierce County. It was at that time, the only way to facilitate the Riverwalk/Foothills connection.

That being said here are my comments based on reading the document

1. I do like the suggestion of moving the pedestrian path to a more aesthetic setting - I think that is a great option. Glad to see that 62 acres (basically 1/3 of the 188 acre site) is being set aside for open space.

2. Speaking of 62 acres of open space, has anyone considered/suggested that the developer create a wetland bank on part of that open space - I thought it used to be 40 acres needed to create a wetland bank (it may have changed). It could off-set the project impacts to the existing on-site wetlands, be a good thing for the Puyallup River, and should provide left over credits the developer can sell. Also many years ago, as a consultant, I did the permitting for the Shaw Road bridge over the railroad at the Knutson farm - it was pointed out that there were several failing drainage tiles installed on Knutson's farm so it isn't that crazy to try and do a wetland bank in that area. It works well with moving the trail which should help with plans to infiltrate the roof run off into the open space area.

3. I think Alt 2 - the reduced warehouse area (the zoning allows several uses, including wineries and distilleries which could be compatible with Farm 12) is definitely the better/preferred option, although it would be good to have a hybrid of Alt 1 and Alt 2 by adding the railroad component to Alt 2 - maybe modify the railroad component eliminating potential conflicts with the trail users and the railroad by working to keep the rail connection on the west side and if possible (it may not be possible) and eliminate or adjust the south side rail connection. As I recall, Claudia Peters with Pierce County Parks many years ago, stated the property underlying the railroad is owned by Pierce County Parks.

4. The outfall - what a mess - they do need a good engineering plan, maybe work with Pierce County Public Works, WDFW and tribal fisheries to develop a better engineered outfall possibly adding large woody material.

5. I am not sure why Roundabouts were not recommended in more places - I am not a traffic engineer, I know there is specific protocols for when a Roundabout is warranted. Prior to retiring I know WSDOT Olympic Region had plans for a few of them including one of the intersections in Sumner reviewed in this study, but it was only in the planning stage.

Theresa Turpin, AICP



Tacoma, WA

**Commenter:** Valencia, Emilio

**Source:** Webform

**Date:** 1/17/2024 6:18:20 PM

**Comment:**

No action

**Commenter:** VanLierop, Anne

**Source:** Webform

**Date:** 1/10/2024 6:28:55 AM

**Comment:**

I'm a stakeholder watching this crime unfold on that precious property for several decades now. If you don't stop it, it's gone forever. How can you master plan that? How can you develop land that you don't know? It belongs to the mountain, valley and river ...we are only temporary stewards. Warehouses will never thrive on that soil. Ever.

**Commenter:** E Vernon, Pamela

**Source:** Webform

**Date:** 3/1/2024 5:49:00 PM

**Comment:**

I believe you are doing the community a great disservice if you allow the proposed warehouse park to grow to more than 3 additional buildings next to the Van Lierop Park. I would ask the City of Puyallup to consider the impact this development would have on the citizens of Puyallup. First, the traffic in this area is now congested with the bike path, Farm 12, and the park users. To add additional large trucks in this area is only asking for vehicle accidents, as well as possible injuries and deaths with the considerable amount of pedestrians crossing and using the road. Second, the wild life that is attracted to the farm and fields, the geese and birds, would disappear with no more large areas to land and feed. This would be a crime against our children to have no place to see, enjoy and connect with the experience of seeing these amazing wild birds. Third and probably most important, this would absolutely destroy what has become a "vital family recreational area" in Puyallup. Open, inviting for families, the elderly and active people of all ages. It is a beautiful area not only because of the fields of the park but the farm lands and views of Mount Rainer. To change the small town feel of this area, the "welcome to Puyallup", and "here is what we have to offer families", to be nothing more than a spot between buildings in the middle of a urban sprawl would be criminal. Puyallup is a small town. It should never loose that attraction. That feeling of belonging and home. As we grow and expand we must keep what Puyallup is and should be, foremost in every consideration of how we develop. This is too important of an area for our community, to destroy it with this development!

**Commenter:** Vines, Forrest

**Source:** Webform

**Date:** 1/2/2024 12:26:41 PM

**Comment:**

Consider the impact of the project on views of the valley from surrounding hillsides - consider roof construction that does not reflect light and helps buildings blend in to their surroundings. Consider traffic impact between Traffic Ave in Sumner and 410/167, which already backs up during peak commutes and may require additional mitigation.

**Commenter:** Vittetoe, Barbara

**Source:** Email

**Date:** 1/30/2024

**Comment:**

I am opposed to covering more of our valuable and disappearing farmland with warehouses, especially in the floodplain of the Puyallup River. A better place for the warehouses would be on either hilltop, such as the Graham or Fredrickson areas. An alternative use for the farmland would be to create a wetland or flood zone to capture excess water. Someday, we are going to wish we had that fertile soil back again to use in growing local food. Once destroyed, that rich soil will be very difficult to restore. Warehouses can be built anywhere, farms cannot.

Barbara Vittetoe

**Commenter:** Walker, Staci

**Source:** Webform

**Date:** 1/6/2024 8:41:24 PM

**Comment:**

This is very sad. Downtown Puyallup does not have the infrastructure for this and sitting in years of construction waiting for "road expansion" and this concrete monstrosity to be built is terrible. Letting warehouses be built in the valley and lack of infrastructure to hold this will make Puyallup lose its character. People will leave and warehouse traffic jams will win. \$\$\$\$ over quality of life- very sad, and disappointing. Do better Puyallup.

**Commenter:** Wallace, Scott

**Source:** Email

**Date:** 3/14/2024

**Comment:**

Hi there. I am asking the council to consider the preferred alternative plan.

A better "reduced intensity" alternative is shown in the attached picture. This alternative reduces the number of new warehouses to three (1M sq. ft.), reduces traffic impacts by 60%, protects farmland surrounding Van Lierop Park and Farm 12 that connects to the floodplain, protects wetlands, reduces the stormwater impacts to salmon, and concentrates the new warehouses in one area near the existing warehouse. Thanks for your careful consideration.

Scott Wallace [image of reduced intensity alternative]

**Commenter:** Waltier, Christie

**Source:** Webform

**Date:** 2/19/2024 7:11:00 PM

**Comment:**

Hello, My primary concern is around semi-truck traffic around the Foothills trail, the Farm 12 property, and Van Lierop Park. Any option that would keep trucks off of E Pioneer and 8th Ave SE/80th St E would be better than the current plan. It's critical that we maintain a stroller/bike/pedestrian friendly road near the park and trail! I would be in favor of the alternative to reduce the new warehouses from 7 to 3, reduce the amount of traffic, and protect the areas directly adjacent to the park.

**Commenter:** Wambold, Bailey

**Source:** Webform

**Date:** 1/7/2024 8:21:55 AM

**Comment:**

This plan is an ecological tragedy. Pierce County and Puyallup especially are rapidly losing important green spaces that provide ecosystem services which make our area a healthy and safe place to live. Developing on an already-crowded flood plain in an era of climate change ensures that if and when there are extreme weather or flooding events, the damage (physical and economical) will be even greater and harder to recover from. The increased traffic around the region will contribute to air and water pollution, harming sensitive wildlife populations, like salmon. There are many underutilized warehouse spaces in the area that could be remodeled instead of ruining some of our increasingly precious farmland. Over 70% of Pierce County's farmland has been developed since 1950, making it hard for the local community to produce affordable, healthful food to sustain itself and not be wholly dependent on "imported" goods. This project will harm water and air quality, destroy increasingly-rare wildlife habitats, contribute noise pollution and eyesores to the area, decrease the ability to enjoy local parks in the area, and is generally unsustainable. Any new development in Puyallup should be wholly dedicated to affordable housing.

**Commenter:** Wambold, Bailey

**Source:** Webform

**Date:** 1/20/2024 12:35:30 PM

**Comment:**

I am writing to request the City of Puyallup recommend the "No Action" alternative to the current project proposal. The increased truck traffic in the area will require expensive infrastructure improvements and still result in greatly reduced level of service on adjacent roadways. Additionally, a recent presentation on Puyallup's Comprehensive Plan update noted that community survey responses feel the area around the project site should be used to build needed housing, not industrial warehouses.

**Commenter:** Wambold, Bailey

**Source:** Webform

**Date:** 2/5/2024 5:26:17 PM

**Comment:**

Please choose the 'No Action' Alternative. On the short drive from Puyallup to Fife along Valley Ave. E and 70th Ave. E, there are at least 9 warehouses with "Available for Rent/Lease" signs. There are many other warehouse spaces available in the area as well, such as those on Canyon Rd. and in Fife and Sumner. Puyallup and the greater valley area do not need any more empty warehouses, especially when there is a severe housing shortage. The 'No Action' Alternative must be recommended to if we wish to protect the health and vitality of our community and local environment.

**Commenter:** Wambold, Bailey

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Thank you. I'll try and be quick. My name is Bailey Wambold, B-A-I-L-E-Y. Wambold is W-A-M-B-O-L-D. And just to reiterate what everyone said, thank you, City, so much for putting this together. It was so informative. I know a lot of work went into it and I (inaudible) your stance on it. Just some numbers real quick that I could find before I came here today. This is from 2021, but in the decade before 2021, Pierce County lost over 10,000 farmland acres, and we've lost over 75 percent of it since the 1950s.

I like that Ashley brought up the food banks because I'm a volunteer at a food bank, and we get so much great produce from our local farms and they provide an enormous service to our clients.

And food insecurity is only growing in Pierce County, and we cannot rely on shipping in food. We need to be able to grow our own. We can't be getting rid of farmland. It's too precious. And yeah, I'm also curious as to who would maintain the trail that is in the alternative, if it would be private lands, what's that going to look like? And then also, this plan goes against Pierce County's comprehensive plan. They describe a county-wide need to maintain and enhance natural resource-based industries by preserving and enhancing the agricultural land base. And this just seems to fly right in the face of that. And honestly, our Puyallup companies also want to support farmland and also promote access to healthy food, which I think this is just taking us steps away from getting more of our community better, easier, more affordable access to healthy food. And I'm just going to leave it at that. But thank you.

**Commenter:** Wambold, Bailey

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns and agricultural impacts

**Commenter:** Warhol, Carolyn

**Source:** Email

**Date:** 2/3/2024

**Comment:**

Here are my comments and concerns about the future of the Knutson Farms area:

I would really like the walking/biking trail to stay along the river as much as possible. I am glad to see that is the intent in Figure 4-56. Yes, parts of it may flood but low points on the existing Riverwalk flood now, i.e. at the blue cube buildings, at the 11th St. NW trailhead, and further west on the trail heading for the end of the trail at 18th St. NW. The Park Department does a good job of clearing away the mud when the waters recede. Also the trail itself seems to hold up very well to the floodwaters and the mud scraping.

The view of Mt. Rainier from Dixie's swing needs to be maintained. Please, please, please don't let a warehouse be built that interferes with that view. The whole of what exists now in Van Lierop Park is aimed at that view.

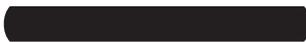
Figure 4-57 showing rail tracks among the warehouses is very concerning along with the dotted yellow trail line skirting the warehouses. People are always looking for the shortest route between two spots. Although a high fence might detour most people, there will always be those that will find a way to cross between the warehouses to get to Van Lierop Park. Train tracks just seem to be a disaster ready to happen. The traffic in that whole area will be ramped up (it is already a mess on weekends now with Farm 12 and the Foothills Trailhead) but to add train cars coming and going would be very dangerous. As for the dotted yellow line proposed pedestrian trail, it is too close to the buildings to be visually pleasing and too close to Van Lierop Park for someone not to be tempted to figure out a shortcut to the park.

Thank you,

Carolyn Warhol

Member of the Friends of the Puyallup Riverwalk

President of Daffodil Valley Volkssport Association



**Commenter:** Wertz, Amy

**Source:** Email

**Date:** 3/12/2024

**Comment:**

In regard to the Knutson Farm Warehouse District, it is my desire to see a reduced minimized warehouse plan that will include open green space and park usage. It is also important that there is a direct connection from the Foothills Trail to the Van Lierop Park to the Riverwalk Trail. I am very concerned that the connection that is currently proposed is part of a natural flood plain that will significantly impact the usage and maintenance of the Trail connection. It is also important that the Trail connection is aesthetically pleasing and located near roads and warehouses.

Sincerely,

Alma Wertz

████████████████████

██

██████████

**Commenter:** Wetzel, David

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Concerned with KF warehouses, Traffic concerns, Impervious surface concern

**Commenter:** Whiteside, Kristal

**Source:** Webform

**Date:** 1/11/2024 3:36:47 PM

**Comment:**

Do not build the proposed development (No Action)

**Commenter:** Wickett, Jody

**Source:** Webform

**Date:** 1/17/2024 8:43:47 AM

**Comment:**

As a person who uses the van leirop park, farm 12 and the trail near there I am opposed to the warehouse project covering all the land in that area - it will change the whole environment in a very negative way. I would vote for no action but at minimum the least impactful course of action. I realize I no longer live in the city limits of puyallup (this is recent) but I still view this area as my community from the other side of the valley and would like the area to stay usable for families and wildlife especially. Adding semi trucks on these roads will be horrible for everyone -

**Commenter:** Wickoren, Michael

**Source:** Email

**Date:** 3/14/2024

**Comment:**

So sad that future generations will not have kids playing in the dirt or even farmers farming! So sorry to see, the time will come when the decisions you all make today will hurt the sustainability of humans to live here

Please don't let this come to our community

Thank you for thoughtful consideration , Mike Wickoren

**Commenter:** Wilkins, Alixandre

**Source:** Webform

**Date:** 1/17/2024 10:17:06 PM

**Comment:**

Please consider the environmental impacts and traffic congestion that this building will cause. Alternative 2 is a good step in the right direction but all must be done to limit the environmental harm and traffic congestion that will occur.

**Commenter:** Willard, Tiffany

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

I love riding my bike with friends and family on the Foothills Trail. It is also so refreshing to walk this area after a visit to Farm 12. Ever since Van Lierop Park was constructed I have been looking forward to a connection to the Riverwalk Trail, however, this connection cannot be surrounded by warehouses. The beauty of this trail is an escape from city life and concrete buildings. I have taken so many beautiful pictures of my nieces with the wild lupines and view of Mt. Rainier, it would be devastating if this view is tampered with warehouses. Please do not allow warehouses to interfere with the beauty surrounding the park and trail. NO more warehouses should be added to this area, but absolutely the warehouses F and G cannot be built that would interfere with the trail and the Mt. Rainier view. I do agree with Alternative 2 that A and C should not be built so close to the shoreline of the river and trail connection there as well. I also worry about traffic further negatively affecting South Hill. It already is very difficult at times to travel from South Hill to the valley and downtown Puyallup, this would make it harder. People in South Hill, Orting, and Graham would be adversely affected from more traffic congestion on Shaw Road, Meridian, and Orting Highway. I am also concerned about the safety of my nieces who live walking distance from the park if semi-trucks are moving in and out of this area. Thank you for providing me a space to put in my voice opposing this warehouse development.

**Commenter:** Williams, Teena

**Source:** Webform

**Date:** 2/21/2024 4:41:00 PM

**Comment:**

The EIS is deficient in being the least impactful alternative. Knutson already owns land it could build warehouses on instead of paving over the most fertile farmland in the western US. Make Knutson consider putting a gigantic warehouse where they have a flower farm now. Oh wait, they make a crap ton of money selling people overpriced pumpkins and hayrides at that property, not really farming at all. 23 year puyallup resident and Hate seeing the loss of our precious farmland.

**Commenter:** Williams, Maren

**Source:** Email

**Date:** 3/15/2024

**Comment:**

Good afternoon. My name is Maren Williams. I work in downtown Puyallup and own a house here in the Puyallup valley. Growing up in Fife showed me, in person, what warehouses do to a city. Adding this many warehouses to Puyallup, especially a part of a city with no additional infrastructure in place could be disastrous. Puyallup is a special place. And we should keep it that way.

Puyallup holds a special place in my heart as the hometown of my mom's family. The Gronens were prominent members of the community, owning daffodil farms, working the land, and literally shaping it by engineering the levy system that keeps the flood waters from our homes in the valley. Today, I own the house my grandparents built back in 1958 with the hopes that it will be my retirement home.

But I didn't grow up in Puyallup. I grew up in Fife. Across the river, Fife used to be very much like Puyallup- at least 40 years ago. Plenty of farmland, good businesses, and good neighbors. A lot of that has changed as the warehouses moved in, one by one. It made sense for the location. Fife has very easy access to I5, and many people to fill jobs as needed. While the warehouses were built, the infrastructure was built up around it. Streets were widened, lanes were added, but nevertheless, the traffic started to pile up. What used to be fertile farmland was paved over. The pollution is significant.

Unlike Fife, Puyallup has no direct access to freeways, especially where the warehouses are proposed. Unlike Fife, no freeway already runs through Puyallup. Because of this, traffic will increase dramatically- even with possible road improvements. And in the end, improving the roads will do nothing to mitigate the pollution caused by thousands of extra trucks.

I don't think investors from out of state realize what is at stake besides their own profits. I would like to think we can invest our land, time, and resources toward protecting what is special about Puyallup.

Thank you for your time,

**Commenter:** Williams, Gail

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

I'm just going to say I echo every word that Karen said. No project. No project is my view. So thank you for everything you're doing. And please take into consideration every single thing she said. We're still new out here to Puyallup, so I don't know all these intricate things that she knew. But it is so important. And like she said, once this land is gone, it is gone. It is gone. And for our children coming up, it is gone. And there's no way to even travel now. I work downtown. Trying to get home some nights, especially during the fair, it takes me over an hour. Over an hour. It's just crazy. So, please. You know, whatever you can do just to protect our land. Please. And please, like she said, tell everybody. That's all.

**Commenter:** Winger, Michael

**Source:** Verbal Comment

**Date:** 1/17/2024

**Comment:**

Okay. Well, I'd like to go on record

I think I sent in an email when the project was first

proposed. I don't know if that still counts as a comment on this EIS -- EIS form. It was like several years ago. But anyway, I'd like to go on record as being opposed to the entire project, of course, and I would definitely be in favor of reducing that plan. And I mean, the amount of traffic on Shaw and Pioneer is going to be unbearable. I'm not sure how many -- well, they're talking about several thousand trips a day with semi-trucks, so I'm definitely opposed to that, and I'd appreciate anything the

city can do to lessen that impact. That's pretty much all I have to say.

**Commenter:** Wisness, Sarah

**Source:** Webform

**Date:** 1/3/2024 8:09:00 PM

**Comment:**

Requesting that the building of warehouses in the proposed Knutson Farms be reconsidered. There must be a better, different option than to build warehouses in that location. Other options that should be considered include housing or places for small businesses. Warehouses do not offer places for families to exist and thrive.

**Commenter:** Wisness, Sarah

**Source:** Webform

**Date:** 1/3/2024 8:09:37 PM

**Comment:**

Requesting that the building of warehouses in the proposed Knutson Farms be reconsidered. There must be a better, different option than to build warehouses in that location. Other options that should be considered include housing or places for small businesses. Warehouses do not offer places for families to exist and thrive.

**Commenter:** Wolff, Sara

**Source:** Email

**Date:** 2/26/2024

**Comment:**

Comment on preference of the reduced intensity plan.

I prefer no new warehouses, but if that's impossible, then reduced intensity of course.

Sara Wolff

**Commenter:** Wolken, Ann

**Source:** Webform

**Date:** 12/15/2023 12:31:18 PM

**Comment:**

Is there an opportunity to have an athletic facility here? This seems like a great opportunity to have a community center for youth in this area.

**Commenter:** Woodke, Mark

**Source:** Email

**Date:** 3/10/2024

**Comment:**

Of the three choices, I'd prefer only three warehouses to reduce the traffic impacts, protect farmland surrounding Van Lierop Park and Farm 12, protect wetlands, reduce the stormwater impacts to salmon, and concentrate the new warehouses in one area near the existing warehouse. I live just up Shaw Road and already have a partial view of the current warehouse; the fewer new warehouses I can see from my house, the better. Thanks, Mark Woodke.

**Commenter:** Woodke, Michelle

**Source:** Webform

**Date:** 3/10/2024

**Comment:**

I prefer the alternative with only three warehouses. I actually would like zero new warehouses if at all possible!!!!

**Commenter:** Yanasak, Chris

**Source:** Webform

**Date:** 2/12/2024 9:08:24 PM

**Comment:**

It is a shame to see this happening, for all the reasons many are stating; the loss of valuable Puyallup heritage and agricultural character, the loss of the fog on the farmlands and Mount Rainier in the distance on an early morning Sounder train, and of course all the more measurable negative impacts to our salmon, floodplains, traffic, pollution, etc. BUT if we must have them, the final report should call for the proposed "reduced intensity" option, with 3 warehouses, as the "preferred alternative." This should be the only acceptable compromise.

**Commenter:** Yanasak, Chris

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Hello. If we must have these warehouses, then the “Preferred Alternative” should be the proposed “Reduced Intensity” option: [image of reduced intensity alternative] This is the only way to salvage this piece of Puyallup’s character, protect its floodplains, and reduce the negative impacts of the warehouses.

Chris Yanasak

**Commenter:** Yost, Debra

**Source:** Email

**Date:** 2/26/2024

**Comment:**

Hello,

I vote for the Reduced Intensity alternative way to go.

This will minimize Traffic, farmland loss, storm water runoff, the killing of Salmon, wetland destruction, noise & visual impacts, impacts to the Foothill trailhead, Van Lierop Park and Farm 12.

Sounds like a NIGHTMARE & Not any kind of progress!

Thanks

Debra & Michael Yost

City of Puyallup residents 69 years now

**Commenter:** Yost, Deb

**Source:** Verbal Comment

**Date:** 3/12/2024

**Comment:**

Warehouse development concerns, Traffic concerns, Environmental impact concerns

**Commenter:** Zimmerman, Joni

**Source:** Webform

**Date:** 3/14/2024

**Comment:**

I am completely opposed to this project on environmental, social, and practical grounds, and don't think it should be considered further. What struck me most strongly when I read this EIS is the fact that this property is zoned terribly wrong if this project could even be considered, and the zoning should be changed immediately. It is currently and historically farm land, next to property that is also currently and historically farmland now zoned for residential development. There is no way that residential property should be next to giant warehouses. What were they thinking??? At a time when cities all over the country and the world are reclaiming their river fronts from past industrial uses and abuses and misguided policies of the early 20th century at a cost of millions and billions, why is Puyallup considering taking beautiful riverfront farmland and turning it into an industrial wasteland and a nightmare for future residents to clean up??? And then, to add insult to injury, considering routing trails through it. As if anyone wants to go for a nice hike on a sunny day through warehouses and hear the lovely sounds of diesel trucks?? Why? Pierce County should also be considering the impacts of climate change while considering this project. With climate change, there will be less international and cross country shipping. People will, out of necessity, be making do with less - recycling, reusing etc. Giant warehouses are becoming obsolete. We should be supporting and encouraging the growth of small, local businesses using locally sourced materials rather than the continued mass consumption of consumer goods that require warehouses. And then there are the environmental changes due to climate change. Just this year, the world is worried about the lack of snowfall, the water table, etc. We must preserve what we have. We should be encouraging sustainable and regenerative farming on this land. I am very concerned about the possibility of contamination of the river and killing of salmon. It sounds as though the storm water handling for the existing warehouse has been extremely casual and careless. All it would take would be one incident and there would be no coming back. The fish would be dead. Pierce County and Puyallup should also consider the concept of need vs speculation, and build what the local community needs now and into the future. Presumably, warehouses are more needed closer to I-5 and major railway and shipping channels. Building warehouses here where there is not current demand would add unnecessary truck traffic and contribute to climate change by making trucks make unnecessary trips to and from the I-5 corridor. If the land must be developed for commercial use, it should be for something compatible with farmland, recreation and residential. Create a village with permeable landscaping, retaining and improving at least some of the farmland. It could be an area with small, locally owned shops and manufacturing businesses like custom woodworking, signs, and artist spaces, with parks interspersed. There could be a community garden and perhaps larger plots for farm to table restaurants and local produce stands. There could even be no motorized vehicles allowed throughout - parking could be on the edge, with permeable roads going through with fire access only, bikes and pedestrians, electric carts to carry people who can't walk so far, and even trams from downtown, the fairgrounds, the train station, Safeway, and Farm 12. Then the area would be a place where people would want to take a trail to on a sunny day!

And people might actually want to live near it. We have friends who live in the area, and we have been seriously considering moving to that area, but there is no way that we would if this absurd warehouse project is allowed.

**Commenter:** , Anonymous

**Source:** Email

**Date:** 1/11/2024 8:37:00 PM

**Comment:**

Please do not destroy the little bit of farmland we have left in this community.

**Commenter:** , Anonymous

**Source:** Email

**Date:** 1/11/2024 8:43:00 PM

**Comment:**

Consider the climate and environmental impacts of this proposal to destroy this land and cover it up with blacktop. This is the opposite of environmental sustainment. This country, state and county is continuing to destroy our environment. Your impact statement said something about ensuring the environment is protected, building this project is the opposite. Forget the tax dollars you're focusing on and consider not destroying our county

**Commenter:** , Anonymous

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Where have all the trees gone? Where has all the farmland gone?

Where can the ducks, frogs, mice, raccoons and or deer get their food?

Why all the congested roads?

What about concern for "climate change".

Seems we citizens of Pierce County can't survive unless we're destroying more natural habitat and covering it with more concrete. Why can't we just make the decision to limit any further destruction? Tell the financiers to go somewhere else to build their warehouses. We don't need the \$, we need the nature.

**Commenter:** , Anonymous

**Source:** Email

**Date:** 2/13/2024

**Comment:**

Preferred alternative is most reasonable

## Protect Puyallup

**Commenter:** Puyallup, Protect

**Source:** Email

**Date:** 3/15/2024

### Comment:

Dear City of Puyallup,

On behalf of 5,601 citizens that have signed the below petition, we recommend a preferred alternative that significantly scales back this proposal to mitigate significant impacts.

Link to Petition

<https://chnng.it/ZYh2kpcykF>

Petition Title

Stop the 7 Mega Warehouse Proposal in the Puyallup Shaw/Pioneer Farmland Area

Petition Text

A St. Louis, Missouri developer has proposed turning the Shaw/Pioneer farmland area into a massive industrial warehouse district. This proposal runs counter to the City of Puyallup's land use plan that was adopted in 2009 after years of working with the landowners, citizens, and Pierce County. The developer is trying to take advantage of existing County zoning and ignoring past agreements and the adopted plan for this Puyallup future annexation area. The impacts of the proposal would be enormous: traffic congestion from 8,000 trucks and cars per day, costs to Puyallup citizens to repair roadways, decreased property values, destruction of farmland, incompatibility with the City's Van Lierop Park, and an overall diminishment of land use character and quality of life for the Puyallup community.

Thanks to the nearly 5,000 citizens that have signed this petition and to the City of Puyallup, a Draft Environmental Impact Statement (DEIS) has been prepared to assess the impacts of the project and propose mitigation measures. The public can provide comment on the DEIS until March 15, 2024.

The proposed mitigation in Alternative 2 of the DEIS is a good start. Alternative 2: 1) reduces the number of warehouses; 2) preserves some open space/farmland; 3) limits truck traffic; 4) provides some traffic mitigation; 5) protects Van Lierop Park from sound and visual impacts; and 6) reduces impacts to the Puyallup River and wetlands.

However, the proposed mitigation in Alternative 2 is not sufficient. Reducing warehouse square footage and traffic volume by 35% is not enough. Even at that scale, the project is too massive for this area. At least 50% reduction is needed. Fewer warehouses, less traffic, and more farmland protection should be incorporated into the City of Puyallup's preferred alternative in the Final EIS to Protect Puyallup.

Name	City	State	Postal Code	Country	Signed On
Protect Puyallup	Puyallup	WA		US	5/15/2016
Eileen Hennessy	Melrose	MA	2176	US	5/15/2016
Misti Stamon	Puyallup	WA	98372	US	5/17/2016
Tom Taylor	Puyallup	WA	98372	US	5/17/2016
Phil Schumock	Mount Angel	OR	97362	US	5/17/2016
Juanita Hembrow	Puyallup	WA	98371	US	5/17/2016
Chad Salisbury	Puyallup	WA	98372	US	5/17/2016
Cari Palmer	Puyallup	WA	98372	US	5/17/2016
Liz Manley	Puyallup	WA	98374	US	5/17/2016
Beth McDonnell	Puyallup	WA	98372	US	5/17/2016
David Myers	Puyallup	WA	98372	US	5/17/2016
Penny eims	Puyallup	WA	98372	US	5/17/2016
Art Seeley	Puyallup	WA	98374	US	5/17/2016
Carmen Bosch	Puyallup	WA	98372	US	5/17/2016
Stephanie Wilke-Swanson	Puyallup	WA	98372	US	5/17/2016
rachel kesler	Puyallup	WA	98371-7335	US	5/17/2016
Patricia Hawley	Puyallup	WA	98374	US	5/17/2016
shonn devroy	Orting	WA	98360	US	5/17/2016
Erica bodwin	Puyallup	WA	98371	US	5/17/2016
michael hawley	Puyallup	WA	98374	US	5/17/2016
Kathryn Moran	Puyallup	WA	98374	US	5/17/2016
Aaron Johnson	Puyallup	WA	98374	US	5/17/2016
Gayle Raschkow	Puyallup	WA	98372	US	5/17/2016
Demorie Smith	Puyallup	WA	98371	US	5/17/2016
Lloyd & Brenda Tibeau	Puyallup	WA	98372	US	5/17/2016
Sarah Haude	Tacoma	WA	98445	US	5/17/2016
Tiffany Bennett	Puyallup	WA	98374	US	5/17/2016
Jenel Yeamans	Puyallup	WA	98372	US	5/17/2016
Rick Westerfield	Puyallup	WA	98372	US	5/17/2016
Chris Chisholm	Puyallup	WA	98371	US	5/17/2016
Kathy Warren	Puyallup	WA	98374	US	5/17/2016
Stephanie McDaniel	Puyallup	WA	98374	US	5/17/2016
Laura Kearns	Puyallup	WA	98374	US	5/17/2016
Colleen Valadez	Puyallup	WA	98371	US	5/17/2016
Julie Hrivnak	Puyallup	WA	98374	US	5/17/2016
Yvonne McCullough	Puyallup	WA	98374	US	5/17/2016
Chantelle Loffer	Federal Way	WA	98023	US	5/17/2016
Jennifer Barrager	puyallup	WA	98374	US	5/17/2016
Amy Ormiston	Puyallup	WA	98372	US	5/17/2016
Ambria Affriseo	Tacoma	WA	98446	US	5/17/2016
Caitlin Hutt-Martindale	Puyallup	WA	98371	US	5/17/2016
Debi Tonsager	Puyallup	WA	98371	US	5/17/2016
Victoria Binkiewicz	Puyallup	WA	98374	US	5/17/2016
Jennifer Davies	Puyallup	WA	98371	US	5/17/2016
Sherry Dega	Puyallup	WA	98373	US	5/17/2016
Karen Edwards	Puyallup	WA	98374	US	5/17/2016

Shelley Friend	Puyallup	WA	98372 US	5/17/2016
Brett Lucas	Puyallup	WA	98374 US	5/17/2016
Nicole Martineau	Puyallup	WA	98371 US	5/17/2016
Michelle Arnett	Puyallup	WA	98373 US	5/18/2016
Robin Sherman	Puyallup	WA	98371 US	5/18/2016
Shannon Perry	Mount Vernon	WA	98274 US	5/18/2016
Tiffany Olson	Puyallup	WA	98375 US	5/18/2016
Tessa Hudgens	Puyallup	WA	98374 US	5/18/2016
Jennifer Lampman	Puyallup	WA	98374 US	5/18/2016
Katharina Rondeau	Puyallup	WA	98374 US	5/18/2016
Monica Henley	Puyallup	WA	Puyallup US	5/18/2016
Angela Bolam	Puyallup	WA	98374 US	5/18/2016
Linda Morgan	Puyallup	WA	98371 US	5/18/2016
Janet Sears	Puyallup	WA	98373 US	5/18/2016
Elizabeth Weyer	Enumclaw	WA	98022 US	5/18/2016
Cindy Poysnick	Puyallup	WA	98374 US	5/18/2016
Leslie Wilcox	Puyallup	WA	98374 US	5/18/2016
Frank Hulse	Tacoma	WA	98445 US	5/18/2016
Victoria Fontana	Puyallup	WA	98374 US	5/18/2016
Tiffany Brandelmayr	Puyallup	WA	98374 US	5/18/2016
Shaw Come Together	Puyallup	WA	98372 US	5/18/2016
Melanie Fuemmeler	Puyallup	WA	98371 US	5/18/2016
Elizabeth Ice	Puyallup	WA	98374 US	5/18/2016
Sara Husarik	Puyallup	WA	98374 US	5/18/2016
Jillida McBroom	Auburn	WA	98001 US	5/18/2016
Laura williams	Puyallup	WA	98374 US	5/18/2016
Kris Stamon	Puyallup	WA	98372 US	5/18/2016
Kelsey Treat	Puyallup	WA	98374 US	5/18/2016
Kathleen Mondragon	Puyallup	WA	98371 US	5/18/2016
Amy Barnard	Puyallup	WA	98374 US	5/18/2016
Luis Landaverde	Puyallup	WA	98374 US	5/18/2016
Zoann Hudspeth	Puyallup	WA	98374 US	5/18/2016
Ebony O'Neill	Puyallup	WA	98372 US	5/18/2016
Caitlin Moats	Puyallup	WA	98374 US	5/18/2016
Tracie Shepard	Puyallup	WA	98375 US	5/18/2016
Terri Vesey	Tacoma	WA	98446 US	5/18/2016
Michael James	Puyallup	WA	98374 US	5/18/2016
Emma Colley	Puyallup	WA	98372 US	5/18/2016
Linda Brown	Puyallup	WA	98372 US	5/18/2016
Noreen Landaverde	Puyallup	WA	98374 US	5/18/2016
Michael Watt	Puyallup	WA	98372 US	5/18/2016
David Stilli	Puyallup	WA	98374 US	5/18/2016
Amanda O'Brien	Auburn	WA	98092 US	5/18/2016
Angela Huber	Puyallup	WA	98374 US	5/18/2016
Carol Whipple	Puyallup	WA	98372 US	5/18/2016
Kimberly Amsden	Puyallup	WA	98374 US	5/18/2016
Patty Schumock	Puyallup	WA	98372 US	5/18/2016

Jennifer Roberts	Puyallup	WA	98371 US	5/18/2016
Tracy Ihlan	Puyallup	WA	98372 US	5/18/2016
Jennifer Roberts	Puyallup	WA	98371 US	5/18/2016
Nanette Palo	Puyallup	WA	98372 US	5/18/2016
KC Lowery	Tacoma	WA	98444 US	5/18/2016
Lauri Lackey	Puyallup	WA	98374 US	5/18/2016
Kerry Bloomquist	puyallup	WA	98374 US	5/18/2016
Cherie Miller	Puyallup	WA	98374 US	5/18/2016
Dana Llanos	Puyallup	WA	98373 US	5/18/2016
Cindee Reilly	Puyallup	WA	98374 US	5/18/2016
Denise Griffin	Puyallup	WA	98371 US	5/18/2016
Erin Smith	Tacoma	WA	98446 US	5/18/2016
Laura Jones	Puyallup	WA	98372 US	5/18/2016
Jason Effrig	Puyallup	WA	98372 US	5/18/2016
Jana Dinham	Puyallup	WA	98372 US	5/18/2016
Jennifer Matuz	Puyallup	WA	98373 US	5/18/2016
Hillary Barrett	Puyallup	WA	98374 US	5/18/2016
Ben Griffin	Puyallup	WA	98371 US	5/18/2016
Robin Helbich	Puyallup	WA	98372 US	5/18/2016
Ray Johnson	Dayton	OH	45405 US	5/18/2016
Rebecca Hartmann	Puyallup	WA	98372 US	5/18/2016
Colin Guthrie	Puyallup	WA	98372 US	5/18/2016
Jaimie Houser	Puyallup	WA	98374 US	5/18/2016
Treva Adkins	Puyallup	WA	98373 US	5/18/2016
Heidi Walsh	Portland	OR	97225 US	5/18/2016
Judi Rinehart	Puyallup	WA	98374 US	5/18/2016
Denelle Petschl	Puyallup	WA	98373-9112 US	5/18/2016
Jessica Michalski	Puyallup	WA	98372 US	5/18/2016
Katie Hall	Puyallup	WA	98375 US	5/18/2016
Lloyd Berge	Tacoma	WA	98443 US	5/18/2016
Paul Grange	Puyallup	WA	98372 US	5/18/2016
Francie Adams	Puyallup	WA	98374 US	5/18/2016
Leilani Cuarisma	Fort Worth	TX	76132 US	5/18/2016
Zori Perantie	Puyallup	WA	98374 US	5/18/2016
Kristy Fisher	Puyallup	WA	98374 US	5/18/2016
Jennifer Regalado	Puyallup	WA	98371 US	5/18/2016
Andy Goelzer	Puyallup	WA	98371 US	5/18/2016
Jayne Conway	Puyallup	WA	98372 US	5/18/2016
Frank Mestas	Puyallup	WA	98374 US	5/18/2016
Paul Mahoney	Puyallup	WA	98374 US	5/18/2016
Jonathan Stewart	Puyallup	WA	98372 US	5/18/2016
Karen Thomas	Puyallup	WA	98372 US	5/18/2016
Don boggs	Graham	WA	98338 US	5/18/2016
Jennifer Beck	Puyallup	WA	9837:04:00 US	5/18/2016
Kandy Stillwell	Puyallup	WA	98372 US	5/18/2016
Sarah Al-marri	Puyallup	WA	98371 US	5/18/2016
Tanisha Ystad	Pacific	WA	98047 US	5/18/2016

Heather Park	Puyallup	WA	98373 US	5/18/2016
Laura Stuart	Graham	WA	98338 US	5/18/2016
John Harrington	Orting	WA	98360 US	5/18/2016
Laura Thompson	Puyallup	WA	98371 US	5/18/2016
Jacob E Hall	Puyallup	WA	98371 US	5/18/2016
Patrick rugh	Puyallup	WA	98373 US	5/18/2016
Cathy Overfield	Puyallup	WA	98373 US	5/18/2016
Joost Claeys	Puyallup	WA	98371 US	5/18/2016
Justin Holley	Puyallup	WA	98374 US	5/18/2016
Lorrie Hutt	Puyallup	WA	98372 US	5/18/2016
Lisa Schultz	Puyallup	WA	98371 US	5/18/2016
Amy DeWitt	Puyallup	WA	98373 US	5/18/2016
Christi Leon	Puyallup	WA	98371 US	5/18/2016
Jennifer Evans	Puyallup	WA	98375 US	5/18/2016
Bill Stock	Puyallup	WA	98371 US	5/18/2016
Cheryl Myers	Puyallup	WA	98373 US	5/18/2016
Jacob Casement	Puyallup	WA	98371 US	5/18/2016
Katrina Paris	Tacoma	WA	98446 US	5/18/2016
Ryan Barker	Puyallup	WA	98372 US	5/18/2016
Carrie Loshbough	Puyallup	WA	98371 US	5/18/2016
Crystal Johnson	Sumner	WA	98390 US	5/18/2016
Scott Dubay	Puyallup	WA	98374 US	5/18/2016
Laura Deforrest	Puyallup	WA	98374 US	5/18/2016
Elowyn Jones	Puyallup	WA	98374 US	5/18/2016
Leslie Boyle	Puyallup	WA	98371 US	5/18/2016
Tracy Hotte	Schenectady	NY	12302 US	5/18/2016
Nate Conrad	Puyallup	WA	98374 US	5/18/2016
Lynn MacBradaigh	Puyallup	WA	98374 US	5/18/2016
Amy Tevis	Puyallup	WA	98374 US	5/18/2016
Sheri Sprague	Puyallup	WA	98372 US	5/18/2016
Christine Perrin	Puyallup	WA	98372 US	5/18/2016
Kristin funderburk	Puyallup	WA	98375 US	5/18/2016
Chandra Laskowski	Puyallup	WA	98373 US	5/18/2016
Marie Askew	Puyallup	WA	98374 US	5/18/2016
Peaches Ladwig	Puyallup	WA	98372 US	5/18/2016
Ronald J Tracy	Puyallup	WA	98374 US	5/18/2016
Susan Snoke	Puyallup	WA	98372 US	5/18/2016
Kay Tracy	Puyallup	WA	97374 US	5/18/2016
John Kolano	Puyallup	WA	98372 US	5/18/2016
Daleen Wilson	Puyallup	WA	98372 US	5/18/2016
Susan Savo	Puyallup	WA	98372 US	5/18/2016
Perry Ormiston	Puyallup	WA	98372 US	5/18/2016
Daniel Hoaas	Puyallup	WA	98372 US	5/18/2016
Kristin Lane	Kent	WA	98042 US	5/18/2016
Mckenzie Roberts	Puyallup	WA	98372 US	5/18/2016
Breanne crabbe	Yelm	WA	98597 US	5/18/2016
Heidi Leonard	Puyallup	WA	98371 US	5/18/2016

Monique Thometz	Puyallup	WA	98371 US	5/18/2016
Lisa Manners	Puyallup	WA	98371 US	5/18/2016
Kristy Hobson	Puyallup	WA	98374 US	5/18/2016
Chris Hawkins	Puyallup	WA	98371 US	5/18/2016
Carissa Cassell	Puyallup	WA	98371 US	5/18/2016
Terence Nelson	Puyallup	WA	98372 US	5/18/2016
Jen Pardo	Orting	WA	98360 US	5/18/2016
Luis Flores	Puyallup	WA	98372 US	5/18/2016
Tory Grant	Puyallup	WA	98372 US	5/18/2016
Hilary Stubbs	Puyallup	WA	98371 US	5/18/2016
Angela Johnson	Puyallup	WA	98374 US	5/18/2016
Samantha Mann	Puyallup	WA	98373 US	5/18/2016
Nancy Zahn	Bonney Lake	WA	98391 US	5/18/2016
Ariel Stuart	Puyallup	WA	98374 US	5/18/2016
Michael Michalski	Puyallup	WA	98372 US	5/18/2016
David Sundahl Jr	Puyallup	WA	98372 US	5/18/2016
Destry Johnson	Puyallup	WA	98374 US	5/18/2016
Jamie Dana	Puyallup	WA	98374 US	5/18/2016
Tina Schaller	Puyallup	WA	98371 US	5/18/2016
Kenneth Griffin	Puyallup	WA	98374 US	5/18/2016
Ashley Fererro	Buckley	WA	98321 US	5/18/2016
Cheryl Densmore	Puyallup	WA	98372 US	5/18/2016
Alicia Taylor	Federal Way	WA	98003 US	5/18/2016
Florence Myers	Tacoma	WA	98424 US	5/18/2016
Andrew Akers	Spanaway	WA	98387 US	5/18/2016
Deb Rowland	Puyallup	WA	98375 US	5/18/2016
Kathryn Haithcox	Puyallup	WA	98371 US	5/18/2016
Heather Booth	Puyallup	WA	98371 US	5/18/2016
Jennifer Kuhlman	Lake Tapps	WA	98391 US	5/18/2016
Bebe Colley	Puyallup	WA	98372 US	5/18/2016
Sharon Beslow	Puyallup	WA	98374 US	5/18/2016
Russell Clark	Tacoma	WA	98466 US	5/18/2016
Dan Oyama	Puyallup	WA	98371 US	5/18/2016
Jennifer Champagne	Puyallup	WA	98374 US	5/18/2016
Donelle Vance	Puyallup	WA	98374 US	5/18/2016
Tara McFarland	Wentzville	MO	63385 US	5/18/2016
Kelli Sallander	Puyallup	WA	98373 US	5/18/2016
Tracie Gloor	Puyallup	WA	98372 US	5/18/2016
chris mitchell	Puyallup	WA	98374 US	5/18/2016
Carrie Webb	Bonney Lake	WA	98391 US	5/18/2016
Luke Weaver	Orting	WA	98360 US	5/18/2016
Guy Schaub	Puyallup	WA	98372 US	5/18/2016
Martin Anderson	Puyallup	WA	98371 US	5/18/2016
Danielle Berger	Puyallup	WA	98374 US	5/18/2016
Jason Hickman	Puyallup	WA	98372 US	5/18/2016
Deborah Reed	Puyallup	WA	98301 US	5/18/2016
Charles Craft	Puyallup	WA	98374 US	5/18/2016

Erica Hansen	Tacoma	WA	98404 US	5/18/2016
Dawn Brightwell	Tacoma	WA	98402-3127 US	5/18/2016
Patricia Shandrow	Tacoma	WA	98443 US	5/18/2016
Luke Heath	Puyallup	WA	98371 US	5/18/2016
Lucas Bonner	Puyallup	WA	98371 US	5/18/2016
Devon Watson	Puyallup	WA	98375 US	5/18/2016
Catherine Larrow	Puyallup	WA	98372 US	5/18/2016
Gail Strobl	Puyallup	WA	98372 US	5/18/2016
Mark Temple	Puyallup	WA	98374 US	5/18/2016
Ko Wibowo	Tacoma	WA	98402 US	5/18/2016
Michael Larson	Puyallup	WA	98371 US	5/18/2016
Maya Brown	Graham	WA	98338 US	5/18/2016
Angel Hagedorn	Tacoma	WA	98404 US	5/18/2016
Dennis Farnes	Orting	WA	98360 US	5/18/2016
Amy Manzo	Seattle	WA	98146 US	5/18/2016
Suzan DeWitt	Puyallup	WA	98374 US	5/18/2016
Jerry Isaksen	Puyallup	WA	98373 US	5/18/2016
Marilynn Long	Puyallup	WA	98372 US	5/18/2016
Aaron Dillon	Puyallup	WA	98373 US	5/18/2016
Christine Johnson	Tacoma	WA	98424 US	5/18/2016
Josh Taylor	Roy	WA	98580 US	5/18/2016
Ryan Krohn	Puyallup	WA	98371-9416 US	5/18/2016
Danielle Douglas	Puyallup	WA	98374 US	5/18/2016
Anna Stanton	Sumner	WA	98390 US	5/18/2016
Kristi Bodnar	Tacoma	WA	98445 US	5/18/2016
Mary Kohli	Puyallup	WA	98371 US	5/18/2016
Mikayla Montgomery	Puyallup	WA	98372 US	5/18/2016
Krystal Peltzer	Puyallup	WA	98375 US	5/18/2016
Marek Quesnel	Puyallup	WA	98372 US	5/18/2016
Shaelee McCormick	Puyallup	WA	98375 US	5/18/2016
Juli Duck	Puyallup	WA	98371 US	5/18/2016
Cathrine Reuter	Puyallup	WA	98374 US	5/18/2016
Kristina pia	Buckley	WA	98321 US	5/18/2016
Breanna Soliz	Bellingham	WA	98229 US	5/18/2016
Jill Phillips	Puyallup	WA	98374 US	5/18/2016
Cheryl Osborne	Puyallup	WA	98373 US	5/18/2016
Jeny Buckley	Tacoma	WA	98446 US	5/18/2016
Missy Hartley	puyallup	WA	98373 US	5/18/2016
Kelsie Kolomyjec	Puyallup	WA	98371 US	5/18/2016
Melissa Huston	Sumner	WA	98390 US	5/18/2016
Kathleen Braaten	Tacoma	WA	98404 US	5/18/2016
Helen Arnestad	Tacoma	WA	#NAME? US	5/18/2016
Mallorie Stubbs	Puyallup	WA	98373 US	5/18/2016
Sadie Johnson	Puyallup	WA	98371 US	5/18/2016
Amanda Pagel	Lakewood	WA	98498 US	5/18/2016
Mark Tronson	Olympia	WA	98506 US	5/18/2016
Justin Gizinski	Spanaway	WA	98387 US	5/18/2016

Valerie Watt	Puyallup	WA	98374 US	5/18/2016
Zach Veltung	Rødovre	WA	98371 Denmark	5/18/2016
Zoe Smith	Puyallup	WA	98374 US	5/18/2016
John Olson	Puyallup	WA	98373 US	5/18/2016
Alannah Bruizeman	Orting	WA	98360 US	5/18/2016
Krystin McMahon	Puyallup	WA	98372 US	5/18/2016
Jason Rundell	Olympia	WA	98513 US	5/18/2016
Darice Hermann	Puyallup	WA	98371 US	5/18/2016
Angela Baker	Puyallup	WA	98371 US	5/18/2016
Brittany Hoffman	Puyallup	WA	98371 US	5/18/2016
kirsten weaver	Puyallup	WA	98375 US	5/18/2016
Hannah Davidson	Puyallup	WA	98372 US	5/18/2016
Chris Erickson	Tacoma	WA	98443 US	5/18/2016
TERESA BLISCHE	Puyallup	WA	98374 US	5/18/2016
Amal Walia	Puyallup	WA	98372 US	5/18/2016
Stacy Deaton	Puyallup	WA	98371 US	5/18/2016
Julia Bowen	Sumner	WA	98390 US	5/18/2016
Jaime Newberry	Puyallup	WA	98375 US	5/18/2016
Anita Dwight	Puyallup	WA	98371 US	5/18/2016
Rich Carr	Puyallup	WA	98372 US	5/18/2016
Jamie Gronka	sumner	WA	98390 US	5/18/2016
sarah giampapa	Puyallup	WA	98371 US	5/18/2016
Tom Reed	Puyallup	WA	98372 US	5/18/2016
Cyndi Johnson	Tacoma	WA	98443 US	5/18/2016
Victoria Knight	Sumner	WA	98390 US	5/18/2016
Donovan Mann	Puyallup	WA	98374 US	5/18/2016
David Westwood	Puyallup	WA	98374 US	5/18/2016
tabitha benton	puyallup	WA	98371 US	5/18/2016
Jessica Grisham	Puyallup	WA	98373 US	5/18/2016
Jennifer Jackson	Puyallup	WA	98375 US	5/18/2016
Debbie Rammell	Puyallup	WA	98371 US	5/18/2016
Sylvia Miller	Yelm	WA	98597 US	5/18/2016
Penny Harvill	SPANAWAY	WA	98387-8498 US	5/18/2016
Kimberley Stoner	Puyallup	WA	98371 US	5/18/2016
Mat Sharff	Puyallup	WA	98372 US	5/18/2016
derek weber	Sumner	WA	98390 US	5/18/2016
Debbie Myers	Puyallup	WA	98372 US	5/18/2016
Karen Freeman	Bonney Lake	WA	98391 US	5/18/2016
Michael Leuzzi	Puyallup	WA	98375 US	5/18/2016
Taysa Charnell	Tacoma	WA	98408 US	5/18/2016
Charlene Lenti	Tacoma	WA	98404 US	5/18/2016
Sheri Thomas	Bonney Lake	WA	98391 US	5/18/2016
Kim Hamilton	Puyallup	WA	98375 US	5/18/2016
Allison Champion	Milton	WA	98354 US	5/18/2016
joshua carsen	puyallup	WA	98375 US	5/18/2016
Chase Hamodey	Sumner	WA	98390 US	5/18/2016
April Schmidt	Puyallup	WA	98375 US	5/18/2016

Ashley Witzens	Puyallup	WA	98372 US	5/18/2016
Kyle Kaelin	Puyallup	WA	98374 US	5/18/2016
Sharon Fetter	Puyallup	WA	98371 US	5/18/2016
Carissa Paul	Puyallup	WA	98374 US	5/18/2016
Anjanette Breault	Sumner	WA	98390 US	5/18/2016
Chester Coskie	Puyallup	WA	98375 US	5/18/2016
Jean Shelton	Puyallup	WA	98375 US	5/18/2016
Carole Holfeld	Puyallup	WA	98372 US	5/18/2016
Pamela Larsen	Tacoma	WA	98446 US	5/18/2016
Devon Nichols	Lacey	WA	98503 US	5/19/2016
Vanessa Walsh	Puyallup	WA	98375 US	5/19/2016
Brett Baumann	Puyallup	WA	98371 US	5/19/2016
Elizabeth Gilbert	Puyallup	WA	98375 US	5/19/2016
Edward Atkins	Lewiston	ID	83501 US	5/19/2016
charles jacobson	Puyallup	WA	98372 US	5/19/2016
Madeline Morehouse	Sacramento	CA	95812 US	5/19/2016
Elizabeth Engle	Puyallup	WA	98371 US	5/19/2016
David Miller	Graham	WA	98338 US	5/19/2016
Kathy Turner	Puyallup	WA	98371 US	5/19/2016
Carolyn Graham	Puyallup	WA	98372 US	5/19/2016
Brenda Nelson	Puyallup	WA	98372 US	5/19/2016
Lisa Hoaas	puyallup	WA	98372 US	5/19/2016
Nancy Garrett-Payton	Puyallup	WA	98374 US	5/19/2016
Jessica Gilderoy	Puyallup	WA	98372 US	5/19/2016
Lauren Andreasen	Sumner	WA	98390 US	5/19/2016
Trissa Walstrom	Puyallup	WA	98373 US	5/19/2016
Suzelle Cowan	Sumner	WA	98390 US	5/19/2016
Angela Messer	Puyallup	WA	98373 US	5/19/2016
Julie Stout	Puyallup	WA	98374 US	5/19/2016
Brian Mays	Puyallup	WA	98371 US	5/19/2016
Lance Forshay	Puyallup	WA	98374 US	5/19/2016
John Webber	Puyallup	WA	98371 US	5/19/2016
Nancy Johnson	Tacoma	WA	98444 US	5/19/2016
Maegen Blue	Puyallup	WA	98374 US	5/19/2016
Ramdas Vaidyanathan	Puyallup	WA	98372 US	5/19/2016
Kathleen Adragna	ORLANDO	FL	32814 US	5/19/2016
Brandon Tabor	Puyallup	WA	98372 US	5/19/2016
Leeroy Perkins	Puyallup	WA	98375 US	5/19/2016
Robert Trimmer	Puyallup	WA	98375 US	5/19/2016
Ella Stanton	Sumner	WA	98390 US	5/19/2016
Nancy Marcordes	Sumner	WA	98390 US	5/19/2016
Christine Gillins	Puyallup	WA	98372 US	5/19/2016
Liesl Judah	Puyallup	WA	98372 US	5/19/2016
Heather Trimmer	Puyallup	WA	98375 US	5/19/2016
Melissa Brown	Puyallup	WA	98372 US	5/19/2016
Adam Post	Auburn	WA	98092 US	5/19/2016
Michael Iverson	Puyallup	WA	98374 US	5/19/2016

Patsy Bruinsma	Puyallup	WA	98373 US	5/19/2016
Rebecca Boor	Puyallup	WA	98375 US	5/19/2016
Jeff Edenfield	Puyallup	WA	98372 US	5/19/2016
Forrest Nameniuk	Puyallup	WA	98371 US	5/19/2016
Crystal Gorham	Puyallup	WA	98372 US	5/19/2016
Demarie McCurdu	Puyallup	WA	98372 US	5/19/2016
Emilie Yabe	Puyallup	WA	98371 US	5/19/2016
Andrew Rausch	Puyallup	WA	98371 US	5/19/2016
Lori Logan	Puyallup	WA	98374 US	5/19/2016
Cindy Olson	Puyallup	WA	98374 US	5/19/2016
Jodi Morris	Puyallup	WA	98374 US	5/19/2016
Scott Perrin	Puyallup	WA	98372 US	5/19/2016
Jacki Oakes	Puyallup	WA	98375 US	5/19/2016
Kurt Evensen	Puyallup	WA	98371 US	5/19/2016
Jennifer Smay	Puyallup	WA	98372 US	5/19/2016
Pat Woodrow	Puyallup	WA	98374 US	5/19/2016
Krista Linden	Puyallup	WA	98372 US	5/19/2016
Stacey May	Puyallup	WA	98371 US	5/19/2016
Amanda Foster	Puyallup	WA	98372 US	5/19/2016
Courtney Farrington	Puyallup	WA	98374 US	5/19/2016
linda papke	Tacoma	WA	98403 US	5/19/2016
Eve Rugarber	Puyallup	WA	98373 US	5/19/2016
Angela Wolf	Puyallup	WA	98374 US	5/19/2016
Rhiannon Wise	Puyallup	WA	98371 US	5/19/2016
Carrie Saagim	Graham	WA	98338 US	5/19/2016
Katlyn Bender	Puyallup	WA	98373 US	5/19/2016
Jacqueline Horn	Puyallup	WA	98373 US	5/19/2016
Patti Bostwick	Puyallup	WA	98371 US	5/19/2016
Eva Sanchez	Spanaway	WA	98387 US	5/19/2016
Randy Horn	Puyallup	WA	98373 US	5/19/2016
Pat Beug	Puyallup	WA	98372 US	5/19/2016
Renae Green	Puyallup	WA	99383 US	5/19/2016
Stephanie Norman	Puyallup	WA	98372 US	5/19/2016
Kim Flegel	Tacoma	WA	98446 US	5/19/2016
Christy Schaefer	Puyallup	WA	98374 US	5/19/2016
Laurie Patoff	Puyallup	WA	98371 US	5/19/2016
Lauren Williams	Puyallup	WA	98371 US	5/19/2016
Maggie Smith	Puyallup	WA	98371 US	5/19/2016
Julie Chisholm	Puyallup	WA	98372 US	5/19/2016
Jennifer Turner	Puyallup	WA	98372 US	5/19/2016
Denise Babineaux	puyallup	WA	98374 US	5/19/2016
Jay Shideeler	Bonney Lake	WA	98391 US	5/19/2016
Daniel Langel	Puyallup	WA	98373 US	5/19/2016
Dorien Pierce	Puyallup	WA	98374 US	5/19/2016
Alicia Pingul	Puyallup	WA	98374 US	5/19/2016
Hannah Palmer	Puyallup	WA	98372 US	5/19/2016
Becky Konigsfeld	Puyallup	WA	98373 US	5/19/2016

Deanna Andrade	Graham	WA	98338 US	5/19/2016
Clay Parker	Puyallup	WA	98372 US	5/19/2016
Michael Matos	Puyallup	WA	98371 US	5/19/2016
Barbara Fahnstrom	Puyallup	WA	98374 US	5/19/2016
Marlisa Kertson	Puyallup	WA	98372 US	5/19/2016
Donna hAGAR	Puyallup	WA	98374 US	5/19/2016
Brenda Shaw	Puyallup	WA	98374 US	5/19/2016
Britny Sirotak	Puyallup	WA	98375 US	5/19/2016
Ka Smith	puyallup	WA	98371 US	5/19/2016
Brad Butler	Puyallup	WA	98372 US	5/19/2016
Alison Cavanaugh	Puyallup	WA	98372 US	5/19/2016
Shannon Shish	Buckley	WA	98321 US	5/19/2016
Stephanie Aceves	Puyallup	WA	98375 US	5/19/2016
Tracie Pierce	Puyallup	WA	98375 US	5/19/2016
Stacy Piper	Puyallup	WA	97382 US	5/19/2016
J Adams	puyallup	WA	98371 US	5/19/2016
Amy Query	South Colby	WA	98384 US	5/19/2016
Alexa Knight	Puyallup	WA	98374 US	5/19/2016
Karen Shearer	Federal Way	WA	98003 US	5/19/2016
Mary Ann Quann	Fresno	CA	93711 US	5/19/2016
Ramona Griffin	Puyallup	WA	98373 US	5/19/2016
Fredrick LaPeer	Puyallup	WA	98373 US	5/19/2016
Darius Taylor	Puyallup	WA	98375 US	5/19/2016
Lindsay Smolko	Tacoma	WA	98424 US	5/19/2016
Laureen Manera	Kennewick	WA	99336 US	5/19/2016
Joshua Skilton	Puyallup	WA	98374 US	5/19/2016
Brenda Miller	Puyallup	WA	98371 US	5/19/2016
Julie Hatfield	Tacoma	WA	98424 US	5/19/2016
Breanna MCGLAUFLIN	Puyallup	WA	98375 US	5/19/2016
Nykole rojers	Sumner	WA	98390 US	5/19/2016
sherri Bones-Moore	Puyallup	WA	98372 US	5/19/2016
Tim Knapp	Puyallup	WA	98372 US	5/19/2016
Sarah Le Compte	Tacoma	WA	98409 US	5/19/2016
sierra dasilva	Puyallup	WA	98374 US	5/19/2016
Tammy Wescovich	Puyallup	WA	98374 US	5/19/2016
Julio Jauregui	Puyallup	WA	98373 US	5/19/2016
Anjovon Alhadeff	Puyallup	WA	98372 US	5/19/2016
Elizabeth Larson	Puyallup	WA	98374 US	5/19/2016
Krista Bishop	Puyallup	WA	98371 US	5/19/2016
Leslie Andrews	Puyallup	WA	98372 US	5/19/2016
shawna cozby	Puyallup	WA	98374 US	5/19/2016
Madison Haase	Midland	TX	79705 US	5/19/2016
Danny Rehms	Graham	WA	98338 US	5/19/2016
Lissa Rogoway	Puyallup	WA	98374 US	5/19/2016
Beth Smith	Puyallup	WA	98375 US	5/19/2016
Leah Gendreau	Puyallup	WA	98371 US	5/19/2016
Yvonne Larosee	Marietta	GA	30066 US	5/19/2016

Kelli Gailfus	Tacoma	WA	98407 US	5/19/2016
Susan Cherry	Puyallup	WA	98373 US	5/19/2016
Brian Massey	Puyallup	WA	98372 US	5/19/2016
Travis Codde	Puyallup	WA	98371 US	5/19/2016
Christi Doe	Issaquah	WA	98027 US	5/19/2016
Tina Larson	Covington	WA	98042 US	5/19/2016
Damian Lierley	Auburn	WA	98092 US	5/19/2016
Cindy Fruge	Puyallup	WA	98372 US	5/19/2016
Heidi Fitzgerald	Tacoma	WA	98405 US	5/19/2016
Duane Kanar	Tacoma	WA	98466 US	5/19/2016
Robert Golden	Puyallup	WA	98374 US	5/19/2016
Erik Mullan	Puyallup	WA	98371 US	5/19/2016
Ally's Adams	Puyallup	WA	98374 US	5/19/2016
Kristy Plaud	Puyallup	WA	98371 US	5/19/2016
Tom Biermann	Overland Park	KS	66223 US	5/19/2016
Brenda Zak	Puyallup	WA	98372 US	5/19/2016
Sarah Ruppert	Tacoma	WA	98446 US	5/19/2016
Laurel Monson	Puyallup	WA	98374 US	5/19/2016
Natalie Bowie	Bonney Lake	WA	98391 US	5/19/2016
Daryn Fahnstrom	Puyallup	WA	98374 US	5/19/2016
Leslie Bloom	Puyallup	WA	98374 US	5/19/2016
Frances Kehrberg	Puyallup	WA	98372 US	5/19/2016
Meghann Douvier	Spanaway	WA	98387 US	5/19/2016
Lindsey Hurley	Puyallup	WA	98374 US	5/19/2016
Char Adams	Puyallup	WA	98374 US	5/19/2016
Tracy League	Puyallup	WA	98372 US	5/19/2016
Christina Hennessy	Spanaway	WA	98387 US	5/19/2016
Kelsey Harrison	Puyallup	WA	98371-5219 US	5/19/2016
Carol Harder	Puyallup	WA	98374 US	5/19/2016
Katie Thibodeau	University Place	WA	98467 US	5/19/2016
Shelby Stanley	Puyallup	WA	98372 US	5/19/2016
Tina Pogue	Tacoma	WA	98408 US	5/19/2016
Bethany Gapsch	Puyallup	WA	98374 US	5/19/2016
Gloria Aaserud	Puyallup	WA	98374 US	5/19/2016
Gloria Bradbury	Buckley	WA	98321 US	5/19/2016
Patricia Ivey	Puyallup	WA	98375 US	5/19/2016
Cody Zimmerman	Sumner	WA	98390 US	5/19/2016
Regina Rollis	Puyallup	WA	98374 US	5/19/2016
Jared Ball	Woodburn	OR	97071 US	5/19/2016
Julie Anderson	Federal Way	WA	98003 US	5/19/2016
Jessica Collet	Puyallup	WA	98375 US	5/19/2016
Joy Gonzales	Puyallup	WA	98372 US	5/19/2016
Joshua Daley	Puyallup	WA	98371 US	5/19/2016
Brandy Wade	Puyallup	WA	98372 US	5/19/2016
Audrey Fellenberg	Bonney Lake	WA	98391 US	5/19/2016
Damian Alhadeff	Puyallup	WA	98372 US	5/19/2016
Katy Everson	Puyallup	WA	98372 US	5/19/2016

Tressie Lowe	Puyallup	WA	98374 US	5/19/2016
carly hendricks	Tacoma	WA	98466 US	5/19/2016
Krystal Budd	Puyallup	WA	98372 US	5/19/2016
Brek Hansen	Bonney Lake	WA	98391 US	5/19/2016
Christina Schneider	Puyallup	WA	98375 US	5/19/2016
Tiffanie Harse	Puyallup	WA	98373 US	5/19/2016
William Cotton	Orting	WA	98360 US	5/19/2016
Cristen Duff	South Hill	WA	98374 US	5/19/2016
Michael Brown	Puyallup	WA	98371 US	5/19/2016
Nikki Murrell	Puyallup	WA	98373 US	5/19/2016
Victoria McManus	Tacoma	WA	98418 US	5/19/2016
Karri Montague	Sumner	WA	98390 US	5/19/2016
Jennifer Welch	Puyallup	WA	98371 US	5/19/2016
Karen Fisher	Puyallup	WA	98371 US	5/19/2016
brian brock	auburn	WA	98001 US	5/19/2016
Christy Ferber	Puyallup	WA	98372 US	5/19/2016
Courtney Joslin	Puyallup	WA	98374 US	5/19/2016
Courtney Nichols	Seattle	WA	98117 US	5/19/2016
Melanie Walch	Bonney Lake	WA	98391 US	5/19/2016
Rhonda Coombes	Tacoma	WA	98418 US	5/19/2016
Mike Driscoll	Puyallup	WA	98374 US	5/19/2016
Georgia Horton	Tacoma	WA	98408 US	5/19/2016
John Fiorini	Federal Way	WA	98003 US	5/19/2016
Marla Keethler			10040 Portugal	5/19/2016
Lacey Ezell	Graham	WA	98338 US	5/19/2016
Trevor Cassidy	Bellingham	WA	98229 US	5/19/2016
Taylor Keith	Puyallup	WA	98372 US	5/19/2016
Christi Baker	Graham	WA	98338 US	5/19/2016
Julie Miller	Tacoma	WA	98403 US	5/19/2016
Tosha Betts	Puyallup	WA	98374 US	5/19/2016
Jolene Butterfield	Eatonville	WA	98328 US	5/19/2016
Kevin McClellan	Tacoma	WA	98407 US	5/19/2016
Ryan Reed	Graham	WA	98338 US	5/19/2016
Krista Rosebrook	Puyallup	WA	98374 US	5/19/2016
Marge Kerwin	Spokane	WA	99203 US	5/19/2016
Lorin Relano	Puyallup	WA	98372 US	5/19/2016
Linda Kachinsky	Puyallup	WA	98371 US	5/19/2016
Rosalie Clarke	The Dalles	OR	97058 US	5/19/2016
Amanda Snoke	Puyallup	WA	98374 US	5/19/2016
Kristin Yamada	Puyallup	WA	98371 US	5/19/2016
Cathi Mahan	Puyallup	WA	98371 US	5/19/2016
Jennifer Rhyne	Puyallup	WA	98373 US	5/19/2016
Jennie Drake	Tacoma	WA	98445 US	5/19/2016
Patty Thomas	puyallup	WA	98372 US	5/19/2016
sonjia warren	Tenino	WA	98589 US	5/19/2016
Corei Bean	Port Orchard	WA	98366 US	5/19/2016
Chris Pfeiffer	Puyallup	WA	98373 US	5/19/2016

Meredith Bailey	Bothell	WA	98012 US	5/19/2016
christine lingk	Bonney Lake	WA	98391 US	5/19/2016
Jamie Capp	Everett	WA	98203 US	5/19/2016
Kelly Laher	Puyallup	WA	98384 US	5/19/2016
Mandy Hatcher	Puyallup	WA	98371 US	5/19/2016
Whitney Mack	Puyallup	WA	98372 US	5/19/2016
Rebecca Thompson	Puyallup	WA	98372 US	5/19/2016
Arlene towne	Milton	WA	98354 US	5/19/2016
Colin McDonnell	Puyallup	WA	98372 US	5/19/2016
Andy Simmons	Kent	WA	98032 US	5/19/2016
Brian Klug	Puyallup	WA	98372 US	5/19/2016
Raeanne Heath	Puyallup	WA	98371 US	5/19/2016
Ash Mearns	Puyallup	WA	98371 US	5/19/2016
Darlene Kerwin	Puyallup	WA	98375 US	5/19/2016
Robert Carnes	Puyallup	WA	98374 US	5/19/2016
Tiffany Flaherty	Puyallup	WA	98372 US	5/19/2016
Jeanette Gregory	Puyallup	WA	98374 US	5/19/2016
Teri Kavanaugh	Puyallup	WA	98375 US	5/19/2016
Melissa Guay	Puyallup	WA	98374 US	5/19/2016
Bailey Hockaday	Puyallup	WA	98374 US	5/19/2016
Pamela Knight	Puyallup	WA	98371 US	5/19/2016
timthy retynski	Sumner	WA	98390 US	5/19/2016
Barbara Tibeau	Puyallup	WA	98371 US	5/19/2016
Ashley Alhadeff	Seattle	WA	98136 US	5/19/2016
MOLLY BALLANTYNE	Bonney Lake	WA	98391 US	5/19/2016
Jim Scott	Puyallup	WA	98371 US	5/19/2016
Veltung LeeAnn	Puyallup	WA	98371 US	5/19/2016
Xylia Scott	Puyallup	WA	98371 US	5/19/2016
cole munson	Bonney Lake	WA	98391 US	5/19/2016
Teena Williams	South Prairie	WA	98375 US	5/19/2016
Lacey Jeffries	Puyallup	WA	98372 US	5/19/2016
PETER LANDRU	Puyallup	WA	98373 US	5/19/2016
Aaron Maruska	Tacoma	WA	98407 US	5/19/2016
Laurence Welch	Puyallup	WA	98371 US	5/19/2016
AUDEANE MCMORROW	Enumclaw	WA	98022 US	5/19/2016
CHRISTINA HOPKINS	Auburn	WA	98002 US	5/19/2016
Gabrielle Pitzler	Seattle	WA	98103 US	5/19/2016
Elaine Alhadeff	Seattle	WA	98116 US	5/19/2016
Shaun Munson	Bonney Lake	WA	98391 US	5/19/2016
Cody Buck	Kent	WA	98042 US	5/19/2016
HAROLD ANDERSON	Federal Way	WA	98003 US	5/19/2016
Kaitlyn Johnson	Auburn	WA	98092 US	5/19/2016
Sharyn Amalla	Auburn	WA	98001 US	5/19/2016
Cindy Fuhr	Bonney Lake	WA	98391 US	5/19/2016
Julie Shackett	Puyallup	WA	98372 US	5/19/2016
Janine Westfall	Puyallup	WA	98372 US	5/19/2016
Rob Vajko	Port Orchard	WA	98367 US	5/19/2016

Carla Ritchie	Sumner	WA	98390 US	5/19/2016
Pamela Franklin	Puyallup	WA	98371 US	5/19/2016
Rebecca Waller	Puyallup	WA	98372 US	5/19/2016
Lori Hinz	Puyallup	WA	98374 US	5/19/2016
Lori Garrett	Spanaway	WA	98387 US	5/19/2016
Adrienne Weller	Seattle	WA	98144 US	5/19/2016
jan speer	auburn	WA	98002 US	5/19/2016
Michael Vincent	Puyallup	WA	98372 US	5/19/2016
Jacob Sidmore	Puyallup	WA	98375 US	5/19/2016
Gwen Hillius	Puyallup	WA	98371 US	5/19/2016
Annette Jeffrey	Philadelphia	PA	19147 US	5/19/2016
Vanessa Allen	Puyallup	WA	98373 US	5/19/2016
Jennifer Kennedy	maple Valley	WA	98038 US	5/19/2016
Annette Klapstein	Seattle	WA	98107 US	5/19/2016
Terra Lewis	Tacoma	WA	98404 US	5/19/2016
Alisa O'Haver-Ayala	Puyallup	WA	98374 US	5/19/2016
James Miller	Puyallup	WA	98374 US	5/19/2016
Carol Spurgin	Spokane	WA	99208 US	5/19/2016
Dennice Bryan	Tacoma	WA	98446 US	5/19/2016
Kari Horvat	Puyallup	WA	98374 US	5/19/2016
Kristen Renee	Puyallup	WA	98374 US	5/19/2016
rex lortz	Tacoma	WA	98445 US	5/19/2016
Jurren Bouman	Auburn	WA	98001 US	5/19/2016
Robert Holsberry	Seattle	WA	98116 US	5/19/2016
Melissa Riley	Puyallup	WA	97374 US	5/19/2016
kim gerstmann	Puyallup	WA	98372 US	5/19/2016
Miriam Irwin	Puyallup	WA	98373 US	5/19/2016
Kelly Comer	Puyallup	WA	98371 US	5/19/2016
Virginia Wodtli	Lakewood	WA	98499 US	5/19/2016
Kristi Higgins	Puyallup	WA	98374 US	5/19/2016
leslie stephens	des moines	WA	98198 US	5/19/2016
Maxine Ross	Puyallup	WA	98373 US	5/19/2016
Michael Bostwick	Puyallup	WA	98373 US	5/19/2016
Beth Brooks	Puyallup	WA	98371 US	5/19/2016
Robert Marion	Puyallup	WA	98371 US	5/19/2016
Nikki Brummond	Puyallup	WA	98374 US	5/19/2016
R Baghirov	PUYALLUP	WA	98373 US	5/19/2016
Kathy Schmidt			98372 Italy	5/19/2016
Diana Martin	Puyallup	WA	98371 US	5/19/2016
Dayna Miller	Puyallup	WA	98372 US	5/19/2016
Chris Berch	Puyallup	WA	98371 US	5/19/2016
Kareen Shanks	Puyallup	WA	`98374 US	5/19/2016
Linda Langebetg	Tacoma	WA	98418 US	5/19/2016
Ashley Isenberger	Puyallup	WA	98374 US	5/19/2016
John Adams	Tacoma	WA	98443 US	5/19/2016
Ashley Smith	Puyallup	WA	98372 US	5/19/2016
Casey Nutting	Puyallup	WA	98373 US	5/19/2016

Alexandria Steele	Puyallup	WA	98373 US	5/19/2016
Diane Bero	Puyallup	WA	98372 US	5/19/2016
Devin Ehli	Pullman	WA	99164 US	5/19/2016
Sherri Hoy	Graham	WA	98338 US	5/19/2016
Loraina McMahon	Puyallup	WA	98373 US	5/19/2016
Prince Galligan	Tacoma	WA	98444 US	5/19/2016
Hillary Moss	Bonney Lake	WA	98391 US	5/19/2016
Jason Wood	Puyallup	WA	98372 US	5/19/2016
Laura Baker	Auburn	WA	98092 US	5/19/2016
Janet Nix	Puyallup	WA	98371 US	5/19/2016
Sandra Waddell	Puyallup	WA	98371 US	5/19/2016
Angelica Miramontes	Puyallup	WA	98374 US	5/19/2016
Kody Granger	Auburn	WA	98001 US	5/19/2016
Tracy Gallaway	Auburn	WA	98092 US	5/19/2016
christine wilson	puyallup	WA	98374 US	5/19/2016
Kaleb Granger	Spanaway	WA	98387 US	5/19/2016
Christine Wood	Seattle	WA	98106 US	5/19/2016
Hannah Rotness	Puyallup	WA	98372 US	5/19/2016
Shara Wallace	Auburn	WA	98002 US	5/19/2016
Janelle Fricks	Puyallup	WA	98372 US	5/19/2016
Iris Fung	Bellevue	WA	98015 US	5/19/2016
Kari Champine	Puyallup	WA	98372 US	5/19/2016
Anna Coghill	Lynnwood	WA	98036 US	5/19/2016
Bonnie Char	Edgewood	WA	98371 US	5/19/2016
Alison Tabalno	Puyallup	WA	98373 US	5/19/2016
Ann Todd	Puyallup	WA	98374 US	5/19/2016
Jennifer Kingsland	Edmonds	WA	98026 US	5/19/2016
Angela Rouse	Puyallup	WA	98374 US	5/19/2016
Caitlin Abel	Federal Way	WA	98023 US	5/19/2016
Rachel Lin	Puyallup	WA	98373 US	5/19/2016
Kim Chisholm	Puyallup	WA	98371 US	5/19/2016
Kimberly Doering	Federal Way	WA	98023 US	5/19/2016
Hannah Comerford	Spanaway	WA	98387 US	5/19/2016
kellie rust	Lake Tapps	WA	98391 US	5/19/2016
Rebecca Passic	Puyallup	WA	98375 US	5/19/2016
Martha Temus	Puyallup	WA	98373 US	5/19/2016
Jessica Hernandez	Graham	WA	98338 US	5/19/2016
Heidi Sweet	Puyallup	WA	98374 US	5/19/2016
Sherry Fobes	Puyallup	WA	98371 US	5/19/2016
Shaelynn Zurcher	Tacoma	WA	98445 US	5/19/2016
Katharine Stringer	Puyallup	WA	98374 US	5/19/2016
Jonas Cline	Puyallup	WA	98373 US	5/19/2016
Sarah Paranada	Puyallup	WA	98375 US	5/19/2016
Ashley Hill	Tacoma	WA	98424 US	5/19/2016
Sunjoo Park	Puyallup	WA	98372 US	5/19/2016
jenah Williams	Puyallup	WA	98374 US	5/19/2016
Morris Hernandez	Puyallup	WA	98372 US	5/19/2016

Mary Price	Puyallup	WA	98374 US	5/19/2016
Diane Henke	Puyallup	WA	98372 US	5/19/2016
Cindy Holshouser	Puyallup	WA	98372 US	5/19/2016
Judith Cowell	Tacoma	WA	98445 US	5/19/2016
Maggie Rogers	South Bend	WA	98586 US	5/19/2016
Zack Person	Bakersfield	CA	93309 US	5/19/2016
Larry & Emilie Wright	Puyallup	WA	98372 US	5/19/2016
Robert Hart	Sumner	WA	98390 US	5/19/2016
Janine Parque	Puyallup	WA	98372 US	5/19/2016
Kaitlynn Samuelson	Puyallup	WA	98372 US	5/19/2016
James Schatza	Puyallup	WA	98375 US	5/19/2016
Natalie Rainey	Puyallup	WA	98372 US	5/19/2016
Erin Tadche	Puyallup	WA	98374 US	5/19/2016
Virginia Facer	Puyallup	WA	98373 US	5/19/2016
Victoria Ross	Puyallup	WA	98372 US	5/19/2016
Allan Holtz	Buckley	WA	98321 US	5/19/2016
Cassandra Nocifora	Naples	FL	34104 US	5/19/2016
Kathy Skipper	Puyallup	WA	98371 US	5/19/2016
Sue Burton	Puyallup	WA	98374 US	5/19/2016
Alana Kasner	Puyallup	WA	98373 US	5/19/2016
carolanne knoll	Enumclaw	WA	98022 US	5/19/2016
Nichole Crosbie	Puyallup	WA	98371 US	5/19/2016
Joy Parker	Spanaway	WA	98387 US	5/19/2016
Kimberly Nee	Orting	WA	98360 US	5/19/2016
Judy Visser	renton	WA	98057 US	5/19/2016
Callie Hyman	Puyallup	WA	98374 US	5/19/2016
kathleen fifer	Mesa	AZ	85213 US	5/20/2016
Alyssa Bentzoni	Puyallup	WA	98374 US	5/20/2016
Elaine Schultz	Puyallup	WA	98371 US	5/20/2016
JoLynn Hilsendeger	Puyallup	WA	98373 US	5/20/2016
Laurie Bolton	Bonney Lake	WA	98391 US	5/20/2016
Shelley Shipe	Puyallup	WA	98374 US	5/20/2016
Sally Connors	Seattle	WA	98133 US	5/20/2016
Lisa Hansen	Spanaway	WA	98387 US	5/20/2016
Kaelin Koppelman	Puyallup	WA	98372 US	5/20/2016
Bruce Wilson	Puyallup	WA	98372 US	5/20/2016
susan Morrison	Puyallup	WA	98374 US	5/20/2016
Rebekka Nicholson	Puyallup	WA	98374 US	5/20/2016
Janice Celski	Puyallup	WA	98371 US	5/20/2016
Mercedes Bower	Puyallup	WA	98375 US	5/20/2016
Janet Sainati	Kent	WA	98032 US	5/20/2016
Vickie Blodgett	Puyallup	WA	98372 US	5/20/2016
Crystal Cutter	Puyallup	WA	98374 US	5/20/2016
Michael Palko-Dean	Auburn	WA	98002 US	5/20/2016
Darsa Moffat	Federal Way	WA	98003 US	5/20/2016
Gretchen Momberg	Port Angeles	WA	98362 US	5/20/2016
Marcia Cline	Woodinville	WA	98077 US	5/20/2016

Ben Goucher	Tacoma	WA	98443 US	5/20/2016
Amanda gillespie	Orting	WA	98360 US	5/20/2016
Kim Kolzow	Auburn	WA	98092 US	5/20/2016
Mary Jordan	Puyallup	WA	98374 US	5/20/2016
Alvias Davis	Puyallup	WA	98372 US	5/20/2016
Carmen Houghton	Puyallup	WA	98371 US	5/20/2016
Gary Hoy	Federal Way	WA	98003 US	5/20/2016
Leslie Snyder	Tahuya	WA	98588 US	5/20/2016
Jonathan Ames	Aberdeen	WA	98520 US	5/20/2016
Alicia Pashon	Puyallup	WA	98374 US	5/20/2016
Joelle Csuha	Puyallup	WA	98374 US	5/20/2016
Cheryl Weller	Puyallup	WA	99374 US	5/20/2016
sandy LaCasse	Seattle	WA	98103 US	5/20/2016
Kate Mack	Puyallup	WA	98375 US	5/20/2016
Gayle Clemens	Seattle	WA	98166 US	5/20/2016
nancy piper	Spokane	WA	99205 US	5/20/2016
Ruth Schindler	Tacoma	WA	98445 US	5/20/2016
Tayor Vetter	Puyallup	WA	98373 US	5/20/2016
Kevin Fleming	Buckley	WA	98321 US	5/20/2016
Ed Herbert	Tacoma	WA	98405 US	5/20/2016
Mike O'Dell	Puyallup	WA	98372 US	5/20/2016
Sharon Thomas	Puyallup	WA	98371 US	5/20/2016
Pat Lowery	Puyallup	WA	98371 US	5/20/2016
Ellie Kirchner	Puyallup	WA	98373 US	5/20/2016
Jenni Muehlenbruch	Puyallup	WA	98374 US	5/20/2016
James Jennings	Puyallup	WA	98375 US	5/20/2016
Jennifer Matthews	Puyallup	WA	98372 US	5/20/2016
Holly Nutter	Puyallup	WA	98371 US	5/20/2016
Drew Street	Puyallup	WA	98372 US	5/20/2016
Andre Douglas	Puyallup	WA	98372 US	5/20/2016
Kaylie Fernando	Puyallup	WA	98371 US	5/20/2016
Cindy kelley	Puyallup	WA	98374 US	5/20/2016
Toni DePoister	Tacoma	WA	98445 US	5/20/2016
Ellen Rackham	Spanaway	WA	98387 US	5/20/2016
Sean Mahoney	Graham	WA	98338 US	5/20/2016
terrah highet	Graham	WA	98338-9517 US	5/20/2016
Christine Sturgill	Graham	WA	98338 US	5/20/2016
Sara Jacobs	Spanaway	WA	98387 US	5/20/2016
Ashley Markum	Scio	OR	97374 US	5/20/2016
Suzana Lish	Puyallup	WA	98375 US	5/20/2016
Cindy garske	Puyallup	WA	98374 US	5/20/2016
Chuck Bowers	Puyallup	WA	98373 US	5/20/2016
April Janas	Puyallup	WA	98372 US	5/20/2016
Elizabeth Dorsey	Puyallup	WA	98372 US	5/20/2016
Kara Centioli	Seattle	WA	98116 US	5/20/2016
Steve Anderson	Kent	WA	98032 US	5/20/2016
Janette Wilcox	Puyallup	WA	98375 US	5/20/2016

Kelsey Little	Pacific	WA	98047 US	5/20/2016
Dianne Huntsman	Spanaway	WA	98387 US	5/20/2016
Sundi Avila	Auburn	WA	98092 US	5/20/2016
Racheal Cummings	Puyallup	WA	98374 US	5/20/2016
Tess Morrison	Puyallup	WA	98374 US	5/20/2016
Karla Giboney	Puyallup	WA	98372 US	5/20/2016
Darrienne weber	Puyallup	WA	98374 US	5/20/2016
Peter Lopez	Puyallup	WA	98372 US	5/20/2016
Deserae Marinelli	Puyallup	WA	98372 US	5/20/2016
Daneysha Drew	Puyallup	WA	98374 US	5/20/2016
Scott Jacobs	Puyallup	WA	98375 US	5/20/2016
Sierra Ducatt	Puyallup	WA	98372 US	5/20/2016
miranda obregon	Puyallup	WA	98372 US	5/20/2016
Linda Kammin	Tacoma	WA	98404 US	5/20/2016
Dianne Dickersonn	Seattle	WA	98133 US	5/20/2016
Jeremy Trenhaile	Seattle	WA	98107 US	5/20/2016
Ashlee Simcoe	Roy	WA	98580 US	5/20/2016
Michael Alhadeff	Seattle	WA	98116 US	5/20/2016
Jeremy Oswald	Lakewood	WA	98499 US	5/20/2016
Larisha Shelman	Tacoma	WA	98466 US	5/20/2016
Anastazia Adams	Puyallup	WA	98372 US	5/20/2016
Robert Hosler	Puyallup	WA	98374 US	5/20/2016
Alyssa Miller	Puyallup	WA	98374 US	5/20/2016
Amanda Barker	Seattle	WA	98119 US	5/20/2016
ROBERT CARR	Puyallup	WA	98372 US	5/20/2016
Kim Slater	Orting	WA	98360 US	5/20/2016
Andrea Ulmer	Puyallup	WA	98371 US	5/20/2016
Nydia Trout	puyallup	WA	98374 US	5/20/2016
Tanmy McCoy	Orting	WA	98360 US	5/20/2016
Jim Rubert	Puyallup	WA	98372 US	5/20/2016
Elisabeth Sims	Auburn	WA	98092 US	5/20/2016
April Clarey	Graham	WA	98338 US	5/20/2016
Michael Weaver	Puyallup	WA	98372 US	5/20/2016
Brandi Gibson	Olympia	WA	98506 US	5/20/2016
Amanda Smith	Tacoma	WA	98445 US	5/20/2016
thomas bishop	Bonney Lake	WA	98391 US	5/20/2016
Deborah Cutbirth	Kelso	WA	98626 US	5/20/2016
christopher scott	Graham	WA	98338 US	5/20/2016
Kathleen Erskine	Puyallup	WA	98375 US	5/20/2016
Christy Kolowitz	Puyallup	WA	98373 US	5/20/2016
Sarah Medberry	Puyallup	WA	98372 US	5/20/2016
Maria Parris	Puyallup	WA	98372 US	5/20/2016
Brad Kraft	Bonney Lake	WA	98391 US	5/20/2016
sunny wilson	Lake Stevens	WA	98258 US	5/20/2016
Korrine Claxton	Bonney Lake	WA	98391 US	5/20/2016
Thomas Cannell	Puyallup	WA	98372 US	5/20/2016
Kaylea Tuttle	Ogden	UT	84404 US	5/20/2016

vincnt ferrau	cannon beach	OR	97110 US	5/20/2016
Ron Perry	Puyallup	WA	98372 US	5/20/2016
Katy Baur	Puyallup	WA	98371 US	5/20/2016
David Schrader	Roy	WA	98580 US	5/20/2016
Carrie Lindsay	Tacoma	WA	98443 US	5/20/2016
Diane Uhrich	Tacoma	WA	98407 US	5/20/2016
Erin Logan-Guffey	Puyallup	WA	98374 US	5/20/2016
Courtney Goter	Puyallup	WA	98372 US	5/20/2016
Linda O'Rourke	Puyallup	WA	98372 US	5/20/2016
David Johnson	Graham	WA	98338 US	5/20/2016
Janessa Stovall	Orting	WA	98360 US	5/20/2016
Eileen McKee	Puyallup	WA	98373 US	5/20/2016
Linda Taylor	Puyallup	WA	98374 US	5/20/2016
carol vandver	Puyallup	WA	98374 US	5/20/2016
Dawn Arbelo	Puyallup	WA	98372 US	5/20/2016
Heather Tscheuschner	Roy	WA	98580 US	5/20/2016
Kimberly Hosler	Puyallup	WA	98374 US	5/20/2016
Shireen Ainsworth	Puyallup	WA	98371 US	5/20/2016
Gail Gause	Bonney Lake	WA	98391 US	5/20/2016
Dave Berry	Puyallup	WA	98371 US	5/20/2016
Daniel Hayes	Puyallup	WA	98372 US	5/20/2016
Elise Rushton	Puyallup	WA	98374 US	5/20/2016
Kaitlyn Tungsvik	Puyallup	WA	98374 US	5/20/2016
Kieli Blenheim	Turk	WA	98543 US	5/20/2016
Barbara Shepherd	Auburn	WA	98092 US	5/20/2016
Cheryl McColm	Lakewood	WA	98499 US	5/20/2016
Brittanie Doering	Buckley	WA	98321 US	5/20/2016
Traci Davey	Puyallup	WA	98373 US	5/20/2016
Martin Larsen	Puyallup	WA	98374 US	5/20/2016
Carol Berry	Puyallup	WA	98374 US	5/20/2016
TERRY COPELAND	Tacoma	WA	98446 US	5/20/2016
John Runland	Puyallup	WA	98374 US	5/20/2016
Ray Rezab	Puyallup	WA	98372 US	5/20/2016
Erica Bates	Puyallup	WA	98375 US	5/20/2016
Gail Sampson	Puyallup	WA	98374 US	5/20/2016
Joan Lynn	Puyallup	WA	98372 US	5/20/2016
Becky Stanczyk	Graham	WA	98338 US	5/20/2016
sami pendergress	Lacey wa	WA	98503 US	5/20/2016
Robert Gallwas	Puyallup	WA	98372 US	5/20/2016
Mike Herzog	Bonney Lake	WA	98391 US	5/20/2016
Tara Wilson	Puyallup	WA	98372 US	5/20/2016
Melissa McConkey	Bonney Lake	WA	98391 US	5/20/2016
Kirsten Herzog	Bonney Lake	WA	98391 US	5/20/2016
Amy Kelley	Puyallup	WA	98372 US	5/20/2016
Erika Harnett	Seattle	WA	98106 US	5/20/2016
Krystal Matthew	Spanaway	WA	98387 US	5/20/2016
Chris Kucklick	Puyallup	WA	98371 US	5/20/2016

Charlotte Burns	Puyallup	WA	98372 US	5/20/2016
Jessica Kelly	Olympia	WA	98501 US	5/20/2016
Amanda OMalley	Puyallup	WA	98372 US	5/20/2016
sarah curtis	Puyallup	WA	98375 US	5/20/2016
Sandy Marcy	Portland	OR	97213 US	5/20/2016
Andreia Hagel	Puyallup	WA	98371 US	5/20/2016
Johanna Sabado	Puyallup	WA	98374 US	5/20/2016
Dominique Temple	Eatonville	WA	98328 US	5/20/2016
Jo Ann Munce	Tacoma	WA	98446 US	5/20/2016
Stephanie Christel-Lewis	Graham	WA	98338 US	5/20/2016
Marsha Arnold	Puyallup	WA	98371 US	5/20/2016
Lori Hart	Puyallup	WA	98374 US	5/20/2016
Christian Amburgy	Sumner	WA	98390 US	5/20/2016
Mitchell Dawley	Puyallup	WA	98374 US	5/20/2016
Sarah Matthews	Puyallup	WA	98372 US	5/20/2016
Jerry & Susan Scelzi	Puyallup	WA	98371 US	5/20/2016
SUZANNE VICTORIA	Olympia	WA	98506 US	5/20/2016
Jennifer Crawford	Puyallup	WA	98372 US	5/20/2016
Travis Wilson	Puyallup	WA	98372 US	5/20/2016
Alfred Emerson	Puyallup	WA	98372 US	5/20/2016
Debra Countryman	Puyallup	WA	98374 US	5/20/2016
John Olan	Puyallup	WA	98372 US	5/20/2016
Cheri McKay	Spanaway	WA	98387 US	5/21/2016
Janet Walter	Puyallup	WA	98371 US	5/21/2016
Mike boggs	Puyallup	WA	98372 US	5/21/2016
Tracey Arney	Puyallup	WA	98372 US	5/21/2016
Morgan Olan	Puyallup	WA	98372 US	5/21/2016
Robin Reed	Sumner	WA	98390 US	5/21/2016
Karl Hagel	Puyallup	WA	98371 US	5/21/2016
Jennifer Austin	Tacoma	WA	98471 US	5/21/2016
Jerilyn Gerhardstein	Puyallup	WA	98371 US	5/21/2016
Danna Bandle	Puyallup	WA	98374 US	5/21/2016
Chris Mcgrew	Puyallup	WA	98375 US	5/21/2016
Jeff Husband Jenney	Puyallup	WA	98372 US	5/21/2016
Lisa Jenney	Puyallup	WA	98372 US	5/21/2016
Kathy McKinley	Puyallup	WA	98374 US	5/21/2016
Chelsea Grace	Puyallup	WA	98371 US	5/21/2016
Alec Taylor	Puyallup	WA	98372 US	5/21/2016
Jerome Schroer	Puyallup	WA	98372 US	5/21/2016
Trisa Hansen	Puyallup	WA	98374 US	5/21/2016
Jill Spaulding	Tacoma	WA	98446 US	5/21/2016
Ryan Shervanick	Puyallup	WA	98372 US	5/21/2016
Craig Strand	Puyallup	WA	98372 US	5/21/2016
Tracy Strand	Puyallup	WA	98372 US	5/21/2016
Isabelle Brennan	Puyallup	WA	98371 US	5/21/2016
Nick Marinelli	Puyallup	WA	98372 US	5/21/2016
Holly Luna	Orting	WA	98360 US	5/21/2016

Patricia Dalton	Bonney Lake	WA	98391 US	5/21/2016
Tessa Eyer	Graham	WA	98338 US	5/21/2016
Barbara Shaw	Puyallup	WA	98372 US	5/21/2016
Donna Anderson	Puyallup	WA	98375 US	5/21/2016
Erin Lakin	Tacoma	WA	98408 US	5/21/2016
Deborah Chadwick	Orting	WA	98360 US	5/21/2016
Alyssa Lunde	Puyallup	WA	98373 US	5/21/2016
Jessica Sandoval	Puyallup	WA	98374 US	5/21/2016
Josh Bedwell	Puyallup	WA	98372 US	5/21/2016
Stacy Olivas	Puyallup	WA	98372 US	5/21/2016
Susan Wahlund	Puyallup	WA	98372 US	5/21/2016
Seth Pierce	Puyallup	WA	98375 US	5/21/2016
Bryan Christianson	Puyallup	WA	98372 US	5/21/2016
Karl Olson	Sumner	WA	98390 US	5/21/2016
Tracy Wheeler	Puyallup	WA	98371 US	5/21/2016
Henriette Veltung	Roedovre		Denmark	5/21/2016
Dain Everett	Federal Way	WA	98023 US	5/21/2016
Jamie Evangelista	Puyallup	WA	98372 US	5/21/2016
Michele Larson	Tacoma	WA	98424 US	5/21/2016
Laurie Pettit	Puyallup	WA	98372 US	5/21/2016
Jeffrey Falkner	Puyallup	WA	98375 US	5/21/2016
Joyce Twiggs	Puyallup	WA	98373 US	5/21/2016
Kathy Palmer	Puyallup	WA	98371 US	5/21/2016
Peter Veltung	Puyallup	WA	98373 US	5/21/2016
Laura Forstein	Puyallup	WA	98374 US	5/21/2016
Brenna Faker	Tacoma	WA	98405 US	5/21/2016
Robyn Ghorley	Orting	WA	98360 US	5/21/2016
Marjie Madlena	Puyallup	WA	98374 US	5/21/2016
Jeanette Merod	Puyallup	WA	98375 US	5/21/2016
Michael Holbert	Puyallup	WA	98372 US	5/21/2016
Allen Mietzner	Sammamish	WA	98074 US	5/21/2016
Stephen Veltung	Puyallup	WA	98371 US	5/21/2016
Marshelle Rodin	Puyallup	WA	98374 US	5/21/2016
Nancy January	Puyallup	WA	98374 US	5/21/2016
Tracie Johnson	Puyallup	WA	98375 US	5/21/2016
Corey Lynn	Auburn	WA	98002 US	5/21/2016
Dwayne Patterson	Puyallup	WA	98375 US	5/21/2016
Kathy Powers	Puyallup	WA	98372 US	5/21/2016
Kristi Cook	Puyallup	WA	98374 US	5/21/2016
Tammy Hughes	Puyallup	WA	98372 US	5/21/2016
Andrew Rice	Puyallup	WA	98373 US	5/21/2016
Andrea Landron	Puyallup	WA	98371 US	5/21/2016
Bridget Stueve	Puyallup	WA	98371 US	5/21/2016
Nichole San Miguel	Puyallup	WA	98373 US	5/21/2016
Heather Schilling	Tacoma	WA	98406 US	5/21/2016
John Ward	Puyallup	WA	98372 US	5/21/2016
Rachel McLain-Rollins	Puyallup	WA	98374 US	5/21/2016

David Toves	Puyallup	WA	98374 US	5/21/2016
Thomas Gibson	Puyallup	WA	98374 US	5/21/2016
Eric Hadley	Graham	WA	98338 US	5/21/2016
Karen Peddersen	Tacoma	WA	98404 US	5/21/2016
Angel Espindola	Puyallup	WA	98373 US	5/21/2016
Courtney Lott	Puyallup	WA	98373 US	5/21/2016
David Grant	Puyallup	WA	98372 US	5/21/2016
Keri Jorgenson	Puyallup	WA	98372 US	5/21/2016
Diane Bartholomew	Tacoma	WA	98404 US	5/21/2016
April Putz	Puyallup	WA	98372 US	5/21/2016
Michael Goodwin	Sumner	WA	98390 US	5/21/2016
Leanne Blyth	Puyallup	WA	98372-4227 US	5/21/2016
Jody Wickett	Puyallup	WA	98374 US	5/21/2016
Meredith Wright	Puyallup	WA	98375 US	5/21/2016
Jacqualin Prusia	Sumner	WA	98390 US	5/21/2016
Cathlene Morris	Puyallup	WA	98372 US	5/21/2016
Christy Hoffman	Puyallup	WA	98372 US	5/21/2016
Jay Rush	Puyallup	WA	98372 US	5/21/2016
Dorothy Walker	Gig Harbor	WA	98335 US	5/21/2016
Phil Brooke	Tenino	WA	98589 US	5/21/2016
Andrea Lumley	Tacoma	WA	98407 US	5/21/2016
Eve Hart	Puyallup	WA	98374 US	5/21/2016
Janeen Morris	Puyallup	WA	98372 US	5/21/2016
Bradley Thompson	Tacoma	WA	98405 US	5/21/2016
Roxann Murray	Graham	WA	98338 US	5/21/2016
Tina Krogh	Puyallup	WA	98375 US	5/21/2016
Linda Goodwin	Tacoma	WA	98445 US	5/21/2016
Brittany Weible	Puyallup	WA	98372 US	5/22/2016
Lokee Beaver	Puyallup	WA	98372 US	5/22/2016
Paul Kempff	Puyallup	WA	98371-7343 US	5/22/2016
Tiffany Daswani	Seattle	WA	98136 US	5/22/2016
Carrie Silvey	Orting	WA	98360 US	5/22/2016
Amy Forest	Puyallup	WA	98373 US	5/22/2016
JO ann ARTIS	Tacoma	WA	98443 US	5/22/2016
mary stuart-fairburn	Tacoma	WA	98422 US	5/22/2016
Edward Smith	Yelm	WA	98597 US	5/22/2016
Sierra Tryon	Puyallup	WA	98273 US	5/22/2016
cody foster	Tacoma	WA	98418 US	5/22/2016
Angelina Louman	Puyallup	WA	98373 US	5/22/2016
Lydia (Dee) MARGESON	Tacoma	WA	98402 US	5/22/2016
Joshua Schleining	Tacoma	WA	98407 US	5/22/2016
Christine Dodson	Lakewood	WA	98439 US	5/22/2016
Kianna Jaye	Gig Harbor	WA	98332 US	5/22/2016
Lorryn Whisnant	Puyallup	WA	98374 US	5/22/2016
Heather Herlofson	Sumner	WA	98390 US	5/22/2016
Janice Martin	Puyallup	WA	98373 US	5/22/2016
Lana Billesbach	Puyallup	WA	98371 US	5/22/2016

Jeanne Westmark	Puyallup	WA	98372 US	5/22/2016
david mortensen	Puyallup	WA	98372 US	5/22/2016
Michael Brown	Puyallup	WA	98372 US	5/22/2016
Martin Svingen	Puyallup	WA	98374 US	5/22/2016
Mike Lewis	Puyallup	WA	98373 US	5/22/2016
Benjamin McCarter	Auburn	WA	98002 US	5/22/2016
Kerri Robberson	Puyallup	WA	98374 US	5/22/2016
thaddeus thun	Puyallup	WA	98372 US	5/22/2016
Jonathan Benoit	Puyallup	WA	98371 US	5/22/2016
Aaron Bonner	Tacoma	WA	98445 US	5/22/2016
Vicki Hurley	Tacoma	WA	98424 US	5/22/2016
Betty Ohlund	Puyallup	WA	98371 US	5/22/2016
Bret Corey	Puyallup	WA	98373 US	5/22/2016
Danielle Oleary	Graham	WA	98338 US	5/22/2016
Kevin Schoon	Orting	WA	98360 US	5/22/2016
Alysen Rasmussen	Puyallup	WA	98371 US	5/22/2016
Danielle Lee	Puyallup	WA	98372 US	5/22/2016
Lindsey Chavez	Puyallup	WA	98375 US	5/22/2016
Alexis Noonan	Tacoma	WA	98403 US	5/22/2016
Alisha Auditore	Puyallup	WA	98373 US	5/22/2016
Kelly Keone	Puyallup	WA	98372 US	5/22/2016
Krystal Hernandez	Puyallup	WA	98372 US	5/22/2016
Claudia Woodward-Rice	Aberdeen	WA	98520 US	5/22/2016
Erich Weiter	Puyallup	WA	98372 US	5/22/2016
Kevin Kruse	Puyallup	WA	98372 US	5/22/2016
Debi Thierbach	Fife	WA	98424 US	5/22/2016
Nanette Reetz	Tacoma	WA	98422 US	5/22/2016
Linda Chavez	Ephrata	WA	98823 US	5/22/2016
Maureen Dewey	Lacey	WA	98516 US	5/22/2016
Bryan Goffe	Seattle	WA	98198 US	5/22/2016
Kris Wilner	Puyallup	WA	98372 US	5/22/2016
Lawrence Beck	Carnation	WA	98014 US	5/22/2016
Jennifer Fulton	Graham	WA	98338 US	5/22/2016
Sean Young	Puyallup	WA	98375 US	5/22/2016
Jaimey McCormack	Puyallup	WA	98374 US	5/22/2016
Kami Ward	Puyallup	WA	98374 US	5/22/2016
Timothy Kuhlmann	Roy	WA	98580 US	5/22/2016
Jay McNabb	Puyallup	WA	98372 US	5/22/2016
Robert Mitchell	Tacoma	WA	98443 US	5/22/2016
Christina Goold	Puyallup	WA	98373 US	5/22/2016
Hai-Ping Hwang-Twigg	Puyallup	WA	98374 US	5/22/2016
Holly Beck	Carnation	WA	98014 US	5/22/2016
Lynda Gormley	Puyallup	WA	98372 US	5/22/2016
Max-Jayde Romero	Seattle, WA	AL	98103 US	5/22/2016
Hillary Kuehn	Buckley	WA	98321 US	5/22/2016
GORDON ROBERTS	Lynnwood	WA	98036 US	5/22/2016
Judi Otto	Orting	WA	98360-9593 US	5/23/2016

Eduardo Espinoza	Puyallup	WA	98372 US	5/23/2016
Roberta Peterson	Puyallup	WA	98374 US	5/23/2016
Tyler Hooper	Tacoma	WA	98445 US	5/23/2016
Cynthia Sepulveda Walker	Puyallup	WA	98375 US	5/23/2016
Audrey Dubay	Puyallup	WA	98374 US	5/23/2016
loretta savage	Puyallup	WA	98371 US	5/23/2016
Juanita Chavez	Moses Lake	WA	98837 US	5/23/2016
Irving Meyerson	Puyallup	WA	98372 US	5/23/2016
Cory Ready	Puyallup	WA	98372 US	5/23/2016
Melissa Ryan	Puyallup	WA	98372 US	5/23/2016
Gina Partain	Puyallup	WA	98374 US	5/23/2016
Aimee Hickman	Puyallup	WA	98372 US	5/23/2016
Christine Reeder	Sebring	FL	33872 US	5/23/2016
Shannon Tracy	Puyallup	WA	98374 US	5/23/2016
Tim Powers	Puyallup	WA	98372 US	5/23/2016
Patrick Michel	Puyallup	WA	98374 US	5/23/2016
Steve Silver	Puyallup	WA	98372 US	5/23/2016
Michael Peterson	Puyallup	WA	98374 US	5/23/2016
eric ryan	Puyallup	WA	98372 US	5/23/2016
Michelle Braithwaite wagner	Puyallup	WA	98372 US	5/23/2016
Lisa Palmer	Puyallup	WA	98375 US	5/23/2016
Caitlin Schweyen	Puyallup	WA	98372 US	5/23/2016
Niki Horace	Puyallup	WA	98372 US	5/23/2016
Tina Saxowsky	Puyallup	WA	98372 US	5/23/2016
Marc Saxowsky	Puyallup	WA	98372 US	5/23/2016
Michelle Meyers	Puyallup	WA	98371 US	5/23/2016
sharon mudge	Spanaway	WA	98387 US	5/23/2016
Susan Winterowd-Laughman	Puyallup	WA	98372 US	5/23/2016
Daniel Matthews	Puyallup	WA	98372 US	5/23/2016
Renee' Maloney	Bellingham	WA	98225 US	5/23/2016
Michael Sikora	Puyallup	WA	98374 US	5/23/2016
Christina Jensen	Vashon	WA	980970-2893 US	5/23/2016
Ed Herde	Tacoma	WA	98445 US	5/23/2016
Erin Jeffery	Sumner	WA	98390 US	5/23/2016
Andreia Brown	Puyallup	WA	98372 US	5/23/2016
Pete Wingard	Puyallup	WA	98371 US	5/23/2016
Edward Hock	Puyallup	WA	98374 US	5/23/2016
Jon Grundy	Puyallup	WA	98373 US	5/23/2016
Charleen Garman	Nokomis	FL	34275 US	5/23/2016
Stephen Biggerstaff	Puyallup	WA	98372 US	5/23/2016
Tracy Robillard	Puyallup	WA	98372 US	5/23/2016
Tracy Frye	Puyallup	WA	98374 US	5/23/2016
Brock Maxwell	Orting	WA	98360 US	5/23/2016
Ann Hurley	Puyallup	WA	98371 US	5/23/2016
Sylvia Ambrose	Orting	WA	98360 US	5/23/2016
Jill Baines	Tacoma	WA	98446 US	5/23/2016
misti nicodemus	Lake Tapps	WA	98391 US	5/23/2016

Tessa Stutes	Puyallup	WA	98375 US	5/23/2016
Bernard Miller	Puyallup	WA	98374 US	5/23/2016
Jacqueline Chose	Orting	WA	98360 US	5/23/2016
Per Bakken	Puyallup	WA	98372 US	5/23/2016
Taeler felderman	Puyallup	WA	98371 US	5/23/2016
Leilani Whitehurst	Orting	WA	98360 US	5/23/2016
Kris Haynes	Puyallup	WA	98374 US	5/23/2016
Natalie Scowden	Puyallup	WA	98372 US	5/23/2016
Trena Garrison	Orting	WA	98360 US	5/23/2016
Brittney Fleming	Bonney Lake	WA	98391 US	5/23/2016
Coleton Burbridge	Orting	WA	98360 US	5/23/2016
Tanya McNabb	Longmire	WA	98397 US	5/23/2016
Greg Parkhurst	Orting	WA	98360 US	5/23/2016
Stephanie Peters	Orting	WA	98360 US	5/23/2016
Tami Music	Puyallup	WA	Puyallup US	5/23/2016
Tyler Howland	Puyallup	WA	98371 US	5/23/2016
Sheenah Tripp	Orting	WA	98360 US	5/23/2016
Danette Knowles	Tacoma	WA	98407 US	5/23/2016
Jennifer Mahlum	Orting	WA	98360 US	5/23/2016
Janis Johnson	Port Orchard	WA	98367 US	5/23/2016
Crystal Nelson	Sumner	WA	98390 US	5/23/2016
Kaitlin Gleason	Orting	WA	98360 US	5/23/2016
Brandee Britschgi	South Prairie	WA	98360 US	5/23/2016
Erik Cottle	Orting	WA	98360 US	5/23/2016
Robyn Lawrence	Bonney Lake	WA	98391 US	5/23/2016
Dean Backus	Orting	WA	98360 US	5/23/2016
Paul McKean	Puyallup	WA	98374 US	5/23/2016
Melody Farrer	Bonney Lake	WA	98391 US	5/23/2016
Douglas Bishop	Orting	WA	98360 US	5/23/2016
Brittany King	Orting	WA	98360 US	5/23/2016
pat thackeray	Orting	WA	Orting WA US	5/23/2016
Danielle Hall	Orting	WA	98360 US	5/23/2016
Megan Gallwas	Puyallup	WA	98372 US	5/23/2016
Jenni Jones	Orting	WA	98360 US	5/23/2016
Shara Slaughter	Orting	WA	98360 US	5/23/2016
Deanna Cope	Graham	WA	98338 US	5/23/2016
Ariel Grob	Puyallup	WA	98372 US	5/23/2016
John Aldrich	La Push	WA	98350 US	5/23/2016
Rob Campbell	Puyallup	WA	98373 US	5/23/2016
Kim Wainscott	Orting	WA	98360 US	5/23/2016
Lacey Kindred	Tacoma	WA	98443 US	5/23/2016
Eric Adair	Sumner	WA	98390 US	5/23/2016
Jennie Slack	Puyallup	WA	98372 US	5/23/2016
Sharon Walvatne	Puyallup	WA	98375 US	5/23/2016
Amy Maiello	Orting	WA	98360 US	5/23/2016
Teresa Nylander	Shelton	WA	98584 US	5/23/2016
Marlo Frownfelder	Roslyn	WA	98941 US	5/23/2016

Alice Barney	Graham	WA	98338 US	5/23/2016
Cheryl Wagner	Lincoln	IL	62656 US	5/23/2016
Lisa Bakken	Puyallup	WA	98372 US	5/23/2016
Kahmina Monclova	Orting	WA	98360 US	5/23/2016
Beth-el Wilson	Orting	WA	98360 US	5/23/2016
belinda koetje	Buckley	WA	98321 US	5/23/2016
Kirsten Schmid	Tacoma	WA	98404 US	5/23/2016
Shannon Morrison	Puyallup	WA	98374 US	5/23/2016
Erin Rooney	Puyallup	WA	98374 US	5/23/2016
Violet Knapp	Astoria	OR	97103 US	5/23/2016
Kelly Kraemer	Graham	WA	98338 US	5/23/2016
James Mcafee	Tacoma	WA	98446 US	5/23/2016
Ashley Wickeremasuriya	Orting	WA	98360 US	5/23/2016
Burke Long	Lakewood	WA	98498 US	5/23/2016
Jeff Washington	Orting	WA	98360 US	5/23/2016
Dan Buckley	Puyallup	WA	98375 US	5/23/2016
Sara Meath	Puyallup	WA	98373 US	5/23/2016
Karyn Davies	Orting	WA	98360 US	5/23/2016
Don Bartel	Sumner	WA	98390 US	5/23/2016
Jon Winder	Puyallup	WA	98371 US	5/23/2016
Cheryl Moyd	Sumner	WA	98390 US	5/23/2016
Melissa Ellis	Puyallup	WA	98374 US	5/23/2016
Scott Bowman	Orting	WA	98360 US	5/23/2016
Karla Struble	Puyallup	WA	98371 US	5/23/2016
Carol Johnson	Puyallup	WA	98374 US	5/23/2016
Angie Ellison	Kenmore	WA	98028 US	5/23/2016
Shea Aitkens	Orting	WA	98360 US	5/23/2016
Kristel Schmidt	Maple Valley	WA	98038 US	5/23/2016
Robert Jones	Puyallup	WA	98372 US	5/23/2016
Kaelyn Smith	Orting	WA	98360 US	5/23/2016
Autumn Wright	Sumner	WA	98390 US	5/23/2016
Stephany Anderson	Puyallup	WA	98372 US	5/23/2016
Chris Cooper	Puyallup	WA	98373 US	5/23/2016
Calvin Burkhead	University Place	WA	98467 US	5/24/2016
Emma DeFord	Bonney Lake	WA	98391 US	5/24/2016
Rose Wedlund	Tacoma	WA	98445 US	5/24/2016
Beth Johnson	University Place	WA	98467 US	5/24/2016
Diana Wiklund	Issaquah	WA	98027 US	5/24/2016
Drew Newman	Bonney Lake	WA	98391 US	5/24/2016
Thomas weaver	Puyallup	WA	98373 US	5/24/2016
Brandi Morehouse	Puyallup	WA	98375 US	5/24/2016
Jorie Belnap	Spanaway	WA	98387 US	5/24/2016
Colin Jones	Orting	WA	98360 US	5/24/2016
Ricky Zevenbergen	Puyallup	WA	98375 US	5/24/2016
Tamie Nielsen	Puyallup	WA	98372 US	5/24/2016
Tammy Cooley	Orting	WA	98360 US	5/24/2016
Shonna Parks	Puyallup	WA	98371 US	5/24/2016

Julie Krebsbach	Buckley	WA	98321 US	5/24/2016
Stefanie Newsome	Orting	WA	98360 US	5/24/2016
Marlene Miller	Puyallup	WA	98374 US	5/24/2016
Amanda Brown	Puyallup	WA	98375 US	5/24/2016
Renee Osborne	Orting	WA	98360 US	5/24/2016
Amy Thayer	Tacoma	WA	98405 US	5/24/2016
Alix Stuckel	Snohomish	WA	98290 US	5/24/2016
Elisa Hays	Puyallup	WA	98372 US	5/24/2016
Tracy Busick	Graham	WA	98338 US	5/24/2016
Taylor Ford	Puyallup	WA	98374 US	5/24/2016
Amanda Marshall	Orting	WA	98360 US	5/24/2016
Terry Robinson	Citrus Heights	CA	95610 US	5/24/2016
Leanne Horton	Orting	WA	98360 US	5/24/2016
Morgan Brandfors	Lakewood	WA	98499 US	5/24/2016
William McCaffrey	Puyallup	WA	98371 US	5/24/2016
Peter Nelson	Puyallup	WA	98372 US	5/24/2016
Brooke Vasser-Learn	Puyallup	WA	98374 US	5/24/2016
Cale Streepy	Kent	WA	98032 US	5/24/2016
Torie Houghton	Puyallup	WA	98371 US	5/24/2016
Barb Souza	Orting	WA	98360 US	5/24/2016
Carl M. Stultz Stultz	Puyallup	WA	98371 US	5/24/2016
Brian Peterson	Puyallup	WA	98372 US	5/24/2016
Rose Nelson	Tacoma	WA	98446 US	5/24/2016
Robert Herren	Puyallup	WA	98374 US	5/24/2016
Tamara Carr	Puyallup	WA	98372 US	5/24/2016
Richard Berger	Orting	WA	98360 US	5/24/2016
Ronald Kempe	Puyallup	WA	98372 US	5/24/2016
nancy johnson	puyallup	WA	98371 US	5/24/2016
Kim Barnes	White Salmon	WA	98672 US	5/24/2016
Tracie Stipp	Puyallup	WA	98374 US	5/24/2016
Darlene Reilley	Spanaway	WA	98387 US	5/24/2016
Scott Van Wey	Orting	WA	98360 US	5/24/2016
April Phillips	Sumner	WA	98390 US	5/24/2016
Nancy MacDicken	Tacoma	WA	98446 US	5/24/2016
Bronwyn Webster	Orting	WA	98360 US	5/24/2016
Suzanne Sellers	Puyallup	WA	98373 US	5/24/2016
Shannon Fouts	Tacoma	WA	98408 US	5/24/2016
cheryl thacker	Puyallup	WA	98372 US	5/24/2016
Anne Waller	Buckley	WA	98321 US	5/24/2016
michael wise	Tacoma	WA	98446 US	5/24/2016
Charles Fricke	Lakewood	WA	98499 US	5/24/2016
Shawna Abel	Puyallup	WA	98373 US	5/24/2016
Andy Johnson	Tacoma	WA	98404 US	5/24/2016
kathy milano	Tacoma	WA	98403 US	5/24/2016
Candus Barnum	Puyallup	WA	98371 US	5/24/2016
Trisha Carlson	Orting	WA	98360-9343 US	5/24/2016
Joyce Hibma	North Bend	WA	98045 US	5/24/2016

Enid Scarfo	Puyallup	WA	98373 US	5/24/2016
Diane Johnson	Graham	WA	98338 US	5/24/2016
Allison Nelson	Puyallup	WA	98373 US	5/24/2016
Lisa Zander	Puyallup	WA	98372 US	5/24/2016
Inga Isakson	Seattle	WA	98168 US	5/24/2016
Samuel Carbajal	Orting	WA	98360 US	5/24/2016
Tricia Hukee	Puyallup	WA	98372 US	5/24/2016
Joan Brown	Orting	WA	98360 US	5/24/2016
Ross Cook	Kirkland	WA	98033 US	5/24/2016
Mallory Lux	seattle	WA	98119 US	5/24/2016
Casey Littlejohn	Puyallup	WA	98372 US	5/24/2016
Sullivan Rebecca	Puyallup	WA	98372 US	5/24/2016
Shenoah Miller	Lakewood	WA	98498 US	5/24/2016
Nancy Huber	Puyallup,	WA	98372 US	5/25/2016
Dustin Hall	Sumner	WA	98390 US	5/25/2016
Stephanie Mavin	Puyallup	WA	98374 US	5/25/2016
Andrea Snow	Puyallup	WA	98372 US	5/25/2016
Sarah DeWitt	Tacoma	WA	98443 US	5/25/2016
Benjamin DeGoede	Seattle	WA	98103 US	5/25/2016
Katie Porubek	Orting	WA	98360 US	5/25/2016
Craig Mattson	Bonney Lake	WA	98391 US	5/25/2016
Gayle Beckman	Puyallup	WA	98374 US	5/25/2016
Lisa Tylor	Puyallup	WA	98372 US	5/25/2016
debby Herbert	Tacoma	WA	98402 US	5/25/2016
MARY POWELL	Graham	WA	98338 US	5/25/2016
Mstthew Link	Orting	WA	98360 US	5/25/2016
Lisa Satterthwaite	Bonney Lake	WA	98391 US	5/25/2016
Mariah Beckman	Puyallup	WA	98374 US	5/25/2016
Ellie DeGoede	Bonney Lake	WA	98391 US	5/25/2016
Nancy Brennan	Puyallup	WA	98373 US	5/25/2016
marcia wheeler	orting	WA	98360 US	5/25/2016
Janice Andreasen	Sumner	WA	98390 US	5/25/2016
alease frieson	Tacoma	WA	98406 US	5/25/2016
kristen rognaldson	wa	WA	98404 US	5/25/2016
Jana Wennstrom	Tacoma	WA	98404 US	5/25/2016
Sally Radford	Tacoma	WA	98409 US	5/25/2016
Stephanie Bowen	Puyallup	WA	98374 US	5/25/2016
Williamson Kristen	Spanaway	WA	98387-3039 US	5/25/2016
Kristina Brown	Tacoma	WA	98422 US	5/25/2016
Mikala Rushton	Lakewood	WA	98498 US	5/25/2016
Tom Clowers	Puyallup	WA	98373 US	5/25/2016
Kristina Gourdine	Puyallup	WA	98372 US	5/25/2016
Allison Frosr	Puyallup	WA	98374 US	5/25/2016
Ulli Clowers	Puyallup	WA	98373 US	5/25/2016
Mariesa Bus	Tacoma	WA	98443 US	5/25/2016
Jeanette Lively	Tacoma	WA	98404 US	5/25/2016
Sierra Whitney	Milton	WA	98354 US	5/25/2016

John Warwick	Wilkeson	WA	98396 US	5/25/2016
Bryce Allison	Seattle	WA	98122 US	5/25/2016
Kathy McFall-Butler	Federal Way	WA	98023 US	5/26/2016
Kim Tunison	Puyallup	WA	98372 US	5/26/2016
Rachel Prokop	Puyallup	WA	98372 US	5/26/2016
Susan Beilke	Puyallup	WA	98372 US	5/26/2016
Wendy Pedersen	Puyallup	WA	98372 US	5/26/2016
sharlene frantsen	Puyallup	WA	98372 US	5/26/2016
Traci Biss	Puyallup	WA	98372 US	5/26/2016
Cheryl Anderson	Kent	WA	98032 US	5/26/2016
Brenda Ireland	Puyallup	WA	98374 US	5/26/2016
Laurie Brewer	Chico	CA	95973 US	5/26/2016
Kevin Blackburn	Puyallup	WA	98371 US	5/26/2016
Douglas A. Maske	Puyallup	WA	98375 US	5/26/2016
James Chase	Graham	WA	98338 US	5/26/2016
April Madsen	Puyallup	WA	98375 US	5/26/2016
Heidi Jones-Maguire	Puyallup	WA	98374 US	5/26/2016
ERIC FARRER	Puyallup	WA	98371 US	5/26/2016
Jennifer Harris	Tacoma	WA	98443 US	5/26/2016
jeff graham	Puyallup	WA	98372 US	5/26/2016
Elaine Grant	Puyallup	WA	98371 US	5/26/2016
Jeff Siegel	Puyallup	WA	98374 US	5/26/2016
Karen Barker	Puyallup	WA	98374 US	5/26/2016
Alice M Phelps	Graham	WA	98338 US	5/26/2016
Ty Cummings	Puyallup	WA	98374 US	5/26/2016
Maggie Mason	Sumner	WA	98390 US	5/26/2016
Rick heseltine	Puyallup	WA	98372 US	5/27/2016
Flora Wilson	Puyallup	WA	98372 US	5/27/2016
Chastity Mullins	Puyallup	WA	98372 US	5/27/2016
Larry Lozensky	Puyallup	WA	98374 US	5/27/2016
Heather Straub	Puyallup	WA	98372 US	5/27/2016
Celia Clifford	University Place	WA	98467 US	5/27/2016
Mackenzie Fruge	Puyallup	WA	98372 US	5/27/2016
Sumer Hilton	Puyallup	WA	98372 US	5/27/2016
Christopher Clifford	Tacoma	WA	98409 US	5/27/2016
James Siegel	Decatur	IN	46733 US	5/27/2016
Gennah Stocks	Puyallup	WA	98372 US	5/27/2016
Dawn Jensema	Puyallup	WA	98374 US	5/27/2016
John Budzak Jr	Puyallup	WA	98372 US	5/27/2016
Corrin Cabrera	Bonney Lake	WA	98391 US	5/27/2016
Joel Steveson	Puyallup	WA	98372 US	5/27/2016
Sarah Steveson	Puyallup	WA	98372 US	5/27/2016
Isaac Stocks	Puyallup	WA	98372 US	5/27/2016
Lynn Maloney	Bonney Lake	WA	98391 US	5/27/2016
Doug Bartlett	Spanaway	WA	98387 US	5/27/2016
Reanna Pettigrew	Port Orchard	WA	98367 US	5/27/2016
Lisa Crinklaw	Toledo	OR	97391 US	5/27/2016

Nickolas Neville	Puyallup	WA	98374 US	5/27/2016
Kimberly Johnson	Seattle	WA	98109 US	5/27/2016
Anne Phillips	Puyallup	WA	98372 US	5/27/2016
Cortnie Goodin	Bonney Lake	WA	98391 US	5/27/2016
Deborah Kiley-gibson	Tacoma	WA	98446 US	5/27/2016
tony talevich	seattle	WA	98118 US	5/27/2016
Cody Gapp	Seattle	WA	98104 US	5/27/2016
Anna Ferrario	Tacoma	WA	98403 US	5/27/2016
David Klein	Puyallup	WA	98372 US	5/27/2016
April Goffena	Sumner	WA	98390 US	5/27/2016
John Hancock	Sumner	WA	98390 US	5/27/2016
Barb Skinner	Puyallup	WA	98374 US	5/27/2016
Yvonne Perry	Everett	WA	98203 US	5/27/2016
Naveen Mukkollu	Puyallup	WA	98372 US	5/27/2016
Kelly Crouch	Sumner	WA	98390 US	5/27/2016
Holly Christiansen	Puyallup	WA	98374 US	5/27/2016
Tawnya Tucker	Graham	WA	98338 US	5/28/2016
Lauri Lindquist	Tacoma	WA	98407 US	5/28/2016
Robin Pruettt	Puyallup	WA	98374 US	5/28/2016
sharon gustafson	Puyallup	WA	98374 US	5/28/2016
Mandy Zevenbergen	Tacoma	WA	98406 US	5/28/2016
Frederick nelson	Puyallup	WA	98372 US	5/28/2016
Terri Whitehouse	Puyallup	WA	98372 US	5/28/2016
dawn bendon	Bonney Lake	WA	98391 US	5/28/2016
Jill Mansfield-Riddell	Bonney Lake	WA	98391 US	5/28/2016
Rosemary Slater	Las Vegas	NV	89101 US	5/28/2016
Sandra Dickey	Puyallup	WA	98374 US	5/28/2016
Reuben Pielle	Auburn	WA	98002 US	5/28/2016
Emily Pawul	Puyallup	WA	98372 US	5/28/2016
Rick Smith	Puyallup	WA	98374 US	5/28/2016
Julie Grant	Puyallup	WA	98372 US	5/29/2016
Hunter Mullins	Puyallup	WA	98372 US	5/29/2016
jeffery berg	Puyallup	WA	98374 US	5/29/2016
Ramona Strassburg	Puyallup	WA	98372 US	5/29/2016
Jon Turmel	Puyallup	WA	98374-4158 US	5/29/2016
Caitlin hennessy	Spokane	WA	99203 US	5/29/2016
frank dimayuga	Tacoma	WA	98407 US	5/30/2016
Joseph Gray	Puyallup	WA	98372 US	5/30/2016
Emily Gray	Puyallup	WA	98372 US	5/30/2016
Natasha Barnes	Puyallup	WA	98372 US	5/30/2016
Marcy Vslencia	Renton	WA	98058 US	5/30/2016
Shane Heyden	Seattle	WA	98146 US	5/31/2016
Ryan Lund	Kent	WA	98042 US	6/1/2016
Bryce Warning	Puyallup	WA	98372 US	6/1/2016
Alan Thompson	Puyallup	WA	98374 US	6/1/2016
Kaleb Jenson	Puyallup	WA	98372 US	6/1/2016
Hannah Ireland	Tacoma	WA	98446 US	6/1/2016

Anita Yamada	Puyallup	WA	98374 US	6/1/2016
diane wallace	Puyallup	WA	98372 US	6/2/2016
Janine Duhon	Sumner	WA	98390 US	6/2/2016
Arrisa Medina	Puyallup	WA	98373 US	6/2/2016
Sharon Dazey	Puyallup	WA	98374 US	6/2/2016
Frank Door	Puyallup	WA	98374 US	6/2/2016
Michael Brown	Puyallup	WA	98371 US	6/2/2016
Teresa Bailey	Sumner	WA	98390 US	6/2/2016
Stephanie Bailey	Puyallup	WA	98375 US	6/2/2016
Kristina Mccann	Tacoma	WA	98443 US	6/2/2016
Turan Kayaoglu	Puyallup	WA	98371 US	6/2/2016
Dawn Morrell	Puyallup	WA	98374 US	6/2/2016
Dianne Waters	Tacoma	WA	98444 US	6/2/2016
Vicki Cribbin	Puyallup	WA	98374 US	6/2/2016
Suzanne Blackburn	Spanaway	WA	98387 US	6/2/2016
Corry Glucoft	Puyallup	WA	98372 US	6/2/2016
Sally Kronoff	Puyallup	WA	98372 US	6/2/2016
CHARLEY STOKES	Puyallup	WA	98372 US	6/2/2016
Cathie Vance	Puyallup	WA	98372 US	6/2/2016
JoLynn Skilton	Puyallup	WA	98374 US	6/2/2016
Tina Beckendorf	Puyallup	WA	98374 US	6/2/2016
Dave Seabrook	Tacoma	WA	98404 US	6/2/2016
Jessica McVittie	Puyallup	WA	98372 US	6/3/2016
Merrily Fillmore	Puyallup	WA	98372 US	6/3/2016
Jacob Wise	Fife	WA	98424 US	6/3/2016
Laura Nagai	Puyallup	WA	98372 US	6/3/2016
Mary Webster	Puyallup	WA	98373 US	6/3/2016
Marie Lloyd	Puyallup	WA	98372 US	6/3/2016
Guy Reese	Lakewood	WA	98499 US	6/3/2016
Nancy Parsons	Auburn	WA	98092 US	6/3/2016
karen varney	Graham	WA	98338 US	6/3/2016
Peggy Raether	Puyallup	WA	98371 US	6/3/2016
Clara Jacobson	Olympia	WA	98512 US	6/3/2016
Susan Scelzi	Puyallup	WA	98371 US	6/3/2016
Jerry Scelzi	Puyallup	WA	98371 US	6/3/2016
Heather Peizner	Portland	OR	97223 US	6/3/2016
Kathryn Baker	Puyallup	WA	98374 US	6/3/2016
Earline Cox	Puyallup	WA	98372 US	6/3/2016
Sarah Flanagan	Puyallup	WA	98374 US	6/3/2016
Theresa Maher	Puyallup	WA	98374 US	6/4/2016
Charles Bullion	Puyallup	WA	98374 US	6/4/2016
Diana Christian	Puyallup	WA	98374 US	6/4/2016
Shanna Nix	Puyallup	WA	98373 US	6/4/2016
Karrie Vogt	Puyallup	WA	98371 US	6/4/2016
Johanna Cherland	Puyallup	WA	98374 US	6/4/2016
Loretta Cutter	Puyallup	WA	98371 US	6/4/2016
Hannah Russ	Puyallup	WA	98373 US	6/4/2016

Randy Cutter	Puyallup	WA	98371 US	6/4/2016
Aaron Warchola	Puyallup	WA	98374 US	6/4/2016
Janet Bissonette	Puyallup	WA	98374 US	6/4/2016
Chad Kamke	Puyallup	WA	98374 US	6/4/2016
Karri Wicka	Puyallup	WA	98374 US	6/4/2016
Dian Nuxol	Puyallup	WA	98374 US	6/4/2016
Ethan Wilson	Puyallup	WA	98372 US	6/4/2016
Kristina Moats	puyallup	WA	98373 US	6/4/2016
Jennifer Carrington	Roy	WA	98580 US	6/4/2016
Elizabeth Feucht	Puyallup	WA	98374 US	6/4/2016
Tom Thomas	Seattle	WA	98122 US	6/4/2016
Katherine Holley	Puyallup	WA	98374 US	6/5/2016
Branden Soldonia	Buckley	WA	98321 US	6/5/2016
Kim McWillis Ribble	Bellingham	WA	98225 US	6/5/2016
Fred Howard	Puyallup	WA	98372 US	6/5/2016
Aly Hirz	Puyallup	WA	98373 US	6/5/2016
Adam Grapenthin	Sumner	WA	98390 US	6/5/2016
Sharron Taylor	Puyallup	WA	98374 US	6/6/2016
Daniel Smith	Puyallup	WA	98371 US	6/6/2016
John Finigan	Puyallup	WA	98372 US	6/6/2016
Jon Willey	Puyallup	WA	98374 US	6/7/2016
Tyler Rasmussen	Puyallup	WA	98372 US	6/7/2016
ROSEMARIE COSGAYON	Puyallup	WA	98372 US	6/7/2016
Jody Akerman	Pacific	WA	98047 US	6/8/2016
Judith Sawatzky	Federal Way	WA	98023 US	6/8/2016
Chris Akerman	Puyallup	WA	98372 US	6/8/2016
Stephen Hall	Seattle	WA	98103 US	6/8/2016
Heather Witters	Tacoma	WA	98424 US	6/8/2016
Brandon Davis	Tacoma	WA	98424 US	6/9/2016
Kathy Stephens	Graham	WA	98338 US	6/9/2016
Craig Busick	Olympia	WA	98512 US	6/9/2016
Camille Warning	Puyallup	WA	98372 US	6/9/2016
Bonnie Munce	Puyallup	WA	98374 US	6/10/2016
Jeanne Miller	puyallup	WA	98472 US	6/10/2016
Linda Krutenat	Seattle	WA	98116 US	6/10/2016
Josh Davidson	Tacoma	WA	98445 US	6/11/2016
Patricia Warren	Puyallup	WA	98374 US	6/11/2016
Joyce Blatterman	Puyallup	WA	98374 US	6/12/2016
David Hyres	Puyallup	WA	98372 US	6/13/2016
Debra Look	Tacoma	WA	98443 US	6/13/2016
Tinell Skaug	Puyallup	WA	98371 US	6/15/2016
Amyee Pearson	Puyallup	WA	98371 US	6/15/2016
Chantal Jacobsen	Tacoma	WA	98404 US	6/15/2016
Albert Devine	Puyallup	WA	98371 US	6/15/2016
Amy Johnson	Puyallup	WA	98371 US	6/15/2016
Carol Salick	Tacoma	WA	98404 US	6/15/2016
Theresa Taft	Puyallup	WA	98375 US	6/15/2016

Tara Carlsen	Tacoma	WA	98443 US	6/15/2016
Jesse Smith	Puyallup	WA	98372 US	6/15/2016
Tia Cady	Puyallup	WA	98372 US	6/15/2016
Janea Deluz	Puyallup	WA	98374 US	6/15/2016
Pamela Mower	Puyallup	WA	98371 US	6/15/2016
pam huff	Puyallup	WA	98374 US	6/15/2016
Mary-Elizabeth Gendreau	Puyallup	WA	98374 US	6/15/2016
Erin Tangen	Puyallup	WA	98371 US	6/16/2016
Margaret Williams	Puyallup	WA	98371 US	6/16/2016
Nathan Flansburg	Puyallup	WA	98375 US	6/16/2016
roger Crewse	Tacoma	WA	98445 US	6/16/2016
McKenzi Kent	Puyallup	WA	98373 US	6/16/2016
Melissa Davidson	Puyallup	WA	98372 US	6/16/2016
Shannon Wickstrom	Camarillo	CA	93012 US	6/16/2016
Brianne Seston	Puyallup	WA	98371 US	6/16/2016
Andrew Dahl	Puyallup	WA	98372 US	6/16/2016
Heidi colkitt	Puyallup	WA	98371 US	6/16/2016
Sayler Namy	Puyallup	WA	98373 US	6/16/2016
Jami Dahl	Puyallup	WA	98374 US	6/16/2016
Kaylyn Dahl	Puyallup	WA	98372 US	6/16/2016
Alex Williams	Sumner	WA	98390 US	6/17/2016
sandra Hardaway	Graham	WA	98338 US	6/17/2016
Michael Gibbons	Puyallup	WA	98373 US	6/17/2016
Chelsea Kennedy	Puyallup	WA	98373 US	6/17/2016
Tobin Risser	Auburn	WA	98092 US	6/17/2016
Marci Berry	Puyallup	WA	98372 US	6/17/2016
Caleb Livernash	Puyallup	WA	98372 US	6/17/2016
Tori Trujillo	Puyallup	WA	98372 US	6/17/2016
Alec McNamara	Puyallup	WA	98372 US	6/17/2016
Carly Crossley	Tacoma	WA	98466 US	6/17/2016
Tom Smillie	Puyallup	WA	98371 US	6/17/2016
Megan Larson	Puyallup	WA	98372 US	6/17/2016
Jesse Jorgensen	Puyallup	WA	98372 US	6/17/2016
Susan Scott	Tacoma	WA	98404 US	6/18/2016
Spencer Fox	Bonney Lake	WA	98391 US	6/18/2016
Tim Lindstrom	puyallup	WA	98375 US	6/18/2016
Renee smith	Puyallup	WA	98371 US	6/19/2016
Courtney Craft	Puyallup	WA	98374 US	6/20/2016
Kelley Larson	Puyallup	WA	98372 US	6/21/2016
Victor Hernandez	Puyallup	WA	98372 US	6/21/2016
Elizabeth Owen	Puyallup	WA	98372 US	6/23/2016
Nancy Brown	Puyallup	WA	98371 US	6/25/2016
Lynette Mix	Puyallup	WA	98372 US	6/28/2016
Janelle Dittus	Puyallup	WA	98372 US	6/28/2016
Lee Dedinsky	Milton	WA	98354 US	6/28/2016
Alexx Elder	Puyallup	WA	98372 US	6/28/2016
Geoffrey Dairiki	Shoreline	WA	98155-6021 US	6/28/2016

Bud BERG	Milton	WA	98354 US	6/29/2016
Karen McNamara	Puyallup	WA	98372 US	6/29/2016
Jean Watt	Puyallup	WA	98374 US	6/29/2016
Kimberly Ramsey	Puyallup	WA	98372 US	6/29/2016
Heather Dolan	Spanaway	WA	98387 US	6/30/2016
Makayla Dolan	Puyallup	WA	98375 US	6/30/2016
Jake Irvine	Orting	WA	98360 US	6/30/2016
Sarah Smith	Puyallup	WA	98371 US	6/30/2016
Carol Green	Silverdale	WA	98383 US	6/30/2016
Coralie Gustafson	Bonney Lake	WA	98391 US	6/30/2016
William Green	Silverdale	WA	98383 US	6/30/2016
Paige Johansen	Puyallup	WA	98372 US	7/2/2016
Sarah Withrow	Puyallup	WA	98375 US	7/2/2016
Nancy Galloway	Puyallup	WA	98374 US	7/2/2016
Victoria Bower	Puyallup	WA	98373 US	7/2/2016
Shaun Isenberger	Puyallup	WA	98373 US	7/2/2016
elona livingston	Puyallup	WA	98374 US	7/3/2016
Robin Farris	Puyallup	WA	98371 US	7/5/2016
Rachel bakke	Tacoma	WA	98405 US	7/7/2016
Tara Wagener	Puyallup	WA	98373 US	7/13/2016
Dianna Hamilton	Puyallup	WA	98372 US	7/13/2016
Chris Knutson	Puyallup	WA	98373 US	7/13/2016
pam Antelo	Orting	WA	98360 US	7/13/2016
Mary Ugelstad	Puyallup	WA	98374 US	7/13/2016
Rebecca Redford	Sumner	WA	98390 US	7/13/2016
Barbara Samuelson	Puyallup	WA	98371 US	7/13/2016
Sarah Biggerstaff	Puyallup	WA	98372 US	7/13/2016
Jackie James	Graham	WA	98338 US	7/13/2016
Bailey Zydek	Sumner	WA	98390 US	7/13/2016
Vernon Grayson	Sumner	WA	98390 US	7/13/2016
Robert Bartho	Tacoma	WA	98422 US	7/13/2016
Jaleta Coy	Puyallup	WA	98374 US	7/13/2016
Nicole McClung	Puyallup	WA	98375 US	7/13/2016
Sandra Kittelson	Tacoma	WA	98422 US	7/13/2016
Kyra Erickson	Tacoma	WA	98443 US	7/13/2016
Jacob Umar	Issaquah	WA	98027 US	7/13/2016
Danyel Pink	Puyallup	WA	98371 US	7/13/2016
Lauren Ferrin	Puyallup	WA	98375 US	7/13/2016
judy robling	Spanaway	WA	98387 US	7/13/2016
Randy Meier	Seattle	WA	98107 US	7/13/2016
Susan Soros	Buckley	WA	98321 US	7/13/2016
Rob Graham	Puyallup	WA	98374 US	7/13/2016
Marissa Ostrem	Puyallup	WA	98375 US	7/13/2016
al hall	Puyallup	WA	98375 US	7/13/2016
Kirsten DeWittie	Puyallup	WA	98374 US	7/13/2016
Julie Gallardo-Lemke	Puyallup	WA	98374 US	7/13/2016
Penny Nufer	Sumner	WA	98390 US	7/13/2016

Jackie Chaplin	Puyallup	WA	98372 US	7/13/2016
Paul Rodrigues	Puyallup	WA	98372 US	7/13/2016
roger seela	bonneylake	WA	98391 US	7/13/2016
Les Berenson MD	Seattle	WA	98103 US	7/14/2016
Andrea Young	Puyallup	WA	98371 US	7/14/2016
Brice Yarbrough	Bonney Lake	WA	98391 US	7/14/2016
Bambi Hiron	Lake Tapps	WA	98391 US	7/14/2016
Marian Berejikian	Gig Harbor	WA	98332 US	7/14/2016
Heather McFarlane	Burley	WA	98322 US	7/15/2016
Tracy Engels	Puyallup	WA	98374 US	7/15/2016
Keith Mayo	Gig Harbor	WA	98335 US	7/15/2016
Kim Jasionkowski	Puyallup	WA	98372 US	7/15/2016
Sandra Wusterbarth Brown	Puyallup	WA	98375 US	7/16/2016
Brian Carroll	Puyallup	WA	98371 US	7/19/2016
Ashley Benson	Puyallup	WA	98373 US	7/19/2016
ANTHONY PETRUCCELLI	Puyallup	WA	98372 US	7/20/2016
Patrick Lathrop	Puyallup	WA	98373 US	7/21/2016
Tim Parham	Seattle	WA	98118 US	7/23/2016
Jeff Schroeder	Puyallup	WA	98372 US	7/30/2016
Chawewan LeFord	Puyallup	WA	98372 US	7/31/2016
Tyler Doty	Puyallup	WA	98371 US	8/7/2016
James Clinton	Puyallup	WA	98372 US	8/16/2016
Donald Partington	Bonney Lake	WA	98391 US	8/18/2016
Dixie Gatchel	Puyallup	WA	98372 US	8/21/2016
Julie Drobny	Bonney Lake	WA	98391 US	8/21/2016
Eugene Marsh	Fox Island	WA	98333 US	8/21/2016
Jim Kastama	Puyallup	WA	98372 US	8/21/2016
Ryan Drobny	Puyallup	WA	98374 US	8/21/2016
Linda Bandli	Puyallup	WA	98371 US	8/21/2016
Nicki Stocks	Puyallup	WA	98372 US	8/22/2016
Michelle Lombard	Puyallup	WA	98374 US	8/22/2016
Shannon Knoben	Seattle	WA	98103 US	8/22/2016
hobear1087@msn.com land	Scappoose	OR	97056 US	8/22/2016
Jane Moore	Tacoma	WA	98402 US	8/23/2016
Danny Harden	Puyallup	WA	98374 US	8/24/2016
Anisa Parks	Graham	WA	98338 US	8/24/2016
Carolyn Warhol	Puyallup	WA	98375 US	8/25/2016
Rebecca Mann	Puyallup	WA	98371 US	8/25/2016
Russell Matthews	Kent	WA	98042 US	8/26/2016
Andrew Bodman	North Bend	WA	98045 US	8/29/2016
Hannah Miner	Tacoma	WA	98405 US	8/30/2016
Ken Kinyon	Port Orchard	WA	98366 US	9/11/2016
Karl Mecklenburg	Tacoma	WA	98404 US	9/12/2016
Terry Schneider	Puyallup	WA	98372 US	9/12/2016
Nicole Johnson	Puyallup	WA	98371 US	9/12/2016
TONI fINDT	Milton	WA	98354 US	9/12/2016
Sherry Schneider	Puyallup	WA	98372 US	9/12/2016

Kurt Anderson	Puyallup	WA	98374 US	9/13/2016
Marie Moren	Puyallup	WA	98375 US	9/13/2016
Jane Campos Johnson	Puyallup	WA	98371 US	9/13/2016
Joanne Anderson	Puyallup	WA	98373 US	9/13/2016
Wayne Clifford	Rochester	WA	98579 US	9/14/2016
Nichole Clifford	Pasco	WA	99301 US	9/14/2016
Marilyn Eby	Pasco	WA	99301 US	9/14/2016
Laura Goggins	Kirkland	WA	98033 US	9/14/2016
Joseph Colombo	Puyallup	WA	98373 US	9/14/2016
Carole Geis	Puyallup	WA	98371 US	9/14/2016
Laura Muir	Puyallup	WA	98374 US	9/14/2016
Robert Johnson	Puyallup	WA	98371 US	9/14/2016
Amy Bartnes	Puyallup	WA	98372 US	9/15/2016
Corrine Weidman	Tacoma	WA	98445 US	9/15/2016
Lonie Kragelund	Puyallup	WA	98375 US	9/15/2016
Shannon Winslow	Filer	ID	82338 US	9/15/2016
Lindzie Kuzmer	Puyallup	WA	98374 US	9/16/2016
Jean McKenzie	Puyallup	WA	98374 US	9/16/2016
Eric Murry	Puyallup	WA	98375 US	9/16/2016
Karen Gustafson	Puyallup	WA	98372 US	9/16/2016
Michelle Shandrow	Puyallup	WA	98372 US	9/16/2016
Sue Reynolds	Cheyenne	WY	82007 US	9/17/2016
Helen Mamic	Puyallup	WA	98374 US	9/17/2016
Tyler Van	Anderson Island	WA	98303 US	9/20/2016
scott eliot	Auburn	WA	98092 US	9/30/2016
becca mejia	Puyallup	WA	98372 US	10/2/2016
Rebecca Holton	Oroville	WA	98844 US	10/18/2016
John Brohan	Puyallup	WA	98372 US	11/30/2016
Tina Johnson	Puyallup	WA	98375 US	11/30/2016
Randall Priest	Tacoma	WA	98424 US	12/14/2016
Julia Priest	Tacoma	WA	98424 US	12/19/2016
Allison Lew	Los Angeles	CA	90024 US	1/5/2017
Tracy Abdella	Puyallup	WA	98374 US	1/28/2017
Lauri Cotton	Puyallup	WA	98374 US	1/28/2017
Carol Paul	Puyallup	WA	98374 US	1/28/2017
Jeff Billings	Puyallup	WA	98374 US	1/28/2017
ELLEN PROUTY	Puyallup	WA	98374 US	1/30/2017
Tammi Farnese	Puyallup	WA	98375 US	1/30/2017
jeff keller	Puyallup	WA	98372 US	3/6/2017
Jennifer Grey	Edgewood	WA	98372-9229 US	3/6/2017
David Hsu	Puyallup	WA	98374 US	3/6/2017
Angelina Lease	Puyallup	WA	98372 US	3/6/2017
Megan Klein	Puyallup	WA	98372 US	3/6/2017
Beth Herner	Puyallup	WA	98374 US	3/6/2017
Nikole Fargher	Puyallup	WA	98372 US	3/6/2017
Eva Hagerman	Tacoma	WA	98404 US	3/6/2017
Jeff Yelton	Puyallup	WA	98374 US	3/6/2017

Sara Edwards	Puyallup	WA	98372 US	3/6/2017
Eddie Richie	Puyallup	WA	98374 US	3/6/2017
Andrea Lopez	Puyallup	WA	98372 US	3/6/2017
Raymond McKinney	Puyallup	WA	98372 US	3/6/2017
Marilyn Harlan	Puyallup	WA	98374 US	3/6/2017
Katrina James	Puyallup	WA	98375 US	3/6/2017
Heidi Sanders	Puyallup	WA	98374 US	3/6/2017
crystal hartwell	Puyallup	WA	98374 US	3/6/2017
Maura Shawhan	Puyallup	WA	98374 US	3/6/2017
Christina Lauderdale	Puyallup	WA	98372 US	3/6/2017
Barbara Richards	Puyallup	WA	98374 US	3/6/2017
Muriel WOhrl	PUYALLUP	WA	98372-4041 US	3/6/2017
Joe Fitzsimmons	Puyallup	WA	98372 US	3/6/2017
Michael Winger	Puyallup	WA	98374 US	3/6/2017
Debbie Garrison	Lakewood	WA	98499 US	3/6/2017
Debbie Whitestine	Puyallup	WA	98374 US	3/6/2017
Kathleen Sarten	Puyallup	WA	98372 US	3/6/2017
George Gray	Sumner	WA	98390 US	3/6/2017
Liz Lombardo	Puyallup	WA	98372 US	3/6/2017
Geraldine Recalde	Puyallup	WA	98372 US	3/6/2017
Jason Lombardo	Puyallup	WA	98372 US	3/6/2017
Laura Hickerson	Puyallup	WA	98372 US	3/6/2017
Jaemie Robinson	Puyallup	WA	98372 US	3/6/2017
Taylor Evana	Puyallup	WA	98372 US	3/6/2017
Susan Kolwitz	Puyallup	WA	98372 US	3/6/2017
Avis Jobrack	Puyallup	WA	98372 US	3/6/2017
Dennis Davenport	Puyallup	WA	98374 US	3/6/2017
Melanie Danielsen	Puyallup	WA	98372 US	3/6/2017
Deanna Feiring	Puyallup	WA	98372 US	3/7/2017
Jeffrey Feiring	Puyallup	WA	98372 US	3/7/2017
Stacy Troupin	Puyallup	WA	98372 US	3/7/2017
Donald Norris	Puyallup	WA	98374 US	3/7/2017
Sue Jackson	Spanaway	WA	98387 US	3/7/2017
Destiny Johnson	Puyallup	WA	98372 US	3/7/2017
Longnecker Jacob	Puyallup	WA	98372 US	3/7/2017
Robert King	Puyallup	WA	98374 US	3/7/2017
Kevin Miller	Puyallup	WA	98372 US	3/7/2017
Alison Ball	Puyallup	WA	98372 US	3/8/2017
Mary Hall	Puyallup	WA	98374 US	3/8/2017
Miriam Beckstrom	Carbondale	IL	62902 US	3/8/2017
Lynnell Young	Tacoma	WA	98445 US	3/8/2017
Bertha Turner	Knoxville	TN	37919 US	3/8/2017
m h	austin	TX	76813 US	3/8/2017
Raleigh Koritz	PLYMOUTH	MN	55442 US	3/8/2017
krista Brown	Royersford	PA	19468 US	3/8/2017
Elaine Smith	Puyallup	WA	98374 US	3/8/2017
Tracy Edenfield	Puyallup	WA	98372 US	3/10/2017

George Sedlack	Puyallup	WA	98374 US	3/11/2017
Kim Weule	Puyallup	WA	98374 US	3/16/2017
Michelle Ragle	North Platte	NE	69101 US	5/3/2017
Ada Rosman	Puyallup	WA	98371 US	5/5/2017
Johanna Moeller	Tacoma	WA	98403 US	5/5/2017
Rachel Leip	Jefferson City	MO	65109 US	5/5/2017
Debra Collins	Puyallup	WA	98374 US	5/5/2017
Jess Sprague	Puyallup	WA	98373 US	5/5/2017
Annie Anson	Puyallup	WA	98375 US	5/5/2017
christina Schneider	South Hill	WA	98375 US	5/5/2017
Racheal Cummings	Carbonado	WA	98323 US	5/5/2017
mandi ashworth	Puyallup	WA	98374-2966 US	5/5/2017
Stephanie Whisenhunt	Sumner	WA	98390 US	5/5/2017
Eric True	Puyallup	WA	98374 US	5/5/2017
Cheryl Wichers	Puyallup	WA	98373 US	5/5/2017
Audrey Cones	Puyallup	WA	98374 US	5/5/2017
Michelle Woolvs	Saint Louis	MO	63105 US	5/5/2017
Lisa Rosseels	Puyallup	WA	98371 US	5/5/2017
Terri Poulin	Puyallup	WA	98372 US	5/5/2017
Jamie Evangelista	Puyallup	WA	98372 US	5/5/2017
Cheryl Boswell	Graham	WA	98338 US	5/5/2017
Andrea Macy	Tacoma	WA	98446 US	5/5/2017
Shauna Holmes	Puyallup	WA	98371 US	5/5/2017
Melissa McCollom	Seattle	WA	98146 US	5/5/2017
Britt Coates	Puyallup	WA	98372 US	5/5/2017
Erin Lalonde	Puyallup	WA	98372 US	5/5/2017
Arianna Dalceredo	Puyallup	WA	98372 US	5/5/2017
Toni Gates	Puyallup	WA	98374 US	5/5/2017
Cindy Scerri	Puyallup	WA	98373 US	5/5/2017
Diana Tilson	Puyallup	WA	98374 US	5/5/2017
Kim Lackey	Buckley	WA	98321 US	5/5/2017
Courtney Craft	Puyallup	WA	98374 US	5/5/2017
Tim Schumacher	Spanaway	WA	98387 US	5/5/2017
Jack Morey	Puyallup	WA	98374 US	5/5/2017
Deanna Percy	Puyallup	WA	98373 US	5/5/2017
Nicole Lam	Puyallup	WA	98372 US	5/5/2017
Darlene Morford	Sumner	WA	98390 US	5/5/2017
Ashley May	Puyallup	WA	98371 US	5/5/2017
Hesther Snyder	Puyallup	WA	98374 US	5/5/2017
Robert and Janice Gailfus	Puyallup	WA	98374-9425 US	5/5/2017
Karen Bratvold	Sumner	WA	98390 US	5/5/2017
Samantha Webber	Puyallup	WA	983u4 US	5/5/2017
Don Jacobsen	Puyallup	WA	98374 US	5/5/2017
sridevi mopidevi	Puyallup	WA	98372 US	5/5/2017
Susan Wetherford	Eatonville	WA	98328 US	5/5/2017
Jordan Ramsey	Puyallup	WA	98375 US	5/5/2017
betty lybeck	Sumner	WA	98390 US	5/5/2017

nola sienkiewicz	Puyallup	WA	98374 US	5/5/2017
Scott Smith	Puyallup	WA	98372 US	5/5/2017
Alexandria Vanderveen	Puyallup	WA	98372 US	5/5/2017
Charlotte Burns	Puyallup	WA	98372 US	5/5/2017
david taggart	Woodbridge	VA	22193 US	5/5/2017
Annie Fleck	Molalla	OR	97038 US	5/5/2017
Toshia Dunagan	Puyallup	WA	98372 US	5/5/2017
Ericka Wallace	Tacoma	WA	us, 98445 US	5/5/2017
Seshu Uriti	Puyallup	WA	98372 US	5/5/2017
Lisa Bruce	Puyallup	WA	98372 US	5/5/2017
Siva Maram	Renton	WA	98059 US	5/5/2017
Allison Morton	Puyallup	WA	98372 US	5/5/2017
Sean Morton	Puyallup	WA	98372 US	5/5/2017
Erin Oneall	Puyallup	WA	98372 US	5/5/2017
Shannon Densmore	Puyallup	WA	97372 US	5/5/2017
Toni McConnell	Puyallup	WA	98374 US	5/5/2017
AMY SLAGLE	Puyallup	WA	98374 US	5/5/2017
Shireen Ainsworth	Puyallup	WA	98371 US	5/5/2017
Holly Herrera	Puyallup	WA	98374 US	5/5/2017
Michael Gruber	Puyallup	WA	98371 US	5/5/2017
Kelly edwards	Puyallup	WA	98374 US	5/5/2017
Judith Duggan	Orting	WA	98360 US	5/5/2017
Brenda Higham	Tacoma	WA	98445 US	5/5/2017
Theresa Lawrence	Puyallup	WA	98474 US	5/5/2017
Amber Butcher	Puyallup	WA	98375 US	5/5/2017
Scott Wallace	Puyallup	WA	98374 US	5/5/2017
stephanie moffett	Kent	WA	98042 US	5/5/2017
Renita Arbelo-Buller	Puyallup	WA	98375 US	5/5/2017
Joshua Thompson	Puyallup	WA	98372 US	5/5/2017
Ashly Dale	Milton	WA	98354 US	5/5/2017
Brendon Reed	Bend	OR	97701 US	5/5/2017
joan harrelson	Puyallup	WA	98371 US	5/5/2017
Jason INGRAHAM	Puyallup	WA	98374 US	5/5/2017
Diana Tilson	Puyallup	WA	98374 US	5/5/2017
Mark Blaney	Puyallup	WA	98371 US	5/5/2017
Nathaniel Tilson	Puyallup	WA	98374 US	5/5/2017
Hollie Burt	Puyallup	WA	98374 US	5/5/2017
Sarah H	Puyallup	WA	98372 US	5/5/2017
LISA PETERSEN	Puyallup	WA	98374 US	5/5/2017
Brianne Minga	Puyallup	WA	98375 US	5/5/2017
Patricia Hughes	Spanaway	WA	98387 US	5/5/2017
Judy Duthie	Sumner	WA	98390 US	5/5/2017
Rachael Baltazar	Puyallup	WA	98373 US	5/5/2017
janet glos	puyallup	WA	98372 US	5/5/2017
Brandy Sandana	Puyallup	WA	98372 US	5/5/2017
Cheri Caywood	Graham	WA	98338 US	5/5/2017
Lisa Fraizer	Tacoma	WA	98445 US	5/5/2017

Lisa Norris	Reno	NV	89506 US	5/5/2017
Debby Lowe	Puyallup	WA	98374 US	5/5/2017
Michelle Ochs	Puyallup	WA	98374 US	5/5/2017
Kasie Scott	Puyallup	WA	98372 US	5/5/2017
Lori Lidyad	Puyallup	WA	98373 US	5/5/2017
Jessica Walters	Tacoma	WA	98443 US	5/5/2017
Maegan Edds	Puyallup	WA	98374 US	5/5/2017
Christina Scott	Puyallup	WA	98372 US	5/5/2017
Martin Moffett	Puyallup	WA	98373 US	5/5/2017
Shawn CROSBY	Puyallup	WA	98374 US	5/5/2017
Nikki Taylor	Puyallup	WA	98372 US	5/5/2017
suzanne joyner	Puyallup	WA	98373 US	5/5/2017
Steve Watson	Puyallup	WA	98374 US	5/5/2017
Melissa Pattee	Puyallup	WA	98372 US	5/5/2017
Michele Suiter	Bonney Lake	WA	98391 US	5/5/2017
Ryan Ulver	Buckley	WA	98321 US	5/5/2017
Marie Zemanek	Puyallup	WA	98372 US	5/5/2017
Leslie Bloom	Puyallup	WA	98374 US	5/5/2017
Forrest Vines	Bonney Lake	WA	98391 US	5/5/2017
Ashley Raymond	Puyallup	WA	98372 US	5/5/2017
Marilyn Irvine Solomonson	Puyallup	WA	98374 US	5/5/2017
Robin McCoy	Puyallup	WA	98372 US	5/5/2017
Connie Best	Puyallup	WA	98374 US	5/5/2017
Miguel Galtman	Puyallup	WA	98372 US	5/5/2017
Corrine Berryman	Eatonville	WA	98328 US	5/5/2017
Jean Wiggins	Puyallup	WA	98371 US	5/5/2017
Salena Taylor	Puyallup	WA	98374 US	5/5/2017
Peter Baerny	Puyallup	WA	98372 US	5/5/2017
Vanessa Witkowski	Puyallup	WA	98374 US	5/5/2017
Teresa Treakle	Tacoma	WA	98406 US	5/5/2017
John Dimas	Wayne	IL	60184 US	5/5/2017
Katherine Knox	Puyallup	WA	98374 US	5/5/2017
Ann Reed	Honolulu	HI	96822 US	5/5/2017
Sierra Witkowski	Sumner	WA	98390 US	5/6/2017
Wendy Willats	Seattle	WA	98102 US	5/6/2017
Jody Burns	Puyallup	WA	98372 US	5/6/2017
Charles Woolley	Puyallup	WA	98373 US	5/6/2017
Erik Barrett	Puyallup	WA	98374 US	5/6/2017
Ricky James	Puyallup	WA	98373 US	5/6/2017
Elyssa Massey	Puyallup	WA	98372 US	5/6/2017
JoAnn Clark	Puyallup	WA	98374 US	5/6/2017
Karen Harding	Puyallup	WA	98374 US	5/6/2017
Mary Davis	Puyallup	WA	98374 US	5/6/2017
Jenny Willey	Puyallup	WA	98375 US	5/6/2017
Jeremy Hamel	Puyallup	WA	98372 US	5/6/2017
Zach Wilson	Puyallup	WA	98372 US	5/6/2017
Marcie Mahoney	Puyallup	WA	98374 US	5/6/2017

Sandra Bandel	Graham	WA	98338 US	5/6/2017
Christy Bergerson	Buckley	WA	98321 US	5/6/2017
Leona Shaklee	Puyallup	WA	98375 US	5/6/2017
Jean Coy	Renton	WA	98058 US	5/6/2017
Linda Martin	Graham	WA	98338 US	5/6/2017
Brian and Sue McCluskey	Carbonado	WA	98323 US	5/6/2017
mike mcguire	Tacoma	WA	98405 US	5/6/2017
Shari Glos	Puyallup	WA	98372 US	5/6/2017
Kathryn Mobley	Puyallup	WA	98374 US	5/6/2017
Christina DesRosiers	Puyallup	WA	98374 US	5/6/2017
Danielle Vinluan	Bonney Lake	WA	98391 US	5/6/2017
Sandra Kean	Spanaway	WA	98387 US	5/6/2017
Steve Brown	Puyallup	WA	98371 US	5/6/2017
Renee Brewer	Puyallup	WA	98373 US	5/6/2017
Kelly Jo Cane	Puyallup	WA	98374 US	5/6/2017
Karen Powers	Puyallup	WA	98374 US	5/6/2017
Valerie Franks	Spanaway	WA	98387 US	5/6/2017
Yelena Mock	Kent	WA	98032 US	5/6/2017
Holly Jensen	Renton	WA	98058 US	5/6/2017
Cheryl Habgood	Puyallup	WA	98372 US	5/6/2017
Katrina Wilson	Puyallup	WA	98372 US	5/6/2017
Jessica Acord	Renton	WA	98058 US	5/6/2017
Janet Newby	Puyallup	WA	98372whi US	5/6/2017
Anna Leach	Puyallup	WA	98372 US	5/6/2017
Whitney Deehr	Tacoma	WA	98445 US	5/6/2017
Krista Kenyon	Orting	WA	98360 US	5/6/2017
Becky Dickerson	Puyallup	WA	98371 US	5/6/2017
Sara Eichenlaub	Puyallup	WA	98371 US	5/6/2017
Brenda Milewski	Puyallup	WA	98373 US	5/6/2017
Karen Kime	Puyallup	WA	98372 US	5/6/2017
Gwen Joseph	Puyallup	WA	98372 US	5/6/2017
Jessica Simmons	Puyallup	WA	98373 US	5/6/2017
Jamie Prak	Puyallup	WA	98374 US	5/6/2017
Cindy Schill	Puyallup	WA	98371 US	5/6/2017
Scott Newkirk	Tacoma	WA	98443 US	5/6/2017
Lorraine gakin	Puyallup	WA	98375 US	5/6/2017
Ashley Parrish	Puyallup	WA	98375 US	5/6/2017
Kristin O'Brien	Milton	WA	98354 US	5/6/2017
brittney helmlinger	South Prairie	WA	98385 US	5/6/2017
Michelle McClure	Tacoma	WA	98404 US	5/6/2017
Laura Vos	Tacoma	WA	98444 US	5/6/2017
Hailey Powers	missoula	MT	59801 US	5/6/2017
leslie schoneman	Puyallup	WA	98371 US	5/6/2017
Audrie Garrison	Puyallup	WA	98372 US	5/6/2017
Krista Francis	Puyallup	WA	98372 US	5/6/2017
Brianna Hislop	Bonney Lake	WA	98391 US	5/6/2017
Mary Zimmerman	Puyallup	WA	98374 US	5/6/2017

Gayle Franks	Graham	WA	98338 US	5/6/2017
Mary Stegman	Puyallup	WA	98371 US	5/6/2017
Jeffery Roane	Albion	MI	49224 US	5/6/2017
Karen Cacciatore	Renton	WA	98055 US	5/6/2017
Iony heidt	Tacoma	WA	98445 US	5/6/2017
Jennifer Alessandra	Puyallup	WA	98372 US	5/6/2017
Jason Bellerive	Graham	WA	98338 US	5/6/2017
Betsy Ceccanti	Puyallup	WA	98375 US	5/6/2017
Richard Hanson	Puyallup	WA	98372 US	5/6/2017
Heather Morris	Buckley	WA	98321 US	5/6/2017
Catherine Schweitzer	Wenham	MA	1984 US	5/6/2017
colleen kimball	Puyallup	WA	98372 US	5/6/2017
Margene Schotz	Tacoma	WA	98424 US	5/6/2017
Angela Edwards	Puyallup	WA	98372 US	5/6/2017
Dorothy Hoey	Tacoma	WA	98445 US	5/6/2017
Matthew Creley	Puyallup	WA	98372 US	5/6/2017
Jean Fletcher	Sequim	WA	98382 US	5/6/2017
Elizabeth Olsen	Roy	WA	98580 US	5/6/2017
Denise Sharff	Edgewood	WA	98372 US	5/6/2017
Ken Wilson	Pt. Pleasant	PA	18950 US	5/6/2017
Anna Lundstrom	Puyallup	WA	98372 US	5/6/2017
Mark Folk	Puyallup	WA	98374 US	5/6/2017
Hannah Hollicky	Puyallup	WA	98373 US	5/6/2017
Tamara Udovich	Puyallup	WA	98372 US	5/6/2017
Brandi Jameson	Sumner	WA	98390 US	5/6/2017
David Schram	Puyallup	WA	98372 US	5/6/2017
Deborah Erdmann	Federal Way	WA	98003 US	5/6/2017
Sally Mildren	Puyallup	WA	98371 US	5/6/2017
Tamara Majors	Bonney Lake	WA	98391 US	5/6/2017
Debra Cunningham	Bonney Lake	WA	98391 US	5/6/2017
Lisa Elliott	Puyallup	WA	98374 US	5/6/2017
Phil Schumock	Puyallup	WA	98372 US	5/6/2017
Tona Thun Taylor	Seattle	WA	98116 US	5/6/2017
Sharon Jaramillo	Puyallup	WA	98374 US	5/6/2017
Terri Manza	Puyallup	WA	98372 US	5/6/2017
Jennifer McPherson	Seattle	WA	98106 US	5/6/2017
Lynnette Butler	Puyallup	WA	98374 US	5/6/2017
Gordon Ferber	Puyallup	WA	98371 US	5/6/2017
Kathleen Pace	Puyallup	WA	98371 US	5/6/2017
michael cobb	Puyallup	WA	98374 US	5/6/2017
Jennifer Moe	Puyallup	WA	98374 US	5/6/2017
andra shinaul	Beaverton	OR	87006 US	5/6/2017
Linda Stratton	Puyallup	WA	98374 US	5/6/2017
Kenneth Welsh	Puyallup	WA	98375 US	5/6/2017
Margaret englund	Puyallup	WA	98373 US	5/6/2017
Vicky Altamiranda	Puyallup	WA	98374 US	5/6/2017
Augustine Canales	Puyallup	WA	98371 US	5/6/2017

Jim Kilgus	Puyallup	WA	98373 US	5/6/2017
Pam Heath	Orting	WA	98360 US	5/6/2017
Shannon York	Puyallup	WA	98372 US	5/6/2017
Paige brannon	Puyallup	WA	98374 US	5/6/2017
Joseph Ball	Puyallup	WA	98374 US	5/6/2017
Deborah Kurtz	Puyallup	WA	98374 US	5/6/2017
Sean Mclellan	Renton	WA	98056 US	5/6/2017
Susan Garrett	Green Valley	AZ	85622 US	5/6/2017
LISA kim	Puyallup	WA	98372 US	5/6/2017
LJ KREUGER	Puyallup	WA	98372 US	5/6/2017
TINA LONDON	Puyallup	WA	98374 US	5/6/2017
Glenda Spray	Puyallup	WA	98374 US	5/6/2017
Melissa Hollenbeck	Puyallup	WA	98371 US	5/6/2017
Steven Daskam	Puyallup	WA	98373 US	5/6/2017
Kimberly Johnson	Puyallup	WA	98374 US	5/6/2017
Theresa Pierce	Puyallup	WA	98374 US	5/6/2017
Rachel Berry	Puyallup	WA	98372 US	5/6/2017
Emily Frantz	Puyallup	WA	98374 US	5/6/2017
Lisa Puppe Crouse	Puyallup	WA	98374 US	5/6/2017
Lizzy Bartoy	Puyallup	WA	98373 US	5/6/2017
Carol Walker	Puyallup	WA	98374 US	5/6/2017
Kate Reinking	Milton	WA	98354 US	5/6/2017
Angela Hembroff	Bremerton	WA	98310 US	5/6/2017
JJ Williams	Puyallup	WA	98372 US	5/6/2017
Ashley Ayling		0 WA	0 US	5/6/2017
Steve Gregg	Puyallup	WA	98374 US	5/6/2017
Diana Hansen	Milton	WA	98354 US	5/6/2017
Daphine Niquette	Puyallup	WA	98372 US	5/6/2017
Seth Miller	Puyallup	WA	98372 US	5/6/2017
Colleen Paeper	Puyallup	WA	98374 US	5/6/2017
Harry Hansen	Bonney Lake	WA	98391 US	5/6/2017
Shelby Russell	Bonney Lake	WA	98391 US	5/6/2017
Stephanie Vuleta	Puyallup	WA	98372 US	5/6/2017
Nancy Kreuger	Puyallup	WA	98372 US	5/6/2017
Karah Dickson-Landoe	Puyallup	WA	98374 US	5/6/2017
Brien Briggs	Tacoma	WA	98404 US	5/6/2017
Steve Vuleta	Puyallup	WA	98371 US	5/6/2017
Rex Dickson	Puyallup	WA	98372 US	5/6/2017
Maia Caroline	Puyallup	WA	98374 US	5/6/2017
Christine Ziegler	Palm Desert	CA	92211 US	5/6/2017
Shannon Haavik	Sumner	WA	98390 US	5/6/2017
Kyndra Williams	Seattle	WA	98115 US	5/6/2017
Amy Carnes	Puyallup	WA	98372 US	5/6/2017
Madison Mineard	puyallup	WA	98374 US	5/6/2017
Julia Fozzard	Tacoma	WA	98444 US	5/6/2017
Melanie Greenman	Bonney Lake	WA	98391 US	5/6/2017
Chris Fournier	Tacoma	WA	98424 US	5/6/2017

Margret Bethel	Milton	WA	98354 US	5/6/2017
Monica Henley	Puyallup	WA	98374 US	5/6/2017
Jenneby Chambers	Puyallup	WA	98372 US	5/6/2017
Rachelle Johnson	Puyallup	WA	98375 US	5/6/2017
Caleb Marrs	Puyallup	WA	98374 US	5/6/2017
Bradley Brown	Bonney Lake	WA	98391 US	5/6/2017
Dana Matsukawa	puyallup	WA	98374 US	5/6/2017
Frank Saxton	Graham	WA	98338 US	5/6/2017
Sarah Bedlion	Puyallup	WA	98371 US	5/6/2017
Valerie Ruhle	Puyallup	WA	98374 US	5/6/2017
Toni Conway	Puyallup	WA	98371 US	5/6/2017
Lisa Powell	Washtucna	WA	99371 US	5/6/2017
Eva Jaycoc	Puyallup	WA	98373 US	5/6/2017
Eric De Sart	Puyallup	WA	98372 US	5/6/2017
Nichol Krambule	Puyallup	WA	98374 US	5/6/2017
Kelly Rhodes	Puyallup	WA	98372 US	5/6/2017
Daniel Krambule	Puyallup	WA	98374 US	5/6/2017
Serena Fulton	Puyallup	WA	98374 US	5/6/2017
Drew Hobson	Seattle	WA	98144 US	5/7/2017
Kristina Miles	Graham	WA	98338 US	5/7/2017
Rachel Diaz	Spanaway	WA	98387 US	5/7/2017
Meichele Boisvert	Puyallup	WA	98374 US	5/7/2017
Meg Suzuki	Spanaway	WA	98387 US	5/7/2017
Cory Wilson	Puyallup	WA	98372 US	5/7/2017
Steve Duffy	Sumner	WA	98390 US	5/7/2017
Brian cooper	Puyallup	WA	98372 US	5/7/2017
Rebecca Pedersen	Eatonville	WA	98328 US	5/7/2017
Toby Donovan	Puyallup	WA	98372 US	5/7/2017
Alyssa Lettiere	Puyallup	WA	98372 US	5/7/2017
Kyle Crawford	Puyallup	WA	98372 US	5/7/2017
Alexandra Stephens	Tacoma	WA	98446 US	5/7/2017
Aaron Watt	Puyallup	WA	98372 US	5/7/2017
Jon Watt	Puyallup	WA	98373 US	5/7/2017
Charlene Peden	Orting	WA	98360 US	5/7/2017
Teann Rivera	Lakewood	WA	98499 US	5/7/2017
Cynthia Rose	Puyallup	WA	98374 US	5/7/2017
Melissa Gage	Puyallup	WA	98373 US	5/7/2017
Paulette Drewry	Puyallup	WA	98372 US	5/7/2017
Christine Harrington	Scio	OR	97374 US	5/7/2017
Chad Edmonds	Puyallup	WA	98374 US	5/7/2017
angela Bartlett	puyallup	WA	98371 US	5/7/2017
Dorlisa Haley	Federal Way	WA	98023 US	5/7/2017
Tracey Tennis	Puyallup	WA	98374 US	5/7/2017
Terri Olson	Sumner	WA	98390 US	5/7/2017
Ryan Banner	Puyallup	WA	98372 US	5/7/2017
Andrea Vuleta	Claremont	CA	91711 US	5/7/2017
Meagan Moore	Puyallup	WA	98372 US	5/7/2017

Todd Tracer	Puyallup	WA	98374 US	5/7/2017
Amy Asbjornsen	Puyallup	WA	98371 US	5/7/2017
Nick Padilla	Puyallup	WA	98374 US	5/7/2017
newt Russell	Elbe	WA	98330 US	5/7/2017
Patricia Schneider	Puyallup	WA	98373 US	5/7/2017
Nate Benson	Bonney Lake	WA	98391 US	5/7/2017
Graham Abbott	Puyallup	WA	98374 US	5/7/2017
Brett Kapels	Puyallup	WA	98373 US	5/7/2017
Loni Brown	Puyallup	WA	98371 US	5/7/2017
Cory Chavez	Puyallup	WA	98373 US	5/7/2017
Tracy Teed	Camano Island	WA	98282 US	5/7/2017
Caroline saulen	Puyallup	WA	98371 US	5/7/2017
Virginia Van Keuren	Puyallup	WA	98375 US	5/7/2017
Madison Davis	Puyallup	WA	98374 US	5/7/2017
Kyle Sidor	Puyallup	WA	98374 US	5/7/2017
Aliesha Alexandar	Fox Island	WA	98333 US	5/7/2017
Charles Banner	Puyallup	WA	98372 US	5/7/2017
Beverly Cascio	Puyallup	WA	98372 US	5/7/2017
Lorri Stoyles	Puyallup	WA	98374 US	5/7/2017
Wendy Hoashi-Erhardt	Puyallup	WA	98371 US	5/7/2017
Richard Uhrich	Puyallup	WA	98372 US	5/7/2017
Megan Lawless	Puyallup	WA	98373 US	5/7/2017
Alexandra Pierce	Fullerton	CA	92832 US	5/7/2017
Nancy McFarland	Tacoma	WA	98417 US	5/7/2017
Carla Smith	Puyallup	WA	98371 US	5/7/2017
Susan Bartlett	Port Orchard	WA	98367 US	5/7/2017
William Petersen	Puyallup	WA	98372 US	5/7/2017
Andrea Michie	Puyallup	WA	98374 US	5/7/2017
Kaci beyer	Puyallup	WA	98374 US	5/7/2017
Kenneth Bishop	Puyallup	WA	98371 US	5/7/2017
Connor Bates	Puyallup	WA	98374 US	5/7/2017
Katelyn Guesford	Puyallup	WA	98371 US	5/7/2017
Danielle Mahnken	Puyallup	WA	98371 US	5/7/2017
Brooke Zerh	Puyallup	WA	98375 US	5/7/2017
Jessica Northover	Puyallup	WA	98374 US	5/7/2017
Siri Troutman	Puyallup	WA	98372 US	5/7/2017
rosalie clarke	Puyallup	WA	98373 US	5/7/2017
Jami Overby	Puyallup	WA	98374 US	5/7/2017
Florence Petersen	Puyallup	WA	98372 US	5/7/2017
Craig Mattson	Puyallup	WA	98372 US	5/7/2017
Dustin Ellison	Puyallup	WA	98375 US	5/7/2017
Dan Brown	Puyallup	WA	98372 US	5/7/2017
Megan Barnes	Puyallup	WA	98374 US	5/7/2017
Vonna Graham	Eatonville	WA	98328 US	5/7/2017
Kelsey Littleton	Puyallup	WA	98374 US	5/7/2017
Dovey Zukowski	Puyallup	WA	98374 US	5/7/2017
Jacob Josti	Puyallup	WA	98374 US	5/7/2017

Jordan Loucks	Tacoma	WA	98416 US	5/7/2017
Patrick Leonard	Puyallup	WA	98374 US	5/7/2017
Brian Nelson	Puyallup	WA	98372 US	5/7/2017
Chase Charaba	Puyallup	WA	98374 US	5/7/2017
Matthew Kusche	Puyallup	WA	98374 US	5/7/2017
Anne Dean	olympia	WA	98501 US	5/7/2017
Brandy Wagenblast	Puyallup	WA	98374 US	5/7/2017
Paige Beyer	Apache Junction	AZ	85119 US	5/7/2017
Jacob Woodbury	Puyallup	WA	98374 US	5/7/2017
Kristy Golden	Puyallup	WA	98375 US	5/7/2017
Mindy Nelson	Puyallup	WA	98375 US	5/7/2017
Kathy Campbell	Bonney Lake	WA	98391 US	5/7/2017
Myrna Christian	Scottsdale	AZ	85258 US	5/7/2017
Kym Schodron	sumner	WA	98390 US	5/7/2017
Dana Shumake	Roanoke	TX	76262 US	5/7/2017
Mallory Halverson	Puyallup	WA	98371 US	5/7/2017
Allyson Goldsberry	Graham	WA	98338 US	5/7/2017
Tara Knutsen	Puyallup	WA	98374 US	5/7/2017
julie morris	Tacoma	WA	98406 US	5/7/2017
Ginger Sovay	Puyallup	WA	98372 US	5/7/2017
Jenel Yeamans	Puyallup	WA	98372 US	5/7/2017
Linda Shampine	Milton	WA	98354 US	5/7/2017
Austin Bechtold	Puyallup	WA	98372 US	5/7/2017
Matthew Wells	Puyallup	WA	98374 US	5/7/2017
Brianna Early	Puyallup	WA	98374 US	5/7/2017
Shana Delight	Orting	WA	98360 US	5/7/2017
Randy Cherland	Puyallup	WA	98374 US	5/7/2017
Kasey Loucks	Puyallup	WA	98374 US	5/7/2017
Leonard McMullen	Tacoma	WA	98466 US	5/7/2017
Aaron Stanley	Puyallup	WA	98374 US	5/7/2017
Sharon Strong	Castro Valley	CA	94546 US	5/7/2017
Shanna Sheppard	Puyallup	WA	98374 US	5/7/2017
Kristy Moe	Puyallup	WA	98374 US	5/7/2017
Stephan Dominguez	Puyallup	WA	98372 US	5/7/2017
KATHRYN CASEY	Puyallup	WA	98374 US	5/7/2017
William Visser	Puyallup	WA	98371 US	5/7/2017
Katrina Chambers	Tacoma	WA	98445 US	5/7/2017
Austin Babcock	Puyallup	WA	98374 US	5/7/2017
Kayla Fernandez	Spanaway	WA	98387 US	5/7/2017
Tammy Reasor	Puyallup	WA	98374 US	5/7/2017
Randall Sherwood	Puyallup	WA	98374 US	5/7/2017
Cat Pirolo	Puyallup	WA	98374 US	5/7/2017
Jordon Speckhardt	Puyallup	WA	98371 US	5/7/2017
Angus Rakes	Orting	WA	98360 US	5/7/2017
Bjorn Tinglum	Puyallup	WA	98374 US	5/7/2017
Kimberly Becker Stonack	Puyallup	WA	98371 US	5/7/2017
Harrison Wisti	Puyallup	WA	98374 US	5/7/2017

Dominic Choi	Puyallup	WA	98374 US	5/7/2017
Jason Barnett	Puyallup	WA	98374 US	5/7/2017
oscar lepe	Puyallup	WA	98374 US	5/7/2017
Mary Dearing	Sumner	WA	98390 US	5/7/2017
Jon Bryan	Puyallup	WA	98374 US	5/7/2017
Victor Gratzner	Puyallup	WA	98374 US	5/7/2017
Robin Stanton	Puyallup	WA	98372 US	5/7/2017
Doreen Weimann	Sammamish	WA	98075 US	5/7/2017
Sasha Minor	Shelton	WA	98584 US	5/7/2017
Jamie Knutsen	Puyallup	WA	98374 US	5/7/2017
Heather Johnson	Puyallup	WA	98372 US	5/7/2017
Jacob Tomashek	Puyallup	WA	98374 US	5/7/2017
Andy Minor	Shelton	WA	98584 US	5/7/2017
Dawn Akers	Puyallup	WA	98371 US	5/7/2017
Melanie Bowers	Puyallup	WA	98373 US	5/7/2017
Elise VanderLinda	Puyallup	WA	98371 US	5/7/2017
Sierr Meyer	Puyallup	WA	98374 US	5/7/2017
William Crosley	Puyallup	WA	98374 US	5/7/2017
Brian Mays	Puyallup	WA	98372 US	5/7/2017
Dianna Breunig	Puyallup	WA	98374 US	5/7/2017
Ronald Thompson	Puyallup	WA	98371 US	5/7/2017
Jordan Barne	Puyallup	WA	98374 US	5/7/2017
Helen King	Edgewood	WA	98372-1524 US	5/7/2017
Elizabeth Miller	Renton	WA	98055 US	5/7/2017
Cody Heyrman	Puyallup	WA	98374 US	5/7/2017
Sam Miller	Puyallup	WA	98374 US	5/7/2017
Karen Chappell	Bonney Lake	WA	98391 US	5/7/2017
Ching Yap	Puyallup	WA	98374 US	5/7/2017
Brandon Wigington	Puyallup	WA	98375 US	5/7/2017
Amanda Gomez	Puyallup	WA	98374 US	5/7/2017
Amanda Curran	Puyallup	WA	98374 US	5/7/2017
Shaelyn Lizee	Bonney Lake	WA	98391 US	5/7/2017
Connor Lynch	Puyallup	WA	98374 US	5/7/2017
Madison Colley	Puyallup	WA	98372 US	5/7/2017
Connor Bates	Puyallup	WA	98374 US	5/7/2017
Monica Kimmerly	Puyallup	WA	98374 US	5/7/2017
Julie Fosnick	Puyallup	WA	98371 US	5/7/2017
Rhonda Kirkes	Spanaway	WA	98387 US	5/7/2017
Michele Price	Puyallup	WA	98374 US	5/7/2017
Julia Wright	Puyallup	WA	98374 US	5/7/2017
Morgan Perry	Puyallup	WA	98372 US	5/7/2017
Susan Balogh	Puyallup	WA	98375 US	5/7/2017
Miranda Falgout	Puyallup	WA	98374 US	5/7/2017
Matthew Perea	Tacoma	WA	98447 US	5/7/2017
Stephanie Thompson	Tacoma	WA	98445 US	5/7/2017
Carter Barnett	Puyallup	WA	98374 US	5/7/2017
Natalie Gendreau	Centralia	WA	98531 US	5/7/2017

Courtney Tapia	Puyallup	WA	98372 US	5/7/2017
Alicia Decker	Puyallup	WA	98374 US	5/7/2017
Paul McKinney	Puyallup	WA	98372 US	5/7/2017
Logan Oelkers	Belgrade	MT	59714 US	5/7/2017
Darleen Allen	Puyallup	WA	98374 US	5/7/2017
Sammy Sponenburg	Sumner	WA	98390 US	5/7/2017
Ethan Olson	Puyallup	WA	98372 US	5/7/2017
Dana Montevideo	Puyallup	WA	98374 US	5/7/2017
Ryan Gutierrez	Graham	WA	98338 US	5/7/2017
Sarah Quinn	Tacoma	WA	98446 US	5/7/2017
Andrew Curran	Puyallup	WA	98374 US	5/7/2017
Jenna Dally	Bonney Lake	WA	98391 US	5/7/2017
Bastian McKeen	Puyallup	WA	98373 US	5/7/2017
Kort Skoda	Puyallup	WA	98373 US	5/7/2017
Sarah Paiva	Puyallup	WA	98374 US	5/7/2017
Alyssa Oneal	Puyallup	WA	98374 US	5/7/2017
Myles Barrow	Seattle	WA	98178 US	5/7/2017
Gabriella Penfield	Puyallup	WA	98374 US	5/7/2017
Ashley Carrasco	Tacoma	CA	98447 US	5/7/2017
Dolores Barrett	Puyallup	WA	98372 US	5/7/2017
Jessie Saunders	Puyallup	WA	98373 US	5/7/2017
MARIAN BOLSTAD	Puyallup	WA	98372 US	5/7/2017
Grace Jeffries	Puyallup	WA	98374 US	5/7/2017
Nkole Mwansa	Puyallup	WA	98373 US	5/7/2017
Michaela Zaro	Puyallup	WA	98374 US	5/7/2017
Sheryl Lathrop	Bonney Lake	WA	98391 US	5/7/2017
Matthew Miller	Puyallup	WA	98372 US	5/7/2017
Melinda Sidor	Puyallup	WA	98374 US	5/7/2017
Cynthia McKeen	Puyallup	WA	98373 US	5/7/2017
Tammy Nguyen	Seattle	WA	98105 US	5/7/2017
Kelly Krupke	Buckley	WA	98321 US	5/7/2017
Diedre Feinauer	Milton	WA	98354 US	5/7/2017
Carrie Silvey	Orting	WA	98360 US	5/7/2017
James Petak	Puyallup	WA	98371 US	5/8/2017
Kurt Foerster	Puyallup	WA	98371 US	5/8/2017
Jerry Tillery	Puyallup	WA	98372 US	5/8/2017
Lorraine Hirakawa	Puyallup	WA	98374 US	5/8/2017
Mardene Patton	Fife	WA	98424 US	5/8/2017
Carherine Burke	Puyallup	WA	98373 US	5/8/2017
Thomas Dever	Puyallup	WA	98374 US	5/8/2017
L Hirakawa	Puyallup	WA	98374 US	5/8/2017
Laline Carda	Puyallup	WA	98374 US	5/8/2017
james brayton	Roy	WA	98580 US	5/8/2017
Myiesha Raphael	Tacoma	WA	98446 US	5/8/2017
Kristi Tvedt	Puyallup	WA	98374 US	5/8/2017
Marga Overby	Puyallup	WA	98375 US	5/8/2017
Katie Stewart	Puyallup	WA	98372 US	5/8/2017

Katherine Miller	Tacoma	WA	98446 US	5/8/2017
Briana Dowey	Puyallup	WA	98374 US	5/8/2017
Norma Champlain	Graham	WA	98338 US	5/8/2017
Brian Barrager	Puyallup	WA	98374 US	5/8/2017
Marilyn McCoy	Puyallup	WA	98371 US	5/8/2017
Maya Lewis	Puyallup	WA	98374 US	5/8/2017
Joyce Edwards	Puyallup	WA	98374 US	5/8/2017
Curtis Young	Spanaway	WA	98387 US	5/8/2017
Shauna Rogers	Puyallup	WA	98374 US	5/8/2017
David nation	Puyallup	WA	98374 US	5/8/2017
Mariah Turco	Puyallup	WA	98373 US	5/8/2017
Michelle Gunter	Puyallup	WA	98373 US	5/8/2017
Christopher Powell	Puyallup	WA	98372 US	5/8/2017
Brandon Swanson	Puyallup	WA	98374 US	5/8/2017
Kylie Fossum	Bonney Lake	WA	98391 US	5/8/2017
Kellylouise Yount	Temecula	CA	92592 US	5/8/2017
Jeffrey Valdanbrini	Puyallup	WA	98375 US	5/8/2017
Annmarie Childs	Tacoma	WA	98446 US	5/8/2017
Destany Burke	Puyallup	WA	98374 US	5/8/2017
Scot Johnson	Puyallup	WA	98374 US	5/8/2017
Kaleb Hinz	Puyallup	WA	98374 US	5/8/2017
Kylai Cooley	Bonney Lake	WA	98391 US	5/8/2017
Jonathon Seaborn	Yakima	WA	98902 US	5/8/2017
Robertson Christina	Puyallup	WA	98374 US	5/8/2017
Cinthia Piedra	Puyallup	WA	98375 US	5/8/2017
Laura Eklund	Puyallup	WA	98375 US	5/8/2017
Jeff Stillman	Puyallup	WA	98371 US	5/8/2017
Jennifer Lowe	Puyallup	WA	98375 US	5/8/2017
Jen Dotson	Puyallup	WA	98373 US	5/8/2017
Hope Roberts	Puyallup	WA	98372 US	5/8/2017
Veronica Stout	Auburn	WA	98001 US	5/8/2017
Sharon Mc Andrews	Puyallup	WA	98373 5259 US	5/8/2017
William Childs	Tacoma	WA	98446 US	5/8/2017
Matt Cario	Puyallup	WA	98373 US	5/8/2017
Amy Engels	Puyallup	WA	98373 US	5/8/2017
Ariana Knesebeck	Tacoma	WA	98444 US	5/8/2017
Lia Craven	Tacoma	WA	98403 US	5/8/2017
Raquel Jasmer	Puyallup	WA	98373 US	5/8/2017
Austin Kile	Tacoma	WA	98443 US	5/8/2017
Wendi Trummert	Puyallup	WA	98374 US	5/8/2017
Cara Akau	Puyallup	WA	98371 US	5/8/2017
Eileen Hedrick	Puyallup	WA	98374 US	5/8/2017
Jodi Wheat	Puyallup	WA	98375 US	5/8/2017
Charlene Conrad	Kent	WA	98032 US	5/8/2017
Carolyn Draper	Renton	WA	98055 US	5/8/2017
Jacob Kile	Tacoma	WA	98443 US	5/8/2017
Tammy Allen	Puyallup	WA	98375 US	5/8/2017

Jami Scholz	Puyallup	WA	98371 US	5/8/2017
Kristen Dhaese	Puyallup	WA	98374 US	5/8/2017
Lisa Rowan	Puyallup	WA	98375 US	5/8/2017
Michael Alkiré	Puyallup	WA	98374 US	5/8/2017
Bobbi Gardner	Graham	WA	98338-0745 US	5/8/2017
Melanie Arntsen	Bonney Lake	WA	98391 US	5/8/2017
sandy wagner	Puyallup	WA	98371 US	5/8/2017
Tina Blackford	Puyallup	WA	98374 US	5/8/2017
Jon Rakes	Sumner	WA	98390 US	5/8/2017
Teresa Dwyer	Sumner	WA	98390 US	5/8/2017
DANA ELZIE	Tacoma	WA	98445 US	5/8/2017
Nichole Krueger	Newport Beach	CA	92663 US	5/8/2017
Thomas Courtney	Puyallup	WA	98372 US	5/8/2017
Scott Farabee	Puyallup	WA	98374 US	5/8/2017
Corina Hills	Orting	WA	98360 US	5/8/2017
Terri Moss	Puyallup	WA	98374 US	5/8/2017
Renelle Fernandez	Puyallup	WA	98371 US	5/8/2017
jeanne stoddard	Walla Walla	WA	99362 US	5/8/2017
Ronn Stearns	Spanaway	WA	98387 US	5/8/2017
Larry Hills	Bonney Lake	WA	98391 US	5/8/2017
Tracie Ross	Sumner	WA	98390 US	5/8/2017
Donald McElyea	Bonney Lake	WA	98391 US	5/8/2017
Heather Dire	Tacoma	WA	98424 US	5/8/2017
Elizabeth Davies	Puyallup	WA	98371 US	5/8/2017
DeeAnn Pfeiffer	Spanaway	WA	98387 US	5/8/2017
Allison Crandall	Orting	WA	98360 US	5/8/2017
Davene Eyres	Seattle	WA	98125 US	5/8/2017
Jacob Rowe	Puyallup	WA	98373 US	5/8/2017
Michelle Vaughan	Tacoma	WA	98405 US	5/8/2017
Rachel Mattson	Sumner	WA	98390 US	5/8/2017
Marilyn Long	Bonney Lake	WA	98391 US	5/8/2017
Adriana rowe	Puyallup	WA	98374 US	5/8/2017
Thomas Brendelson	Graham	WA	98338 US	5/8/2017
Mary Barbee	Puyallup	WA	98371 US	5/8/2017
Sharon Wilbanks	Sumner	WA	98390 US	5/8/2017
Jaqueline Wilkins	Puyallup	WA	98372 US	5/8/2017
Meredith Matheson	Auburn	WA	98092 US	5/8/2017
Candice Comfort	Edgewood	WA	98372 US	5/8/2017
Joan Templin	Tacoma	WA	98443 US	5/8/2017
Jerrett Cox	Puyallup	WA	98371 US	5/8/2017
Leslie Kellner	Puyallup	WA	98372 US	5/8/2017
Brandon Comfort	Puyallup	WA	98372 US	5/8/2017
Tracy Pitzer	Puyallup	WA	98372 US	5/8/2017
Jode Murphy	Sumner	WA	98390 US	5/8/2017
John Smith	Puyallup	WA	98373 US	5/8/2017
Cabrina Hollandsworth	Puyallup	WA	98373 US	5/8/2017
Mary Bengtson	Sekiu	WA	98381 US	5/8/2017

Jaimie Mittelstadt	Tacoma	WA	98445 US	5/8/2017
Stacie Thompson	Puyallup	WA	98371 US	5/8/2017
James Reaves	Puyallup	WA	98371 US	5/8/2017
Elizabeth Sheffler	Puyallup	WA	98371 US	5/8/2017
Naomi Berry	Puyallup	WA	98371 US	5/8/2017
V Doyle	Tacoma	WA	98422 US	5/8/2017
Jana Maki	Puyallup	WA	98371 US	5/8/2017
Denise Jones	Buckley	WA	98321 US	5/8/2017
Rodney Gratzer	Puyallup	WA	98371 US	5/8/2017
Sandy Basquette	Renton	WA	98056 US	5/8/2017
Nick Carpenter	Tacoma	WA	98444 US	5/8/2017
Ron DeGroot	Tacoma	WA	98444 US	5/8/2017
Tessa Fowler	Tacoma	WA	98409 US	5/8/2017
Taylor Lindberg	Tacoma	WA	98424 US	5/8/2017
Michael Peters	Puyallup	WA	98371 US	5/8/2017
Debbie Huber	Blackwood	NJ	8012 US	5/8/2017
Marcia Hess	Buckley	WA	98321 US	5/8/2017
Taylor Bailey	Santa Rosa	CA	95404 US	5/8/2017
Freda Carda	Puyallup	WA	98374 US	5/8/2017
Melody Rath	Sumner	WA	98390 US	5/8/2017
Fawn Palkovich	Auburn	WA	98002 US	5/8/2017
Reg Doty	Puyallup	WA	98374 US	5/8/2017
Jenny Jensen	Auburn	WA	98092 US	5/8/2017
mercedes powers	Suquamish	WA	98392 US	5/8/2017
Lindsay Serpa	Renton	WA	98058 US	5/8/2017
Lynda Vaughan	Tacoma	WA	98402 US	5/8/2017
Robin Crabb	Puyallup	WA	98372 US	5/8/2017
Deanna Hamlin	Puyallup	WA	98372 US	5/8/2017
Jane Brimhall	Puyallup	WA	98371 US	5/8/2017
ivana gabrovec	Kent	WA	98032 US	5/8/2017
Elizabeth Bral	Puyallup	WA	98371 US	5/8/2017
Alison Hanson	Puyallup	WA	98372 US	5/8/2017
Nancy Retynski	Puyallup	WA	98372 US	5/8/2017
Erin Miller	Tacoma	WA	98404 US	5/8/2017
Justine Bankhead	Puyallup	WA	98374 US	5/8/2017
TAMMIE LEWIS	Ocean Park	WA	98640 US	5/8/2017
Jordan Brennan	Puyallup	WA	98371 US	5/8/2017
Marlene Harrison	Auburn	WA	98002 US	5/8/2017
Kitt Bradley	Renton	WA	98059 US	5/8/2017
Amanda Knoop	Puyallup	WA	98372 US	5/8/2017
Mark Gobble	Puyallup	WA	98372 US	5/8/2017
Carina Cao	Puyallup	WA	98372 US	5/8/2017
Judith M. Smith	Kent	WA	98042 US	5/8/2017
Brett Gorrell	Snoqualmie	WA	98065 US	5/8/2017
Ruth Yang	San Jose	CA	95131 US	5/8/2017
Karen Whitmore	Ocean Park	WA	98640 US	5/8/2017
Jamie Simons	Puyallup	WA	98374 US	5/8/2017

JACLYN ANDERSON	Puyallup	WA	98372 US	5/8/2017
Jennifer Spane	Puyallup	WA	98371 US	5/8/2017
Rebecca Smith	Puyallup	WA	98371 US	5/8/2017
Iris Poynter	Puyallup	WA	98375 US	5/8/2017
Jerry Broadus	Puyallup	WA	98371 US	5/8/2017
Brittney Landoe	Puyallup	WA	98371 US	5/8/2017
Lisa Liu	Puyallup	WA	98372 US	5/8/2017
Stacy Broussard	Puyallup	WA	98372 US	5/8/2017
Stacy Williams	Puyallup	WA	98373 US	5/8/2017
Crystal Harrison	Puyallup	WA	98372 US	5/8/2017
Ted Ochsenhirt	Tacoma	WA	98407 US	5/8/2017
Bill Dit	Sumner	WA	98390 US	5/8/2017
Jonathan Elliott	Gig Harbor	WA	98335 US	5/8/2017
Tina Mcneeley	Puyallup	WA	98374 US	5/8/2017
Phil Brewer	Puyallup	WA	98372 US	5/8/2017
Christopher Skilton	Puyallup	WA	98374 US	5/8/2017
Colin Fielding	Sumner	WA	98390 US	5/8/2017
Gemlene Schaudies	Puyallup	WA	98373 US	5/8/2017
Hope Ziegler	Puyallup	WA	98372 US	5/8/2017
Karen Nolan	Puyallup	WA	98371 US	5/8/2017
Shirley Healy	Puyallup	WA	98374 US	5/8/2017
Todd Ulsund	Puyallup	WA	98374 US	5/8/2017
Nathan Fluger	Puyallup	WA	98375 US	5/8/2017
Robert Rowe	Puyallup	WA	98373 US	5/8/2017
Vancie Daniels	Puyallup	WA	98372 US	5/9/2017
Kathy Garceau	Puyallup	WA	98374 US	5/9/2017
Lesley Baczuk	Puyallup	WA	98374-1708 US	5/9/2017
Darin Wagner	Puyallup	WA	98371 US	5/9/2017
Jeremy Moore	Puyallup	WA	98374 US	5/9/2017
Taylor Ford	Puyallup	WA	98374 US	5/9/2017
Elias Garoutte	Puyallup	WA	98371 US	5/9/2017
Betty Ransom-Peterson	Olympia	WA	98512 US	5/9/2017
Curtis Yanasak	Puyallup	WA	98371 US	5/9/2017
Jeff Mclean	Enumclaw	WA	98022 US	5/9/2017
Carla Canter	Puyallup	WA	98371 US	5/9/2017
Mandy Sandhorst	Puyallup	WA	98371 US	5/9/2017
Tamara Christensen	Puyallup	WA	98371 US	5/9/2017
Chelsea Hebert	Hudson	WI	54016 US	5/9/2017
Chelle Haire	Puyallup	WA	98374 US	5/9/2017
Judy Ripoli	Puyallup	WA	98372 US	5/9/2017
Don Montague	Tacoma	WA	98408 US	5/9/2017
James Barrows	Orting	WA	98360 US	5/9/2017
Rena Wendland	Puyallup	WA	98372 US	5/9/2017
Dian Lord	Sumner	WA	98390 US	5/9/2017
Nora Strothman	Bothell	WA	98011 US	5/9/2017
Suzan Robertson	Tacoma	WA	98443 US	5/9/2017
Glenda Griebel	Seattle	WA	98178 US	5/9/2017

Julie Cleary	Auburn	WA	98001 US	5/9/2017
Jay Inderbitzin	Puyallup	WA	98373 US	5/9/2017
Ardys Curtis	Puyallup	WA	98374 US	5/9/2017
Sheila Brush	Des Moines	WA	98032 US	5/9/2017
Brian Curtis	Puyallup	WA	98374 US	5/9/2017
marcy Bradshaw	Puyallup	WA	98372 US	5/9/2017
Katie Curtis	Puyallup	WA	98374 US	5/9/2017
Diana Hernandez	Toledo	OH	43611 US	5/9/2017
Anna Chu	Puyallup	WA	98373 US	5/9/2017
pamq miller	Puyallup	WA	98374 US	5/9/2017
Tezra Jolley	Graham	WA	98338 US	5/9/2017
Brian Fenton	Puyallup	WA	98373 US	5/9/2017
ronald knotek	Puyallup	WA	98374 US	5/9/2017
Jerald Moran	Puyallup	WA	98375 US	5/9/2017
Laural Chudecke	Puyallup	WA	98372 US	5/9/2017
Kirstin Linkem	Puyallup	WA	98372 US	5/9/2017
Laura Santos	Puyallup	WA	98372 US	5/9/2017
Diane Picha	Puyallup	WA	98371 US	5/9/2017
Virginia Christmas-Crow	Bonney Lake	WA	98391 US	5/9/2017
Sarah Sherry	Puyallup	WA	98372 US	5/9/2017
Timothy Stidhams	Puyallup	WA	98372 US	5/9/2017
Laura Vitale	Puyallup	WA	98372 US	5/9/2017
Christine Maguire	Puyallup	WA	98373 US	5/9/2017
Kathryn Barnes	Everett	WA	98204 US	5/9/2017
Arild Lindland	Tacoma	WA	98445 US	5/9/2017
Shirley Waddell	Puyallup	WA	98375 US	5/9/2017
Jeanette Rukke	Puyallup	WA	98372 US	5/9/2017
Tina McKim	Bellingham	WA	98225 US	5/9/2017
Cascadia Deaf Nation	Bellingham	WA	98226 US	5/9/2017
Julie Davidson-Gomez	Puyallup	WA	98372 US	5/9/2017
Jarret Hoskins	Sumner	WA	98390 US	5/9/2017
Allyson Cloyd	Seattle	WA	98198 US	5/9/2017
christopher cleary	Puyallup	WA	98372 US	5/9/2017
Dianne Critchfield	Puyallup	WA	98372 US	5/9/2017
Wendy Kruse	Puyallup	WA	I am concerned! US	5/9/2017
Arla Gustafson	Federal Way	WA	98023 US	5/9/2017
Paula Molund	Puyallup	WA	98371 US	5/9/2017
Lee Rushton	Gig Harbor	WA	98335 US	5/9/2017
Cori Johnson	Spanaway	WA	98387 US	5/9/2017
mimi riley	Seattle	WA	98136 US	5/9/2017
Amanda Grams	Bonney Lake	WA	98391 US	5/9/2017
Jackie Frady	Bonney Lake	WA	98391 US	5/9/2017
Tom Blank	Puyallup	WA	98372 US	5/9/2017
Dena Jensen	Blaine	WA	98230 US	5/9/2017
Amanda Morse	Graham	WA	98338 US	5/9/2017
Sandi Van Kirk	Orting	WA	98360 US	5/9/2017
Brandon Sujka	Puyallup	WA	98371 US	5/9/2017

Paula Walker	Puyallup	WA	98372 US	5/9/2017
Anne Stats	Puyallup	WA	98374 US	5/9/2017
LeeAnn Cogert	Blaine	WA	98230 US	5/9/2017
Linda DuBay	Puyallup	WA	98373 US	5/9/2017
Doreen Harper	Des Moines	WA	98198 US	5/9/2017
Jorge Bustamante	Puyallup	WA	98371 US	5/9/2017
Valerie Hines	Tacoma	WA	98445 US	5/9/2017
Wendy Scott	Seattle	WA	98198 US	5/9/2017
Monika Delle	Federal Way	WA	98003 US	5/9/2017
Boe Baty	Seattle	WA	98102 US	5/9/2017
Nikki Matthews	Portland	OR	97217 US	5/9/2017
Meg Sollenberger	Puyallup	WA	98374 US	5/9/2017
Amy Johnson	Puyallup	WA	98373 US	5/9/2017
Natalie Johnston	Puyallup	WA	98373 US	5/9/2017
Lexi Foster	Puyallup	WA	98375 US	5/9/2017
Patricia Matheny	Spanaway	WA	98387 US	5/9/2017
Garret Lloyd King	Puyallup	WA	98373 US	5/9/2017
Amanda Jackson	Graham	WA	98338 US	5/9/2017
Rose Clark	Auburn	WA	98092 US	5/9/2017
Heather Cogger	Puyallup	WA	98374 US	5/9/2017
Shane Ennis	Puyallup	WA	98371 US	5/9/2017
Denise Harrington	Covington	WA	98042 US	5/9/2017
Richard Snyder	Puyallup	WA	98373 US	5/9/2017
Jesse Sandhorst	Puyallup	WA	98371 US	5/9/2017
Doyle Husted	Bonney Lake	WA	98391 US	5/9/2017
Randy Johnson	Puyallup	WA	98374 US	5/10/2017
Ashland Thornton	Puyallup	WA	98374 US	5/10/2017
Cheramie Henley	Puyallup	WA	98371 US	5/10/2017
Audra Brown	Tacoma	WA	98446 US	5/10/2017
Angie Camp	Buckley	WA	98321 US	5/10/2017
Erin Oliver	Puyallup	WA	98371 US	5/10/2017
James Boswell	Puyallup	WA	98371 US	5/10/2017
Martin Katzberg	Puyallup	WA	98372 US	5/10/2017
Karl Honegger	Des Moines	WA	98198-9007 US	5/10/2017
Kathy Bartoy	Buckley	WA	98321 US	5/10/2017
Ross Sharp	Sumner	WA	98390 US	5/10/2017
Juliet Collins	Puyallup	WA	98374 US	5/10/2017
Karen Chambers	Puyallup	WA	98374 US	5/10/2017
Becky Peters	Sumner	WA	98390 US	5/10/2017
Gena Minnick	Orting	WA	98360 US	5/10/2017
Colleen Gregoire	Bellevue	WA	98008 US	5/10/2017
Mike L Thornton	Long Beach, WA		98631 Canada	5/10/2017
Lynn Pascoe	Federal Way	WA	98023 US	5/10/2017
Charlene Chen	Seattle	WA	98198 US	5/10/2017
Dawn Kurtz	Sumner	WA	98390 US	5/10/2017
Natalie Armbrust	Puyallup	WA	98371 US	5/10/2017
Jennifer Lamfers	Maple Valley	WA	98038 US	5/10/2017

Wendy Wagener	Puyallup	WA	98374 US	5/10/2017
Darlene Baldwin	Langley	WA	98260 US	5/10/2017
Tami Grylls	Puyallup	WA	98374 US	5/10/2017
Nathan Seipp	Puyallup	WA	98374 US	5/10/2017
MaryAnne Espinoza	Sumner	WA	98390 US	5/10/2017
Janel Allison	Puyallup	WA	98371 US	5/10/2017
Wendy Rayne	Wilkeson	WA	98396 US	5/10/2017
Cherrilee Matthews	Tacoma	WA	98404 US	5/10/2017
Josh Garber	Bonney Lake	WA	98391 US	5/10/2017
Derek Goodier	Bonney Lake	WA	98391 US	5/10/2017
alex elgas	Bonney Lake	WA	98391 US	5/10/2017
Paige Smith	Spanaway	WA	98387 US	5/10/2017
Jacob Mcknight	Puyallup	WA	98372 US	5/10/2017
Suzy Kraft	Bonney Lake	WA	98391 US	5/10/2017
Caleb Kelley	Puyallup	WA	98372 US	5/10/2017
Jessica Querubin	Puyallup	WA	98374 US	5/10/2017
John McRae	Sumner	WA	98390 US	5/10/2017
tiffany jeanblanc	Puyallup	WA	98371 US	5/10/2017
Greg Stevens	Sumner	WA	98390 US	5/10/2017
Jessica Mays	Graham	WA	98338 US	5/10/2017
Brent Haugen	Puyallup	WA	98374 US	5/10/2017
leah martin	Federal Way	WA	98023 US	5/10/2017
Tessia Rusk	Sumner	WA	98390 US	5/10/2017
Charity Richardson	Puyallup	WA	98372 US	5/10/2017
Eve Jorgensen	Puyallup	WA	98374 US	5/10/2017
Viola Marcoe	Puyallup	WA	98373 US	5/10/2017
Mike Bingham	Sumner	WA	98390 US	5/10/2017
Jolaine Aho	Puyallup	WA	98371 US	5/10/2017
Cody Greenfield	Puyallup	WA	98374 US	5/10/2017
Sivilina Ancheta	Wilkeson	WA	98396 US	5/10/2017
Kimberly ann Small	Bonney Lake	WA	98391 US	5/10/2017
James Miller	Puyallup	WA	98372 US	5/10/2017
Lorrainr Chambers	Tacoma	WA	98443 US	5/10/2017
Kelly Adams	Puyallup	WA	98371 US	5/10/2017
Jessica Adams	Puyallup	WA	98371 US	5/10/2017
Ben Cook	Lakewood	WA	98499 US	5/10/2017
Jamie Lorenz	tacoma	WA	98403 US	5/10/2017
Mary Minsk	Puyallup	WA	98373 US	5/10/2017
Dylan Riche	Graham	WA	98338 US	5/10/2017
Diana Cootsona	Puyallup	WA	98374 US	5/10/2017
Kenneth Wulf	Rhineland	WI	54501 US	5/10/2017
ash ash	Puyallup	WA	98375 US	5/10/2017
Ruth Pellettieri	Canaan	NH	3741 US	5/10/2017
Kristine Mendez	Puyallup	WA	98374 US	5/10/2017
Jessamy Whitwell	Puyallup	WA	98374 US	5/10/2017
Meghan Sharp	Sumner	WA	98390 US	5/11/2017
Kiarra Larson	South Prairie	WA	98385 US	5/11/2017

Carol Ann Clark	Tacoma	WA	98406 US	5/11/2017
Marvin Clark	Tacoma	WA	98406 US	5/11/2017
Kristina Monk	Puyallup	WA	98372 US	5/11/2017
Ashley Becker	Buckley	WA	98321 US	5/11/2017
Penny Webb	Clinton	WA	98236 US	5/11/2017
June Everson	Tacoma	WA	Tacoma US	5/11/2017
Steven Shortt	Bonney Lake	WA	98391 US	5/11/2017
Christine Oliver-Hammond	Bonney lake		98391 Canada	5/11/2017
Kathlyne Pineda	Mountlake Terrace	WA	98043 US	5/11/2017
Jessica Barr	Lake Tapps	WA	98381 US	5/11/2017
Connie Hunter	Puyallup	WA	98373 US	5/11/2017
Mary Austin	Puyallup	WA	98373 US	5/11/2017
Kelsey Rose	Puyallup	WA	98373 US	5/11/2017
Jill Kruger	Randle	WA	98377 US	5/11/2017
Stephanie livers	Tacoma	WA	98424 US	5/11/2017
Linda Walrath	Bonney Lake	WA	98391 US	5/11/2017
Erika Nelson	Sumner	WA	98390 US	5/11/2017
Aaron Latimer	Puyallup	WA	98373 US	5/11/2017
Cynthia Whitman	Puyallup	WA	98371 US	5/11/2017
Leonard Oliver	Puyallup	WA	98372 US	5/11/2017
Jo Wittmier	Maple Valley	WA	98038 US	5/11/2017
Tamara mcstay	Enumclaw	WA	98022 US	5/11/2017
Shannon Whipple	Puyallup	WA	98375 US	5/11/2017
Randy Wilson	Bothell	WA	98021 US	5/11/2017
Doris Schick	Puyallup	WA	98372 US	5/11/2017
Ashley Wakeley	Puyallup	WA	98371 US	5/11/2017
Leann Warmbo	Puyallup	WA	98371 US	5/11/2017
Tracy Cutler	Puyallup	WA	98372 US	5/12/2017
Lynn Smith	Puyallup	WA	98373 US	5/12/2017
Melodie Welch	Herrin	IL	62948 US	5/12/2017
Brayden Moreno	Palm Bay	FL	32909 US	5/12/2017
Becca Christenson	Tacoma	WA	98406 US	5/12/2017
Ashley Fuchs	Milton	WA	98354 US	5/12/2017
Sarah Pierce	Memphis	TN	38128 US	5/12/2017
CAMILLE Depalo	Denver	CO	80218 US	5/12/2017
MARLYS PYKE	Puyallup	WA	98375 US	5/12/2017
Maranda Colwell	Sumner	WA	98390 US	5/13/2017
Richard Baccus	Puyallup	WA	98371 US	5/13/2017
Lori McFarland	Bonney Lake	WA	98391 US	5/13/2017
kristin mcbeth	Salt Lake City	UT	84123 US	5/13/2017
Mike Schlecht	Bonney Lake	WA	98391 US	5/13/2017
Olivia Wilhite	St. Peters	MO	63376 US	5/13/2017
Jo Ann Draughon	Carlsbad	CA	92008 US	5/13/2017
carole hagen	Hillsboro	OR	97124 US	5/13/2017
Jeri Imhof	Tacoma	WA	98424 US	5/13/2017
Aiken Angela	Tacoma	WA	98405 US	5/13/2017
Kelly Stopka	Temple	OK	73568 US	5/13/2017

Marla Blazer	Seatac	WA	98188 US	5/13/2017
Karraann Larson	Tacoma	WA	98404 US	5/13/2017
Elliott Jorgenson	Tacoma	WA	98409 US	5/13/2017
Carla Hau	Bonney Lake	WA	98391 US	5/13/2017
Esmeralda Ramirez	Incline Village	NV	89451 US	5/14/2017
Jonathan Strivens	Tacoma	WA	98404 US	5/14/2017
Jessica Lober	Mountian home	ID	83647 US	5/15/2017
Natalie Lencioni	Puyallup	WA	98375 US	5/15/2017
Robert Lencioni	Puyallup	WA	98375 US	5/15/2017
Jaimee Larsen	Graham	WA	98338 US	5/15/2017
Katherine Gatherer	Darrington	WA	98241 US	5/23/2017
Dan Mahoney	Puyallup	WA	98374 US	5/26/2017
Teresa Lerew	Puyallup	WA	98374 US	5/28/2017
Elvin Lerew	Puyallup	WA	98374 US	5/28/2017
Amy Agnew	Seattle	WA	98391 US	5/28/2017
Christa Jeffreys	Puyallup	WA	98374 US	5/29/2017
Robert Stalnaker	Puyallup	WA	98372 US	5/29/2017
Rebekah Hagen	Puyallup	WA	98372 US	5/29/2017
Tara Lerew	Seattle	WA	98109 US	5/29/2017
Briana Morgan	Bonney Lake	WA	98391 US	5/30/2017
Nola Renz	Seattle	WA	98390 US	6/2/2017
Brent Watterson	Renton	WA	98055 US	6/3/2017
Melody Alnes	Tacoma	WA	98418 US	6/3/2017
Brendan Jeffreys	Puyallup	WA	98374-3321 US	6/3/2017
Wendy Kent	Bonney Lake	WA	98391 US	6/5/2017
Juliana Jamison	Bonney Lake	WA	98391 US	6/5/2017
Steven Lipp	Tacoma	WA	98446 US	6/5/2017
Kenneth Cohen	Sumner	WA	98390 US	6/5/2017
Sean Lynn	Puyallup	WA	98371 US	6/5/2017
Christina Williams	Puyallup	WA	98373 US	6/5/2017
Sara Wolff	Puyallup	WA	98373 US	6/6/2017
Lisa Baril	Puyallup	WA	98374 US	6/6/2017
Natalie White	Puyallup	WA	98372 US	6/6/2017
Joyce Lowery	Tacoma	WA	98371 US	6/6/2017
Jacob Cassidy	Puyallup	WA	98373 US	6/6/2017
Ryan Hanson	Puyallup	WA	98372 US	6/6/2017
Michael Clawson	Seattle	WA	98021 US	6/6/2017
KEVIN WATKINS	Seattle	WA	98391 US	6/6/2017
Lacey Helf	Puyallup	WA	98373 US	6/7/2017
J Westmark	Puyallup	WA	98371 US	6/7/2017
Joseph Weiler	Tacoma	WA	98374 US	6/7/2017
Shannon Hallam	Tacoma	WA	98373 US	6/7/2017
brad helle	Puyallup	WA	98371 US	6/7/2017
Robert Helle	Puyallup	WA	98371 US	6/7/2017
bodenstadt lottie	Kent	WA	98558 US	6/8/2017
Elizabeth Minnich	Duvall	WA	98019 US	6/8/2017
Kathryn Sheldon	Graham	WA	98374-3591 US	6/8/2017

Steve Vermillion	Puyallup	WA	98372 US	6/12/2017
Kris Colburn	Puyallup	WA	98375 US	6/13/2017
Mary Jane Zeiger	Puyallup	WA	98371 US	6/16/2017
Christine Krueger	Tacoma	WA	98373 US	6/16/2017
Gayle Toney	Seattle	WA	98106 US	6/18/2017
Darci Friebe	Puyallup	WA	98373 US	6/19/2017
sunjoo park	Puyallup	WA	98372 US	6/20/2017
Laila Tova	Puyallup	WA	98374 US	6/23/2017
Gino Pagano	Puyallup	WA	93872 US	7/1/2017
Candace Pagano	Puyallup	WA	98372 US	7/2/2017
Amber Stanley	Puyallup	WA	98373 US	7/3/2017
Katherine Sanford	Seattle	WA	98166 US	7/20/2017
leigh fenster		WA	US	7/22/2017
d helms	puyallup	WA	98373 US	7/28/2017
Susan Whipple	Puyallup	WA	98374 US	8/4/2017
Katherine L. Ragsdale	West Des Moines	IA	50266 US	8/13/2017
Faith Gordon	Kirkland		98034 US	8/20/2017
Melissa McNary	Spanaway	WA	98387 US	8/20/2017
Hannah Wilson	Spanaway	WA	98387 US	8/20/2017
Eileen Kosel	Puyallup	WA	98374 US	8/25/2017
Leslee Jo Peterson	Seattle		98198 US	9/16/2017
Rachael Lyon	Spanaway		98387 US	9/16/2017
John Holland	Auburn		98001 US	9/21/2017
April Baez	Bonney Lake	WA	98391 US	10/10/2017
Milena Acuna	Bonney Lake	WA	98391 US	11/7/2017
Paul Monaghan	Tacoma	WA	98416 US	11/16/2017
Traci messer	Puyallup	WA	98373 US	11/28/2017
Dawn VanDerschelden	Olympia	WA	98516 US	12/22/2017
Randall Norstrem	Federal Way	WA	98003 US	1/4/2018
Melinda Giovengo	Tacoma	WA	98444 US	4/16/2018
Cynthia Warner	Puyallup	WA	98372 US	4/28/2018
Kelly Cole	Puyallup	WA	98374 US	7/13/2018
Beth Jersey	Lakewood	WA	98498 US	7/13/2018
Michael Powers	Puyallup	WA	98374 US	7/13/2018
Danelle Rempfer	Auburn	WA	98001 US	7/13/2018
Jaime Richeson	Tacoma	WA	98446 US	7/13/2018
Dorothy Lewis	Milton	WA	98354 US	7/13/2018
Patty Gratz	Puyallup	WA	98374 US	7/13/2018
Larry Stigers	Puyallup	WA	98371 US	7/13/2018
Marie Trenhaile	Puyallup	WA	98374 US	7/13/2018
Marc Robertson	Tacoma	WA	98443 US	7/13/2018
Florence McLaughlin	Bonney Lake	WA	98391 US	7/13/2018
Diane Carey	puyallup	WA	98371 US	7/13/2018
Violet Gonzales	Bonney Lake	WA	98391 US	7/13/2018
Andi Burch	Lakewood	WA	98499 US	7/13/2018
Dana Cella	Auburn	WA	98002 US	7/13/2018
Susan Waterman	Federal Way	WA	98003 US	7/13/2018

Elizabeth Crowell	Seattle	WA	98104 US	7/13/2018
Kathleen Grover	South Hill	WA	98374 US	7/13/2018
Sean Shoup	Puyallup	WA	98374 US	7/13/2018
Amy Canada	Lakewood	WA	98499 US	7/13/2018
nils anderson	Puyallup	WA	98374 US	7/13/2018
Gary Hahn	Buckley	WA	98321 US	7/13/2018
Patricia Styles			US	7/13/2018
Mary Gates	Puyallup	WA	98371 US	7/13/2018
Eli Sparks	Tacoma	WA	98405 US	7/13/2018
Susan Machen	Bonney Lake	WA	98391 US	7/14/2018
Stacey Richards	Federal Way	WA	98023 US	7/14/2018
Charlene Cook	Puyallup	WA	98374 US	7/14/2018
Laura Kinne	Puyallup	WA	98375 US	7/14/2018
Pat Halaszyn	Dupont	WA	98327 US	7/14/2018
Bonnie McGuire	Tacoma	WA	98466 US	7/14/2018
Janie Dymond	Puyallup	WA	98373 US	7/14/2018
Sarah Storey	Fall City	WA	98024 US	7/14/2018
Alise Lanfot	South Hill	WA	98374 US	7/14/2018
Todd Berry			US	7/14/2018
Mari Conrads	St. Clair Shores	MI	48080 US	7/14/2018
Jenny Canter	Seattle	WA	98107 US	7/14/2018
stefano petucci			Italy	7/14/2018
Dyan Centen	Auburn	WA	98408 US	7/14/2018
Sharon Hanson	Puyallup Tacoma	WA	98373 US	7/14/2018
Jennifer Lawrence	Tacoma	WA	98404 US	7/14/2018
Carrie Buddle	Puyallup	WA	98374 US	7/14/2018
Racine Maroni	Puyallup	WA	98372 US	7/14/2018
Maddie Centen	Auburn	WA	98002 US	7/14/2018
Todd Morton	Milton	WA	98354 US	7/14/2018
Robert Whalen	Milton	WA	98354 US	7/14/2018
patrick austin	Puyallup	WA	98373 US	7/14/2018
Dwain Phillips	Puyallup	WA	98372 US	7/15/2018
Patricia Rigney	Puyallup	WA	98372 US	7/15/2018
Susan Johnson	Milton	WA	98354 US	7/15/2018
K K			US	7/15/2018
Mike McElaney			US	7/15/2018
Angelo Marcialis			US	7/15/2018
Donita Moody	Milton	WA	98354 US	7/15/2018
Ric Horjus	Puyallup	WA	98371 US	7/15/2018
Christina Lovan			US	7/15/2018
Anna DeMarre	Bonney Lake	WA	98391 US	7/15/2018
Josh Paxman			US	7/15/2018
Sharon Herbst			US	7/16/2018
Shari Nylander	Seattle	WA	98408 US	7/16/2018
Kristi Hall	Clarksville	IN	47129 US	7/16/2018
Julia Wolbert	Tacoma	WA	98444 US	7/17/2018
DARREL Prado			US	7/17/2018

priscilla romans	Puyallup	WA	98372 US	7/17/2018
Brenda Bowhay	Puyallup	WA	98371 US	7/17/2018
Nancy Oliver	Puyallup	WA	98371 US	7/17/2018
Richard Carlson	Auburn	WA	98001 US	7/17/2018
Sunny Gotewal			US	7/17/2018
Geoff Yearber			US	7/18/2018
Sarah McPherson			US	7/18/2018
Aaron Smith			US	7/18/2018
Monica Alvarado	El Paso	TX	79934 US	7/18/2018
Grey Jones			US	7/19/2018
Jc Leverich			US	7/19/2018
Kobe Coney			US	7/20/2018
jaime palacios	Seattle	WA	98168 US	7/21/2018
Oliver Jahn	Puyallup	WA	98375 US	7/21/2018
Steven Winsor	Auburn	WA	98092 US	7/21/2018
John Glenn	Puyallup	WA	98374 US	7/23/2018
Evan Rens	Orting	WA	98360 US	7/23/2018
bill allison	Orting	WA	98360 US	7/23/2018
Amit Makharia	Puyallup	WA	98374 US	7/23/2018
Amy Palacios	Puyallup	WA	98375 US	7/23/2018
Vikki Ernst	Chattaroy	WA	99003 US	7/23/2018
Jeff Arens	Puyallup	WA	98374 US	7/23/2018
Brooke Douglas	Puyallup	WA	98374 US	7/23/2018
David Anderson	Puyallup	WA	98374 US	7/23/2018
Christopher Cutler	Puyallup	WA	98371 US	7/23/2018
Jeremy Garcia			US	7/23/2018
Stephen Pickard	Rosendaal		6891 Netherland	7/23/2018
Michelle Cutler	Puyallup	WA	98371 US	7/23/2018
Betty Cutler	Issaquah	WA	98029 US	7/23/2018
Robert Newsome	Orting	WA	98360 US	7/23/2018
Lov Patel			US	7/23/2018
Evan Rosebrook	Puyallup	WA	98374 US	7/23/2018
Leanne Norseth	Puyallup	WA	98374 US	7/23/2018
Michele Jangula	Puyallup	WA	98374 US	7/23/2018
Jill Schwerzler Herrera	Puyallup	WA	98374 US	7/23/2018
Lori Wales	Puyallup	WA	98374 US	7/23/2018
Linda Wakeley	Puyallup	WA	98374 US	7/23/2018
Lani Curtis	Tacoma	WA	98446 US	7/23/2018
Stacy Frazier	Puyallup	WA	98374 US	7/23/2018
Christina Caldwell	Orting	WA	98360 US	7/23/2018
Amy Hunter	Puyallup	WA	98374 US	7/23/2018
Miriam Hatcher	Puyallup	WA	98373 US	7/23/2018
Claire Hunter	Puyallup	WA	98374 US	7/23/2018
Julianna Ruisla	Puyallup	WA	98374 US	7/23/2018
Carri Enright	Orting	WA	98360 US	7/23/2018
Martin Ihlen	Orting	WA	98360 US	7/23/2018
Carroll Muck	Puyallup	WA	98374 US	7/23/2018

Jason Wargo	Graham	WA	98338 US	7/23/2018
Tracy Dron	Orting	WA	98360 US	7/23/2018
Mario Amadeo	ORTING	WA	98360 US	7/23/2018
Leslie Lipischak	Puyallup	WA	98375 US	7/23/2018
Rakes donnyreb@gmail.com	Orting	WA	98360 US	7/23/2018
Justin T	Carlsbad	CA	92009 US	7/23/2018
Lewis Honick	Orting	WA	98360 US	7/23/2018
Michael Draper			US	7/23/2018
Dianne Gano	Federal Way	WA	98003 US	7/23/2018
jaimie cale	Puyallup	WA	98374 US	7/23/2018
Courtney Hippo	Puyallup	WA	98374 US	7/23/2018
Judy Carpenter-Ross	Puyallup	WA	98372 US	7/23/2018
Belinda Heil Stark	Puyallup	WA	98371 US	7/23/2018
Renee Osborne	Poulsbo	WA	98370 US	7/23/2018
Derrinda Stone	Graham	WA	98338 US	7/23/2018
Jaana Junttila	Helsinki		Finland	7/23/2018
Ann Crawford	Puyallup	WA	98372 US	7/23/2018
Diane Langer	Puyallup	WA	98374 US	7/23/2018
Lisa Mceathron	Puyallup	WA	98374 US	7/23/2018
Donald Dougherty	Tacoma	WA	98409 US	7/23/2018
Averett Diakhate			US	7/23/2018
Madeline Neigel	Seattle	WA	98109 US	7/23/2018
James Steenson	Puyallup	WA	98372 US	7/23/2018
Jennifer Bules	Puyallup	WA	98374 US	7/23/2018
connor rojas			US	7/23/2018
Ryan Lee			US	7/23/2018
Joshua Gabrielson	Puyallup	WA	98371 US	7/23/2018
Karen Henry	Seattle	WA	98118 US	7/23/2018
J Streeter	Bonney Lake	WA	98391 US	7/23/2018
Roberta Gwynn	Orting	WA	98360 US	7/23/2018
Hilerie Garrison	Toledo	OH	43605 US	7/23/2018
Jerie Jensen-withim	Bonney Lake	WA	98391 US	7/23/2018
Christopher Sims	Tacoma	WA	98422 US	7/23/2018
Carie Sauders	Puyallup	WA	98375 US	7/23/2018
Shelley Parker			US	7/23/2018
Aaron Blondin	Puyallup	WA	98374 US	7/23/2018
Jano Dornisch	Puyallup	WA	98374 US	7/23/2018
david ruff	tacoma	WA	98445 US	7/23/2018
cindy williamd	San Leandro	CA	94578 US	7/23/2018
Nicole Duppenthaler	Puyallup	WA	98374 US	7/23/2018
Hannah Gray			US	7/23/2018
Rob Enright	Orting	WA	98360 US	7/24/2018
Tami Nelson	Puyallup	WA	98375 US	7/24/2018
Melanie Kelly	Tacoma	WA	98445 US	7/24/2018
Lilly Linn	Aurora	CO	80013 US	7/24/2018
Shane Pomerinke	Puyallup	WA	98034 US	7/24/2018
Nichole McNiven	Bonney Lake	WA	98391 US	7/24/2018

Rachelle Garon	Bonney Lake	WA	98391 US	7/24/2018
Paul Hunter	Puyallup	WA	98374 US	7/24/2018
Karen Wilson	Red Feather Lakes	CO	80545 US	7/24/2018
Michelle Kavesh	Seattle	WA	98112 US	7/24/2018
Denise Blake	Puyallup	WA	98375 US	7/24/2018
Kim Weule	Puyallup	WA	98375 US	7/24/2018
Elizabeth Harris	Puyallup	WA	98374 US	7/24/2018
Laura Smith	Puyallup	WA	98375 US	7/24/2018
Joe Salins	Puyallup	WA	98374 US	7/24/2018
Pamela Sharp	Tacoma	WA	98408 US	7/24/2018
Jaimie Houser	Puyallup	WA	98374 US	7/24/2018
Deborah Owens	Puyallup	WA	98374 US	7/24/2018
Lisa Parker Hendricksen			US	7/24/2018
Anne McGinn	Puyallup	WA	98371 US	7/24/2018
Dara Deal	Fircrest	WA	98466 US	7/24/2018
Casey Smith	Puyallup	WA	98375 US	7/24/2018
Tina McNeeley	Puyallup	WA	98375 US	7/24/2018
Karen jermstad Jermstad	Graham	WA	98338 US	7/24/2018
Debbie Jankanish	Seattle	WA	98126 US	7/24/2018
Hannah Bachelder	Kent	WA	98032 US	7/24/2018
MaryJo Young	Puyallup	WA	98374 US	7/24/2018
Jessica Hunt	Spanaway	WA	98387 US	7/24/2018
Kari Hammargren	Kingston	WA	98346 US	7/24/2018
debbie huntington	seattle	WA	98117 US	7/24/2018
Debbie Sheets			US	7/24/2018
Kelly Brown	Renton	WA	98055 US	7/24/2018
Tracey Morris	Renton	WA	98057 US	7/24/2018
Nick Chandler	Puyallup	WA	98372 US	7/24/2018
Dani Schanz	Puyallup	WA	98372 US	7/24/2018
Karen Jackson	Sumner	WA	98390 US	7/24/2018
Andrea Swickard	Burien	WA	98166 US	7/24/2018
Meri Collins	Yelm	WA	98597 US	7/24/2018
Jan White	Eatonville	WA	98328 US	7/24/2018
Raymond Rogers	Puyallup	WA	98374 US	7/24/2018
Nancy Smith	Tacoma	WA	98446 US	7/24/2018
Cathy Ceely	Tacoma	WA	98422 US	7/24/2018
Lisa Sells	San Antonio	TX	78254 US	7/24/2018
Brandon Petersen	Renton	WA	98059 US	7/24/2018
Jessica Petersen	Renton	WA	98055 US	7/24/2018
Patrick Kane	South Hill	WA	98374 US	7/24/2018
Theresa Pfeiffer	University Place	WA	98467 US	7/24/2018
lynne Smith	Federal Way	WA	98023 US	7/24/2018
Christiane Carder	Puyallup	WA	98372 US	7/24/2018
Susan Garrett	Seattle	WA	98133 US	7/24/2018
Emily Garza	Puyallup	WA	98373 US	7/24/2018
Nancy Wolverton	Snoqualmie	WA	98065 US	7/24/2018
Debbie Bryson	Puyallup	WA	98374 US	7/24/2018

Randy Collins	yelm	WA	98597 US	7/24/2018
Danielle Drake	Puyallup	WA	98375 US	7/24/2018
Ashley Yerxa	Puyallup	WA	98372 US	7/24/2018
Lynn Mair	Spokane	WA	99223 US	7/24/2018
Brittany Bertolini	Colorado Springs	CO	80918 US	7/24/2018
ron muck	Puyallup	WA	98375 US	7/24/2018
Mandy Davis			US	7/24/2018
Hollylu Coon	Puyallup	WA	98371 US	7/24/2018
Denise Milles	Tacoma	WA	98404 US	7/24/2018
Nickie Robbs	Graham	WA	98338 US	7/24/2018
Dana Wright	Kent	WA	98030 US	7/24/2018
Valerie Tung	Seattle	WA	98103 US	7/24/2018
Betty Kistenmacher Kistenmacher	Seattle	WA	98116 US	7/24/2018
Jane Sparks	Bonney Lake	WA	98391 US	7/24/2018
Claudia Garren	Asheville	NC	28806 US	7/24/2018
Melissa Hake	Puyallup	WA	98373 US	7/24/2018
Linda Kolano	Sumner	WA	98390 US	7/24/2018
Anthony Sells	San Antonio	TX	78254 US	7/24/2018
Frankie Marazzi	Denver	WA	80247 US	7/24/2018
Christy Lewis	Edmonds	WA	98026 US	7/24/2018
Kristy McGregor			US	7/24/2018
Annette Briggs	Lacey	WA	98516 US	7/25/2018
Kelsi Wusterbarth	Enumclaw	WA	98022 US	7/25/2018
Jodi Franks	Tacoma	WA	98407 US	7/25/2018
Cheryl Evans	Tacoma	WA	98446 US	7/25/2018
Violet Rivera			US	7/25/2018
Peggy Trummell	Puyallup	WA	98373 US	7/25/2018
Kim Kvamme	Puyallup	WA	98375 US	7/25/2018
Michelle Sullivan	Seattle	WA	98117 US	7/25/2018
Twilda Breiland	Puyallup	WA	98374 US	7/25/2018
Kimberly Tarr	Puyallup	WA	98374 US	7/25/2018
Sam Boice			US	7/25/2018
Don Myhre	Puyallup	WA	98374 US	7/25/2018
Tracey Ternyak Myhre			Greece	7/25/2018
Paige Anderson	Tacoma	WA	98422 US	7/25/2018
Mary Myhre	Denver	CO	80210 US	7/25/2018
Debbie Lacefield	Puyallup	WA	98375 US	7/25/2018
Kory Barnes			US	7/25/2018
Michelle Woodke	Puyallup	WA	98372 US	7/25/2018
Kathleen Capettini	Tacoma	WA	98445 US	7/25/2018
Christopher Castro	Puyallup	WA	98374 US	7/25/2018
Christine Bozarth	Puyallup	WA	98375 US	7/25/2018
Jesse Strowbridge	Puyallup	WA	98373 US	7/25/2018
Mari Hinderman	Everett	WA	98203 US	7/25/2018
Melissa Lapp	Bonney Lake	WA	98391 US	7/25/2018
Rossana Aquino	Federal Way	WA	98023 US	7/25/2018
Jill Foster	Tacoma	WA	98404 US	7/25/2018

Kayla Hiner	Puyallup	WA	98374 US	7/25/2018
Stacey Reeve	Puyallup	WA	98375 US	7/25/2018
Julie Anderson	Sumner	WA	98390 US	7/25/2018
James Omvig Jr	Sparks	NV	89436 US	7/25/2018
Cheryl Hodgson	Puyallup	WA	98373 US	7/25/2018
Gelmut Nayberger	Puyallup	WA	98374 US	7/25/2018
Julee Spalinger	spanaway	WA	98387 US	7/25/2018
Kile Rury	Puyallup	WA	98372 US	7/25/2018
Kendra Reeve	Puyallup	WA	98375 US	7/25/2018
Sandy Peterson	Puyallup	WA	98374 US	7/25/2018
Kayla Bradford	Seattle	WA	98118 US	7/25/2018
Benjamin Petersen	Baltimore	MD	21209 US	7/25/2018
Yolan Novak	Colton	CA	92324 US	7/25/2018
Stephanie Munson	Puyallup	WA	98371 US	7/25/2018
dawn Guerrero	Fontana	CA	92337 US	7/25/2018
Allison Linder			US	7/25/2018
Christina Forster	Puyallup	WA	98375 US	7/25/2018
William Harris	South Hill	WA	98374 US	7/25/2018
samantha waterman			US	7/25/2018
Sherri Fortin	Seattle	WA	98168 US	7/25/2018
Samantha Magin	Puyallup	WA	98375 US	7/25/2018
Rachel Turner	Puyallup	WA	98374 US	7/25/2018
Tammi Duis	Franklin	IN	46131 US	7/25/2018
Phillip Tate	Puyallup	WA	98374 US	7/25/2018
Karla Gormley	Puyallup	WA	98375 US	7/25/2018
Emily Rigg	Everett	WA	98208 US	7/25/2018
Isaiah Wilson	Puyallup	WA	98372 US	7/25/2018
Tina Clark	Maple Valley	WA	98038 US	7/25/2018
Rodney Steinke			US	7/25/2018
Kaaren Porter	Seattle	WA	98109 US	7/25/2018
Lance Shotwell Shotwell	Sumner	WA	98390 US	7/25/2018
Tiffany Vigil	Puyallup	WA	98375 US	7/25/2018
Kelli Gay	puyallup	WA	98375 US	7/25/2018
Robert Kleinhanz	Puyallup	WA	98375 US	7/25/2018
Haylie Holov	Puyallup		98374 US	7/25/2018
Kristie Gibney	Tacoma	WA	98411 US	7/25/2018
Kristine Kwok	Eatonville	WA	98328 US	7/25/2018
Celia Smith	Seattle	WA	98168 US	7/25/2018
James Sommer	Puyallup	WA	98374 US	7/25/2018
Ruthie Bertrand	PUYALLUP	WA	98372 US	7/25/2018
Susanne Hippke	Sumner	WA	98390 US	7/26/2018
Heather Freed			US	7/26/2018
Dawn Baughman	Gig Harbor	WA	98335 US	7/26/2018
Oliver Price			US	7/26/2018
Annalee Coon	Puyallup	WA	98371 US	7/26/2018
Tina Olson	Wilkeson	WA	98396 US	7/26/2018
Jerae Goldsby	Puyallup	WA	98372 US	7/26/2018

Lela Perkins	Everett	WA	98208 US	7/26/2018
Andrea Barry	Puyallup	WA	98372 US	7/26/2018
Tony Maupin	Puyallup	WA	98374 US	7/26/2018
Jill Brandaberry	Spanaway		98387 US	7/26/2018
Katie White			US	7/26/2018
Ray Marks			US	7/26/2018
Heather Flieman	Bethesda	MD	20817 US	7/26/2018
Dana Parrish	Seattle	WA	98126 US	7/26/2018
Ashley Gray	Puyallup	WA	98375 US	7/26/2018
Carissa Poyfair	Puyallup	WA	98372 US	7/26/2018
alan macdonald	Seattle	WA	98111 US	7/26/2018
William Ulsund	Puyallup	WA	98372 US	7/26/2018
Sandy Arend	Tacoma	WA	98404 US	7/26/2018
Carolee Gulbransen	PUYALLUP	WA	98374 US	7/26/2018
Autumn Echols	Kent	WA	98031 US	7/26/2018
Nancy Conrad	Puyallup	WA	98373 US	7/26/2018
Debbie Summers	Puyallup	WA	98374 US	7/26/2018
Chase Charaba	Puyallup	WA	98374 US	7/26/2018
Tasha Allen	Puyallup	WA	98375 US	7/26/2018
Alicia Hulse	Puyallup	WA	98371 US	7/26/2018
lindsay baker	puyallup	WA	98374 US	7/26/2018
Chad Baker	Bremerton	WA	98337 US	7/26/2018
Dianna Bailey	Puyallup	WA	98374 US	7/26/2018
Janell Krack	Chevy Chase	MD	20815 US	7/26/2018
Judy Vogel	Tacoma	WA	98408 US	7/27/2018
Brooke Wilkinson	Seattle	WA	98144 US	7/27/2018
Terri JUNEMAN	Puyallup	WA	98374-2091 US	7/27/2018
Rick Juneman	Puyallup	WA	98374 US	7/27/2018
Shanan Bjelland	Puyallup	WA	98374 US	7/27/2018
Bruce Mercer	Puyallup	WA	98375 US	7/27/2018
Mariah Gertson	Puyallup	WA	98373 US	7/27/2018
Bruce Leonardy	Spanaway	WA	98387 US	7/27/2018
Gregory Clark	Tacoma	WA	98404 US	7/27/2018
lisa giovannone	Tacoma	WA	98404 US	7/27/2018
Sam Phillips	Spanaway	WA	98387 US	7/27/2018
Alpina Chilton			US	7/27/2018
Pam Walker	Lacey	WA	98503 US	7/27/2018
Linda Riley	Ocean Shores	WA	98569 US	7/27/2018
Christian Young	KIRKLAND	WA	98034 US	7/28/2018
Erin Kane	Tacoma	WA	98445 US	7/28/2018
Gerlinda Isaak	Puyallup	WA	98374 US	7/28/2018
Bonita Ahart			US	7/28/2018
Jami Weaver	Edgewood	WA	98372 US	7/28/2018
Angela Keller	Indianapolis	IN	46221 US	7/29/2018
Ian Zabawa	Puyallup	WA	98375 US	7/29/2018
Ashley Armatiss	Puyallup	WA	98374 US	7/29/2018
Chasie Beyer	Seattle	WA	98125 US	7/30/2018

Joe Silva				US	7/30/2018
Karen Deckel				US	7/30/2018
NanCee Peterson	Puyallup	WA	98374	US	7/30/2018
Kristy Heineman				US	7/30/2018
Alex Jones				US	7/31/2018
Cathy Rasinin				US	7/31/2018
Danny Grossman				US	8/1/2018
Lesley Harris	Puyallup	WA	98373	US	8/1/2018
Liam Sharpe				US	8/4/2018
Jon Perry				US	8/10/2018
Jessica Anderson	Covington	LA	70433	US	9/5/2018
Melodia Gutierrez	Salt Lake City	UT	84101	US	9/20/2018
Carissa Harwood	Seattle	WA	98111	US	9/22/2018
Leah Banner	Puyallup	WA	98372	US	9/29/2018
Tiffany Wiley	Palm Springs	CA	92264	US	9/29/2018
Marilyn Lieurance	Spanaway	WA		US	9/29/2018
Melissa Stauffer	Bellevue	WA	98004	US	9/29/2018
Katty George	Puyallup	WA	98375	US	10/3/2018
Jdjckkxx Jxjdhxhehcjsj				Sri Lanka	10/5/2018
halls halls				US	10/8/2018
Christine Timme	Puyallup	WA	98372	US	11/30/2018
hannah mcco	Puyallup		98373	US	12/3/2018
Maren Ellingson	Tacoma	WA	98418	US	12/3/2018
Austin Piper	Morgantown		26501	US	12/3/2018
Renee Hayes	Matthews		28105	US	12/3/2018
Connor Bossong	Lancaster		17603	US	12/3/2018
Shawn Searles	Gardiner		4345	US	12/3/2018
Brianna Arteaga	Corona	CA	92883	US	12/3/2018
Lily Geric	Seattle		98168	US	12/3/2018
Ashley Platen	Cleveland		44192	US	12/3/2018
Isabelle Toader	Sammamish		98075	US	12/3/2018
Zach Skubiszewski	Ashburn		20149	US	12/3/2018
Omar Ojeda	Inglewood		90303	US	12/3/2018
calvin kinndling	Wolfeboro		3894	US	12/3/2018
Charlotte Hale	Puyallup	WA	98372	US	12/3/2018
Samuel Jimenez	El Paso		79902	US	12/3/2018
Kareem Hunt	New York		10013	US	12/3/2018
Nicole Donnelly	Breezy Point		11697	US	12/3/2018
David Thorsen	Sumner	WA	98391	US	12/3/2018
Vanessa Pixley	Puyallup	WA	98374	US	12/3/2018
Bryan Deyo	Jurupa Valley		91752	US	12/3/2018
Zach B	Gravois Mills		65037	US	12/3/2018
Gabrielle Sawyer	Puyallup		98375	US	12/3/2018
cathy rupp	Pittsburgh		15213	US	12/3/2018
Denisse Romano	Miami		33180	US	12/3/2018
Jordan Blakely	Columbus		43209	US	12/3/2018
leila kramer	makawao		96768	US	12/3/2018

Ashley Bacani	Fort walton beach		32547 US	12/3/2018
Jennifer Sis	Los Angeles		90044 US	12/3/2018
olland richardson	san deigo		92071 US	12/3/2018
Alex Clark	Lexington		40502 US	12/3/2018
Suzanne Timmer	Beacon		12508-2906 US	12/3/2018
Jackie Stubbs	Sumner	WA	98390 US	12/3/2018
Zach Bookman	Cleveland		44105 US	12/3/2018
Krystian Fitzpatrick	Bridgewater		8807 US	12/3/2018
John Casebere	Tacoma	WA	98405 US	12/3/2018
Bryce Busa	Tampa		33602 US	12/3/2018
alex jilka			US	12/3/2018
Gabe Lail	Murfreesboro		37130 US	12/3/2018
Marion Marsh	Elm Grove		53122 US	12/3/2018
Scott M	Kansas City		64119 US	12/3/2018
Brett Williamson	Detroit		48228 US	12/3/2018
Maddy Mancuso	Beverly Hills		34465 US	12/3/2018
cameron hunter	Lexington		29072 US	12/3/2018
Waylon Robbins	Tacoma		98499 US	12/3/2018
Shane Rethlake	Greensburg		47240 US	12/3/2018
Farmer Joe	Orlando		32822 US	12/3/2018
Christopher McDonald	Spokane		99206 US	12/4/2018
Trent Gibson	Burleson		76028 US	12/4/2018
robert ford	La Center		98629 US	12/4/2018
Daniel Sanchez	New York		10025 US	12/4/2018
Ryley Washburn	Camarillo		93011 US	12/4/2018
Jacob Lanning	Minneapolis		55428 US	12/4/2018
Jaden Lopez-Yick	Fresno		US	12/4/2018
Georgia Mollrem	Excelsior		55331 US	12/4/2018
Albert Gordon	Renton		98055 US	12/4/2018
ANONYMOUS ANONYMOUS	Griffin		30223 US	12/4/2018
c h	Los Angeles		90016 US	12/4/2018
Thomas Edica	Yakima		98902 US	12/4/2018
Marco Polo	Brooklyn		11237 US	12/4/2018
Andrew Bilky			US	12/4/2018
Ambrozia McGettes	San Angelo		76909 US	12/4/2018
Mackenzie Williams	Seagoville		75159 US	12/4/2018
Markise Daniels	Roanoke		24014 US	12/4/2018
James Reed	Bellflower		90707 US	12/4/2018
Angel Rodriguez	Independence		64055 US	12/4/2018
steven bunn	Ferndale		18921 US	12/4/2018
Safari Court	Wverett		98201 US	12/4/2018
Jake Brown	Apo		96367 US	12/4/2018
Jacob Owen	Carlisle		17013 US	12/4/2018
Charles Hupp	Ellabell		31308 US	12/4/2018
richard mann	Elko		89801 US	12/4/2018
Ashley Reid	Derby		67037 US	12/4/2018
Alex Mercer	Grand Rapids		49507 US	12/4/2018

Donald Fields	Mallie	KY	41836 US	12/4/2018
Bekkah Jeneane	Auburn		13021 US	12/4/2018
Stephen Martin	New Bern		28560 US	12/4/2018
Ma'Kayla Hunter	Streamwood		60107 US	12/4/2018
Micah Fisher	Springville		89101 US	12/4/2018
Jennifer Maestas	Auburn	WA	98002 US	12/4/2018
Kenneth Crespo	Fort Lauderdale		33309 US	12/4/2018
Candace Rogers	Lakewood		98498 US	12/4/2018
Laura Perry	Tacoma		98445 US	12/4/2018
jesse nester	Hudson		34669 US	12/4/2018
Lucinda Harvey	Fort Worth		76133 US	12/4/2018
Janae Gladulich	Snellville		30078 US	12/4/2018
will moye	Rocky Mount		27804 US	12/4/2018
Jayde Smith	Cleveland		44102 US	12/4/2018
Sean O'Connor	Seattle		98144 US	12/4/2018
Cierra Fromme	Goliad		77963 US	12/4/2018
Joseph Alegre	Mission Viejo		92691 US	12/4/2018
Pene Negro Grande Muy Gra	United stats		54732 US	12/4/2018
Wren Elliott	Statesville		28677 US	12/4/2018
Curtis Talbott-Osborne	Riverside		92503 US	12/4/2018
John Schellhouse	Detroit		44089 US	12/5/2018
Luis B	San Jose		95123 US	12/5/2018
Danny Weiss	Lakewood		8701 US	12/5/2018
Anna Holle	Saint Paul		53210 US	12/5/2018
Alejandro Lujan	Bel Air		21014 US	12/5/2018
Aaron Parry	Erie		16511 US	12/5/2018
james tanner	Salt Lake City		84119 US	12/5/2018
Shaniya Woodard	Chicago		60827 US	12/5/2018
Michelle Morrisette	Fort Collins		80525 US	12/5/2018
Madison Boswell	Owensboro		42301 US	12/5/2018
Albert Alamina	Yakima		98901 US	12/5/2018
Stephen Speidel	Laguna Woods	CA	92637 US	12/5/2018
Mariano Garcia	Mission		78574 US	12/5/2018
Dennis Connelly	New Orleans	LA	70119 US	12/5/2018
Jack Johnson	New Port Richey		34655 US	12/5/2018
Jose Emanuel Garcia	Lake Stevens		98258 US	12/5/2018
Ryan Laatsch			US	12/5/2018
Antonio Walden	Augusta		30906 US	12/5/2018
Sebastian Rodriguez	Cobleskill		12043 US	12/5/2018
Alex Boanta	West Lafayette		47907 US	12/5/2018
Kaitlin Wilson	College Station		77840 US	12/5/2018
Jason Mathis	Lancaster		93536 US	12/5/2018
JayJay Libao	Barrigada		96913 US	12/5/2018
Sofia Henke	East Lansing		48823 US	12/5/2018
Jessica Anglade			US	12/5/2018
Noah Bousson	Orlando		32862 US	12/5/2018
Marc Feliciano	Paterson		7501 US	12/5/2018

Robert Mundell	Bellwood		60104 US	12/5/2018
Uriah Wallace	Columbus		43229 US	12/5/2018
ryan robichaud	Surprise		85379 US	12/5/2018
Angela Crowe	Memphis	TX	38103 US	12/5/2018
Kim Hernandez	North Hills		91343 US	12/5/2018
Cara Evers	Warrensburg		64093 US	12/5/2018
nathaniel williams	Belmar		7719 US	12/5/2018
Guadalupe Yanez	El Paso		79938 US	12/5/2018
owen payne	Tucson		85743 US	12/5/2018
Sam Miller	Park City		84098 US	12/5/2018
philippize@45kti.xyz philippi:	Tomball		77377 US	12/5/2018
Brandon L	Cincinnati		45219 US	12/5/2018
LaRae Schell	Kaukauna		54130 US	12/5/2018
Mai Mochizuki	Greeley		80634 US	12/5/2018
Jameson Sine	Brentwood		94513 US	12/5/2018
Evelyn Cassidy	Columbus		43211 US	12/5/2018
matthew drexler	louisville		40205 US	12/5/2018
Darlene Miller	Naples	ID	83847 US	12/5/2018
susan biggs	columbia city		46725 US	12/5/2018
isabel meleski	tomball		77375 US	12/5/2018
JoAnne Soest	Fort Wayne		46818 US	12/5/2018
Saurav Karekar	Alpharetta		30022 US	12/5/2018
Nemo Lee	Los Angeles		90048 US	12/5/2018
Maribel Marulanda	New York		11106 US	12/5/2018
Patrick Musulin	Riverside		92504 US	12/6/2018
Cory Cook	Georgetown		78628 US	12/6/2018
John Flanagan	Brooklyn		11209 US	12/6/2018
Jonatan Ramos	Spring		77382 US	12/6/2018
Gerzson Nyiri	San Antonio		78219 US	12/6/2018
Leonardo Velasco Bodart	Kirkland		98033 US	12/6/2018
Shiloh Alwine	Chicago		60014 US	12/6/2018
Ezekiel Lemma	Seattle		98119 US	12/6/2018
Ethan Caviness	Saltillo		38866 US	12/6/2018
Reagen Harris	Collierville		38017 US	12/6/2018
Jack Jones	Bothell		98021 US	12/6/2018
Hannah April	Jackson		8527 US	12/6/2018
Holly Harmon	Redford		48239 US	12/6/2018
Jonas Hellenau			US	12/6/2018
jamie butler	Edgewater		32141 US	12/6/2018
Douglas Johnson	Plainfield		60586 US	12/6/2018
Katherine Romero	La Crescenta		91214 US	12/6/2018
Deborah Anne Oney	West Barnstable		2668 US	12/6/2018
Arianna Nguyen	Indianapolis		46204 US	12/6/2018
Peter Murawski	Marblehead		1945 US	12/6/2018
Ellen Wassenaar	Saint Paul		55118 US	12/6/2018
Jaren "Landible" Landefeld	Lehi		84043 US	12/6/2018
Arnab Chirua	San Mateo		94402 US	12/6/2018

Laurel Von Ende	DeLand		32723 US	12/6/2018
Carole Hagen	Hillsboro		97124 US	12/6/2018
Kimberly Wade	Malverne		11565 US	12/7/2018
Sara Jackson	Port Saint Lucie		34953 US	12/7/2018
Mila Kristiansen			US	12/7/2018
Maria Acuna	Stanley		58784 US	12/7/2018
Ben Lee	Portland		97211 US	12/7/2018
Ethan Cua	Manassas		20110 US	12/7/2018
Cody Raynor	Bonney Lake		98391 US	12/7/2018
Jesse Chale	Dallas		75235 US	12/7/2018
rackks charlay	Lynbrook		11563 US	12/7/2018
Edwin Perian			US	12/8/2018
Torrey Carroll	Minneapolis		55410 US	12/10/2018
Jamon Joiner			US	12/10/2018
Gary Byers	Richmond	IN	47374 US	12/10/2018
Catherine Barlick	Miami		33131 US	12/10/2018
Lasha Wells	Saint Petersburg		33707 US	12/10/2018
James Neal			US	12/10/2018
Abbas Qureshi	Toronto	M1B 1E7	Canada	12/10/2018
Gail Miller-Shapiro	Middleton		1949 US	12/10/2018
Eyaya Fletcher	Chester		23831 US	12/10/2018
Susan Winsberg	Los Angeles		90028 US	12/11/2018
Beshoy Waheb	Richmond		23294 US	12/11/2018
Cailyn Tubolino	San Diego		92195 US	12/11/2018
Theresia Erari	Vancouver		98683 US	12/11/2018
Anete R	Hillsborough		8844 US	12/11/2018
Carolyn Jacobs	Charleston		29414 US	12/11/2018
Robert Sauter	Detroit		48226 US	12/11/2018
Alice L	Manchester		6040 US	12/12/2018
Sara Faqiri	Hicksville		11801 US	12/12/2018
Sara Goicuria	Philadelphia		19143 US	12/12/2018
Michelle Gill	Coeur D Alene		83815 US	12/12/2018
Dylan Duckworth	Carrollton		75006 US	12/12/2018
Katie Fitzpatrick	Shelton		98584 US	12/12/2018
Corbin Smith	Spotsylvania		22551 US	12/13/2018
max king	Louisville		40220 US	12/13/2018
Casey Berry	Layton		84041 US	12/13/2018
Lauren Kettner	Spokane		99205 US	12/13/2018
Praneeth Annam	Trenton		8638 US	12/13/2018
Brandon Nachtsheim	Buckley		98321 US	12/13/2018
Dane Iussig	Kettle Falls		99141 US	12/14/2018
Andrew Morang	Vicksburg		39180 US	12/14/2018
Christopher Dennis	Puyallup	WA	98371 US	12/20/2018
Michael Friedmann	Bronx		10461 US	12/24/2018
Andy Fiedler	Puyallup	WA	98371 US	2/21/2019
Devorah Zeller	Puyallup		98374 US	3/26/2019
Julie McMahan	Seattle		98126 US	3/26/2019

Vanessa Johnson	Seattle	WA	98118 US	3/26/2019	
Jhan Hidalgo Perez	Orlando		32819 US	3/26/2019	
K P	Federal Way	WA	98003 US	3/27/2019	
Jon Pellow	Franklin		16323 US	3/27/2019	
Stephanie Wilson	Muncie		47304 US	3/27/2019	
Nathan Colson	Westfield		1085 US	3/28/2019	
Anna Gurl	Springfield		1089 US	3/29/2019	
Kelsey Bee	Athol		1331 US	4/1/2019	
kipp perini	Athol		1331 US	4/2/2019	
Jason Macleod	Gardner		1440 US	4/2/2019	
Dylan Routhier	Westfield		1085 US	4/3/2019	
Tyler Baker	New Bedford		2746 US	4/3/2019	
Brandon Dow	East Templeton		1438 US	4/3/2019	
Denzel Miles	New Bedford		2745 US	4/3/2019	
Alexander Page	Leominster		1453 US	4/3/2019	
Austin Rogers	North Dartmouth		2747 US	4/3/2019	
Austin Judd	Fairhaven		2719 US	4/3/2019	
Eryk Mann	Acton		1720 US	4/3/2019	
Miguel Guzman	New Bedford		2740 US	4/3/2019	
Austin Carvalho	Fall River		2724 US	4/3/2019	
Eric jacobsen	Brooklyn		11214 US	4/4/2019	
Utkarsh Nath	Fremont		94555 US	4/5/2019	
Lucinda Briggs	Bellevue		98008 US	4/5/2019	
Gregory Rathe	Hopewell Junction		12533 US	4/6/2019	
Eric Urbina	Houston		77035 US	4/9/2019	
Jason Richardson	Boca Raton		33431 US	4/9/2019	
Benjamin Harris	Tacoma		98405 US	4/11/2019	
Marco Gagnon	Forestville		GOT 1EO	Canada	4/13/2019
Janet Bennett	Liverpool		L8 9RY	UK	4/16/2019
Winnie Kliewer	Fresno		93710 US	5/2/2019	
Sheri King	Puyallup		98374 US	6/29/2019	
Kyle Haynes	Bonney Lake	WA	98391 US	9/19/2019	
amanda dulaney	johnston city		62951 US	9/19/2019	
George Williams	Puyallup		98372 US	9/19/2019	
Taylor Samsel	Austin		78758 US	9/19/2019	
Adam Thompson			US	9/20/2019	
Chelsea Aldeguer	Sugar Land		77478 US	9/20/2019	
Lorna Zamora	Cainta, Rizal		1900 US	9/20/2019	
Sarah Chaney	Sierra Vista		85635 US	9/20/2019	
Lucy Charlson	Battle ground		98604 US	9/21/2019	
Leslie Miguel	Pompano Beach		33073 US	9/21/2019	
Carlos Garcia	Oakland		94601 US	9/21/2019	
Michael Taylor	Robbins		27325 US	9/22/2019	
Mickey Cox	Lumberton		28358 US	9/22/2019	
Gerald Walters	Puyallup	WA	98374 US	9/24/2019	
Brandon Madsen	Seattle	WA	98112 US	9/25/2019	
Dean Detweiler	Federal Way	WA	98003 US	9/25/2019	

Florence Treboutte			France	10/10/2019
Laura Rogers	Fort Bragg	CA	95437 US	10/16/2019
Daniel Farrington	Puyallup	WA	98374 US	10/16/2019
Monica McKinley	Puyallup	WA	98375 US	10/16/2019
Syd Seablom	Bonney Lake	WA	98391 US	10/16/2019
Miki McKinnon	Lake Tapps	WA	98391 US	10/16/2019
Penny Bitney	Puyallup	WA	98371 US	10/16/2019
Scott Newton	Bellingham	WA	98225 US	10/16/2019
steve jovanovich	Puyallup	WA	98372 US	10/16/2019
Riley Shannon	Puyallup	WA	98371 US	10/16/2019
Jana Prince	Midland	WA	98445 US	10/16/2019
Corinne McKernan	Puyallup	WA	98373 US	10/16/2019
Shelby Kay			US	10/16/2019
Cinnamon Brown	Spanaway	WA	98387 US	10/16/2019
Stephanie Kaeser	Spanaway	WA	98387 US	10/16/2019
Aaron Labiosa	Puyallup	WA	98374 US	10/16/2019
Kim LaBiosa	Puyallup	WA	98374 US	10/16/2019
Joan Cronk	Tacoma	WA	98446 US	10/16/2019
Chris Taylor	Tacoma	WA	98371 US	10/16/2019
Nancy Crouchet	Seattle	WA	98188 US	10/16/2019
Kyna Sharma	Seattle		98118 US	10/16/2019
Trisha Stone	Tacoma	WA	98404 US	10/16/2019
Robin Ordonez	Seattle	WA	98122 US	10/16/2019
Loli Mahoney	Puyallup	WA	98374 US	10/16/2019
Jessica Nelsen	Graham	WA	98338 US	10/16/2019
Cas Toledanes	Odessa	TX	79762 US	10/16/2019
David Holton	Tacoma	WA	98466 US	10/16/2019
Jennifer Frye	Seattle	WA	98188 US	10/16/2019
Jayne Crivello	Seattle	WA	98373 US	10/16/2019
Kenneth-Michael Warren	Puyallup	WA	98374 US	10/16/2019
Melissa Marion	Tacoma	WA	98404 US	10/16/2019
Heather Evanger	Puyallup	WA	98372 US	10/16/2019
Kathryn Lewis	Seattle	WA	98111 US	10/16/2019
Crystal Bylin	Puyallup	WA	98372 US	10/16/2019
Melissa Helm	Tacoma	WA	98445 US	10/16/2019
Holly Dohrman	Tacoma	WA	98404 US	10/16/2019
Nolan Berry	Puyallup	WA	98371 US	10/16/2019
Jamie Pleinis	Puyallup	WA	98374 US	10/16/2019
Gina Poulter	Puyallup	WA	98371 US	10/16/2019
Jonathan Guthrie	Puyallup	WA	98371 US	10/16/2019
Daniel Adamson	Puyallup	WA	98374 US	10/16/2019
Adam Kaluba	Cincinnati		45249 US	10/16/2019
Janet Keel	Puyallup	WA	98375 US	10/16/2019
Austin Kilpatrick	Puyallup	WA	98374 US	10/16/2019
Travis Medberry	Puyallup	WA	98372 US	10/16/2019
Jason Wood	Seattle	WA	98188 US	10/16/2019
Kayla Eiseman	Puyallup	WA	98372 US	10/16/2019

Cramer Refa	Seattle	WA	98111 US	10/16/2019
Donna DuBois	Puyallup	WA	98371 US	10/16/2019
bridget Nutter	Fremont	CA	94536 US	10/16/2019
Susan Ware	Kent	WA	98030 US	10/16/2019
Janis Olmsteas	Tacoma	WA	98418 US	10/16/2019
S V	Puyallup	WA	98375 US	10/16/2019
Jesse Rost	Puyallup	WA	98372 US	10/16/2019
Nicholas Bach	Puyallup	WA	98372 US	10/16/2019
Karen Gustafson	Edgewood	WA	98372 US	10/16/2019
Johnny Spevak	Puyallup	WA	98374 US	10/16/2019
Tracy Squires	Brewster	WA	98812 US	10/16/2019
Courtney Jackson	Puyallup	WA	98375 US	10/16/2019
Kim Chisholm	Bonney Lake	WA	98391 US	10/16/2019
Stephanie Wick	Graham	WA	98338 US	10/16/2019
Jaelyn Walker	Puyallup		98373 US	10/16/2019
Td Bryan	Tacoma	WA	98411 US	10/16/2019
Jake Stevens	Tacoma	WA	98406 US	10/16/2019
Lee Gregory	Puyallup	WA	98374 US	10/16/2019
Elizabeth Suzik	Lakewood	WA	98498 US	10/16/2019
Philip Helmer	Bonney Lake	WA	98391 US	10/16/2019
Sandra Miller	Seattle	WA	98111 US	10/16/2019
emily madding	Puyallup	WA	98375 US	10/16/2019
Rocky Dendurent	Puyallup	WA	98371 US	10/16/2019
Heather Peak	Maple Valley	WA	98038 US	10/16/2019
Turan Kayaoglu	Puyallup	WA	98371 US	10/16/2019
Justine Berry	Tacoma	WA	98402 US	10/16/2019
Stephanie Mayes	Puyallup	WA	98374 US	10/16/2019
Kimberly Zwink	Puyallup	WA	98371 US	10/16/2019
Adam Murphy	Seattle	WA	98168 US	10/16/2019
Sylvia SCHROEDER	Puyallup	WA	98372 US	10/16/2019
koleen vrooman	puyallup	WA	98506 US	10/16/2019
Scott Nutter	Los Angeles	CA	90004 US	10/16/2019
Russ Mazion	Puyallup	WA	97372 US	10/16/2019
Jeanette Foafao-Coates	Graham	WA	98338 US	10/16/2019
Denise Stahnke	Seattle	WA	98109 US	10/16/2019
Kelly Waters	Puyallup	WA	98374 US	10/16/2019
William Specht	Puyallup	WA	98375 US	10/16/2019
Beverly Johnston	Puyallup	WA	98373 US	10/16/2019
LaRee Welch	Puyallup	WA	98371 US	10/16/2019
Shavonne Matulevich	Seattle	WA	98188 US	10/16/2019
Melissa Stephenson	Puyallup	WA	98372 US	10/16/2019
Judy Talik	Seattle	WA	98103 US	10/16/2019
SHAWNA JOHNSON	Puyallup	WA	98371 US	10/16/2019
Russ Barstow	Puyallup	WA	98375 US	10/16/2019
Timothy Dawson	Seattle	WA	98101 US	10/16/2019
Eugene Wagner	Puyallup	WA	98372 US	10/16/2019
Janelle Scheid	Puyallup	WA	98375 US	10/16/2019

Irvin Berndt			US	10/16/2019
Carol Meagher	Lopez Island	WA	98261 US	10/16/2019
DIANE Berry	Puyallup	WA	98373 US	10/16/2019
Evan Rosebrook	Bonney Lake	WA	98391 US	10/16/2019
Krista Rosebrook	Puyallup	WA	98374 US	10/16/2019
Raymond Valentin	Union city		7087 US	10/16/2019
Patrick Watts	Seattle	WA	98188 US	10/17/2019
Ron Bode	Scottsdale	AZ	85257 US	10/17/2019
Megan Boser	Puyallup	WA	98373 US	10/17/2019
Raina Johnson	Auburn		98092 US	10/17/2019
Gabriella Ashford	Port Townsend	WA	98368 US	10/17/2019
Liz McCall	Big Rapids		49307 US	10/17/2019
Shannon King	Issaquah	WA	98027 US	10/17/2019
Alfia Hawley	Puyallup	WA	98374 US	10/17/2019
Rachel Secrest	Puyallup	WA	98375 US	10/17/2019
Allen Covey	Puyallup	WA	98374 US	10/17/2019
Jacob Covey	Lakewood	WA	98499 US	10/17/2019
Shawn Fox	Puyallup	WA	98375 US	10/17/2019
Glenda Specht	Puyallup	WA	98375 US	10/17/2019
Jennifer Wademan	Syracuse		13215 US	10/17/2019
Shannon Silvernail	Puyallup	WA	98373 US	10/17/2019
Cathy Hansen	Puyallup	WA	98372 US	10/17/2019
Ernest Carrillo	Lakewood	WA	98498 US	10/17/2019
RoxAnne Salter	Auburn	WA	98002 US	10/17/2019
Mary Boben	Puyallup		98374 US	10/17/2019
YEKATERINA BOICHENKO	Puyallup	WA	98375 US	10/17/2019
Jane Carden	Puyallup	WA	98374 US	10/17/2019
Cindy johnston	Lakewood	WA	98498 US	10/17/2019
Nathaniel Conrad	Puyallup	WA	98374 US	10/17/2019
Nicole munsonNicoleMunsor	Puyallup	WA	98374 US	10/17/2019
KYLE MAHLSTEDT	Puyallup	WA	98372 US	10/17/2019
Nykwon McMorris	Baltimore		21229 US	10/17/2019
Lisa Lohse-Miranda	Puyallup	WA	98373 US	10/17/2019
Rebecca Johnstone	Sumner	WA	98390 US	10/17/2019
Ty Hander	Seattle	WA	98136 US	10/17/2019
wesley wei	Plano		75025 US	10/17/2019
Kevin Waters	Bonney Lake	WA	98391 US	10/17/2019
Catherine Sorge	Kent	WA	98042 US	10/17/2019
Amber Vaughn	Bellingham	WA	98229 US	10/17/2019
Andrea Dickinson-Gutierrez	Puyallup	WA	98372 US	10/17/2019
Antonia Oh	Puyallup	WA	98373 US	10/17/2019
Maigan Darting	Rainier	WA	98576 US	10/17/2019
Andrea Anzora	Puyallup	WA	98373 US	10/17/2019
Carolyn Balmer	Puyallup	WA	98372 US	10/17/2019
Jason McClain	Puyallup	WA	98372 US	10/17/2019
Hien Nguyen	Tacoma	WA	98403 US	10/17/2019
Kaley Boger	Tacoma	WA	98406 US	10/17/2019

Leslie Beck	Bonney Lake	WA	98391 US	10/17/2019
DEJA Brimhall	Puyallup	WA	98375 US	10/17/2019
Manuel Vega	Dupont	WA	98327 US	10/17/2019
Zachary McLaughlin	Puyallup	WA	98374 US	10/17/2019
Sandra Hackbarth	Puyallup	WA	98372 US	10/17/2019
Llane Rost	Graham	WA	98338 US	10/17/2019
Jennifer Herring	Mooresville	NC	28117 US	10/17/2019
Brandon Baumchen	Bonney Lake	WA	98391 US	10/17/2019
Brooke Robinson	Sumner	WA	98390 US	10/17/2019
Sam Hoyt	Puyallup	WA	98374 US	10/17/2019
Yvonne Miller	Bonney Lake	WA	98391 US	10/17/2019
Ann Marie	Houston		77202 US	10/17/2019
Greg Still	Puyallup	WA	98374 US	10/17/2019
Claire Carver	Seattle	WA	98103 US	10/17/2019
Sharon Austin	Puyallup	WA	98374 US	10/17/2019
Derek Specht	Puyallup	WA	98375 US	10/17/2019
Heather Price	Seattle	WA	98115 US	10/17/2019
Alicia Andersen	Jupiter		33458 US	10/17/2019
Maria Cleveland	Puyallup	WA	98374 US	10/17/2019
Crystal Spear	Enumclaw	WA	98022 US	10/17/2019
Linda Adye-Whitish	Lake Tapps	WA	98391 US	10/17/2019
Heather Bolt	Temple		30179 US	10/17/2019
Roland Dale			US	10/17/2019
Chris Rahn	Puyallup	WA	98374 US	10/17/2019
Charlotte Hicks	Secaucus		7094 US	10/17/2019
Carole Alexander	Auburn	WA	98002 US	10/17/2019
Kathryn mccue	Sarasota		34236 US	10/17/2019
Jamie Bryant	Baltimore		21217 US	10/17/2019
Lucia Underwood	Nicholasville		40356 US	10/17/2019
Lauren Bulzomi	Bonney lake	WA	98391 US	10/17/2019
Bethany Sutton	Puyallup	WA	98371 US	10/17/2019
Kelsie Chenault	Chesapeake		23321 US	10/17/2019
Thomas Tracewell	Puyallup	WA	98372 US	10/17/2019
Julio Garcia	Dallas		75218 US	10/17/2019
Keanna January	Danville		61832 US	10/17/2019
Danielle Flintoff	Enumclaw	WA	98022 US	10/17/2019
Stephen Davis	Puyallup	WA	98374 US	10/17/2019
Miranda Embrey	Puyallup	WA	98374 US	10/17/2019
Reese Junk	Sacramento		95835 US	10/17/2019
Anita Guerrero	Fort Meade		33841 US	10/17/2019
Heather Morgan	Tacoma	WA	98409 US	10/17/2019
Amber Shaffer	Puyallup	WA	98372 US	10/17/2019
Evelyn McAdam	Huntsville	TX	77340 US	10/17/2019
Justin Whisler	Bonney Lake	WA	98391 US	10/17/2019
Rosie Frison	Portland		97211 US	10/17/2019
Kristi Wright	Puyallup	WA	98372 US	10/17/2019
Susan Stallman	Anderson Island	WA	98303 US	10/17/2019

Elizabeth Mueller	Seattle	WA	98188 US	10/17/2019
Michael Peters	Puyallup	WA	98371 US	10/17/2019
Marcia Carpenter			US	10/17/2019
Janet McMahan	Puyallup	WA	98371 US	10/17/2019
Nadine Russum	Fremont	CA	94536 US	10/17/2019
D’Nyah Lyde	College Park		20740 US	10/17/2019
Jennifer Marcum	GLADSTONE	OR	97027 US	10/17/2019
Lori Kappel	Seattle	WA	98118 US	10/17/2019
Eleni Sergiou	Limassol		Cyprus	10/17/2019
Collin Babineau	Aberdeen		98520 US	10/17/2019
Jennifer White	Lapel		46051 US	10/17/2019
Elizabeth Cervantes	Sylmar		91342 US	10/17/2019
Lyndee Berz	Puyallup	WA	98371 US	10/17/2019
Judy Starks	Puyallup	WA	98374 US	10/17/2019
CARL FISHER	Bonney Lake	WA	98391 US	10/17/2019
Daniel Pino	Wenatchee		98801 US	10/17/2019
Linda Hawks	Yelm	WA	98597 US	10/17/2019
Neil Shah	Middlesex		8846 US	10/17/2019
Pennie L. Stevens Stevens	Bonney Lake	WA	98391 US	10/17/2019
Daniel Yadon	Bonney Lake	WA	98391 US	10/17/2019
lila mclaughlin	Port Orchard	WA	98367 US	10/18/2019
Isaiah Ayala	Pomona		91767 US	10/18/2019
Andy Simonsen	Puyallup	WA	98374 US	10/18/2019
Brian Dixon	Dallas	TX	75216 US	10/18/2019
Sarah Boger	Moses Lake	WA	98837 US	10/18/2019
DEBBIE SILVER	Puyallup	WA	98372-5191 US	10/18/2019
Kenneth Spear	Enumclaw		98022 US	10/18/2019
Danielle Kerr	Seattle	WA	98133 US	10/18/2019
Kirsten Silver	Bellingham	WA	98225 US	10/18/2019
Steve Bridgman	Seattle	WA	98119 US	10/18/2019
Karen Messenger	Tempe		85283 US	10/18/2019
Kristin Riveness	Tacoma	WA	98466 US	10/18/2019
Jonathan Carlson	Puyallup	WA	98373 US	10/18/2019
Wendy Carlson	Poulsbo	WA	98370 US	10/18/2019
Siry Fartsalot	Royal Oak		48067 US	10/18/2019
Jim Vaughn	Puyallup	WA	98375 US	10/18/2019
Oscar Mejia	Fort Lauderdale		33331 US	10/18/2019
Cole Gibstein	Glen Head		11545 US	10/18/2019
Jamie Taliulu	Seattle		98047 US	10/18/2019
Jennifer Reiley	Marion		62959 US	10/18/2019
Sonja Schmitt	New York City		11371 US	10/18/2019
Ned Witting	Lakewood	WA	98499 US	10/18/2019
Gema Munoz	Miami		33144 US	10/18/2019
Julia Schmidt	Puyallup	WA	98371 US	10/18/2019
Taylor Payne	Loveland		80538 US	10/18/2019
Amanda Ware	Tacoma	WA	98445 US	10/18/2019
Heather King	Belfair	WA	98528 US	10/18/2019

Garrett Mann	Puyallup	WA	98372 US	10/18/2019
Everett Shultz	Oil City		16301 US	10/18/2019
Sami Davidson	Puyallup	WA	98373 US	10/18/2019
Obama Care	Shirley		11967 US	10/18/2019
Lora Maxwell	Pendleton		46064 US	10/18/2019
Brenda Adamson	Yelm		98597 US	10/18/2019
Sean Bubernak	Hayward		94541 US	10/18/2019
Sara Kelley	Bonney Lake	WA	98391 US	10/18/2019
Keegan Friel	Seattle	WA	98136 US	10/18/2019
Sabrina Mann	Bonney Lake	WA	98391 US	10/18/2019
Kathleen Hill	Tacoma	WA	98444 US	10/18/2019
Jessica Manjarrez	Seattle		98188 US	10/18/2019
Candace Hammans	Seattle	WA	98112 US	10/18/2019
Danita Kelly	Lakewood	WA	98499 US	10/18/2019
Juju Salem	Dayton		45429 US	10/18/2019
Sarah Jones	Fortville		46040 US	10/19/2019
Shahitta Crouch	Newark		7106 US	10/19/2019
Thomas Bartoy	Buckley	WA	98321 US	10/19/2019
Mark Wool	Denver		80211 US	10/19/2019
Kathy Fraino	Pittsburgh		15211 US	10/19/2019
Cindy Schmidt	Puyallup	WA	98374 US	10/19/2019
Christopher West	Nashville		37214 US	10/19/2019
Hayden Lui	Riverside		92507 US	10/19/2019
Shailee StClair	Lake Tapps	WA	98391 US	10/19/2019
Sampson Moyik	New York		10010 US	10/19/2019
Michael Weinstein	Naples		US	10/19/2019
Yolanda Markle	Spanaway	WA	98387 US	10/19/2019
Kaylin Adams	Greensboro		27407 US	10/19/2019
Henry Stubes	Denver		80219 US	10/19/2019
Russell Croker	Ilford		ig1 2sq UK	10/19/2019
PETER GRIFFIN	Birmingham		B15 UK	10/19/2019
Alejandro Fuentes	Fairfax		22038 US	10/19/2019
John Kramer	Marshfield		2050 US	10/19/2019
Sydney Mitchell	Tucson		85705 US	10/20/2019
Jacques Brun	Pointe-du-Chêne		E4P-4Z9 Canada	10/20/2019
sarah gordon	vero beach		32968 US	10/20/2019
AnnaLee Bell	Jamul		91935 US	10/20/2019
Chad Lee	Tacoma	WA	98444 US	10/20/2019
Joel Symons	Douglas,		99824 US	10/20/2019
Maddie Liedtke	Minneapolis		55432 US	10/20/2019
Kim Gray	Sumner	WA	98390 US	10/20/2019
Adan Martinez	Burlington		27217 US	10/20/2019
Светлана Путилова			US	10/20/2019
Angelika Kappus			74889 Germany	10/20/2019
Jack Kanaan	Brockton		2301 US	10/20/2019
Nicholas Kohn	Lafayette		80026 US	10/20/2019
Patricia Sullivan	Tacoma	WA	98424 US	10/21/2019

Dirk Paulsen	Puyallup	WA	98375 US	10/21/2019
Poopy Bum Bum Dick	Coochieville		11216 US	10/21/2019
Camille Paulsen	Puyallup	WA	98375 US	10/21/2019
Nolan Nelson	Brighton		48116 US	10/21/2019
Shannon Schaffer	Ste Gen		63670 US	10/21/2019
Michelle Schmidt	Tacoma	WA	98404 US	10/21/2019
Austen Wheatley	Lynnwood		98087 US	10/21/2019
Alex Palmateer	Littleton		80126 US	10/21/2019
Mitchell Ellis	Memphis		38119 US	10/21/2019
Traci Wachs	Denver		80238 US	10/21/2019
Mike Lee			US	10/21/2019
Sydney Jewell	Utica		48316 US	10/21/2019
Sean Steen	Tacoma		98409 US	10/21/2019
Cory Ebert	Englewood		80150 US	10/21/2019
Mabeth Grout	Puyallup	WA	98374 US	10/21/2019
Andrew Mobley	Puyallup	WA	98374 US	10/22/2019
Hanale Keanaaina	Henderson		89074 US	10/22/2019
Kaitlin Elizabeth	Rochester		3867 US	10/22/2019
Brandon Sparks	Honolulu		96818 US	10/22/2019
Amanda Polzin	Fowlerville		48836 US	10/22/2019
Luke Varilone	Detroit		48219 US	10/22/2019
Nathan Ciak Ciak	Redford		48239 US	10/22/2019
karlene guthrie	Pinckney	MI	48169 US	10/22/2019
Soren Hartsock	Mason		48231 US	10/22/2019
Nora gamboa	Los Angeles		90042 US	10/22/2019
Gail Kucinski	Hollywood		33024 US	10/23/2019
Jerry Johnson	Seattle		98102 US	10/23/2019
Paul Adkins	Belleville		48111 US	10/23/2019
Scarlett Hollingsworth	Brentwood		37027 US	10/23/2019
stephanie Hartkemeyer	West Chester		45069 US	10/23/2019
Todd Holtzclaw	Kent	WA	98032 US	10/23/2019
Courtney Bell	Puyallup	WA	98375 US	10/23/2019
DaShawn Turner	Fairfield		45014 US	10/23/2019
Jasmin Melton	Chicago		60634 US	10/23/2019
Christopher Rojas	Miami		33175 US	10/23/2019
Ariel Laming	Franklin		37064 US	10/23/2019
Arlene Zuckerman	Jamaica		11435 US	10/23/2019
Connor Riehle	West Chester		45069 US	10/23/2019
Peyton Haywood	Cincinnati		45241 US	10/23/2019
dayshun thomas	taylor		48174 US	10/23/2019
Dezmon Navarro	Omaha		68124 US	10/23/2019
Samara Napier	Richmond		40475 US	10/23/2019
Simon Wieselberg	Miami		33169 US	10/23/2019
Lily Schwartz	Cincinnati		45215 US	10/23/2019
Dakota Fronk	Lewis Center		43035 US	10/23/2019
Kerin North-thompson	Hamilton		45013 US	10/23/2019
arianna rogness	mckinney		75071 US	10/23/2019

Z W	Birmingham		35226 US	10/23/2019
Rawr Marchand blume	Miami		33169 US	10/23/2019
Jeff Strobel	Tacoma	WA	98409 US	10/24/2019
Sean Warren	Puyallup	WA	98373 US	10/24/2019
Stephanie Warren	Seattle	WA	98188 US	10/24/2019
Pebbles Kaydus	Puyallup	WA	98374 US	10/24/2019
Kim Field	Tacoma	WA	98403 US	10/24/2019
Christina Ful8	Puyallup	WA	98374 US	10/24/2019
Leslie Trucco-Hilton	Tacoma	WA	98404 US	10/25/2019
Mike MacDonald	Puyallup	WA	98374 US	10/25/2019
Kim Lee	Bonney Lake	WA	98391 US	10/25/2019
Susan Johnson	Seattle	WA	98122 US	10/25/2019
Danae deKruyf	Puyallup	WA	98374 US	10/26/2019
Stephen LaLanne	Post Falls	ID	83854 US	10/26/2019
Lynessa Stone	Puyallup	WA	98374 US	10/26/2019
Emily Willard	Puyallup	WA	98372 US	10/26/2019
Leigh Mahaffie	Graham	WA	98338 US	10/26/2019
Shannon Zimmerly	Spanaway	WA	98387 US	10/29/2019
Francisco Briceno	Coral Gables		33134 US	10/29/2019
Jamie Lorenz	Seattle	WA	98122 US	10/29/2019
Avraam Aristocleous			Cyprus	10/29/2019
Dan Nelson	Puyallup	WA	98371 US	10/29/2019
Keyona Layton	Tampa		33605 US	10/29/2019
brooke herman	Brunswick		44212 US	10/29/2019
Leila Hudson	Miami		33102 US	10/29/2019
Diodore Lopez	Greensboro		27455 US	10/29/2019
Alondra Rybicki	Wichita		67214 US	10/29/2019
Tucker Theiss	Kansas City		66102 US	10/29/2019
Sage Wehrbein	Omaha		68197 US	10/29/2019
Ramon Ruiz	Miami		33135 US	10/29/2019
Bianca Gomez	Salem		97305 US	10/29/2019
Finlay Arnott	Hudson		44236 US	10/29/2019
Charlie Tardio	Diamond Bar		91765 US	10/29/2019
Ava Rasmussen	Saint Paul		55124 US	10/29/2019
Richard Savage	Collierville		38017 US	10/29/2019
Juwan Foreman	Del city		73115 US	10/29/2019
Janel Molina	Mission		78572 US	10/29/2019
Daniel Rubio	San Antonio		78201 US	10/29/2019
Mary Clare Bryant	Lakewood		44107 US	10/29/2019
spirit hoffere	Rochester		14610 US	10/29/2019
charles Adams	Mesa		85210 US	10/29/2019
Tyler Cunningham	Indianpolis		46236 US	10/29/2019
Britanie Vil	New York		10003 US	10/29/2019
jadie kotlc	Chester		7930 US	10/29/2019
Nick Castorina	Philadelphia		19038 US	10/29/2019
Nevaeh Erni	Aurora		80014 US	10/29/2019
Tyler Griffin	Luling		70070 US	10/29/2019

Ganjita Wilson	Tampa		33614 US	10/29/2019
ken corkum	Puyallup	WA	98374 US	10/29/2019
Robert Albritton	Tacoma	WA	98445 US	10/29/2019
KATHLEEN JOHNSON	Tacoma	WA	98422 US	10/29/2019
Eileen Lyons	Tacoma	WA	98424 US	10/30/2019
Alexa Lyons	Puyallup	WA	98374 US	10/30/2019
Jenarae Bach	Bonney Lake	WA	98391 US	10/30/2019
M Adams	Salt Lake City		84090 US	10/30/2019
Jackson Hersch	Moses Lake		98837 US	10/30/2019
Ernest Harmon	Minneapolis		55430 US	10/30/2019
Denaijah Walker	Champlin		55316 US	10/30/2019
karlycia Rcahal	Fort worth		76116 US	10/30/2019
Jaelie Dugger	Chicago		60614 US	10/30/2019
Alex Harris	Utah		85014 US	10/30/2019
Nerissa grimm	Tacoma		98405 US	10/30/2019
Ian Henderson	Lexington		27292 US	10/30/2019
Michael Ortiz	Denver		80206 US	10/30/2019
Rosemarie Wentz	Federal Way	WA	98023 US	10/30/2019
Michéll Espinoza	San Jose		95128 US	10/30/2019
Marvin Johnson	Puyallup	WA	98372 US	10/30/2019
Art Behrman	Seattle	WA	98105 US	10/30/2019
Lynice Cragle	Spanaway	WA	98387 US	10/30/2019
Jason Daniels	Tampa		33614 US	10/30/2019
Kaija Gudenkauf	Ashburn		20149 US	10/30/2019
Cassandra Martinez	Palmview		78574 US	10/30/2019
Kellys Valdes			US	10/30/2019
Ariana Afa			US	10/30/2019
brooklyn capps	Houston		77080 US	10/30/2019
alayshia kong	Providence		2909 US	10/30/2019
Khalil Sennet	San Francisco		94102 US	10/30/2019
Katherine Valladares	Las Vegas		89142 US	10/30/2019
Oscar Garcia	Memphis		38108 US	10/30/2019
Austin Bales	Harrison		37341 US	10/30/2019
Marcos Olivencia	Tacoma		98466 US	10/30/2019
Linda Walls	Berkeley		94703 US	10/30/2019
bitch pussy	Redmond		98052 US	10/30/2019
Alison Bonilla	Goose creek		29445 US	10/30/2019
Kiara Santisteban	Torrance		90501 US	10/30/2019
Chris Kindsfather	Houston		77057 US	10/30/2019
Aidan Wagstaff	Kirkland		98034 US	10/30/2019
Armando Fuentes	Anaheim		92825 US	10/30/2019
allison ching	Honolulu		96816 US	10/30/2019
Triniah Maclin	Tennessee		35805 US	10/30/2019
Raishaad Briggs	Memphis		38118 US	10/30/2019
Emily Asencio	Seattle		98188 US	10/30/2019
Sarah Schutes	Alpharetta		30022 US	10/30/2019
Ryan Seiler	Moody		35004 US	10/30/2019

Duru Bosh	Tacoma		98466 US	10/30/2019
Braedan Kearney	Seattle		98034 US	10/30/2019
Reyana Robles	Austin		78745 US	10/30/2019
Kano Kaeka	Seattle		98188 US	10/30/2019
Autumn Miller	Salt Lake City		84129 US	10/30/2019
Danna Miller	Sandy		84092 US	10/30/2019
Michael Childers	Tacoma	WA	98446 US	10/30/2019
Lori Childers	Seattle	WA	98198 US	10/30/2019
Bruant Barahona	Charlotte		28045 US	10/31/2019
Jordan Guzman	Sanger		93657 US	10/31/2019
Lance Arredondo	Houston		77271 US	10/31/2019
kyle @kyvarro	Torrance		90503 US	10/31/2019
Lillie Smalara	New Lenox		60451 US	10/31/2019
Mariano Diaz	Tucson		85713 US	10/31/2019
MArgret Parongwa	Boardman		97818 US	10/31/2019
ellie throcmorton	Irving		75060 US	10/31/2019
Chloe Trogni	Riverside		6878 US	10/31/2019
Sam Hittepole	Rochester		14609 US	10/31/2019
Jonathan Ni	Chantilly		20151 US	10/31/2019
T Yvette Gates			US	10/31/2019
Oscar Lopez	Tucson		85719 US	10/31/2019
Olman Torres	Lorton		22079 US	10/31/2019
Ainslie Dorrian	Greenwich		6830 US	10/31/2019
Christopher Wilson	Statesville		28625 US	10/31/2019
Grace Dietrich	Rex		30273 US	10/31/2019
Close School	Silver Spring		20910 US	10/31/2019
Capital Steez	Salt Lake City		84120 US	10/31/2019
Theophilus Adu Gyamfi	Virginia		20121 US	10/31/2019
Marcus Owens	Memphis		38127 US	10/31/2019
Dennis Gonzalez	Charlotte		28217 US	10/31/2019
Robby Brownlow	Round Rock		78664 US	10/31/2019
Todd Gurley	Salt Lake City		84109 US	10/31/2019
Ryan Woodhead	Kirkland		98034 US	10/31/2019
Seth DeWhitt	Bellevue		98007 US	10/31/2019
Please Sign	Houston		77091 US	10/31/2019
Aiden Lewis	Arlington		22206 US	10/31/2019
Colby Caffie	Marietta		30064 US	10/31/2019
chef boyardee	Gaithersburg		20878 US	10/31/2019
Lailah Rosario	Pacific		98047 US	10/31/2019
griffin brandt	Saint Paul		55128 US	10/31/2019
C Woof Woof Ribs	Manhattan Beach		90266 US	10/31/2019
Haden Wong	Winston-salem		27104 US	10/31/2019
Alejandra Urgel quiroga	Chantilly		20151 US	10/31/2019
ariana Biermann	Alpharetta		30004 US	10/31/2019
madison quick	Trussville		35173 US	10/31/2019
Davidaniel Pabon	Miami		33162 US	10/31/2019
Daniel Lopez	Pflugerville		78660 US	10/31/2019

Jonathan Li	Germantown		20874 US	10/31/2019
Mathew Buxton	Fremont		94536 US	10/31/2019
Michael Ruidera	Spring Hill		34609 US	10/31/2019
Angela Caudillo	Lockhart		78644 US	10/31/2019
Samuel Zavala	Salinas		93906 US	10/31/2019
Stephano Monge	Katy		77494 US	10/31/2019
Luke Pfeifer	Minneapolis		55421 US	10/31/2019
Melissa Heithaus	Mckinney		75070 US	10/31/2019
Sandra Wertz	Cheyenne		82007 US	11/1/2019
Kimberly Jimenez	Seattle	WA	98117 US	11/1/2019
Tina Simms	Grapeview	WA	98546 US	11/1/2019
James Edwards	Orting	WA	98360 US	11/1/2019
Stacy Lovett	Puyallup	WA	98374 US	11/1/2019
Donald Bykonen	Seattle	WA	98122 US	11/1/2019
Lynn Robertson	Seattle	WA	98111 US	11/1/2019
Dawn Ayers	Puyallup	WA	98373 US	11/1/2019
Jalie Robertson	Auburn		98002 US	11/1/2019
Teresa Hayford	Puyallup	WA	98372 US	11/1/2019
Joseph Covey	Puyallup	WA	98374 US	11/2/2019
Vannesa Esparza	Ennis tx		75155 US	11/2/2019
Erika Machado	Riverdale		20737 US	11/2/2019
Evalyn Acror	Powell		37849 US	11/2/2019
Nicole Belcastro	Brookfield		60513 US	11/2/2019
Richard Gregory	Lake tapps	WA	98391 US	11/2/2019
Olivia Pulliam	Durham		27707 US	11/2/2019
Vicki Michel	Puyallup	WA	98374 US	11/2/2019
Mya Morris	Rsm		92688 US	11/2/2019
Zach Tran	Marrero		70072 US	11/2/2019
Sam Mey	Long Beach		90815 US	11/2/2019
Drew Hill	Washington		20068 US	11/3/2019
Clark Kershner	Puyallup	WA	98373 US	11/3/2019
Silvia Suarez	Los Angeles		90022 US	11/3/2019
Hope Gach	Bellevue		68123 US	11/3/2019
Karen Kershner	Puyallup	WA	98373 US	11/3/2019
Julie Hindman			US	11/3/2019
Anitta Trovato	Bonney Lake		98391 US	11/3/2019
Jency Escobar	Charlotte		28209 US	11/3/2019
Kimberly Rose	Puyallup	WA	98372 US	11/3/2019
Andrew Weeks	Mebane		27302 US	11/3/2019
Kathy Peterson	Sharon		16146 US	11/3/2019
Carol Spilman	Olympia		98503 US	11/3/2019
Sheila Bihag	Honolulu		96813 US	11/3/2019
Logan McDermott	Brooksville		34601 US	11/3/2019
Lucas Spitler	Brooksville		34601 US	11/3/2019
Dymond Anderson	Springfield Gardens		11413 US	11/3/2019
Irina Safiullina	Naberezhnye Chelny		Russia	11/4/2019
Penelope Thompson			US	11/4/2019

Christina Grossruck	Seattle	WA	98134 US	11/4/2019
Eli Roddy	Brentwood		37027 US	11/4/2019
Sandra Gregory	Puyallup	WA	98374 US	11/4/2019
Dawn meadows	Bonney Lake	WA	98391 US	11/4/2019
Latendresse Tina	Beaverton		97006 US	11/4/2019
Roni Keene	Hoover		US	11/4/2019
Michael Rikard	Daphne		36526 US	11/4/2019
Daniel Garcia	Hector		72843 US	11/4/2019
Lane Williams	Mobile		36604 US	11/4/2019
Joseph Conley	Tacoma	WA	98407 US	11/4/2019
Victoria Shih	Plano		75025 US	11/4/2019
Crystal Robles	Houston		77022 US	11/4/2019
Erica White	Philadelphia		19128 US	11/5/2019
Michael Johnson	Honolulu		96818 US	11/5/2019
Angel M	Willow Grove		19090 US	11/5/2019
Jane Hursh	Winter Park	FL	32789 US	11/5/2019
Jasmine Grossruck	Tacoma	WA	98402 US	11/5/2019
cynthia moore	los angeles		90019 US	11/5/2019
Candice Ikinum	Kaneohe		96744 US	11/5/2019
Kyle Batye	Marietta		30062 US	11/5/2019
Atilano Gutierrez	Seattle	WA	98188 US	11/5/2019
Brent Johnson	Yardley		19067 US	11/5/2019
Alycen Creigh	Naples	ID	83805 US	11/5/2019
Sandra Luna	Poulsbo	WA	98370 US	11/5/2019
Jeff Bezos			US	11/5/2019
Dennis Williford	Brooklyn		11201 US	11/5/2019
Ryan Zheng	Tampa		9303 US	11/5/2019
Gigi Hill	Moody		75209 US	11/5/2019
Rennie Armistead	Panama City		32405 US	11/5/2019
Scarlet Tarabillo	Naples		34102 US	11/5/2019
Alisha Robinette	Stanely		28164 US	11/5/2019
Barbara Vicens	West Palm Beach		33417 US	11/5/2019
Luke Tefoe	Honolulu		96744 US	11/5/2019
Jen Ryser	Tacoma	WA	98445 US	11/6/2019
Zachary Skaugset	Puyallup	WA	98374 US	11/7/2019
Steffani Reyes	Puyallup	WA	98375 US	11/8/2019
Matthew Padilla	Puyallup	WA	98374 US	11/13/2019
Rick James	Puyallup	WA	98374 US	11/14/2019
Hannah Lange	Mount Horeb	WI	53572 US	11/20/2019
Justin Hockaday	Puyallup	WA	98373 US	11/28/2019
Chrystal Tepley-luce	Seattle	WA	98188 US	12/13/2019
Loren Gates	Graham	WA	98338 US	1/4/2020
daren P	Puyallup	WA	98374 US	1/11/2020
Shayla Anderson	Puyallup	WA	98372 US	3/16/2020
Cindy Domingo	Seattle	WA	98117 US	6/19/2020
austin ward	Albany	OR	97321 US	8/28/2020
Marla Elam	Puyallup	WA	98372 US	12/4/2020

Vinh Mai	West Chester	OH	45069 US	12/4/2020
Judith Romero	Anaheim	CA	92802 US	12/4/2020
Mike Thomson	Puyallup	WA	98372 US	12/4/2020
Karin Crane	Puyallup	WA	98372 US	12/4/2020
Pauline Thain	Puyallup		98371 US	12/4/2020
Randy Avalos	Menlo Park	CA	94025 US	12/4/2020
Heather Taylor	Puyallup	WA	98374 US	12/4/2020
Richard Houser	Puyallup	WA	98374 US	12/4/2020
Charlene Neugebauer	San Francisco	WA	98374 US	12/4/2020
Katie V	Puyallup	WA	98372 US	12/4/2020
Joshua Crossen	Tacoma	WA	98445 US	12/4/2020
Deanna Charette	Brentwood	CA	94513 US	12/4/2020
Roger Morrow	Puyallup	WA	98372 US	12/4/2020
Myrna Anderson	Puyallup	WA	98374 US	12/4/2020
Jennifer Lewis	Puyallup	WA	98371 US	12/4/2020
amber peninger	Puyallup	WA	98372 US	12/4/2020
Dani Emery	Federal Way	WA	98003 US	12/4/2020
Monica Mckinley	Bonney Lake	WA	98391 US	12/4/2020
Rebecca Wright	Puyallup	WA	98375 US	12/4/2020
Kendra Connor	Puyallup	WA	98371 US	12/4/2020
Crystal Lawson	Graham	WA	98373 US	12/4/2020
Joyce Bowers	Puyallup	WA	98372 US	12/4/2020
James Mckinley	Bonney Lake	WA	98391 US	12/4/2020
Sherri Joe	Puyallup	WA	98374 US	12/4/2020
Stefanie Zulauf Wert	Puyallup	WA	98374 US	12/4/2020
Kaela Belen	Buckley	WA	98321 US	12/4/2020
Ashlynn Ota	Boise	ID	83709 US	12/4/2020
Tonya Burk	Tacoma	WA	98445 US	12/4/2020
Brian Bussard	Puyallup	WA	998 US	12/4/2020
Rachel Corey	Puyallup		98371 US	12/4/2020
Elijah Albro	Puyallup	WA	98446 US	12/4/2020
Lark Powers	Tacoma	WA	98371 US	12/4/2020
Laura Jones	Kent	WA	98372 US	12/4/2020
Jessica Anderson	Puyallup	WA	98375 US	12/4/2020
Nancy Landeis	Puyallup	WA	98374 US	12/4/2020
Brenda Juarez	Puyallup	WA	98374 US	12/4/2020
Kristen Hillman	Seattle	WA	98118 US	12/4/2020
Alec Elias	Maple Valley	WA	98038 US	12/4/2020
Brianna Bussey	Puyallup	WA	98372 US	12/4/2020
Arianna Plascencia	Puyallup	WA	98372 US	12/4/2020
Jennifer Adams	Tacoma	WA	98443 US	12/4/2020
Noah Scott	Puyallup	WA	98373 US	12/4/2020
Judy Lantz	Bonney Lake	WA	98391 US	12/4/2020
Misty Rivas	Katy	TX	77494 US	12/4/2020
Deanna Taylor	Puyallup	WA	98373 US	12/4/2020
Deborah Olin	Puyallup	WA	98372 US	12/4/2020
Kimberly Stachowski	Puyallup	WA	98372 US	12/4/2020

Suzanne Larsen	Sumner	WA	98390 US	12/4/2020
Scott Rhodes	Puyallup	WA	98371 US	12/4/2020
Alyssa Levy	Tacoma	WA	98407 US	12/4/2020
Heidi Brown	Puyallup	WA	98374 US	12/4/2020
Elizabeth Spillers	Puyallup	WA	98371 US	12/4/2020
Kristi Bustamante	Puyallup	WA	98372 US	12/4/2020
Susan Suter	Tacoma	WA	98445 US	12/4/2020
Anne PORTER	Puyallup	WA	98374 US	12/4/2020
Stephanie Swanson	Bonney Lake	WA	98391 US	12/4/2020
Anthony Duncan	Puyallup	WA	98374 US	12/4/2020
Amy Arriza	Puyallup	WA	98372 US	12/4/2020
Shannen Ward	Puyallup	WA	98375 US	12/4/2020
Carolyn Frye	Puyallup	WA	98374 US	12/4/2020
Anthony Cox	Edgewood	WA	98371 US	12/4/2020
Robert Gregory	Puyallup	WA	98372 US	12/4/2020
John Kraynak	Puyallup	WA	98374 US	12/4/2020
Virginia Frazer	Puyallup	WA	98375 US	12/4/2020
jenny Barrager	Puyallup	WA	98374 US	12/4/2020
Sharon Simoneaux	Puyallup	WA	98374 US	12/4/2020
Chantal Jacobsen	Tacoma	WA	98404 US	12/4/2020
Leonard Hunt	Bonney Lake	WA	98391 US	12/4/2020
Greg Skinner	Kent	WA	98374 US	12/4/2020
Tricia Minniti	Puyallup		98373 US	12/4/2020
Matthew Freeman	Puyallup	WA	98031 US	12/4/2020
Rachelle Reichenbach	Puyallup	WA	98372 US	12/4/2020
MICHAEL J WARDELL	Puyallup	WA	98374 US	12/4/2020
Jackie Koonce	Puyallup	WA	98372 US	12/4/2020
Christine Morrow	Puyallup, WA	WA	98372 US	12/4/2020
Matt Becker	Puyallup	WA	98373 US	12/4/2020
Connor Hansen	Puyallup	WA	98373 US	12/4/2020
Tanya Frady	Tacoma	WA	98443 US	12/4/2020
Jennifer Schaapveld	Puyallup	WA	98373 US	12/4/2020
Asika Rosebrough	Puyallup	WA	98374 US	12/4/2020
Lolita Jacobsen	Puyallup	WA	98374 US	12/4/2020
Terry Clifton-Troyer	Puyallup	WA	98373 US	12/4/2020
David Vacknitz	Puyallup	WA	98374 US	12/4/2020
Keven Crowder	Sumner	WA	98390 US	12/4/2020
Darice Hermann	Puyallup	WA	98374 US	12/4/2020
Jonathan Christensen	Puyallup	WA	98392 US	12/5/2020
Jennifer Troccoli	Puyallup	WA	98372 US	12/5/2020
Linda Bulzomi	Bonney Lake	WA	98391 US	12/5/2020
Misty Fox	Centralia	WA	98531 US	12/5/2020
Michelle lo	Oakland	CA	94605 US	12/5/2020
Sheri Meyers	Puyallup	WA	98371 US	12/5/2020
RANDY HAAG	Puyallup	WA	98374 US	12/5/2020
Sue Ervin	Puyallup	WA	98374 US	12/5/2020
Jenny Rothlisberger	Piyallup	WA	98374 US	12/5/2020

Jon Snyder	Puyallup	WA	98371 US	12/5/2020
Kum Ho Chi	Bonney Lake	WA	98391 US	12/5/2020
veronica george	sumner	WA	98390 US	12/5/2020
James Phelps	St. Louis	MO	63130 US	12/5/2020
Sarah Holley	Tacoma	WA	98445 US	12/5/2020
Christine Gustafson	Puyallup	WA	98371 US	12/5/2020
Lorena Christensen	Puyallup	WA	98372 US	12/5/2020
Karen Getchell	Seattle	WA	98146 US	12/5/2020
Shelly Trobaugh	Puyallup	WA	98372 US	12/5/2020
Holly Christensen	Puyallup	WA	98371 US	12/5/2020
Amanda Punsalan	Puyallup	WA	98374 US	12/5/2020
Colleen Littlejohn	Sumner	WA	98390 US	12/5/2020
Kim Guilavogui	Tacoma	WA	98445 US	12/5/2020
Aaron Hare	Puyallup	WA	98371 US	12/5/2020
Carol Schneider	Port Angeles	WA	98362 US	12/5/2020
Charlene Funderburg	Puyallup	WA	98374 US	12/5/2020
Maggie Maki	Tacoma	WA	98371 US	12/5/2020
Richard Fellows	Edgewood	WA	98372 US	12/5/2020
Karina Fjellstad-Pangis	Puyallup	WA	98372 US	12/5/2020
Melinda Smith	Puyallup	WA	98374 US	12/5/2020
Amanda Giesy	Puyallup	WA	98372 US	12/5/2020
Rhondi Smith	Sequim	WA	98382 US	12/5/2020
Mary Seese	Bonney Lake	WA	98391 US	12/5/2020
Ryan Maki	Tacoma	WA	98406 US	12/5/2020
Glen Smith	Sequime	WA	98382 US	12/5/2020
Parker Taylor	Puyallup	WA	98373 US	12/5/2020
Beau Jess	Puyallup	WA	98371 US	12/5/2020
Marilyn Hamel	Puyallup	WA	98371 US	12/5/2020
Joe Hewett	Ringgold	VA	24586 US	12/5/2020
Hannah Huber	Bonney Lake	WA	98391 US	12/5/2020
Kim Anh Tran-Dinh	Puyallup	WA	98372 US	12/5/2020
Pat McMackin	Puyallup	WA	98372 US	12/5/2020
James Chalk	Sacramento	CA	95841 US	12/5/2020
Andrea McMackin	PUYALLUP	WA	98372 US	12/5/2020
Sarah Riley	Tacoma	WA	98443 US	12/5/2020
Charles Knowlton	Puyallup	WA	98374 US	12/5/2020
Don Ellis	Puyallup	WA	98374 US	12/5/2020
Shannon Brown Holland	Tacoma	WA	98409 US	12/5/2020
Christina McClellan	Puyallup	WA	98371 US	12/5/2020
Jerri Lien	Sumner	WA	98390 US	12/5/2020
Brenda Sheridan	Renton	WA	98057 US	12/5/2020
Cyndi Riley	Tacoma	WA	98422 US	12/5/2020
sharon fergusson	Bonney Lake	WA	98391 US	12/5/2020
Kevin Brown	Puyallup	WA	98374 US	12/5/2020
Joshua Hess	Tacoma	WA	98466 US	12/5/2020
Michelle Heatherly	Puyallup	WA	98375 US	12/5/2020
Cindee Rose	Seattle	WA	98375 US	12/5/2020

John Knight	Tacoma	WA	98445 US	12/5/2020
Heather Davies	Puyallup	WA	98373 US	12/5/2020
Kirsten Jones	Puyallup	WA	98374 US	12/5/2020
Jason Klug	Puyallup	WA	98372 US	12/5/2020
Lynne Klug	Puyallup	WA	98372 US	12/5/2020
Beverly Villwock	Seattle	WA	98121 US	12/5/2020
Paula Garcia	Leander	TX	78641 US	12/5/2020
Yvonne Fraser	Bonney Lake	WA	98392 US	12/5/2020
Richard Potter	Puyallup	WA	98372 US	12/5/2020
Alfred Stanford	Bonney Lake	WA	98391 US	12/5/2020
Marjorie Angeline	Puyallup	WA	98374 US	12/5/2020
Dorothy Wilgus	Puyallup	WA	98372 US	12/5/2020
Janet Willid	Puyallup	WA	98371 US	12/5/2020
Paul Blackburn	Elizabethtown	KY	42701 US	12/5/2020
Anna Summerhill	Puyallup	WA	98374 US	12/5/2020
Sara Lawrence	Puyallup	WA	98374 US	12/5/2020
Dean Fettig	Puyallup	WA	98372 US	12/5/2020
Richard Ryan	Kent	WA	98031 US	12/5/2020
Janet Brown	Redmond	WA	98052 US	12/5/2020
Clarice Bauer	Bonney Lake	WA	98391 US	12/5/2020
Ken Krumpos	Bonney Lake	WA	98391 US	12/5/2020
Marisa Ross	Puyallup	WA	98373 US	12/5/2020
Jessie Weber	Bonney Lake	WA	98391 US	12/5/2020
Richard Peterson	Puyallup	WA	98375 US	12/5/2020
Darren Nerland	Seattle	WA	98198 US	12/5/2020
Chris Stockslager	Puyallup	WA	98371 US	12/5/2020
Lori Kramer	Puyallup	WA	98374 US	12/5/2020
Lynn Kaelin	Hayward	CA	94544 US	12/5/2020
Cherie Hamilton	Seattle	WA	98122 US	12/5/2020
Kimmy Tweten	Tacoma	WA	98407 US	12/5/2020
Marcie Mahoney	Puyallup	WA	98374 US	12/5/2020
Heather Gurule	Spanaway	WA	98387 US	12/5/2020
Luda Kamerzan	Tacoma		98405 US	12/5/2020
Christina Hurych	Spanaway	WA	98387 US	12/5/2020
Renee Preston	Puyallup	WA	98374 US	12/5/2020
Megan Pollock	Buckley	WA	98321 US	12/5/2020
Jenny Laycock	Bonney Lake	WA	98372 US	12/5/2020
Ashley Mccartney	Kent	WA	98030 US	12/5/2020
mark bridges	Puyallup	WA	98374 US	12/5/2020
June Coloff	Seattle	WA	98115 US	12/5/2020
Bryan Grozav	Puyallup	WA	98374 US	12/5/2020
Heidi Waltos	Puyallup	WA	98374 US	12/5/2020
Tammy Willyard	Puyallup	WA	98374 US	12/5/2020
Natalie Cardenas	Auburn	WA	98002 US	12/5/2020
Mira Bergsma	Seattle	WA	98146 US	12/5/2020
Shawna Guinn	Puyallup	WA	98371 US	12/5/2020
Rondane Hollar	Bronx	NY	10467 US	12/5/2020

Nancy Martin	Bonney Lake	WA	98391 US	12/5/2020
Sarah Arnett	Puyallup	WA	98375 US	12/5/2020
Tamela Murphy	Gardena	CA	90249 US	12/5/2020
Lynda Fenton	Seattle	WA	98115 US	12/5/2020
Jan Van Court	Auburn	WA	98001 US	12/5/2020
Jamison Stockslager	Puyallup	WA	98471 US	12/5/2020
Susan Harris	PUYALLUP	WA	98371 US	12/5/2020
Jerry Hornsby	Spanaway	WA	98387 US	12/5/2020
Suellen Chilton	Puyallup	WA	98375 US	12/5/2020
Susie Cassens	Fort Pierce	FL	34954 US	12/5/2020
Israel Aguilar	Butte	MT	59701 US	12/5/2020
andrea melanson	puyallup	WA	98374 US	12/5/2020
Denise Ficca	Puyallup	WA	98371 US	12/5/2020
Jonathan Vaughn	Puyallup	WA	98371 US	12/5/2020
Tony Ho	Bonney Lake	WA	98391 US	12/5/2020
Karren Franckowiak	Puyallup	WA	98371 US	12/5/2020
Aura Jusula	Puyallup	WA	98371 US	12/5/2020
Jennifer Skunza	Spanaway	WA	98387 US	12/5/2020
Nicole Munson	Sumner	WA	98390 US	12/5/2020
Claudine LaMere	Bonney Lake	WA	98391 US	12/5/2020
Rod Masterjohn	Marysville	WA	98271 US	12/5/2020
Carrie Levenhagen	Spanaway	WA	98387 US	12/5/2020
Sheryl Orse	Puyallup	WA	98374 US	12/5/2020
Loriel Miller	Olympia	WA	98501 US	12/5/2020
Sarah Holstrom	Puyallup	WA	98372 US	12/5/2020
Claire Witters	Bonney Lake	WA	98391 US	12/5/2020
Bailey Rose	Graham	WA	98338 US	12/5/2020
Heidi Torres	Puyallup	WA	98373 US	12/5/2020
Samantha Foyil	Edgewood	WA	98372 US	12/5/2020
doria wosk	miami	FL	33116-3356 US	12/5/2020
Rachel Roberts	Bonney Lake	WA	98391 US	12/5/2020
Leslie Annis	Puyallup	WA	98373 US	12/5/2020
Elizabeth Knox	Puyallup	WA	98374 US	12/5/2020
Pamela Topping	Puyallup	WA	98374 US	12/5/2020
Allan Huestis	Puyallup	WA	98375 US	12/5/2020
Shannon Berendt	Seattle	WA	98102 US	12/5/2020
Kimberly Baggione	Puyallup	WA	98372 US	12/5/2020
Rae Klepper	Puyallup	WA	98374 US	12/5/2020
Aly Morgan	Clarksville	TN	37040 US	12/5/2020
Michelle Rood	Tacoma	WA	98371 US	12/5/2020
kimberley Kessler	puyallup	WA	98372 US	12/5/2020
Kayla Kavanaugh	Seattle	WA	98122 US	12/5/2020
B. Kay Wong	Puyallup	WA	98371 US	12/5/2020
Brian Suko	Puyallup	WA	98371 US	12/5/2020
Mary Morgan	Spanaway	WA	98387 US	12/5/2020
Sherri Brown	Ellensburg	WA	98926 US	12/5/2020
Kristi Wright	Puyallup	WA	98374 US	12/5/2020

Tarah Walls	Puyallup	WA	98374 US	12/5/2020
Debra Belford	Bonney Lake	WA	98391 US	12/5/2020
Lori Evans	Puyallup	WA	98372 US	12/5/2020
Kelly Curley	Puyallup	WA	98371 US	12/5/2020
Donna Tomlinson	Puyallup	WA	98374 US	12/5/2020
Christine Donnelly			US	12/5/2020
Cheri Dorsey	Puyallup	WA	98371 US	12/5/2020
Tal Altman	Puyallup	WA	98372 US	12/5/2020
Kerry Latin	Puyallup	WA	98373 US	12/5/2020
Toni Caputo	Puyallup	WA	98374 US	12/5/2020
Roger Kulp	Albuquerque	NM	87108 US	12/5/2020
William Bowers	puyallup	WA	98371 US	12/5/2020
Micheal Armstrong	Bonney Lake	WA	98391 US	12/5/2020
Deborah Miller	Puyallup	WA	98371 US	12/5/2020
Shannyn Mujkanovic	Puyallup	WA	98375 US	12/5/2020
Robert Aye	Bonney Lake	WA	98391 US	12/5/2020
Amber Young	Puyallup	WA	98374 US	12/5/2020
Andrea Barta	Seattle	WA	98118 US	12/5/2020
Everett Hale	Puyallup	WA	98446 US	12/5/2020
Mary Kleiber	Puyallup	WA	98372 US	12/5/2020
Tami Haag	Puyallup	WA	98374 US	12/5/2020
Kim Shaw	Tacoma	WA	98443 US	12/5/2020
Karl Hess	Puyallup	WA	98372 US	12/5/2020
Jon Thibodeau	Puyallup	WA	98372 US	12/5/2020
Darlene Belford	Seattle	WA	98122 US	12/5/2020
Brenda Carlock	Puyallup	WA	98373 US	12/5/2020
wendy geldien	Puyallup	WA	98372 US	12/5/2020
GQ Shaw	Spanaway	WA	98387 US	12/5/2020
Jean Bell	Orting	WA	98360 US	12/5/2020
Nolan Scott	Puyallup	WA	98374 US	12/5/2020
Beky Harris	Puyallup	WA	98373 US	12/5/2020
Tamara Enyart	Auburn	WA	98092 US	12/5/2020
Mary Hall	Puyallup	WA	98374 US	12/5/2020
Katie Boone	Puyallup	WA	98374 US	12/5/2020
Cara Swenson	Puyallup	WA	98375 US	12/5/2020
Lana Tuccio	Puyallup	WA	98374 US	12/5/2020
Karen Hegarty			US	12/5/2020
John Dinh	Puyallup	WA	98374 US	12/5/2020
M Eleanor Block	Tacoma	WA	98 US	12/5/2020
Sandy Benson	Tacoma	WA	98422 US	12/5/2020
Steve Androsko	Tacoma	WA	98446 US	12/5/2020
allison mays	Pittsburgh	PA	15226 US	12/5/2020
Kathy Maillard	Puyallup	WA	98372 US	12/5/2020
JC Arndt	Puyallup	WA	98374 US	12/5/2020
Sylvia Sjosten	Sumner	WA	98390 US	12/5/2020
BRETT BOWEN	Tacoma	WA	98444 US	12/5/2020
Stacy Androsko	Tacoma	WA	98446 US	12/5/2020

Amanda Bowen	Puyallup	WA	98374 US	12/5/2020
Judie Burton	Puyallup	WA	98373 US	12/5/2020
April Hart	Puyallup	WA	98374 US	12/5/2020
Jon Hart	Puyallup	WA	98374 US	12/5/2020
Jennifer Redfern	Puyallup	WA	98374-4165 US	12/5/2020
Vanessa McElroy	Puyallup	WA	98374 US	12/5/2020
Lauren Rosales	Flagstaff	AZ	86005 US	12/5/2020
Kathleen Wilson	Tacoma	WA	98407 US	12/5/2020
Ray Rossman	Puyallup	WA	98374 US	12/5/2020
Heidi Eshpeyer	Puyallup	WA	98371 US	12/5/2020
Robert Parrish	Puyallup	WA	98372 US	12/5/2020
Becky Vacanti	Auburn	WA	98001 US	12/5/2020
Heidi Zetterberg	Tacoma	WA	98466 US	12/5/2020
Angela Barbera	Puyallup	WA	98374 US	12/5/2020
Anne Barber	Puyallup	WA	98371 US	12/5/2020
Lynn Garner	Olympia	WA	98502 US	12/5/2020
Dan Floyd	Puyallup	WA	98374 US	12/5/2020
Jamie Eberly	Kent	WA	98042 US	12/5/2020
Victoria Shattuck	Sumner	WA	98390 US	12/5/2020
Lynn Chung	Puyallup	WA	98374 US	12/5/2020
CiCi Ross	Portland	OR	97201 US	12/6/2020
Marian Paulsen	Puyallup	WA	98374 US	12/6/2020
Jim Christensen	Puyallup	WA	98374 US	12/6/2020
Louisa Paulsen	Puyallup	WA	98374 US	12/6/2020
Maria Webster	Seattle	WA	98168 US	12/6/2020
Judith Hofeditz	Woodinville	WA	98072 US	12/6/2020
Kim Hanson	Sumner	WA	98390 US	12/6/2020
hannah Hensley	Stilwell	OK	74960 US	12/6/2020
Nancy Marrapodi	Puyallup	WA	98371 US	12/6/2020
Katie Stemp	Spanaway	WA	98371 US	12/6/2020
Kaden Henderson	Bonney Lake	WA	98391 US	12/6/2020
Maria pascualy	Tacoma	WA	98403 US	12/6/2020
Robert DaSilva	Puyallup	WA	98374-9685 US	12/6/2020
Brian Dempsey	PUYALLUP	WA	98374 US	12/6/2020
Penny Thompson	Orting	WA	98360 US	12/6/2020
Tera Rambo	Puyallup	WA	98374 US	12/6/2020
Ben Cichon	Bonney Lake	WA	98391 US	12/6/2020
Dixie Baeth	Federal Way	WA	98023 US	12/6/2020
Sheila McCann	Puyallup	WA	98373 US	12/6/2020
Natalie Graham	Puyallup	WA	98374 US	12/6/2020
Andrea Botelho	Puyallup	WA	98392 US	12/6/2020
Heidi Beckley	Puyallup	WA	98372 US	12/6/2020
Kristi Fuson	Puyallup	WA	98373 US	12/6/2020
Charlotte Robinson	Sumner	WA	98390 US	12/6/2020
Giovanna Foss	Lakewood	WA	98499 US	12/6/2020
betty winholtz	morro bay	AL	93442 US	12/6/2020
Alyson Trumbull	Gig harbor	WA	98333 US	12/6/2020

Eric Gregg	Puyallup	WA	98374 US	12/6/2020
Kinsey Flauding	Puyallup	WA	98371 US	12/6/2020
Shana Delight	Puyallup	WA	98375 US	12/6/2020
Michelle Alcorn	Puyallup	WA	98375 US	12/6/2020
Paul Fry	Puyallup	WA	98374 US	12/6/2020
paul serrano	Fremont		68025 US	12/6/2020
Erin Jackowski	Puyallup	WA	98374 US	12/6/2020
Heather Lake	Puyallup	WA	92336 US	12/6/2020
Shelly Rubida	Puyallup	WA	98372 US	12/6/2020
Sherry Rhoden	Madison	SD	57042 US	12/6/2020
Kari Westman	PUYALLUP	WA	98373 US	12/6/2020
Deborah Calhoun	Puyallup	WA	98372 US	12/6/2020
Natalia Covan	Livingston	NJ	7039 US	12/6/2020
Jane Tsitsey	Tacoma	WA	98409 US	12/6/2020
Krystal Griffin	Puyallup	WA	98375 US	12/6/2020
Mathew Schatz	Puyallup	WA	98372 US	12/6/2020
Art Pierce	Puyallup	WA	98374 US	12/6/2020
Darren Ross	Puyallup	WA	98374 US	12/6/2020
Katelyn Stringham	Spanaway	WA	98387 US	12/6/2020
MARY ERSKINE	Seattle	WA	98108 US	12/6/2020
Teresa Sweesy	Lake Tapps	WA	98391 US	12/6/2020
Roland Williams	Puyallup	WA	98374 US	12/6/2020
Gail Williams	Myrtle Beach	SC	29579 US	12/6/2020
Cheryl Holien	Puyallup	WA	98371 US	12/6/2020
Carrie Burgess	Puyallup	WA	98374 US	12/6/2020
Kelly Ford	Puyallup	WA	30329 US	12/6/2020
Steve Gann	Puyallup	WA	98373 US	12/6/2020
Susan Parker	Puyallup	WA	98375 US	12/6/2020
Anne Roff	Puyallup	WA	98371 US	12/7/2020
Rose Cowan	Puyallup	WA	98374 US	12/7/2020
Tari Chapman	Puyallup	WA	98375 US	12/7/2020
Bobby Chapman	Puyallup	WA	98375 US	12/7/2020
Sheryl Rogel	Puyallup	WA	98371 US	12/7/2020
LEANNE LEISTIKO	Bonney Lake	WA	98391 US	12/7/2020
Angela Valdez	Puyallup	WA	98374 US	12/7/2020
Margie Van Volkenburg	Puyallup	WA	98372 US	12/7/2020
Margaret McGee	Bonney Lake	WA	98391 US	12/7/2020
Ashley Scott	Puyallup	WA	98371 US	12/7/2020
Brandi Pearson	Puyallup	WA	98374 US	12/7/2020
Natasha McNiven-Dalton	Puyallup	WA	98374 US	12/7/2020
Jane Dee	Puyallup	WA	98375 US	12/7/2020
Tamara Carson	Puyallup	WA	98373 US	12/7/2020
Jonathan Bentley	Puyallup	WA	98371 US	12/7/2020
Maria Bentley	Seattle	WA	98146 US	12/7/2020
Amber Belanger	Puyallup	WA	98372 US	12/7/2020
Sharon Austin	Puyallup	WA	98374 US	12/7/2020
Amanda Wittig	Gilbert	AZ	85234 US	12/7/2020

Catherine Stowers	Puyallup	WA	98374 US	12/7/2020
Brian Nadeau			US	12/7/2020
Jason Dinsmore	Bonney lake	WA	98391 US	12/7/2020
Thomas Bennett	Graham	WA	98338 US	12/7/2020
Stacy Hori	Bonney Lake	WA	98391 US	12/7/2020
Mary Rowe	Puyallup	WA	98374 US	12/7/2020
Stephanie Roberts	Tacoma	WA	98406 US	12/7/2020
Kim Greger	Puyallup	WA	98373 US	12/7/2020
Carrie King	Tacoma	WA	98406 US	12/7/2020
Tabitha Peters	Tacoma	WA	98445 US	12/7/2020
Nancy Purvis	Puyallup	WA	98375 US	12/7/2020
Peter Schiavone	Puyallup	WA	98374 US	12/7/2020
Melissa Diehl	Liberty	MO	64068 US	12/7/2020
Carole Kiesig	Orting	WA	98360 US	12/7/2020
Debbie Phillips	Tacoma	WA	98405 US	12/7/2020
Christina Lindsay	Puyallup	WA	98373 US	12/7/2020
Stacey Beal	Graham	WA	98338 US	12/7/2020
brandon raymond	Puyallup	WA	98372 US	12/7/2020
Dustin Stemp	Puyallup	WA	98371 US	12/7/2020
Barbara Snyder	Bonney Lake	WA	98391 US	12/7/2020
Alex Griffin	Puyallup	WA	98372 US	12/7/2020
Barbara Vittetoe	Puyallup	WA	98374 US	12/7/2020
Philip Corrigan	Puyallup	WA	98372 US	12/7/2020
Audra Swinney	Puyallup	WA	98371 US	12/8/2020
Brandie Fisher	Puyallup	WA	98373 US	12/8/2020
tenzin tsephel	Astoria		11106 US	12/8/2020
Joy Thomas	Seattle	WA	98105 US	12/8/2020
Cheryl Erbe	Bonney Lake	WA	98391 US	12/8/2020
Theresa Chandler	Seattle	WA	98115 US	12/8/2020
Lauren todd	puyallup	WA	98371 US	12/8/2020
Jessica Mcanally	Puyallup	WA	98371 US	12/8/2020
Lisa Sells	Tacoma	WA	98445 US	12/8/2020
Salena Taylor	Puyallup	WA	98375 US	12/8/2020
Patricia Lundell	Putalup	WA	98372 US	12/8/2020
Brett Kapels	Puyallup	WA	98373 US	12/8/2020
Trish Peterson	Puyallup	WA	98374 US	12/8/2020
Brandy Stockwell	Enumclaw	WA	98022 US	12/8/2020
Leanne Kopetski	Puyallup	WA	98371 US	12/8/2020
John Sibley	Bonney Lake	WA	98391 US	12/8/2020
Charla Lang	Puyallup	WA	98372 US	12/8/2020
Douglas Taylor	Bonney Lake	WA	98391 US	12/8/2020
Martha Krotz	Oak Harbor	WA	98277 US	12/8/2020
R V	Enumclaw	WA	98022 US	12/8/2020
Barbara Ellen Williams	Tacoma	WA	98444 US	12/8/2020
Donald serdahl	puyallup	WA	98372 US	12/9/2020
Jaime Mercer	Fife	WA	98424 US	12/9/2020
Quiana Neilson	Tacoma	WA	98466 US	12/9/2020

Paul Conradt	Puyallup	WA	98372 US	12/9/2020
Catherine Young	Puyallup	WA	98391 US	12/9/2020
Larina Fuller	Puyallup	WA	98372 US	12/9/2020
Lucas Erickson	Puyallup	WA	98374 US	12/9/2020
YVONNE CHRISTISON	Stevens Point	WI	54481 US	12/9/2020
Kyrisa Vance	Maple valley	WA	98038 US	12/9/2020
Kimberly Lundell	Puyallup	WA	98372 US	12/9/2020
Dawn Puttler	Puyallup	WA	98374 US	12/10/2020
Eric Puttler	Puyallup	WA	98374 US	12/10/2020
Pam Blauman	Seattle	WA	98119 US	12/10/2020
James Mccray	Opa-locka		33055 US	12/10/2020
Diane Geehan	Phoenix	AZ	85032 US	12/10/2020
Lhen Anthonette Durano			Philippines	12/11/2020
Daniel Delgadillo	Fontana		92336 US	12/11/2020
Ksenia Kalinina	Manvel		77578 US	12/11/2020
Ayva Spears	Renton		98059 US	12/11/2020
Hamza Mahamud	Jamaica Plain		2130 US	12/11/2020
Krisa Kharel	Columbus		43230 US	12/11/2020
Jameson Thornburg	Redmond		18622 US	12/11/2020
Sehr Malik	Fremont		94538 US	12/11/2020
eileen lam			US	12/11/2020
Heather Swain	Homosassa		34448 US	12/11/2020
Kelly Davis	Jacksonville		32221 US	12/11/2020
Melanie Morales	Hialeah		33015 US	12/11/2020
Bailey Gutenberger	Rocklin		95677 US	12/11/2020
Jayah Stogden	Jacksonville		32257 US	12/11/2020
Sadie Hernandez	Newark		7104 US	12/11/2020
Emanuel Alonso	Playa Del Rey		90293 US	12/11/2020
Rylie McKinney	Pomona		91766 US	12/11/2020
Shit Sharty	Newark		7112 US	12/11/2020
Astrid Ceballos	Philadelphia		19124 US	12/11/2020
Lukas Fick	Salem		24153 US	12/11/2020
viviana contreras	Stockton		95204 US	12/11/2020
Maria Ronaldo	Beverly Hills		90210 US	12/11/2020
Gabe Roberts	Fort Worth		76123 US	12/11/2020
Taylor Chaika	Saint Cloud		56399 US	12/11/2020
Sara Padilla	Shorewood		60404 US	12/11/2020
Emma Arsanto	Seattle		98125 US	12/11/2020
Aliese Mendoza	Tucson		85705 US	12/11/2020
Aisilin Sanchez	San Marcos		92078 US	12/11/2020
Shirley Woods	Saint Louis		63136 US	12/11/2020
Kierra Satalino	Albany		12203 US	12/11/2020
ali b	New York City		90067 US	12/11/2020
olivia wilcox	Cranston		2921 US	12/11/2020
Alexis Bellamy	New York		10026 US	12/11/2020
Audrey Popp	Kirkland		98033 US	12/11/2020
Estela Solorio	Waukegan		60087 US	12/11/2020

Chicken Nugget	Medway		2053 US	12/11/2020
Leyia Williams	Richmond		23229 US	12/11/2020
Alfonse Brooke			US	12/11/2020
Erika Navarro	Berkeley		94704 US	12/11/2020
Kayla Johnson	Elkton		21921 US	12/11/2020
Oluwadamilola Daniel	Parkville		21234 US	12/11/2020
Grace O'Connell	Tulsa		74107 US	12/11/2020
Taylor Nixon Poole	Graham		27253 US	12/11/2020
Kenna Teague	Maryland		2009 US	12/11/2020
Jenna Phelps	Puyallup	WA	98373 US	12/12/2020
John Wolinski	Abingdon		21009 US	12/12/2020
Pat Clement	Edgewood	WA	98372 US	12/12/2020
Madelyn Kelly	Boston		2186 US	12/13/2020
Miles Kelly	Washington		20307 US	12/13/2020
Lauren Henley McKinnon	Puyallup	WA	98375 US	12/13/2020
Dan De Yo	Yorba Linda		92886 US	12/13/2020
Corey McKinnon	Puyallup	WA	98374 US	12/13/2020
Brenda Ransom	Puyallup	WA	98372 US	12/13/2020
David Heagle	Tacoma	WA	98445 US	12/13/2020
Lee Cizek	Seattle	WA	98178 US	12/13/2020
Madeline Golden	Puyallup	WA	98371 US	12/13/2020
Stephanie Parido	Puyallup	WA	98375 US	12/14/2020
Julie Harding	Puyallup	WA	98374 US	12/14/2020
Sandra Ransom	Puyallup	WA	98371 US	12/14/2020
Bernice Migita	Puyallup	WA	98003 US	12/14/2020
Marcel Purdy	Tacoma	WA	98422 US	12/14/2020
Shane Bartow	Puyallup	WA	98374 US	12/14/2020
Judith Rhyner	Graham	WA	98338 US	12/14/2020
Gerald Stowe	Seattle	WA	98146 US	12/14/2020
Michael Tungsvik	Puyallup	WA	98374 US	12/14/2020
Autumn Skeen	Federal Way	WA	98023 US	12/14/2020
Jordan Goakey	Spanaway	WA	98387 US	12/14/2020
Allison Lovell	Bellingham	WA	98225 US	12/14/2020
Marguerite Foley	Philadelphia		19149 US	12/14/2020
katherine dander	boston	MA	2114 US	12/14/2020
Brian McDonald	Puyallup	WA	98372 US	12/14/2020
Francis Juarez castillo	Bonney Lake	WA	98391 US	12/14/2020
William Hickey	Danvers	MA	1923 US	12/14/2020
Emma M	Gig Harbor	WA	98332 US	12/14/2020
Shanta HOrlander	Clearwater	FL	98109 US	12/15/2020
John Singcuenco	Puyallup	WA	98373 US	12/15/2020
ariane Singcuenco	Puyallup	WA	98373 US	12/15/2020
Emily Webb	Fishkill		12524 US	12/15/2020
Hina Afzal	Minneapolis		55416 US	12/15/2020
Bella Smith	Hixson		37343 US	12/15/2020
Tiffani Dixon	Arlington		22204 US	12/15/2020
Charlotte Shi	San Jose		95130 US	12/15/2020

Ramon Rodriguez	San Bernardino	92410 US	12/15/2020
Myra Page	Chicopee	1013 US	12/15/2020
Yemily Pedraza	Pleasant Grove	84062 US	12/15/2020
Zoey Hernandez	Los Angeles	90032 US	12/15/2020
Maxine Van	New York	10028 US	12/15/2020
Vivian Combs	Howell	48843 US	12/15/2020
Carol Goerke	Tempe	85287 US	12/15/2020
Miranda Albuquerque	Acton	1720 US	12/15/2020
Christopher Gregory	Pleasant Hill	94523 US	12/15/2020
Natalia Perez	Kirkland	98035 US	12/15/2020
Keyshia Stanley		US	12/15/2020
sophie lanson	Rocklin	95677 US	12/15/2020
victor martinez	Niles	60714 US	12/15/2020
Sharon Sears	Cambridge	2139 US	12/15/2020
Tara Linderborg	Mill Valley	US	12/15/2020
Jackie Deckard	Snow Shoe	16874 US	12/15/2020
Egran Jama	Savage	55378 US	12/15/2020
Cynthia Guima	Dallas	75211 US	12/15/2020
Kendall Mason	Queens Village	11429 US	12/15/2020
Kaelia Oconnor	Colleyville	76034 US	12/15/2020
Damian Yahoo	Boston	2130 US	12/15/2020
Clara Whitmarsh	Gig Harbor	98335 US	12/15/2020
jocelyn mejia	Dallas	75229 US	12/15/2020
Polly Sypowicz	Potomac	20854 US	12/15/2020
Xochitl Alvarado		US	12/15/2020
Josh Minor	Boise	83616 US	12/15/2020
Kristen Brickner	Cleveland	44130 US	12/15/2020
Zack Taft	Pine City	14871 US	12/15/2020
Charles McCue	Miami	33157 US	12/15/2020
Eileen Yan	Denver	80210 US	12/15/2020
Kurissa Traxler	Grand Blanc	48439 US	12/15/2020
Anita Mei	Boston	2127 US	12/15/2020
Lina Morales	Casa Grande	85122 US	12/15/2020
habiba elshafey	morganville	7751 US	12/15/2020
Morgan Daniels	Santa Maria	93455 US	12/15/2020
Morgan Mask	Warwick	10990 US	12/15/2020
Thelma Medel	New Castle	19720 US	12/15/2020
alexa juarez	Bakersfield	93311 US	12/15/2020
Lucia Sanchez	Beltsville	20705 US	12/15/2020
Shaniya King	Fayetteville	28314 US	12/15/2020
mel pittman	Pontiac	48341 US	12/15/2020
Dante Mccurty	Pomona	91766 US	12/15/2020
Zach Holland	Arlington	76015 US	12/15/2020
Liliana Meek	Nogalas	85648 US	12/15/2020
ivy sowers	Seal Beach	90740 US	12/15/2020
Jennifer Krondahl	Madisonville	37354 US	12/15/2020
hannah christensen	Tucson	85755 US	12/15/2020

Amairani Hernandez	Chicago	60612 US	12/15/2020
Morgan S	Palatine	60067 US	12/15/2020
ashley castro	Dendron	23060 US	12/15/2020
Rorbie Auguste	Philadelphia	19116 US	12/15/2020
Persaia Morales	Corpus Christi	78416 US	12/15/2020
Maguelone Ribo	San Jose	95124 US	12/15/2020
Aimee Cambrano	Santa Rosa	95407 US	12/15/2020
Daija Conley	Grand Rapids	49505 US	12/15/2020
Anmary Bautista	Chino	91710 US	12/15/2020
Marianna Osuna	Riverside	92504 US	12/15/2020
Emilie Christie	Fishers	46038 US	12/15/2020
ben pickels	Wexford	15090 US	12/15/2020
Lupa Rosia	Las Vegas	89107 US	12/15/2020
Sophia Santiago	Fair Oaks	95628 US	12/15/2020
Arika Suits	New Hartford	13413 US	12/15/2020
David Swartz	Edinburg TX	78539 US	12/15/2020
Terrie Williams	Vidor	77662-6311 US	12/15/2020
John Doe	Beverly Hills	90210 US	12/15/2020
Samuel Gamet	Canton	48187 US	12/15/2020
cecily pelayo	Oxnard	93030 US	12/15/2020
Ethan Chen	Queens	11428 US	12/15/2020
bryant lopez	Grifton	28530 US	12/15/2020
Eleni Skouros	Long Island City	11109 US	12/15/2020
Nevaeh Paz	Portland	97236 US	12/15/2020
Tiffany Shines	Los Angeles	90032 US	12/15/2020
Tamia Ware	Chicago	60602 US	12/15/2020
Ny'jada Bumpass	Cleveland	44134 US	12/15/2020
Rylie Dehne	Elgin	60124 US	12/15/2020
Kelly Reynolds	Evansville	47747 US	12/15/2020
Riddhi Pandya	Doylestown	18901 US	12/15/2020
Makayla Gandara	Corvallis	59828 US	12/15/2020
Shelby Wallace	Virginia Beach	23462 US	12/15/2020
Yerania Valdovinos	Spokane	99208 US	12/15/2020
Adio Martinez	Elkridge	21075 US	12/15/2020
Adella Brown	Jackson	95642 US	12/15/2020
Cameryn King	Brockton	2301 US	12/15/2020
Kaci Benson	Huntingdon	16652 US	12/15/2020
Jakub Pipota	Huntington Station	11746 US	12/15/2020
Laura Rodrigues dos Santos		US	12/15/2020
Keira Salinas	Weslaco	78596 US	12/15/2020
Lanne Romero	Garden Grove	92840 US	12/15/2020
Dominique Guerrero	Phoenix	85041 US	12/15/2020
Emely Sanchez	Santa Ana	92701 US	12/15/2020
kimberly wilson	Waco	76708 US	12/15/2020
Payton Bade	Crystal Lake	60014 US	12/15/2020
Sharon Wang	Farmington	14425 US	12/15/2020
Joyce Figueroa	Virginia Beach	23452 US	12/15/2020

Chris Jaison	Monroe Township	8831 US	12/15/2020
olivia smith	Louisville	40222 US	12/15/2020
Big Cheese	Clovis	93611 US	12/15/2020
em esp	Arlington	75241 US	12/15/2020
Kimberly Tyndall	Phenix City	36870 US	12/15/2020
fabiana correa	Spring	77382 US	12/15/2020
Arin Poloski	Moline	61265 US	12/15/2020
Emily Alvarez	Bellflower	90706 US	12/15/2020
Amelia stewart	Oklahoma City	73162 US	12/15/2020
Haleemah Ogunniyi	Trenton	8618 US	12/15/2020
Yaravi Landaverde	Arlington	76011 US	12/15/2020
Zafu Abraha	Fresno	93722 US	12/15/2020
Josef Anderson	Scottsdale	85254 US	12/15/2020
Matilda Smith	Huntsville	35893 US	12/15/2020
Dylan Mooney	Piedmont	73078 US	12/15/2020
leila badri		US	12/15/2020
Angela Rodela	San Antonio	78221 US	12/15/2020
Natalie Ayala	San Bernardino	92410 US	12/15/2020
Brittany Munoz	Denver	80204 US	12/15/2020
Rose Vue	Plover	54467 US	12/15/2020
Simon Liam	Sod	24910 US	12/15/2020
Leila Chicas Perez	Hyattsville	20782 US	12/15/2020
Avery Pratt	Bloomfield	7003 US	12/15/2020
nayeli flores	Norwalk	90650 US	12/15/2020
Mary Cummins	Masontown	15461 US	12/15/2020
arushi devpura	West windsor	8520 US	12/15/2020
Courtney Coirin	Yorktown	23693 US	12/15/2020
adriana rivera	valrico	33594 US	12/15/2020
Aleigha McHale	Monroeville	15146 US	12/15/2020
Emma Doherty	Vero Beach	32967 US	12/15/2020
Starrmonnie Farris	Neptune	7753 US	12/15/2020
Emma Fleming	Raleigh	27609 US	12/15/2020
Shania Morris	Riverhead	11901 US	12/15/2020
Amanda Tomlin	Winston-salem	27103 US	12/15/2020
Jennifer Dean	Roanoke	24014 US	12/15/2020
Yatana Romo	Denver	80014 US	12/15/2020
Madeline Jones	East Lansing	48823 US	12/15/2020
isa mikateto	Des Moines	50322 US	12/15/2020
miles steinberg	Los Angeles	90068 US	12/15/2020
Taylor Gerard	Santa Ana	92704 US	12/15/2020
Shawn Hysell	Lawrenceburg	47025 US	12/15/2020
vivian hshs	Chicago	60641 US	12/15/2020
Sidonia Reynolds	Belleville	62220 US	12/15/2020
Adora Thao	Minneapolis	55443 US	12/15/2020
Mario Hernandez	Los Angeles	90015 US	12/15/2020
Lillee Arnest	Burlingame	94010 US	12/15/2020
Litzy Yascaribay	Queens	11372 US	12/15/2020

Michelle Niz	Frankford	19945 US	12/15/2020
morgan stimmel		US	12/15/2020
Gayvin thompson	Dallas	98370 US	12/15/2020
Jessica reyes	Washington	20011 US	12/15/2020
Sandy Amarebo	Florissant	63031 US	12/15/2020
rebecca siciliano	Miami	33182 US	12/15/2020
seikura banks	Harrisburg	17109 US	12/15/2020
Tes Playz	New Haven	48051 US	12/15/2020
Jessica Rondon	Bronx	10453 US	12/15/2020
Anel Castillo	Santa Paula	93060 US	12/15/2020
Ariana Rodriguez		6482 US	12/15/2020
Britney Ulloa	Jamaica	11433 US	12/15/2020
Logan Johnson	Longview	98632 US	12/15/2020
Christian Scott	Philadelphia	19130 US	12/15/2020
Isabella García	Oviedo	32765 US	12/15/2020
Paige Messite	Pelham	10803 US	12/15/2020
Heather Morgan	Norwich	6360 US	12/15/2020
Daniel Andrew	Atascadero	93422 US	12/15/2020
Jim Roberts	Cape coral	33991 US	12/15/2020
Elisabeth Heinze	Minneapolis	55413 US	12/15/2020
Rose Ross	Moreno Valley	92553 US	12/15/2020
Julia Trollinger	Hendersonville	28739 US	12/15/2020
Alana Britt	Elgin	29045 US	12/15/2020
c T	Clinton	20735 US	12/15/2020
Char Lie	Vincennes	47591 US	12/15/2020
Katrina Johnson	Lincolnshire	60069 US	12/15/2020
anita walker	Franklin	53132 US	12/15/2020
Luna Blackfire		US	12/15/2020
Nylah Lebert	Virginia Beach	23456 US	12/15/2020
Laylani Brown	Chicago	60628 US	12/15/2020
Heather Romero	Leadville	80461 US	12/15/2020
Mia Hernandez	Cary	2818 US	12/15/2020
Nikhil Chandra	Aurora	60502 US	12/15/2020
Trinity Smith	Colorado Springs	80908 US	12/15/2020
Alejandro Zepeda	Los Angeles	90047 US	12/15/2020
Holly honeycutt	Four Oaks	27524 US	12/15/2020
magdalena gordon	El Paso	79904 US	12/15/2020
Jesse C. Chavez	Bakersfield	93313 US	12/15/2020
Linda Luschei	Upland	91784 US	12/15/2020
Andre LaPointe	Fort Worth	76114 US	12/15/2020
alyssa ramos	Saint Paul	55106 US	12/15/2020
Mason Culver	Manchester	3104 US	12/15/2020
Joe mama	New Philadelphia	44663 US	12/15/2020
Daniel Cervantes	Los Angeles	90063 US	12/15/2020
Mari Williams	Tampa	33619 US	12/15/2020
Lexi Anderson	Philadelphia	19136 US	12/15/2020
Kennedy Dickerson	Atlanta	31765 US	12/15/2020

Ana Vicens	Indianapolis	46259 US	12/15/2020
Abby Garcia	Waukegan	60085 US	12/15/2020
Marisela Arzate	Orange Cove	93646 US	12/15/2020
Brooke DoVale	Chelsea	2150 US	12/15/2020
Amelia Friend	Springfield	62712 US	12/15/2020
Odalys Rivera	Poway	92064 US	12/15/2020
aracely Amador	Houston	77042 US	12/15/2020
kaylie lim	San Jose	95129 US	12/15/2020
Hannah Johnson	Morris	60450 US	12/15/2020
Sheena Kerbs	Spokane	99202 US	12/15/2020
Ashleigh Tibbetts	Narragansett	2882 US	12/15/2020
Irish Bugayong	Lincoln	95648 US	12/15/2020
Diana Syla	New York	10016 US	12/15/2020
Gisell Chacon	Fountain	80817 US	12/15/2020
Taylor Schmidt	Fayetteville	28304 US	12/15/2020
katie davis	Hinton	25951 US	12/15/2020
Alli Diaz	Placentia	92870 US	12/15/2020
Alyssa Faulkner	San Antonio	78239 US	12/15/2020
Stephanie Rico	Hesperia	92345 US	12/15/2020
makayla coleman	Moyock	27958 US	12/15/2020
Carolyn Ramsay	Pompano Beach	33321 US	12/15/2020
Corrie Dresser	Carlsbad	92009 US	12/15/2020
Dakota Villano	Commack	11725 US	12/15/2020
Lisa Gorewit-Decker	Novato	94947 US	12/15/2020
Amanda Pilotte	Palatine	60067 US	12/15/2020
Mitchell Ladd	Truckee	96161 US	12/15/2020
Wesley Hayes	Houston	77077 US	12/15/2020
cate house	Minneapolis	55419 US	12/15/2020
Aline Salinas	Houston	77049 US	12/15/2020
Jaslene Valdez	Houston	77036 US	12/15/2020
Aryan Vij	Edison	8820 US	12/15/2020
America Perez	Phoenix	85029 US	12/15/2020
cambrya white	queen creek	85142 US	12/15/2020
Navi K	Broomfield	80020 US	12/15/2020
Suze Metayer	Brockton	2301 US	12/15/2020
William Tyson	Buford	30519 US	12/15/2020
Liya Berhanu	Kansas City	64111 US	12/15/2020
Karina Rodriguez	Fort Lauderdale	33316 US	12/15/2020
Christina Guillen	Fresno	93727 US	12/15/2020
joaquin joaquin	Harlingen	78552 US	12/15/2020
Ella Kaczmarek	Raleigh	27606 US	12/15/2020
Tony Dorado	Tampa	33611 US	12/15/2020
Lilly Grdovic	Phillipsburg	8865 US	12/15/2020
Ted Pasieniuk	Orrick	64077 US	12/15/2020
r last	North Bergen	7047 US	12/15/2020
Dayamin Martinez	Granger	97031 US	12/15/2020
Erin Nicholson	Fishkill	US	12/15/2020

Courtney O'Malley	Western Springs		60558 US	12/15/2020
Juliana S	Poughkeepsie		12603 US	12/15/2020
roselyn chavez	Walla Walla		99362 US	12/15/2020
Anniya Sashegyi	Miami		33179 US	12/15/2020
Glendy De leon	Westport		6880 US	12/15/2020
hayley hayley	Louisville		40241 US	12/15/2020
Ava Saracco	Hamden		6514 US	12/15/2020
Brayden Hyde	Athens		37303 US	12/15/2020
Elijah O'Hanlon	Placitas		87043 US	12/15/2020
Piper Nilsen	Carlsbad		92006 US	12/15/2020
Jenna Fallon	Whitestone		11357 US	12/15/2020
Charlotte Campbell	Oak Park		60302 US	12/15/2020
Sabree Musawwir	Antioch		94509 US	12/15/2020
jason suarez	Wayne		7470 US	12/15/2020
Alexis Crawford	Audubon		50025 US	12/15/2020
Layton Stansell	Houston		77059 US	12/15/2020
Shantal Rivera	Joliet		60436 US	12/15/2020
Kylie Thomas	Waynesboro		22980 US	12/15/2020
Cinthia Ramirez	Lehigh Acres		33973 US	12/15/2020
Kellen St John	Tualatin		97062 US	12/15/2020
Carly Greathouse	Oshkosh		54901 US	12/15/2020
sophie dinderman	Gaithersburg		20878 US	12/15/2020
Suspicious Chxrr	Bergenfield		7621 US	12/15/2020
Zach Sidare	Buffalo		14215 US	12/15/2020
Bernard Myrtil	Holbrook		11741 US	12/15/2020
Cameron McElroy	Richland	WA	99352 US	12/15/2020
Ayaris Cade	Valrico		33594 US	12/15/2020
Angela Sang	Charlotte		28214 US	12/15/2020
Alexiss Guallpa	Jackson Heights		11368 US	12/15/2020
zoey bailey	pineville		40977 US	12/15/2020
Heather Grimes	Bruceton Mills		26525 US	12/15/2020
Olivia Gardner	Beverly Hills		90210 US	12/15/2020
luis carranza	alhambra		91803 US	12/15/2020
Izzy Derr	Nashville		37206 US	12/15/2020
Xiadani cruz	Birmingham		35215 US	12/15/2020
Alex Nagy	Fayetteville		13066 US	12/15/2020
Lizzie Carter	Chicago		60609 US	12/15/2020
Rebel Bowie	Edmond		73013 US	12/15/2020
peyton miller	Glenmoore		19343 US	12/15/2020
Lauren Walters	Charlotte		28213 US	12/15/2020
Penelope Parkison	Lake Oswego		97034 US	12/15/2020
Fernanda Bonilla	Pharr		78577 US	12/15/2020
Jaleia Brock	Houston		77019 US	12/15/2020
Kiomi Windbiel	Santa Maria		93455 US	12/15/2020
Natalie Burciaga	Las Vegas		89120 US	12/15/2020
Zxsion Owusu	Detroit		48227 US	12/15/2020
Misty Babcock	Falconer		14733 US	12/15/2020

Tamia Boyd	Missouri City		77489 US	12/15/2020
Hannah Badgley	Matawan		7747 US	12/15/2020
ryan d	Louisville		40216 US	12/15/2020
melissa molera	Benson		85602 US	12/15/2020
Trinity C	Glen Burnie		21061 US	12/15/2020
Helen Ortega	Santa Fe	NM	87505 US	12/15/2020
Sam Hunt	Kingsley		49649 US	12/15/2020
Elisha Fabian	Los Angeles		90027 US	12/15/2020
Isabel Amos	Staten Island		10312 US	12/15/2020
Daniella Shomshonov	Brooklyn		11219 US	12/15/2020
Kessler Giuliano	Fairview		37062 US	12/15/2020
Adriana Magalhaes	Newark		7114 US	12/15/2020
Hisani R	Cleveland		44125 US	12/15/2020
Mercedez Lucero	Las Vegas		89142 US	12/15/2020
Christiane Huckin	Salt Lake City		84104 US	12/15/2020
Mimi Herrera	Appleton		54914 US	12/15/2020
Anissia Jarvis	Aurora		47001 US	12/15/2020
Sa'tina Santana	Lake Worth		33460 US	12/15/2020
Sheerin Subekti	Ridgewood		11385 US	12/15/2020
Kyle Rhodes	Seattle		98134 US	12/15/2020
madisyn darby	Queens		11413 US	12/15/2020
Liane Rainer	New York	NY 10028	US	12/15/2020
Melody Piper	Tracy		95391 US	12/15/2020
Jessica Drake	Los Angeles		90026 US	12/15/2020
Grace ann Henderson	Claremont		91711 US	12/15/2020
Taylor O'Brien	Stow		44224 US	12/15/2020
Kaitlyn Knoll	Annandale		22003 US	12/15/2020
alejandro gonzalez	Pharr		78577 US	12/15/2020
Ava Dardis	Manlius		13104 US	12/15/2020
maddie riddle	Janesville		53545 US	12/15/2020
mack stevens	Las Vegas		89115 US	12/15/2020
brooklyn sanchez	Austin		78736 US	12/15/2020
itzuri s	Madison		53704 US	12/15/2020
Cheyene Lizotte	Las Vegas		89135 US	12/15/2020
Isabela Gonzalez	Chicago		60647 US	12/15/2020
Alfredo Alvarez Jr	Huntington Park		90255 US	12/15/2020
Daniela Calixto	Los Angeles		90008 US	12/15/2020
Elaina Heywood-Gratton	Milton		5468 US	12/15/2020
River Marie	Antioch		37013 US	12/15/2020
Blanca S.S	Houston		77099 US	12/15/2020
Jennifer Fuentes	Hawthorne		90250 US	12/15/2020
Stephanie Sides	Maumelle		72113 US	12/15/2020
mia campos	Foothill Ranch		92610 US	12/15/2020
James Holtsclaw	Fennville		49408 US	12/15/2020
Alli Pense	Traverse City		49686 US	12/15/2020
Anthony McDonough	Ann Arbor		48103 US	12/15/2020
Brian Randall	Anchorage		99507 US	12/15/2020

Brittany Mora	Mesquite	75150 US	12/15/2020
Sofia Saldana	Anaheim	92805 US	12/15/2020
klaudia kozzarek	Clifton	7013 US	12/15/2020
Matthew Morrison	Manhattan	12512 US	12/15/2020
Sydney Kroll	Grand Haven	49417 US	12/15/2020
Genessi Hernandez	Denver	80211 US	12/15/2020
chayse young	Perrysburg	43551 US	12/15/2020
Mikayla Brucoli	Chalfont	18914 US	12/15/2020
Rebecca Mello	Bedford	3110 US	12/15/2020
bob jon	Sterling Heights	48313 US	12/15/2020
Emma Ely	Canonsburg	15317 US	12/15/2020
Emily Jaje	Jacksonville	32211 US	12/15/2020
Kamiya Ray	Anniston	36206 US	12/15/2020
Biggy Cheese	Burleson	76028 US	12/15/2020
Kaylee Andrew	Tupelo	38804 US	12/15/2020
Jaqueline Corona	Riverside	92501 US	12/15/2020
Karina Valencia	Aptos	95003 US	12/15/2020
Bailey Ross	Altoona	16602 US	12/15/2020
Aaliya Cancio	Lordsburg	88045 US	12/15/2020
Lola Geoffroy	Williamsburg	23188 US	12/15/2020
Miranda Garcia	Big Bear City	92314 US	12/15/2020
Lily Roberts	Longmont	80503 US	12/15/2020
Aliyah Whitcomb	Sheffield	16347 US	12/15/2020
Emily Lindwall	Wausau	54403 US	12/15/2020
Marisol Acevedo	Garden Grove	92843 US	12/15/2020
Shantay Heaven	Philadelphia	19133 US	12/15/2020
Amirah Johnson	Durham	27713 US	12/15/2020
Caroline Coleman	West Plains	65775 US	12/15/2020
Hailey Cookston	Stockton	95204 US	12/15/2020
Hannah Fish	Columbus	43214 US	12/15/2020
Josie Van eyk	Greenwood	29649 US	12/15/2020
Marli Watson	Millington	38054 US	12/15/2020
Jelien Vazquez	Queens	11372 US	12/15/2020
Gavin Sadler	Oakland	97462 US	12/15/2020
Isabela Venegas	Lodi	7644 US	12/15/2020
Jayzic Campos	Fullerton	92831 US	12/15/2020
Amber Nash	Pleasanton	66075 US	12/15/2020
naila desila	havertown	19083 US	12/15/2020
mason lou	Reston	20191 US	12/15/2020
mia jaramillo	rio rancho	87124 US	12/15/2020
Annie Lin	New York	10003 US	12/15/2020
ciara mahina	West Jordan	84081 US	12/15/2020
alexa lelong	Simi Valley	93065 US	12/15/2020
Jacqueline Barron	Memphis	38141 US	12/15/2020
Anastasia Balmaceda	San Diego	92124 US	12/15/2020
Kristian Barnes	Tucson	85704 US	12/15/2020
Olivia Katsuki	San Jose	95127 US	12/15/2020

Karissa Powless	Pheonix		85396 US	12/15/2020
kacey manos	West Chester		19382 US	12/15/2020
fuck morgz	Manhattan Beach		90266 US	12/15/2020
Sidny Thorne	Memphis		38122 US	12/15/2020
Julissa Esparza	Stockton		95206 US	12/15/2020
Jasmine Bobino	Houston		77071 US	12/15/2020
Abigail Olson	Chicago		60625 US	12/15/2020
Marissa Balzli	New York		10035 US	12/15/2020
Andre Owens			US	12/15/2020
Daykendra Scott	Milwaukee		53216 US	12/15/2020
Rita G Foley	Pittsburgh		15223 US	12/15/2020
Alexis Baker	Danville		24541 US	12/15/2020
Rouhan Bhojan	Northville		48167 US	12/15/2020
kylee fuentes	Arlington		76013 US	12/15/2020
Anna Rose	Bend		97751 US	12/15/2020
Hailee Rogers	Roanoke		24016 US	12/15/2020
Adrian Rodriguez	Lawrenceville		30044 US	12/15/2020
Jessica Barley	Aurora		80013 US	12/15/2020
Rae Mahan	Newburyport		1950 US	12/15/2020
Valentina Castellanos Luna	Palm Desert		92260 US	12/15/2020
Marin Halvorson	Alexandria		59716 US	12/15/2020
Nia Wang	Atlanta		30308 US	12/15/2020
Kat Anthony	Copperas Cove		76522 US	12/15/2020
Alisson Rivera			US	12/15/2020
Zoë Lee	Cda		83814 US	12/15/2020
Moyinoluwa Erinfolami	Staten Island		10314 US	12/15/2020
Ella Wilkinson	Asheville		1114 US	12/15/2020
angelie martinez	Zanesville		43701 US	12/15/2020
Angelixa Morales	Kissimmee		34759 US	12/15/2020
Taylor Butler	Cleveland		44103 US	12/15/2020
annabelle cassimatis	centereach		11720 US	12/15/2020
Lauren Edwards	Orlando		32828 US	12/15/2020
Susan Ulloa	Crestline		92325 US	12/15/2020
Chloe Heft	Lebanon		17046 US	12/15/2020
Lizette Toscano	Seattle		98092 US	12/15/2020
Kendra Waddell	Cypress		90630 US	12/15/2020
samantha logue	stonington		62568 US	12/15/2020
Ana Lacea	Everett		98203 US	12/15/2020
brian mcdonald	Spanaway	WA	98387 US	12/16/2020
Vance Butler	Bonney Lake	WA	98391 US	12/17/2020
Jee Hamburg	Puyallup	WA	98374 US	12/18/2020
Barb Foster	Puyallup	WA	98372 US	12/25/2020
Brian Dewell	Seattle	WA	98178 US	12/30/2020
Alex Kocina	Bonney lake	WA	98391 US	1/26/2021
Site Admin			US	1/30/2021
Briana Somers	Burlington		US	2/2/2021
Jade Knapp	Puyallup	WA	98374 US	4/27/2021

Corry Ward	Puyallup	WA		98372 US	5/14/2021
AFTON BACHMANN	Seattle	WA		98112 US	5/27/2021
Mark Messer	Puyallup	WA		98374 US	6/9/2021
Ashlee Rassat	Puyallup	WA		98372 US	8/13/2021
Christopher Nammour	Puyallup	WA		98374 US	9/27/2021
Jake Brown	Tacoma	WA		98445 US	10/29/2021
Kayla Hughes	Puyallup	WA		98374 US	10/29/2021
Kate Conway	Wellsville	MO		63384 US	9/10/2022
Charlotte A Conley	Puyallup	WA		98374 US	10/19/2022
Aneeqa Malik	Toronto		M5V	Canada	6/19/2023
connor potts	Alfreton	ENG	DE55	UK	8/2/2023
Keaton Craft	Puyallup	WA		98374 US	9/4/2023
Chris Ransom	Puyallup	WA		98373 US	9/29/2023
Anthony Miller	Puyallup	WA		98371 US	10/30/2023
LLOYD WILBER	Graham	WA		98338 US	12/14/2023
Drayden Hagood	Lexington			29053 US	12/15/2023
Kathryn Saliccioli	Farmington			48336 US	12/16/2023
John Palmer	Tacoma	WA		98444 US	1/4/2024
Adrienne Clark	North Bend	WA		98045 US	1/14/2024
kennedy clark	Snoqualmie	WA		98065 US	1/14/2024
faith felner	Snoqualmie	WA		98065 US	1/14/2024
Russ Ratliff	Federal Way	WA		98003 US	1/14/2024
jill angelichio	charlotte			28204 US	1/15/2024
Chase Levie	Altoona	PA		16601 US	1/15/2024
Dana Burgess	Arvada			80004 US	1/15/2024
Wil Guenther	Kansas City			64121 US	1/15/2024
Alese Shannon	Flushing			11367 US	1/15/2024
Leo Ruvalcaba	Victorville			92395 US	1/15/2024
Skylar Rock	Crofton			21114 US	1/15/2024
andrea tadlock	Wickes			71973 US	1/15/2024
Gloria Welch	powell			37849 US	1/15/2024
kynnedi rodriguez	Eunice			70535 US	1/16/2024
JIN Ahn	Dallas			75243 US	1/16/2024
Sandra Wusterbarth-Brown	Puyallup	WA		98375 US	1/16/2024
David De Groot	Puyallup	WA		98374 US	1/16/2024
Beverly Query	Tacoma	WA		98374 US	1/16/2024
Elizabeth Collins	Puyallup	WA		98372 US	1/16/2024
Kelly Comer	Puyallup	WA		98371 US	1/16/2024
Richard Gerren	Seattle	WA		98144 US	1/16/2024
Paula Salisbury	Renton	WA		98056 US	1/16/2024
Paul Rometsch	Puyallup	WA		98372 US	1/16/2024
Sydney Rometsch	Puyallup	WA		98372 US	1/16/2024
Alan Boyle	Puyallup	WA		98371 US	1/16/2024
Vicki Henke	Seattle	WA		98168 US	1/16/2024
Jon Inwood	Brooklyn	NY		11226 US	1/17/2024
Stacy Zetterberg	Puyallup	WA		98374 US	1/17/2024
David Heagle	Tacoma	WA		98374 US	1/17/2024

Macey Salisbury	Tacoma	WA	98372 US	1/17/2024
Angela Ingalsbe	Puyallup	WA	98374 US	1/17/2024
Alana Preziosi	Swedesboro		8085 US	1/17/2024
Sharon Carter	Puyallup	WA	98374 US	1/17/2024
Morgan Shewmaker	Puyallup	WA	98372 US	1/17/2024
Mi'kayla White	Aurora		60506 US	1/17/2024
Jiwon Park	Peachtree Corners		30092 US	1/17/2024
Michelle Arnett	Tacoma	WA	98404 US	1/17/2024
Chris Swanson	Puya	WA	98373 US	1/17/2024
Rickysia Arnise	Brunswick		31525 US	1/17/2024
Jane McCabe	Puyallup	WA	98372 US	1/17/2024
John A. McCabe	Redmond	WA	98053 US	1/17/2024
Jeannie Smth	Puyallup	WA	98372 US	1/17/2024
Lori LaFontaine	Tacoma	WA	98404 US	1/17/2024
lina reinoso	New York City		11101 US	1/17/2024
Bethany Kirk	PUYALLUP	WA	98371 US	1/17/2024
Ayla Ciano-Buckley	Westport		6880 US	1/17/2024
William Griffin	Puyallup	WA	98374 US	1/17/2024
Alysse Blackwell	Frisco		75036 US	1/17/2024
Jade Duncan	Wimberley		78676 US	1/17/2024
Carter Saul	Grove City		43123 US	1/17/2024
Sirichandana Pulikonda	Tracy		95391 US	1/18/2024
Erin Phillips	Tacoma	WA	98374 US	1/18/2024
Abigayle Crosslin	Seattle	WA	98168 US	1/18/2024
Steven Cabrera	Tacoma	WA	98374 US	1/18/2024
Vincent Cabrera	Puyallup	WA	98372 US	1/18/2024
Ren Kenny	Temple Hills		20748 US	1/18/2024
Mary Lou Soccia	Portland	OR	97212 US	1/18/2024
Kim Woolhouse	Puyallup	WA	98371 US	1/18/2024
Emily Crosslin	Graham	WA	98338 US	1/18/2024
Dawn Foster	Seattle	WA	98144 US	1/18/2024
Alixandre Wilkins	Tacoma	WA	98446 US	1/18/2024
Mohammad Imran	Washington		20020 US	1/18/2024
briell boozier	Aurora		80012 US	1/18/2024
Anjovon alhadeff	puyallup	WA	98372 US	1/18/2024
Susan Raybon	Summerville	SC	29485 US	1/18/2024
Eduardo Guanuchi	Peekskill		10566 US	1/18/2024
Linda INFANTE	West Granby		6090 US	1/19/2024
Ashley Y	Tacoma	WA	98445 US	1/19/2024
Teresa Maston	Puyallup	WA	98372 US	1/19/2024
Sarah Maston	Puyallup	WA	98372 US	1/19/2024
Jen Rittenhouse	Puyallup	WA	98371 US	1/19/2024
Justin Kaufman	Fort Wayne		46806 US	1/20/2024
Rebecca Denbrook	Puyallup	WA	98371 US	1/20/2024
Katie England	Puyallup	WA	98374 US	1/20/2024
Andrew Florez	Flushing		11367 US	1/20/2024
Jose luis Vazquez	Sacramento		95823 US	1/20/2024

benjamin fisher	Ponchatoula		70454 US	1/20/2024
Audrey Eguia	Rancho Cucamonga		91730 US	1/20/2024
Kara Scanlan	Puyallup	WA	98373 US	1/20/2024
Franko Ortega	Brooklyn		11221 US	1/20/2024
Bernie Copeland	Byrdstown	TN	38549 US	1/21/2024
Stephanie Gitlin	Long Island City		11101 US	1/21/2024
Laureen Nitz	Bryan		77807 US	1/21/2024
Jamee Patrick	Orange Park		32073 US	1/21/2024
Christian Argudo	Queens		11368 US	1/21/2024
Melissa Penn	Apo		9012 US	1/21/2024
Josh Fair	New Castle		16101 US	1/22/2024
Sardou Nerette	Brockton		2301 US	1/22/2024
Nancy Davis	Puyallup	WA	98374 US	1/22/2024
Caryl Bittenbender	Puyallup	WA	98372 US	1/22/2024
Ben Polsk	Bellevue		98006 US	1/22/2024
david saint brave	Orlando		32822 US	1/22/2024
Caitlin Mahan	Spring city		43181 US	1/23/2024
Jennifer Mcneill	Puyallup	WA	98372 US	1/23/2024
jennifer valentine	Massapequa		11758 US	1/23/2024
Karen Stewart	Lakewood		90712 US	1/23/2024
Joyce Wilson	Laytonville		95454 US	1/23/2024
jim Arnold	denver		80014 US	1/23/2024
MICHAEL TREGONING	Ottawa		61350 US	1/24/2024
Renee Bryant	Eatonville	WA	98328 US	1/24/2024
Sherry Davis	Seattle	WA	98109 US	1/24/2024
Denise Farrell	Bonney Lake	WA	98391 US	1/24/2024
Patricia Beaver	Seattle	WA	98116 US	1/24/2024
Shirley Graves	Lake Tapps	WA	98391 US	1/24/2024
Rose Rogers	Tacoma	WA	98375 US	1/24/2024
Cheryl Gruwell	Federal Way	WA	98003 US	1/24/2024
Brian Wheeler	Puyallup	WA	98374 US	1/24/2024
Karri Wolgamot	Tacoma	WA	98445 US	1/24/2024
Katie Randolph	Oak Brook		60523 US	1/25/2024
Michele Sequist	Tacoma	WA	98466 US	1/25/2024
Shirel Espejo Westermeyer	Woodland Hills		91367 US	1/25/2024
Kyra Withycombe	Worthington		47471 US	1/25/2024
Jill Jurgens	Puyallup	WA	98372 US	1/25/2024
Cristian Mendoza	Puyallup	WA	98375 US	1/25/2024
Joshua Curphey	Peterborough		US	1/25/2024
Lydia Rodriguez			US	1/25/2024
Carter Stamon	Puyallup	WA	98372 US	1/25/2024
Victor Quebrado	Greensboro		27413 US	1/25/2024
Mikayla Rogers	Gulfport		39503 US	1/26/2024
Rosemary H.	Detroit		48207 US	1/26/2024
Franklin Sarmiento	Jersey City		7105 US	1/26/2024
Kam Ford	Detroit		48235 US	1/26/2024
Earl Pardue	Ozark		72949 US	1/26/2024

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Josh Standiford	Lake Zurich		60047 US	1/27/2024
Patricia Comaduran	Grove city		US	1/27/2024
Eric Sims	Puyallup	WA	98374 US	1/27/2024
Kenneth whalen	Seattle	WA	98198 US	1/27/2024
Vicky Hopp	Puyallup	WA	98374 US	1/27/2024
Jack Iddins	Puyallup	WA	98373-4035 US	1/27/2024
Jacqui Judd	Puyallup	WA	98374 US	1/27/2024
Madi Brower	Belleville		7109 US	1/27/2024
Karen Sedlack	Tacoma	WA	98374 US	1/28/2024
Barbara O'Brien	Puyallup	WA	98374 US	1/28/2024
Olga Subbotin	Puyallup	WA	98374 US	1/28/2024
Samantha Gutierrez	Tacoma	WA	98404 US	1/28/2024
Gabriella Gutierrez	Seattle	WA	98104 US	1/28/2024
Israel Gutierrez	Tacoma	WA	98375 US	1/28/2024
Khalie Liston	Bonney Lake	WA	98391 US	1/28/2024
Delia Trujillo	Seattle	WA	98118 US	1/28/2024
Deborah Kraynak	Tacoma	WA	98374 US	1/28/2024
Erika Thomas	Waldorf		20603 US	1/28/2024
April VanLierop	Seattle	WA	98188 US	1/28/2024
Carey Hartmann	Puyallup	WA	98374 US	1/28/2024
Mahpara Qureshi	Frederick		21704 US	1/28/2024
LeAnn Weber	Milton	WA	98354 US	1/28/2024
Kat Holman	Tacoma	WA	98375 US	1/28/2024
Jill Rossetto	Tacoma	WA	98856 US	1/28/2024
Yvonne Le Havre	Puyallup	WA	98372 US	1/28/2024
Rosemari Ogden	Seattle	WA	98102 US	1/28/2024
Julianna Mauga	Tacoma	WA	98372 US	1/28/2024
Bit kennedy	Los Angeles		90041 US	1/28/2024
Susan Hartmann	Puyallup	WA	98374 US	1/29/2024
Gilbert Han	Irving	TX	75063 US	1/29/2024
Colleen Littlejohn	Sumner	WA	98390 US	1/29/2024
Carol Maul	Gig Harbor	WA	98332 US	1/29/2024
MMAam ando	Auburn		98001 US	1/29/2024
Barbara Heins	Puyallup	WA	98371 US	1/29/2024
Kelsey English	Seattle	WA	98103 US	1/29/2024
Anne VanLierop	Olalla	WA	98359 US	1/29/2024
Cheryl Babo	Anderson island	WA	98303 US	1/29/2024
Marnie Vandeway	Seattle	WA	98104 US	1/29/2024
David Barnes	Sumner	WA	98390 US	1/29/2024
Eddie Dolan	Seattle	WA	98188 US	1/30/2024
Cheryl Fuller	Puyallup	WA	98374 US	1/30/2024
Krista Griffin	Puyallup	WA	98372 US	1/30/2024
Cameron Griffin	Puyallup	WA	98372 US	1/30/2024
Jaiden Dubois	New York		13601 US	1/30/2024
Kelley Emanuelson	Tacoma	WA	98374 US	1/30/2024
Kendall McLaughlin	Tacoma	WA	98466 US	1/30/2024
Hayley Logan	Portland		97217 US	1/30/2024

Malia Hong-Cook	Puyallup	WA	98371 US	1/30/2024
Hayder Alkurdi	Woodbridge		22192 US	1/30/2024
Kat Boyle	Tacoma	WA	98371 US	1/30/2024
Cynthia Netzer	Tacoma	WA	98408 US	1/30/2024
Amy Brown	Edgewood	WA	98372 US	1/30/2024
Jamison Harris	PUYALLUP	WA	98372 US	1/30/2024
Nichole Mcintosh	Auburn	WA	98092 US	1/30/2024
Terri Treakle	Puyallup	WA	98371 US	1/30/2024
Kathy Bates Hansen	Puyallup	WA	98375 US	1/30/2024
Leslie DeRoche	Puyallup	WA	98374 US	1/30/2024
April Darden	Kalamazoo		49001 US	1/31/2024
elijah netzer	Mesa	AZ	85202 US	1/31/2024
Karen Beck	Puyallup	WA	98371 US	1/31/2024
Don Staley	Puyallup	WA	98372 US	1/31/2024
Michael Richmond	Puyallup	WA	98372 US	1/31/2024
Hawks Lisa	Memphis		38111 US	1/31/2024
Catherine Elwess	Puyallup	WA	98373 US	1/31/2024
Jonathan Miller	Fort Wayne		46808 US	1/31/2024
Diane Helm	Sumner	WA	98390 US	1/31/2024
Michael Lindholm	Puyallup	WA	98374 US	1/31/2024
Mary Williams	Tacoma	WA	98443 US	1/31/2024
Deanna Rink	Puyallup	WA	98374 US	2/1/2024
JoAnn Mwrola Merola	Tacoma	WA	98374 US	2/1/2024
S.E. Case	Seattle	WA	98102 US	2/1/2024
Mary Horton	Tacoma	WA	98405 US	2/1/2024
Karen Adams	Seattle	WA	98103 US	2/1/2024
Faith McKillip	Bonney Lake	WA	98391 US	2/1/2024
Colleen Clark	Sumner	WA	98390 US	2/1/2024
APRIL PABLOFF	Tacoma	WA	98371 US	2/1/2024
Marissa Biscocho	Fife	WA	98424 US	2/1/2024
Daniel Ernst	Tacoma	WA	98466 US	2/1/2024
Frederica Soine	Tacoma	WA	98443 US	2/1/2024
Loeryn Roberts	Tacoma	WA	98424 US	2/1/2024
Michael Wickoren	Tacoma	WA	98373 US	2/1/2024
Russell Burr	Seattle	WA	98168 US	2/1/2024
Susan Skieens	Puyallup	WA	98375 US	2/2/2024
Jeremiah Johnson	Puyallup	WA	98372 US	2/2/2024
Jesus Ortiz	La Puente California		91746 US	2/2/2024
Mike Sheffer	Puyallup	WA	98375 US	2/2/2024
Jennifer Kellar	Caliente		89008 US	2/2/2024
Lacey J	Puyallup	WA	98374 US	2/2/2024
Caitlyn Cecchetto	Puyallup	WA	98372 US	2/3/2024
Josh Miller			US	2/3/2024
Paul Croft	Puyallup	WA	98372 US	2/3/2024
Wendy Novini	Tacoma	WA	98373 US	2/3/2024
DeAnn DeRosier	Puyallup	WA	98374 US	2/3/2024
Katherine Hanna	Puyallup	WA	98374 US	2/3/2024

Kimberly Longo	Seattle	WA	98101 US	2/3/2024
Myron Spiegel	Puyallup	WA	98374 US	2/3/2024
Tammy Reed	Houston		77084 US	2/4/2024
Isabella Letic	Old Westbury		11590 US	2/4/2024
Charlene Young	Yuma		85364 US	2/4/2024
Peggy Mckenna	Puyallup	WA	98374 US	2/4/2024
Maria Macias	Yuba City		95991 US	2/5/2024
Pamela Holbert	Tucson	AZ	85711 US	2/5/2024
Melissa Bassi	Buena Park		90620 US	2/5/2024
Shauna Fetterman	Puyallup	WA	98373 US	2/5/2024
Nora Blair	Ames		50010 US	2/6/2024
Tom Heino	Sumner	WA	98390 US	2/6/2024
Judith levy	Tacoma	WA	98374 US	2/6/2024
Saanvi Thakur	Aldie		20105 US	2/7/2024
maddie barott	Minneapolis		55413 US	2/7/2024
Luc Munslow			US	2/8/2024
Anahi Valenzuela	Los Angeles		90033 US	2/8/2024
Erika Rikhiram	Clermont		34711 US	2/8/2024
Mitzi Lea vanetten	Hudson		12534 US	2/8/2024
Hope Montgomery	Omaha		68112 US	2/8/2024
Kiara Scott	Stockton		95209 US	2/9/2024
Kaye Roye	Portland		4756 US	2/9/2024
Nicole Gonzalez			34771 US	2/9/2024
Krys Williams	Montello	WI	53949 US	2/10/2024
Cyd Marckmann	Puyallup	WA	98372 US	2/11/2024
heidi polk		FL	US	2/11/2024
Gloria Navan	Lawrenceville	GA	30042 US	2/12/2024
Desirée Kelshall	Wichita Falls	TX	76309 US	2/12/2024
Jayden Boston			US	2/12/2024
George Turnage	Philadelphia	PA	19141 US	2/12/2024
Shane Turner	Augusta	KS	67010 US	2/13/2024
Heather Hernandez	Puyallup	WA	98372 US	2/13/2024
Chris Yanasak	Tacoma	WA	98466 US	2/13/2024
Katherine Hamilton	Puyallup	WA	98374 US	2/13/2024
Therese Juntunen	Puyallup	WA	98374 US	2/13/2024
Cory Hamilton	Tacoma	WA	98374 US	2/13/2024
BECKY MCDOWELL	Puyallup	WA	98372 US	2/13/2024
Darryl Heine	Barrington	IL	60010 US	2/13/2024
Nicholas Winterton	Seattle	WA	98118 US	2/13/2024
Austin Anderson			US	2/13/2024
Sylvia Zellmer	Seattle	WA	98108 US	2/13/2024
Jordan Ryckman	Bonney Lake	WA	98391 US	2/13/2024
Danielle Julson	Puyallup	WA	98375 US	2/13/2024
Laura Miles	Puyallup	WA	98374 US	2/13/2024
Christine Carr	Sumner	WA	98390 US	2/13/2024
Jason Carr	Sumner	WA	98390 US	2/13/2024
Philip Barber	Bonney Lake	WA	98391 US	2/13/2024

Diana Bullen	PUYALLUP	WA	98371 US	2/13/2024
Bob Myrick	Tacoma	WA	98408 US	2/13/2024
Damon Spark	Tacoma	WA	98466 US	2/13/2024
Thomas Ripley	Tacoma	WA	98465 US	2/13/2024
Sarah Crane	Bonney Lake	WA	98391 US	2/13/2024
Andrew Floyd			US	2/13/2024
Joyce Eslinger	Fife	WA	98424 US	2/14/2024
Jennifer Strong	Tacoma	WA	98466 US	2/14/2024
Avana Stampley	Chicago	IL	60651 US	2/14/2024
Timothy Downs	Puyallup	WA	98374 US	2/14/2024
Angelique Soto	Brooklyn		11212 US	2/14/2024
Franz- Silas Muse	spanaway	WA	98387 US	2/14/2024
Cheryl Tobie	Seattle	WA	98118 US	2/14/2024
Diane Bero	Puyallup	WA	98372 US	2/14/2024
Kim deVries	Puyallup	WA	98374 US	2/14/2024
Douglas Terrien	Gig Harbor	WA	98335 US	2/14/2024
Jami Roland	Tacoma	WA	98374 US	2/14/2024
Stacy Roland	Puyallup	WA	98372 US	2/14/2024
Deborah Hibbs	Puyallup	WA	98383 US	2/14/2024
Craig Roland	Puyallup	WA	98372 US	2/14/2024
whitney watters	Ormond Beach	FL	32174 US	2/14/2024
gary zellmer	Puyallup	WA	98371-7428 US	2/14/2024
Cameron Herrin	Palo Alto	CA	94303 US	2/14/2024
Daniel Aaron	Puyallup	WA	98372 US	2/14/2024
Chao Yuan	Brooklyn	NY	11235 US	2/14/2024
Bryan Obi	Carrollton	TX	75007 US	2/14/2024
Gail Downs	Puyallup	WA	98374 US	2/14/2024
Damaris Torres	Springfield	MA	1107 US	2/14/2024
Kayla Seiler	Green Bay	WI	54313 US	2/14/2024
Zeppa Prince	Norcross	GA	30071 US	2/15/2024
Margi Barsamian	Edmonds	WA	98026 US	2/15/2024
Cynthia Allen	Lawrenceville	GA	30043 US	2/16/2024
Preston Smith	Puyallup	WA	98372 US	2/16/2024
Jolene Neuman	Two Rivers	WI	54241 US	2/16/2024
Amber Lamb	Horicon		53032 US	2/16/2024
Kylene simonar	Denver	CO	80247 US	2/16/2024
Jake Pool	Puyallup	WA	98374 US	2/16/2024
Mary Arlowe	Helena	MT	59601 US	2/16/2024
Madison Brown	Oxon Hill	MD	20745 US	2/16/2024
Eddie Dawson	Arvada	CO	80004 US	2/16/2024
Sophia Carney	Flower Mound		75057 US	2/16/2024
Barbara Mowder	Johnson City	TN	37601 US	2/16/2024
Timothy Lusk	Bonney Lake	WA	98391 US	2/17/2024
Cindy Chellin	Bonney Lake	WA	98391 US	2/17/2024
Dreglin Hensen	Lakewood	WA	98499 US	2/17/2024
Autumn Pegues	Arlington	TX	76013 US	2/17/2024
Julien Lalonde	Puyallup	WA	98372 US	2/17/2024

Christian Lalonde	Puyallup	WA	98360 US	2/17/2024
Joel Kellner	Puyallup	WA	98374 US	2/17/2024
Kimberly Hansen	Puyallup	WA	98374 US	2/17/2024
Mason Hansen	Puyallup	WA	98372 US	2/17/2024
Graciela Diaz	Puyallup	WA	98372 US	2/17/2024
Keelan Nauriyal	Seattle	WA	98188 US	2/17/2024
Jacqueline Nauriyal	Seattle	WA	98115 US	2/17/2024
Pei Wu	Puyallup	WA	98372-1902 US	2/17/2024
Crono Chiu	Puyallup	WA	98372 US	2/17/2024
Jason Hickman	Bonney Lake	WA	98391 US	2/17/2024
Siobhan Williamson	Puyallup	WA	98372 US	2/17/2024
Dawn Palumbo	Puyallup	WA	98372 US	2/17/2024
Gerard Williamson	Bonney Lake	WA	98391 US	2/17/2024
Juniper Williamson	Puyallup	WA	98372 US	2/17/2024
Allison Smith	Renton	WA	98056 US	2/17/2024
Lizzie Brown	Laurel	MT	59044 US	2/17/2024
Madeline Bovard	West Chester	PA	19382 US	2/18/2024
William Specht	DuPont	WA	98374 US	2/18/2024
Heidi Somes	Puyallup	WA	98374 US	2/18/2024
Chris Gardner	Puyallup	WA	98391 US	2/18/2024
Hendery W.	Woodridge	IL	60517 US	2/18/2024
Kennedy Percy	Puyallup	WA	98372 US	2/18/2024
Austin Blake	Puyallup	WA	98374 US	2/18/2024
Michael Heiden	Bonney Lake	WA	98391 US	2/18/2024
Siobhan Heiden	Puyallup	WA	98372 US	2/18/2024
John Palumbo	Puyallup	WA	98372 US	2/18/2024
Vadim Varkentin	Puyallup	WA	98372 US	2/18/2024
Vasyl Romanchuk	Los Angeles	CA	90035 US	2/18/2024
Lindy Von Dohlen	Pasco	WA	99301 US	2/18/2024
Jessica Robichaud	Dallas	TX	75270 US	2/18/2024
Devin Giles	Pomona	CA	91766 US	2/18/2024
Janet Kozan	West Palm Beach	FL	33411 US	2/18/2024
Lorrie Hodge	Seattle	WA	98199 US	2/18/2024
James Presley			US	2/18/2024
Hydie Sanchez		98371 WA	98257 US	2/18/2024
Karen McNamara	Puyallup	WA	98372 US	2/18/2024
Roman Kaminskyi	Rahway	NJ	7065 US	2/18/2024
Kelly Murphy	Stonehaml	MA	2180 US	2/18/2024
Matthew westfall	Bonney Lake	WA	98391 US	2/18/2024
Aubrey Terrill	Tacoma	WA	98493 US	2/19/2024
Lisa Dierckins	Puyallup	WA	98374 US	2/19/2024
Julie Edmonds	Puyallup	WA	98374 US	2/19/2024
William Copeland	Puyallup	WA	98374 US	2/19/2024
Danika Miller	Puyallup	WA	80126 US	2/19/2024
Ian Carter	Doylestown	PA	18902 US	2/19/2024
Elaine Powell	Puyallup	WA	98374 US	2/19/2024
Rosemary Hartless	Washington		20012 US	2/19/2024

Mackenzie Mehaffey	Houston	TX	77023 US	2/19/2024
John Palumbo	Kent	WA	98031 US	2/20/2024
Kari Shields	Seattle	WA	98101 US	2/20/2024
Kathy Holbert	Bonney Lake	WA	98391 US	2/20/2024
Jaclyn Zinck	Newburyport	MA	1950 US	2/20/2024
Hannah Carlisle	Puyallup	WA	98374 US	2/20/2024
Elissa Sorenson	Puyallup	WA	98372 US	2/20/2024
Christie Waltier	Orting	WA	98360 US	2/20/2024
Greg stril	PUYALLUP	WA	98372 US	2/20/2024
Valerie Neuman	Enumclaw	WA	98022 US	2/20/2024
Abbie Bragg	Vancouver	WA	98661 US	2/20/2024
Larry Rodgers	Puyallup	WA	98374 US	2/20/2024
Laurah Rodgers	Puyallup	WA	98374 US	2/20/2024
Jose Nieves	Puyallup	WA	98373 US	2/20/2024
Anna Johnson	Puyallup	WA	98373 US	2/20/2024
Hee Do Park	Puyallup	WA	98372 US	2/20/2024
Ryan Shultz	Puyallup	WA	98372 US	2/20/2024
Melissa Dominguez	Albuquerque	NM	87108 US	2/20/2024
Derek Powell	Puyallup	WA	98374 US	2/20/2024
Ann Guthrie	Puyallup	WA	98372 US	2/20/2024
Kelly Loney	Puyallup	WA	98372 US	2/21/2024
Faith Graeber	Sumner	WA	98092 US	2/21/2024
Matthew Anderson	Sumner	WA	98390 US	2/21/2024
diane taylor	Federal Way	WA	98023 US	2/21/2024
Christine Arend	Seattle	WA	98188 US	2/21/2024
Sergey Milishchuk	Puyallup	WA	98372 US	2/21/2024
Liudmyla Milishchuk	Puyallup	WA	98372 US	2/21/2024
Stephanie Shein	Puyallup	WA	98371 US	2/21/2024
Liboria Arend	Seattle	WA	98188 US	2/21/2024
Bruce Taymond	Puyallup	WA	98372 US	2/21/2024
Jojo Li	Brooklyn	NY	11223 US	2/21/2024
Jessica Ouimet	Constantia	NY	13044 US	2/21/2024
Lauren Colson	Tacoma	WA	98445 US	2/21/2024
Carrie Pederson	Seattle	WA	98107 US	2/21/2024
Cami Schmidt	Puyallup	WA	98374 US	2/21/2024
Monae Munchrath	Seattle	WA	98372 US	2/21/2024
Samuel Munchrath	Puyallup	WA	98374 US	2/21/2024
Jamie Johnson	Puyallup	WA	98372 US	2/21/2024
rocky chin	Puyallup	WA	98373 US	2/21/2024
Emily Ledgard	Puyallup	WA	98374 US	2/21/2024
Judy Allan	Puyallup	WA	98373 US	2/21/2024
Cherish England	Bonney Lake	WA	98391-6360 US	2/21/2024
Vanessa Barry	Puyallup	WA	98374 US	2/21/2024
Matthew Ducatt	Seattle	WA	98198 US	2/21/2024
Lisa Hansen	Bonney Lake	WA	98391 US	2/21/2024
David Dennis	Puyallup	WA	98372 US	2/21/2024
Katharine Bittenbender	Puyallup	WA	98371 US	2/21/2024

Judy Hartman	Puyallup	WA	98374 US	2/21/2024
Bethany Rigtrup	Orting	WA	98360 US	2/21/2024
Melanie Nirider	Tacoma	WA	98445 US	2/21/2024
Talia Pernorio	Buckley	WA	98321 US	2/21/2024
Zach Brown	Milwaukee	WI	53208 US	2/21/2024
Nita Muir	Puyallup	WA	98374 US	2/21/2024
Lisa Breshears	Puyallup	WA	98374 US	2/21/2024
Julia Torpey	Bonney Lake	WA	98391 US	2/21/2024
Donald Hunt	Puyallup	WA	98374 US	2/21/2024
Alexa Pedersen	Tacoma	WA	98404 US	2/21/2024
Connie Challender	Puyallup	WA	98374 US	2/21/2024
Makenna Carlisle	Puyallup	WA	98371 US	2/21/2024
Natalie Reyes	Stockton	CA	95201 US	2/21/2024
Michael Shipman	Sumner	WA	98390 US	2/21/2024
Harriet Acord	Puyallup	WA	98372 US	2/21/2024
Allison Rudder	Bonney Lake	WA	98391 US	2/21/2024
Janie Koch	Puyallup	WA	98375 US	2/21/2024
Jarna Rainey	Bonney Lake	WA	98391 US	2/21/2024
Lisa Reeves	Puyallup	WA	98372 US	2/21/2024
Windy Kruse	Tacoma	WA	98405 US	2/21/2024
Simone Melancon	Puyallup	WA	98374 US	2/21/2024
Keith Melancon	Seattle	WA	98133 US	2/21/2024
Justin Andersen	Seattle	WA	98168 US	2/21/2024
Aden Lynch	Tualatin	OR	97062 US	2/21/2024
Erin Calata	Puyallup	WA	98372 US	2/22/2024
Allyssa Thornhill	Puyallup	WA	98373 US	2/22/2024
Nancy Tibeau	Bonney Lake	WA	98391 US	2/22/2024
Ana Carranza	Seahurst	WA	98062 US	2/22/2024
Robin Schneider	Spanaway	WA	98387 US	2/22/2024
Cheryl Trivette	Puyallup	WA	98372 US	2/22/2024
Ryan Bittenbender	Puyallup	WA	98371 US	2/22/2024
evelyn sarmiento	Canoga Park	CA	91304 US	2/22/2024
John Trivette	Puyallup	WA	98372 US	2/22/2024
Katie Omri	Puyallup	WA	98372 US	2/22/2024
Anna Laidler	East Stroudsburg	PA	18301 US	2/22/2024
Mohammed Al Nidawi	Puyallup	WA	98372 US	2/22/2024
Ella Williams	Puyallup	WA	98372 US	2/22/2024
Sheralyn Martin	Puyallup	WA	98374 US	2/22/2024
Karoline Berge	Puyallup	WA	98374 US	2/22/2024
Erika Chang	Seattle	WA	98117 US	2/22/2024
Katie Brizuela	Spanaway	WA	98387 US	2/22/2024
Kirsten Bland	Puyallup	WA	98373 US	2/22/2024
Julie Foster	Puyallup	WA	98372 US	2/22/2024
Eric Clark	Puyallup	WA	98374 US	2/22/2024
Heather Edson	Puyallup	WA	98372 US	2/22/2024
Jessica Gillette	Bonney Lake	WA	98391 US	2/22/2024
Heather Hubert	Seattle	WA	98198 US	2/22/2024

Stephanie Hackler	Bonney Lake	WA	98391 US	2/22/2024
Kristina Hempel	Seattle	WA	98198 US	2/22/2024
Carol Lorton	Bonney Lake	WA	98391 US	2/22/2024
Lucy Boswell	Puyallup	WA	98374 US	2/22/2024
Benjamin Marinoni	Seattle	WA	98111 US	2/22/2024
Laurie Gaiter	Puyallup	WA	9874 US	2/22/2024
Cinthia Lozada	Puyallup	WA	98373 US	2/22/2024
James Hunt	Puyallup	WA	98373 US	2/22/2024
Sabrina Baker	Bonney Lake	WA	98391 US	2/22/2024
Amy Boers	Tacoma	WA	98354 US	2/22/2024
Byron Martz	Puyallup	WA	98375 US	2/22/2024
Lukas Winterowd-Laughman	Puyallup	WA	98372 US	2/22/2024
Larry Worthington	Puyallup	WA	98372 US	2/22/2024
Zoe Worthington	Puyallup	WA	98372 US	2/22/2024
Andrea Haney	Lakewood	WA	98499 US	2/22/2024
Ali Hanson	Puyallup	WA	98372 US	2/23/2024
Bailey Harris	Puyallup	WA	98374 US	2/23/2024
Christina Blakeley	Federal Way	WA	98003 US	2/23/2024
Janet Russell	Seattle	WA	98198 US	2/23/2024
Aura MacArthur	Puyallup	WA	98371 US	2/23/2024
Stephanie Foyle	Tacoma	WA	98403 US	2/23/2024
J F		NY	US	2/23/2024
Tim Nivala	Tacoma	WA	98445 US	2/23/2024
VIRGIL JOHNSON			US	2/23/2024
Nicole Parshall	Puyallup	WA	98372 US	2/23/2024
Danielle Spencer	Puyallup	WA	98374 US	2/23/2024
Heather Hanson	Puyallup	WA	98372 US	2/23/2024
Marc Kendall	Puyallup	WA	98372 US	2/23/2024
Ian Kendall	Puyallup	WA	98372 US	2/23/2024
Injin Kim	Puyallup	WA	98372 US	2/23/2024
Laurie Kajca	Puyallup	WA	98374 US	2/23/2024
Marina Sednev	Clovis	CA	93619 US	2/23/2024
Ute Hümmer			US	2/23/2024
Sara Barber	Puyallup	WA	98371 US	2/23/2024
KyLee Denning	Bonney lake	WA	98391 US	2/23/2024
rosemary torossian	Sherman Oaks	CA	91423 US	2/23/2024
Jason Barthel	Chicago	IL	60156 US	2/24/2024
kathy Sutlick	Aurora	CO	80010 US	2/24/2024
Travis Homestead	Puyallup	WA	98374 US	2/24/2024
Kassie Scott	Puyallup	WA	98372 US	2/24/2024
Angela March	Federal Way	WA	98023 US	2/24/2024
Aaron Shelton	Puyallup	WA	98372 US	2/24/2024
Sharon Ward	Puyallup	WA	98373 US	2/24/2024
M Ward	Puyallup	WA	98373 US	2/24/2024
Jerry Jean	Gresham	OR	97080 US	2/25/2024
Dalton Soriano	Puyallup	WA	98372 US	2/25/2024
Phally Soeung	Federal Way	WA	98023 US	2/25/2024

Christopher Williams	Sanford	NC	27330 US	2/25/2024
Connor Monroe	Carlsbad	CA	92009 US	2/25/2024
Trish Brown	San Bernardino	CA	92413 US	2/25/2024
Zeljko Miksic	Rancho Cordova	CA	95670 US	2/25/2024
Liam Doherty	Auburn	MA	1501 US	2/25/2024
Viorica Durllesteanu	Arlington Heights	IL	60004 US	2/25/2024
Madison Shawver	Winchester	VA	22601 US	2/25/2024
Orva M Gullett	Marion	OH	43302-8435 US	2/25/2024
Kelsi Barton	Roanoke	VA	24015 US	2/25/2024
Christine Dolan	Hillsboro	MO	63050 US	2/25/2024
Irene Levy	Oak Lawn	IL	60453 US	2/25/2024
Emily Davidson	Bonney Lake	WA	98391 US	2/25/2024
Michael Peterson	Seattle	WA	98160 US	2/25/2024
Faith Ducatt	Puyallup	WA	98374 US	2/25/2024
Gary Botelho	Lakewood	WA	98498 US	2/25/2024
Jennifer Botelho	Puyallup	WA	98372 US	2/25/2024
Annie Dawkins	Puyallup	WA	98372 US	2/25/2024
Lorie McCracken	Puyallup	WA	98374 US	2/25/2024
Sarah Erickson	Puyallup	WA	98374 US	2/26/2024
Barbara Cendejas	Tacoma	WA	98406 US	2/26/2024
Richard Plasencia	Puyallup	WA	98375 US	2/26/2024
Inez Gregg	Seattle	WA	76244 US	2/26/2024
Rachel B	Brooklyn	NY	11208 US	2/26/2024
Michele Karns	Lakewood	WA	98499 US	2/26/2024
Amber Moak	Puyallup	WA	98372 US	2/26/2024
Ameer Ismaeel	Minneapolis	MN	55478 US	2/26/2024
Kayla Hattle	Puyallup	WA	98371 US	2/26/2024
Stina Barsoski	Puyallup	WA	98371 US	2/26/2024
Mason Halos	Puyallup	WA	98374 US	2/26/2024
Jack Winterowd	Puyallup	WA	98372 US	2/26/2024
David Rickles	Manhattan Beach	CA	90266 US	2/26/2024
Ryan Dildine	Seattle	WA	98102 US	2/26/2024
Cindy Berger	Tacoma	WA	98466 US	2/26/2024
Christopher Adams	Washington	DC	20011 US	2/26/2024
Patrick Mitchell	Auburn	WA	98001 US	2/26/2024
Jason Chavez	Puyallup	WA	98372 US	2/26/2024
Christine Chavez	Bonney Lake	WA	98391 US	2/26/2024
Jason Salmi	Puyallup	WA	98372 US	2/26/2024
Jerriann Salmi	Bonney Lake	WA	98391 US	2/26/2024
Michelle Mosley	Puyallup	WA	98372 US	2/26/2024
CRAIG EDMONDS	Puyallup	WA	98374 US	2/26/2024
Paula Emerson-Glade	Spanaway	WA	98387 US	2/27/2024
Naomi Emerson-Glade	Tacoma	WA	98444 US	2/27/2024
jesus Segura	Chicago	IL	60651-3944 US	2/27/2024
Liz Tomski	Seattle	WA	98121 US	2/27/2024
Ty Nielsen	Denver	CO	80252 US	2/27/2024
Jayelle Carson	Las Vegas	NV	89102 US	2/27/2024

Vas Mr	Edison	NJ	8817 US	2/27/2024
Danielle Bennett	Puyallup	WA	98371 US	2/27/2024
Nova Hunley			US	2/27/2024
Charlene Huttenlocker	Buckley	WA	98321 US	2/27/2024
Norm Stogsdill	Puyallup	WA	98375 US	2/27/2024
Michael Garza	Atascosa	TX	78002 US	2/27/2024
sherry curran	Puyallup	WA	98375 US	2/27/2024
Kathleen Luxa	Seattle	WA	98112 US	2/28/2024
Paul Markillie	Grand Blanc Towns	MI	48439 US	2/28/2024
John Staton	Newton Falls	OH	44444 US	2/28/2024
Ryan Gray	Puyallup	WA	98371 US	2/28/2024
Daniel Clemens	Puyallup	WA	98374 US	2/28/2024
Tanya Imke	Puyallup	WA	98372 US	2/28/2024
Roberta Coleman	Philadelphia	PA	19143 US	2/28/2024
Travis Ohlde	Seattle	WA	98114 US	2/28/2024
Brock Dittus	West Linn	OR	97068 US	2/28/2024
Renee Gray	Puyallup	WA	98371 US	2/28/2024
Craig Mac	Seattle	WA	98116 US	2/28/2024
Dan Harris	Puyallup	WA	98374 US	2/28/2024
sareina kito	San Francisco	CA	94110 US	2/28/2024
heaven thigpen	Waveland	MS	39576 US	2/28/2024
Peyton Krumlauf	Akron	OH	44319 US	2/28/2024
Charlotte Becker	Puyallup	WA	98374 US	2/29/2024
Evan Moak	Puyallup	WA	98392 US	2/29/2024
Melissa Cromwell			US	2/29/2024
Kim Leifsen	Puyallup	WA	98374 US	2/29/2024
April Cerrato	Vineyard Haven	MA	2568 US	2/29/2024
Marjorie Angeline	Tacoma	WA	98409 US	2/29/2024
Shayna Baughman	Muskogee	OK	74403 US	2/29/2024
Monce Alvarez	Salem	OR	97302 US	2/29/2024
Lori Dersch	Lake Tapps	WA	98391 US	2/29/2024
Paige Clark	havre de grace	MD	21078 US	2/29/2024
Juanita Nilsby	Puyallup	WA	98372 US	2/29/2024
Amanda Butler	Sumner	WA	98390 US	2/29/2024
David Wetzel	Puyallup	WA	98374 US	2/29/2024
TAMMYLYNN Bentley	Sumner	WA	98390 US	3/1/2024
Lydia Fontana	Puyallup	WA	98374 US	3/1/2024
Susan Crawford	Puyallup	WA	98372 US	3/1/2024
Arthur Behrman	Tacoma	WA	98424 US	3/1/2024
Elen Duenas	Phoenix	AZ	85028 US	3/1/2024
Jim Olson	Shoreline	WA	98133 US	3/1/2024
Alexa Lyons	Puyallup	WA	98375 US	3/1/2024
Virginia Hubka	Draper	UT	90420 US	3/1/2024
Winston Tatelock	Manchester	NH	92064 US	3/1/2024
Christine Kithcart	Seattle	WA	98103 US	3/1/2024
Mel W	Phoenix	AZ	85014 US	3/1/2024
Lacinda Pescinska	Brunswick	OH	44212 US	3/1/2024

Susan Reed	Puyallup	WA	98372 US	3/1/2024
Laurie Rose	Puyallup	WA	98373 US	3/1/2024
Michael Reed	Puyallup	WA	98372 US	3/1/2024
Frans Keylard			US	3/1/2024
Audrey Ty	Narrows	VA	24124 US	3/2/2024
Monica Sanz	Lakeside	CA	92040 US	3/2/2024
Madi Stansfield	Edgewood	WA	98372 US	3/2/2024
Randy Tripp	Puyallup	WA	98375 US	3/2/2024
Monike Clark	Little Rock	AR	72209 US	3/2/2024
Monica Ledesma	Las Vegas	NV	89128 US	3/2/2024
skylar Paino	Valley Stream	NY	11580 US	3/2/2024
Jay-me Morita	Bonney Lake	WA	98391 US	3/2/2024
Patricia Miller	Puyallup	WA	98372 US	3/2/2024
Kaitlyn Andrews	Manchester	NH	3102 US	3/3/2024
Lydia Birnbaum	Minnetonka	MN	55345 US	3/3/2024
Megan Cooley	Topeka	KS	66606 US	3/3/2024
Kurt Mueller	Tacoma	WA	98405 US	3/3/2024
C N	Portland	OR	97210 US	3/3/2024
Angel la Nanton	Puyallup	WA	98374 US	3/4/2024
Aidan Sullivan	Londonderry	NH	3053 US	3/4/2024
Martin Confurius	Brooklyn	NY	11228 US	3/4/2024
Ty Phay	Puyallup	WA	98372 US	3/4/2024
Donna Tennison	San Francisco	CA	94124 US	3/4/2024
Odin Withrow			US	3/5/2024
Andrew Meyer	Puyallup	WA	98374 US	3/5/2024
Toledo McNeil	Tampa	FL	33614 US	3/5/2024
YODIT ABRAHA	Washington	MD	20005 US	3/5/2024
Angelica Walker	Santa Rosa	CA	95401 US	3/5/2024
Enya Cartwright	Columbia	TN	38401 US	3/6/2024
Gail Nelson	Seattle	WA	98101 US	3/6/2024
Gordon Poston	Kingstree	SC	29556 US	3/6/2024
Michael Hassur	Puyallup	WA	98373 US	3/6/2024
Arliss Reilly	Duluth	GA	30097 US	3/6/2024
Daisy Queen	Lancaster	PA	17603 US	3/6/2024
Whitney Paige	Saint Clair Shores	MI	48081 US	3/6/2024
Harley Longer	North Charleston	SC	29415 US	3/6/2024
Robert Hansen	Seattle	WA	98118 US	3/6/2024
Mark Anderson	Puyallup	WA	98371 US	3/6/2024
Robert Bruce	Puyallup	WA	98372 US	3/7/2024
Theresa Coonce	Clinton		47842 US	3/7/2024
Abigail Keissar	Sherman Oaks	CA	91411 US	3/7/2024
Monica Besteman	Puyallup	WA	98372 US	3/7/2024
Reginald Spengler	Jac	FL	32205 US	3/7/2024
Karla Struble	Puyallup	WA	98371 US	3/7/2024
Jessica Williams	Puyallup	WA	98375 US	3/7/2024
MARK PALMER	Boydton	VA	23917 US	3/7/2024
Paul Rote	Hayward	CA	94544 US	3/7/2024

Deborah Kirkpatrick	Cave Junction	OR	97523 US	3/7/2024
Gail Atkinson	Puyallup	WA	98372 US	3/7/2024
tickle tipson	amogus	WA	nien US	3/7/2024
Helen M Mamic	Tacoma	WA	98405 US	3/7/2024
Gerrit Foster	Puyallup	WA	98374 US	3/7/2024
Lucas Foster	Puyallup	WA	98374 US	3/7/2024
Sage Farwell	Puyallup	WA	98372 US	3/8/2024
Ivo Kollmer	Berkeley	CA	94705 US	3/8/2024
Michael Mullins	Federal Way	WA	98003 US	3/8/2024
Luke Henry	Los Angeles	CA	90037 US	3/8/2024
Ryan Menard	Lincoln	NE	68521 US	3/8/2024
Jack Carney	Puyallup	WA	2150 US	3/8/2024
Galina Lobets	Puyallup	WA	98372 US	3/8/2024
Mary Burchin	Concord	NC	28027 US	3/8/2024
Cashil Clark	Zanesville	OH	43701 US	3/9/2024
Lauren Joiner	Chicago	IL	60652 US	3/9/2024
Arely Lucio	Morrow ga	GA	30260 US	3/9/2024
Lynne Walton	Puyallup	WA	98373 US	3/9/2024
Jessica Melby	Bonney Lake	WA	98391 US	3/9/2024
Laura Kase Nagai	Puyallup	WA	98372 US	3/9/2024
Lexi Renee	Katy	TX	77494 US	3/9/2024
Elisabeth Shelly	Temple City	CA	91780 US	3/9/2024
Hannah Sorrell	Warrior	AL	35180 US	3/9/2024
Janetta Moorehead	Bonney Lake	WA	98391 US	3/10/2024
eric mcgrath	Hyattsville	MD	20782 US	3/10/2024
Jack MacDonald-Hilton	Worcester	MA	1609 US	3/10/2024
Richard Salinas	Puyallup	WA	98372 US	3/10/2024
Jacob Kruse	Puyallup	WA	98372 US	3/10/2024
Sam Clemetsen	Greenville	SC	29601 US	3/10/2024
Braeden Scott Freisem	puyallup	WA	98372 US	3/10/2024
Jordan Ricard	Puyallup	WA	98372 US	3/11/2024
Elise kourkoutis	Ithaca	NY	14850 US	3/11/2024
Muyguck Taing	Puyallup	WA	98372 US	3/11/2024
Helen Auwelo	Oakland	CA	94603 US	3/11/2024
Mike Richards	Puyallup	WA	98375 US	3/11/2024
Ryann Kezele	Bonney Lake	WA	98391 US	3/11/2024
Michael Whalen	Tacoma	WA	98446 US	3/12/2024
Sara Allan	Puyallup	WA	98374 US	3/12/2024
Cameron Severns	Tacoma	WA	98466 US	3/12/2024
Denise Oatman	Bonney Lake	WA	98391 US	3/12/2024
Leah Feeney-Esposito	Mililani Town	HI	96789 US	3/12/2024
Tomas Malaga	Washington	DC	20020 US	3/12/2024
Kendra Henson	Everett	WA	98201 US	3/12/2024
Emily Grefalda	Puyallup	WA	98372 US	3/12/2024
Ryan Hager	Puyallup	WA	98371 US	3/12/2024
Rechilda Grefalda	Puyallup	WA	98373 US	3/12/2024
Holly Holmes	Puyallup	WA	98375 US	3/12/2024

Marilyn Hager	Tacoma	WA	98409 US	3/12/2024
Dale Clawson	Puyallup	WA	98372 US	3/12/2024
STACY BROWN	Seattle	WA	98188 US	3/12/2024
Marissa Hager	Puyallup	WA	98371 US	3/12/2024
Rodric Grefalda	Lakewood	WA	98499 US	3/12/2024
Robert Hager	Tacoma	WA	98409 US	3/12/2024
Tanner Orrock	Marblehead	MA	1945 US	3/12/2024
Waltraud Lore Belcher	Puyallup	WA	98375 US	3/12/2024
Julien Hammond			US	3/12/2024
Emily Craft	Seattle	WA	98198 US	3/12/2024
Jennie Slack	Puyallup	WA	98372 US	3/12/2024
M Davis	Puyallup	WA	98372 US	3/12/2024
Marie Clarke	Puyallup	WA	98372 US	3/13/2024
Dave Clarke	Spanaway	WA	98387 US	3/13/2024
Annicca Willis	Spanaway	WA	98387 US	3/13/2024
Ashley Clarke	Graham	WA	98338 US	3/13/2024
Jolenea Sounthone	Puyallup	WA	98375 US	3/13/2024
Nancy Davison	Puyallup	WA	98371 US	3/13/2024
Sheryl Brown	Puyallup	WA	98374 US	3/13/2024
Susan Cannella	Puyallup	WA	98374 US	3/13/2024
Ann Fouts	Olympia	WA	98501 US	3/13/2024
Cassandra Swanson	Birmingham	AL	35215 US	3/13/2024
Pam Gordon	Puyallup	WA	98374 US	3/13/2024
Shannin Troxler	Portland	OR	97232 US	3/13/2024
Nicole Pointer	Puyallup	WA	98374 US	3/13/2024
Cheronda Ingram	Franklin	NC	28734 US	3/13/2024
Robert Bench	Riverton	UT	84096 US	3/13/2024
Cynthia Peterson	Seattle	WA	98103 US	3/13/2024
Laurel Steele	Seattle	WA	98146 US	3/13/2024
Jaclyn Rubly	Seattle	WA	9372 US	3/13/2024
Meredith Bautista	Tacoma	WA	98445 US	3/14/2024
susan tabell	fairview park	OH	44126 US	3/14/2024
Kim Stoltenberg	Tacoma	WA	98466 US	3/14/2024
Neela Rodriguez	San Antonio	TX	78229 US	3/14/2024
Jessica Wallack	Puyallup	WA	98372 US	3/14/2024
Rebecca Williams	Newtown	CT	6470 US	3/14/2024
SUSAN MANALLOOR	PUYALLUP	WA	98372 US	3/14/2024
Janice LaCaille	Puyallup	WA	98371 US	3/14/2024
Linda Saarela	Puyallup	WA	98372 US	3/14/2024
Tula M	Wilmington	NC	28412 US	3/14/2024
Mary Foote	Puyallup	WA	98375 US	3/14/2024
Tracy Engels	Bonney Lake	WA	98391 US	3/14/2024
Kelsey Van Horn	Puyallup	WA	98374 US	3/14/2024
Marcelo Anthony Bautista	Tacoma	WA	98445 US	3/14/2024
Morgan Jones	West Hartford	CT	6107 US	3/14/2024
Renee Quiett	Puyallup	WA	98372 US	3/14/2024
Catelyn Frost	Puyallup	WA	98373 US	3/14/2024

Larry Leveen	Tacoma	WA	98424 US	3/14/2024
Cindy Cyphers	Puyallup	WA	98372 US	3/15/2024
Jane Beck	Puyallup	WA	98372 US	3/15/2024
Joe Delong	Puyallup	WA	98372 US	3/15/2024
Maria estrada	Houston	TX	77036 US	3/15/2024
Melinda Lyon	Seattle	WA	98112 US	3/15/2024
Stacia Rink	Seattle	WA	98107 US	3/15/2024
Karilee Witting	Puyallup	WA	98374 US	3/15/2024

See recommended preferred alternative map.



**PREFERRED ALTERNATIVE - "Reduced Intensity"**  
1.0 Million sq ft of Warehouses